

1 activities in the immediate area which indicates that there is presently  
2 insufficient demand for any resources that may exist within the Project area.  
3 Thus, the proposed Project has no potential to adversely impact mineral  
4 resources in any manner.

5 2. Mitigation.

6 No mitigation is required for direct Project impacts, and no mitigation is  
7 required for cumulative impacts related to mineral resources.

8 L. Noise

9 1. Impacts.

10 *For a project located within an airport land use plan or, where such a plan*  
11 *has not been adopted, within two miles of a public airport or public use*  
12 *airport would the Project expose people residing or working in the Project*  
13 *area to excessive noise levels?*

14 The Project site is located within the French Valley Airport Influence Area;  
15 the site will be exposed to noise levels ranging from 55 to 65 CNEL. The  
16 light industrial and business uses proposed for the Project site are  
17 considered Conditionally Acceptable up to 70 dBA CNEL. The removal of  
18 the second runway at the Airport ensures that aircraft noise will not be  
19 increased due to closer proximity of future aircraft operations on the second  
20 runway. To ensure that the employees and patrons of future businesses will  
21 not be subject to noise levels beyond acceptable levels, an acoustical  
22 analysis will be required prior to building permit issuance. If the analysis  
23 indicates noise levels within the structures will not be acceptable, the  
24 building design must then incorporate noise attenuation design features to  
25 control noise levels to an acceptable CNEL for business operations. Future  
26 building occupants must be notified of periodic loud noise associated with  
27 aircraft operations.

28 As described below, mitigation was added that will substantially lessen the

1 potentially significant impacts associated with future aircraft operations on  
2 future businesses occupying structures near the Airport to a level of less  
3 than significant and no unavoidable adverse impacts would occur.

4 *For a project within the vicinity of a private airstrip, would the Project*  
5 *expose people residing or working in the project area to excessive noise*  
6 *levels?*

7 The Project site is not located within the vicinity of a private airstrip.  
8 Therefore, the Project cannot expose people to noise from such operations.

9 There are no railroads located in the vicinity of the Project site. Therefore,  
10 no adverse railroad noise impact can affect future development on the  
11 Project site.

12 The Project site is located approximately 1/2 mile from the nearest  
13 highway. Therefore, no adverse highway noise impact can affect future  
14 development on the Project site.

15 No other sources of noise have been identified near the Project site.  
16 Therefore, no adverse noise impact can affect future development on the  
17 Project site.

18 *Would the Project result in a substantial permanent increase in ambient*  
19 *noise levels in the Project vicinity above levels existing without the Project;*  
20 *in a substantial temporary or periodic increase in ambient noise levels in*  
21 *the Project vicinity above levels existing without the Project; or, in an*  
22 *exposure of persons to or generation of noise levels in excess of standards*  
23 *established in the local general plan or noise ordinance, or applicable*  
24 *standards of other agencies?*

25 Based on the evaluation presented in the noise sections of the FEIR, the  
26 proposed Project will not result in any Project specific significant adverse  
27 noise impacts during future construction or operations. All Project-related  
28 noise impacts, including cumulative contributions, can be controlled to less

1 than significant levels with implementation of proposed mitigation.

2 The mitigation measures identified are feasible and would avoid or  
3 substantially lessen the potentially significant impacts associated with  
4 future construction noise impacts to a level of less than significant and no  
5 unavoidable adverse impacts would occur.

6 The mitigation measure identified is feasible and would avoid or  
7 substantially lessen the potentially significant impacts associated with  
8 future occupancy noise impacts to a level of less than significant and no  
9 unavoidable adverse impacts would occur.

10 The Project site is exposed to conditionally acceptable levels of noise (up to  
11 70 dBA CNEL) for industrial and business park uses. Thus, future  
12 exposure will not exceed the thresholds established by the Riverside County  
13 General Plan with implementation of adequate noise attenuation within the  
14 structures. Since this is a standard condition, no mitigation is required to  
15 achieve a less than significant impact level.

16 *Would the Project result in exposure of persons to or generation of*  
17 *excessive ground-borne vibration or ground-borne noise levels?*

18 The Project site is not adjacent to any sensitive noise or ground-borne  
19 vibration land uses and the remaining activities, fine grading and building  
20 construction do not involve activities that generate excessive ground-borne  
21 vibration. Thus, the potential adverse vibration impact from future  
22 construction and operation activities will be less than significant.

23 Regarding cumulative impacts, based on the cumulative impact significance  
24 criteria described in Section 4.4.3.1 and summarized in Table 4.4-1 of this  
25 document, the Project contributions to the cumulative noise environment  
26 are as follows. The future construction noise impacts can be controlled to a  
27 less than significant impact with implementation of standard Conditions of  
28 Approval and recommended mitigation measures. Thus, a less than

1 significant cumulative noise impact is expected during construction activity.  
2 The proposed Project contribution to on-site noise levels can also be  
3 reduced to a less than significant level with implementation of the  
4 recommended mitigation measure. Finally, the off-site roadway noise level  
5 increases will not cause any significant impacts to any existing or future  
6 sensitive noise receptors. The analysis in Chapter 4.4 Noise of this DEIR  
7 shows that the Project will NOT create a substantial permanent increase in  
8 traffic-related noise levels or expose persons to noise levels in excess of the  
9 exterior noise level standards, and therefore, no mitigation is required.  
10 Consequently, construction and implementation of the proposed Project  
11 would not result in cumulatively significant noise impacts.

12 2. Mitigation.

13 The Project has been modified to mitigate or avoid the potentially  
14 significant impacts by the following mitigation measure:

- 15 a. MM4.4-1: Prior to approval of grading plans and/or issuance of  
16 building permits, plans shall include a note indicating that noise-  
17 generating Project construction activities shall not occur between the  
18 hours of 6:00 p.m. to 6:00 a.m. during the month of June through  
19 September, and between the hours of 6:00 p.m. and 7:00 a.m. during  
20 the months of October through May.
- 21 b. MM4.4-2: During all Project site construction, the construction  
22 contractors shall equip all construction equipment, fixed or mobile,  
23 with properly operating and maintained mufflers, consistent with  
24 manufacturers' standards. The construction contractor shall place all  
25 stationary construction equipment so that emitted noise is directed  
26 away from the noise sensitive receptors nearest the Project site.
- 27 c. MM4.4-3: The construction contractor shall locate equipment  
28 staging in areas that will create the greatest distance between

1 construction-related noise sources and noise sensitive receptors  
2 nearest the Project site during all Project construction. A review of  
3 the Project site and the location of nearby noise sensitive receptors  
4 indicate that construction equipment staging shall be concentrated in  
5 the southeastern corner of the site, or along the southern property  
6 boundary in the eastern portion of the site.

7 d. MM4.4-4: Prior to grading permit issuance, the County shall review  
8 and approve a Construction Haul Route Exhibit prepared by the  
9 Project Applicant that identifies all public and private roadways that  
10 will be used for haul truck deliveries. Haul routes shall minimize  
11 passage by noise-sensitive land uses. A requirement to comply with  
12 the Construction Haul Route Exhibit shall be noted on all grading  
13 and building plans and also shall be specified in bid documents  
14 issued to perspective construction contractors.

15 e. MM4.4-5: All employees that will be exposed to noise levels greater  
16 than 75 dB over an 8-hour period shall be provided with adequate  
17 hearing protection devices to ensure no hearing damage will result  
18 from construction activities.

19 f. MM4.4-6: Utilize construction methods or equipment that will  
20 provide the lowest level of noise impact, i.e., use newer equipment  
21 that will generate lower noise levels.

22 g. MM4.4-7: Maintain good relations with the local community where  
23 construction is scheduled, such as keeping people informed of the  
24 schedule, duration, and progress of the construction, to minimize the  
25 public objections of unavoidable noise. Communities should be  
26 notified in advance of the construction and the expected temporary  
27 and intermittent noise increases during the construction period.  
28

1 h. MM4.4-8: To satisfy the Noise Level Reduction requirements all  
2 windows shall provide a minimum Sound Transmission Class (STC)  
3 rating of 31. The interior noise analysis shows that the French Valley  
4 Airport Center business park land use will satisfy the County of  
5 Riverside 45 dBA CNEL interior noise level standard with a  
6 minimum STC window rating of 31.

7 M. Population and Housing

8 1. Impacts.

9 *Would the Project displace substantial numbers of existing housing,*  
10 *necessitating the construction of replacement housing elsewhere?*

11 The proposed Project site is graded and currently vacant. Therefore, the  
12 Project will not displace any housing.

13 *Would the Project create a demand for additional housing, particularly*  
14 *housing affordable to households earning 80% or less of the County's*  
15 *median income?*

16 At full development the proposed Project is estimated to provide jobs for  
17 between approximately 1,509 to 3,772 employees. These new jobs have a  
18 potential to create demand for additional housing; however, due to the type  
19 of jobs envisioned by this Project, a high percentage of demand for  
20 affordable housing is not anticipated. Given the current housing market and  
21 related availability of housing and high unemployment, the increase in jobs  
22 is not forecast to cause a significant demand for additional housing.

23 *Would the Project displace substantial numbers of people, necessitating the*  
24 *construction of replacement housing elsewhere?*

25 The proposed Project will not displace any people or housing. No adverse  
26 impact to such human resources can result from Project implementation.

27 *Would the Project affect a County Redevelopment Project Area?*

28 The Project is not located in a County Redevelopment Project Area.

1 Therefore, no potential exists to adversely impact any such area.

2 *Would the Project cumulatively exceed official regional or local population*  
3 *projections?*

4 The Project does not provide housing; thus, it has no potential to cause a  
5 cumulative exceedance in local or regional population projections.

6 *Would the Project induce substantial population growth in an area, either*  
7 *directly (for example, by proposing new homes and businesses) or indirectly*  
8 *(for example, through extension of roads or other infrastructure)?*

9 The Project will provide between about 1,500 and 3,772 new jobs over  
10 several years of development. Based on this job forecast, the proposed  
11 Project will enhance the jobs/housing balance for the southwest portion of  
12 Riverside County. This number of new jobs may induce limited population  
13 growth within the Project area, but it is not anticipated that the demand for  
14 future housing will induce substantial population growth. No indirect  
15 growth would be induced as local infrastructure will not be extended to  
16 other potential areas that could support development.

17 2. Mitigation.

18 No mitigation is required for direct Project impacts, and no mitigation is  
19 required for cumulative impacts related to population and housing  
20 resources.

21 N. Public Services

22 1. Impacts.

23 *Would the Project result in substantial adverse physical impacts associated*  
24 *with the provision of new or physically altered government facilities or the*  
25 *need for new or physically altered governmental facilities, the construction*  
26 *of which could cause significant environmental impacts, in order to*  
27 *maintain acceptable service ratios, response times or other performance*  
28 *objectives for fire services?*

1 The Project area is served by the Riverside County Fire Department. When  
2 the original approval was granted by the County in 2008, the developer was  
3 required to offset demand by this Project through payment of development  
4 impact fees (DIF) for fire enforcement service. Since this requirement has  
5 already been imposed on the Project, the potential impact to fire services is  
6 considered a less than significant impact. Specific fees to be paid will  
7 depend upon the amount of the fee at the time of actual building occupancy.

8 *Would the Project result in substantial adverse physical impacts associated*  
9 *with the provision of new or physically altered government facilities or the*  
10 *need for new or physically altered governmental facilities, the construction*  
11 *of which could cause significant environmental impacts, in order to*  
12 *maintain acceptable service ratios, response times or other performance*  
13 *objectives for sheriff services?*

14 The Project area is served by the Riverside County Sheriff's Department.  
15 When the original approval was granted by the County in 2008, the  
16 developer was required to offset demand by this Project through payment of  
17 development impact fees (DIF) for law enforcement service. Since this  
18 requirement has already been imposed on the Project, the potential impact  
19 to such services is considered a less than significant impact. Specific fees  
20 to be paid will depend upon the amount of the fee at the time of actual  
21 building occupancy.

22 *Would the Project result in substantial adverse physical impacts associated*  
23 *with the provision of new or physically altered government facilities or the*  
24 *need for new or physically altered governmental facilities, the construction*  
25 *of which could cause significant environmental impacts, in order to*  
26 *maintain acceptable service ratios, response times or other performance*  
27 *objectives for schools?*

28 The Project area is served by the Temecula Valley Unified School District.



1 The proposed Project will not generate any direct demand for school  
2 capacity, but may indirectly generate students due to a local increase in  
3 population from new employees. When the original approval was granted  
4 by the County in 2008, the developer was required to offset this potential  
5 indirect demand by this Project through payment of school impact fees for  
6 industrial and business park development. Since this requirement has  
7 already been imposed on the Project, the potential impact to such services is  
8 considered to a less than significant impact. Specific fees to be paid will  
9 depend upon the amount of the fee at the time of actual building occupancy.

10 *Would the Project result in substantial adverse physical impacts associated*  
11 *with the provision of new or physically altered government facilities or the*  
12 *need for new or physically altered governmental facilities, the construction*  
13 *of which could cause significant environmental impacts, in order to*  
14 *maintain acceptable service ratios, response times or other performance*  
15 *objectives for libraries?*

16 The Project area is served by the Riverside County library resources. When  
17 the original approval was granted by the County in 2008, the developer was  
18 required to offset demand by this Project through payment of development  
19 impact fees (DIF) for library services. Since this requirement has already  
20 been imposed on the Project, the potential impact to such services is  
21 considered to a less than significant impact. Specific fees to be paid will  
22 depend upon the amount of the fee at the time of actual building occupancy.

23 *Would the Project result in substantial adverse physical impacts associated*  
24 *with the provision of new or physically altered government facilities or the*  
25 *need for new or physically altered governmental facilities, the construction*  
26 *of which could cause significant environmental impacts, in order to*  
27 *maintain acceptable service ratios, response times or other performance*  
28 *objectives for health services?*

1 Health services are provided by the County and private health care  
2 providers. The proposed Project does not place any direct demand on such  
3 services, with the exception of an accident that could occur during construc-  
4 tion or at one of the future businesses during operation. Recent construction  
5 of the Loma Linda Center in Murrieta and Temecula Valley Rancho Springs  
6 in Temecula ensures adequate emergency capacity within the Project area.  
7 No adverse impact on demand for health services will result from  
8 implementing the proposed Project.

9 2. Mitigation and/or Conditions.

10 No mitigation is required for direct Project impacts, and no mitigation is  
11 required for cumulative impacts related to public services resources.  
12 Standard conditions shall apply to the Project and any impacts will remain  
13 less than significant.

14 O. Recreation

15 1. Impacts.

16 *Would the Project include recreational facilities or require the construction*  
17 *or expansion of recreational facilities which might have an adverse*  
18 *physical effect on the environment; or, would the Project include the use of*  
19 *existing neighborhood or regional parks or other recreational facilities*  
20 *such that substantial physical deterioration of the facility would occur or be*  
21 *accelerated?*

22 The proposed Project does not include any recreational facilities nor will it  
23 directly include the use of existing parks which might experience  
24 substantial physical deterioration of such facilities. No adverse impact to  
25 recreation resources will result from Project implementation.

26 *Is the Project located within a C.S.A. or recreation and park district with a*  
27 *Community Parks and Recreation Plan (Quimby fees)?*

28 The Project site is not located within a CSA or park district with

1 Community Parks and Recreation Plan fees. Therefore, no adverse effect  
2 on such a district will result from Project implementation. Additionally, no  
3 County designated trails are shown on the Project site. Therefore, the  
4 proposed Project has no potential to adversely impact such trails.

5 2. Mitigation and/or Conditions.

6 No mitigation is required for direct Project impacts, and no mitigation is  
7 required for cumulative impacts related to recreation resources. Standard  
8 conditions shall apply to the Project and any impacts will remain less than  
9 significant.

10 P. Transportation and Traffic

11 1. Impacts.

12 *Would the Project conflict with an applicable congestion management*  
13 *program, including, but not limited to level of service standards and travel*  
14 *demand measures, or other standards established by the county congestion*  
15 *management agency for designated roads or highways; substantially*  
16 *increase hazards due to a design feature (e.g., sharp curves or dangerous*  
17 *intersections) or incompatible uses (e.g. farm equipment); cause an effect*  
18 *upon, or a need for new or altered maintenance of roads; cause an effect*  
19 *upon circulation during the Project's construction; result in inadequate*  
20 *emergency access or access to nearby uses; or, conflict with adopted*  
21 *policies, plans or programs regarding public transit, bikeways or*  
22 *pedestrian facilities, or otherwise substantially decrease the performance or*  
23 *safety of such facilities?*

24 The proposed Project would not result in any Project specific significant  
25 circulation system effects during future construction with implementation of  
26 the identified construction mitigation measures. The mitigation measures  
27 identified are feasible and would avoid or substantially lessen the  
28 potentially significant circulation system impacts associated with

1 construction activities impacts to a level of less than significant and no  
2 unavoidable adverse impacts would occur.

3 Implementation of the Project will not conflict with an applicable plan,  
4 ordinance or policy establishing a measure of effectiveness for the  
5 performance of the circulation system, taking into account all modes of  
6 transportation, including mass transit and non-motorized travel and relevant  
7 components of the circulation system, including but not limited to  
8 intersections, streets, highways and freeways, pedestrian and bicycle paths,  
9 and mass transit; conflict with an applicable congestion management  
10 program, including, but not limited to level of service standards and travel  
11 demand measures, or other standards established by the county congestion  
12 management agency for designated roads or highways; substantially  
13 increase hazards due to a design feature (e.g., sharp curves or dangerous  
14 intersections) or incompatible uses (e.g. farm equipment); cause an effect  
15 upon, or a need for new or altered maintenance of roads; cause an effect  
16 upon circulation during the Project's construction; result in inadequate  
17 emergency access or access to nearby uses; and/or, conflict with adopted  
18 policies, plans or programs regarding public transit, bikeways or pedestrian  
19 facilities, or otherwise substantially decrease the performance or safety of  
20 such facilities. County application materials, site-specific analysis,  
21 mitigation measures, standard conditions, and conditions of approval will  
22 ensure that impacts to transportation/traffic resources are fully addressed.  
23 Any impacts are considered less than significant. No unavoidable  
24 significant adverse traffic or circulation system impacts will result from  
25 implementing the proposed Project.

26 *Would the Project result in a change in air traffic patterns, including either*  
27 *an increase in traffic levels or a change in location that results in*  
28 *substantial safety risks?*

1 The proposed Project does not include any activities or actions that could  
2 change air traffic patterns at the French Valley Airport. Based on the  
3 revisions to the airport land use plan for the Airport, the Project will not  
4 constrain either existing or future Airport operations. No impact is forecast  
5 and no mitigation is required.

6 *Would the Project alter waterborne, rail or air traffic?*

7 The proposed Project has no potential to adversely impact or conflict with  
8 existing or future air operations. Since there are no waterborne or rail  
9 transportation facilities in the Project vicinity, the proposed Project has no  
10 potential to adversely impact waterborne or rail traffic activities.

11 Additionally, the Project is not located adjacent to any County designated  
12 bike trails. Therefore, the proposed Project has no potential to adversely  
13 impact such trails.

14 Regarding cumulative impacts, the circulation system impact analysis in  
15 Chapter 4.5 Transportation and Traffic evaluates the potential  
16 environmental impacts of the proposed Project Development of the  
17 proposed Project as described in Chapter 3 of this Draft EIR would result in  
18 the creation of short-term construction-related circulation system impacts  
19 and construction related generation of additional short-term traffic which  
20 could adversely affect local circulation systems. After development, the  
21 proposed Project would contribute to localized traffic impacts. A summary  
22 of the cumulatively impacted study area intersections and recommended  
23 improvements to reduce cumulative impacts to less-than-significant are  
24 described in detail within Section 7.0 EAPC (2016 & 2019) Traffic  
25 Analysis of the TIA Report provided in Appendix 3. Cumulative impacts  
26 are deficiencies in the transportation network's LOS that would not be  
27 directly caused by the Project. The Project, along with other cumulative  
28 development projects, would contribute traffic to these deficient facilities.

1 resulting in a cumulatively considerable impact.

2 A summary of off-site improvements needed to address cumulative traffic  
3 impacts for EAPC (2019) traffic conditions, with Clinton Keith Road  
4 Extension, is included in Table 4.5-6 of the EIR. Improvements found to be  
5 included in Transportation Uniform Mitigation Fee (TUMF), County  
6 Development Impact Fee (DIF) and Southwest Road and Bridge Benefit  
7 District (RBBD) programs have been identified as such. Payment of fees to  
8 these programs may be considered as mitigation for these improvements.  
9 For improvements that do not appear to be in the TUMF, DIF or RBBD  
10 programs, a fair share financial contribution based on the Project's fair  
11 share impact may be imposed in order to mitigate the Project's share of  
12 impacts in lieu of construction. These fees are collected as part of a funding  
13 mechanism aimed at ensuring that regional highways and arterial  
14 expansions keep pace with the projected vehicle trip increases.

15 Additional information related to the Fair Share Calculation, TUMF, DIF  
16 and RBBD programs are contained in Section 9.0 Local and Regional  
17 Funding Mechanisms of the TIA Report provided in Appendix 3 where the  
18 current fee schedule and project transportation impacts fees are shown on  
19 Table 9-2. Since payment of TUMF, DIF and RBBD fees is mandatory to  
20 offset a project's fair share contribution to cumulative demand for  
21 circulation system capacity, no mitigation is required. Payment of fair share  
22 fees is generally deemed to be sufficient to reduce a project's contributions  
23 to cumulative impacts to a less than significant level. Specifically, Section  
24 15130(a)(3) states: "An EIR may determine that a project's contribution to a  
25 significant cumulative impact will be rendered less than cumulatively  
26 considerable and thus is not significant. A project's contribution is less than  
27 cumulatively considerable if the project is required to implement or fund its  
28 fair share of a mitigation measure or measures designed to alleviate the

1 cumulative impact. The lead agency shall identify facts and analysis  
2 supporting its conclusion that the contribution will be rendered less than  
3 cumulatively considerable.”

4 For two reasons this finding may not apply to the proposed project. First,  
5 three intersections on the affected circulation system cannot be fully  
6 mitigated to a less than significant impact, even though the project’s  
7 contribution does not cause the cumulatively considerable adverse impact.

8 Second, although a plain reading of Section 15130(a)(3) indicates that  
9 payment of fair share to required improvements is sufficient to reduce a  
10 specific project’s contribution to a less than cumulatively considerable  
11 contribution, there is a timing factor that cannot be ignored. Fair share  
12 contributions to circulation system impacts often cannot be immediately  
13 implemented. Thus, there is a short-term cumulatively significant impact  
14 between the time when an impact occurs (2019) and the actual completion  
15 of the requisite improvement. Thus in an abundance of caution in  
16 interpreting the level of cumulative impact, this document finds that the  
17 proposed project has a potential to cause a contribution to cumulatively  
18 significant impacts on the area circulation system, at least in the period  
19 between full development and the completion of all required circulation  
20 system improvements identified in Subchapter 4.5 of this EIR.

21 2. Mitigation.

22 The Project has been modified to mitigate or avoid the potentially  
23 significant impacts by the following mitigation measure:

- 24 a. MM4.5-1: Prior to initiating roadway impacts, a construction traffic  
25 management plan must be developed, approved by the County or  
26 City and fully implemented by the construction contractor to  
27 minimize adverse effects on the flow of traffic during construction.  
28 At a minimum this plan shall address, but is not limited to, such

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items as:

- How to minimize disruption of vehicle and alternative modes of traffic at all times, but particularly during periods of high traffic volumes.
- Provision of adequate access to meet safety and emergency vehicle access.
- Adequate signage and other controls, including flag persons, to ensure that traffic can flow adequately during construction.
- The identification of alternative routes that can meet the traffic flow requirements of a specific area, including communication (signs, webpages, etc.) with drivers and neighborhoods where construction activities will occur.
- Time of construction activities (e.g., off-peak hours).
- Truck/Haul routes.
- Construction employee parking.
- Construction equipment staging.
- Potential lane closures.
- Work zone traffic control.
- Control of traffic at any location where short-term hazards cannot be avoided.

The construction traffic management plan is viewed as mitigation for short-term circulation system impacts and must be designed to minimize many of the anticipated impacts associated with the construction activities of the Project.

- b. MM4.5-2: The construction contractor will ensure that traffic safety hazards, such as uncovered or unfilled open trenches, will not be left in roadways during period of time when construction personnel are not present, such as nighttime and weekends.



1 c. MM4.5-3: The construction contractor will repair all roads  
2 adequately after construction to ensure that traffic can move in the  
3 same manner as before construction.

4 d. The following mitigation measures are necessary to reduce impacts  
5 to “less-than-significant” for Phase 1-2016 with Clinton Keith Road  
6 extension traffic conditions (EAP).

7 4.5.1-1 Winchester Road (SR-79) / Benton Road

- 8 • Construct a northbound left turn lane, 3rd through lane  
9 and modify the traffic signal to implement overlap  
10 phasing on the right turn lane.
- 11 • Construct a 3rd southbound shared through-right turn  
12 lane.
- 13 • Construct an eastbound left turn lane, two through  
14 lanes and right turn lane.
- 15 • Construct 2 westbound through lanes and modify the  
16 traffic signal to implement overlap phasing on the right  
17 turn lane.

18 4.5.1-2 Margarita Road / Murrieta Hot Springs Road

- 19 • Mitigation Measure 4.5.1-1 shall apply. No additional  
20 mitigation is required.

21 4.5.2-2 Winchester Road (SR-79) / Thompson Road

- 22 • Construct a 2nd northbound left turn lane.
- 23 • Modify the traffic signal and implement overlap  
24 phasing on the eastbound right turn lane.

25 4.5.3-2 Winchester Road (SR-79) / Auld Road

- 26 • Mitigation Measure 4.5.3-1 shall apply. No additional  
27 mitigation is required.

28 4.5.4-2 Winchester Road (SR-79) / Murrieta Hot Springs Road

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- Mitigation Measure 4.5.4-1 shall apply. No additional mitigation is required.

4.5.5-2 Winchester Road (SR-79) / Nicolas Road

- Mitigation Measure 4.5.5-1 shall apply. No additional mitigation is required.

4.5.6-2 Winchester Road (SR-79) / Margarita Road

- Mitigation Measure 4.5.6-1 shall apply. No additional mitigation is required.

4.5.7-2 Winchester Road (SR-79) / Ynez Road

- Mitigation Measure 4.5.7-1 shall apply. No additional mitigation is required.

4.5.8-2 Winchester Road (SR-79) / I-15 Northbound Ramps

- Mitigation Measure 4.5.8-1 shall apply. No additional mitigation is required.

4.5.9-2 Briggs Road / Auld Road

- Mitigation Measure 4.5.9-1 shall apply.
- Modify the traffic signal and implement overlap phasing on the northbound right turn lane.

4.5.10-2 Industry Way / Auld Road

- Mitigation Measure 4.5.10-1 shall apply. No additional mitigation is required.

4.5.11-2 Pourroy Road-West / Auld Road

- Mitigation Measure 4.5.11-1 shall apply. No additional mitigation is required.

e. The following mitigation measures are necessary to reduce impacts to “less-than-significant” for Phase 2-2019 without Clinton Keith Road extension traffic conditions (EAP).

4.5.13-1 Winchester Road (SR-79) / Willows Avenue

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- Stripe a northbound right turn lane and modify the traffic signal to implement overlap phasing on the northbound right turn lane.

4.5.14-1 Pourroy Road-East / Auld Road

- Install a traffic signal.
- Construct an eastbound left turn lane.

4.5.1-3 Margarita Road / Murrieta Hot Springs Road

- Mitigation Measure 4.5.1-1 shall apply. No additional mitigation is required.

4.5.2-3 Winchester Road (SR-79) / Thompson Road

- Mitigation Measure 4.5.2-1 shall apply.
- Construct a 3rd southbound through lane.

4.5.3-3 Winchester Road (SR-79) / Auld Road

- Mitigation Measure 4.5.3-1 shall apply.
- Construct a 3rd northbound through lane.
- Construct a 2nd southbound left turn lane and 3rd shared through-right turn lane.

4.5.4-3 Winchester Road (SR-79) / Murrieta Hot Springs Road

- Mitigation measure 4.5.4-1 shall apply.
- Construct a 4th northbound through lane.
- Construct a 2nd southbound right turn lane and modify the traffic signal to implement overlap phasing.

4.5.5-3 Winchester Road (SR-79) / Nicolas Road

- Mitigation measure 4.5.5-1 shall apply.
- Construct a 2nd southbound right turn lane.

4.5.6-3 Winchester Road (SR-79) / Margarita Road

- Mitigation Measure 4.5.6-1 shall apply.
- The PM peak hour intersection delay is anticipated to

1 exceed the City of Temecula’s significance threshold  
2 of 2.0 seconds over pre-Project conditions with the  
3 implementation of Mitigation Measure 4.5.6-1. In order  
4 to achieve acceptable peak hour intersection  
5 operations: Construct a 2nd westbound right turn lane  
6 with overlap phasing. This improvement may not be  
7 feasible due to right-of-way constraints.

8 4.5.7-3 Winchester Road (SR-79) / Ynez Road

- 9 • Mitigation Measure 4.5.7-1 shall apply. No additional  
10 mitigation is required.

11 4.5.8-3 Winchester Road (SR-79) / I-15 Northbound Ramps

- 12 • Mitigation Measure 4.5.8-1 shall apply. No additional  
13 mitigation is required.

14 4.5.9-3 Briggs Road / Auld Road

- 15 • Mitigation measure 4.5.9-1 shall apply.
- 16 • Construct a 2nd northbound right turn lane and modify  
17 the traffic signal to implement overlap phasing on the  
18 northbound right turn lanes.

19 4.5.10-3 Industry Way / Auld Road

- 20 • Mitigation Measure 4.5.10-1 shall apply. No additional  
21 mitigation is required.

22 4.5.11-3 Pourroy Road-West / Auld Road

- 23 • Mitigation Measure 4.5.11-1 shall apply. No additional  
24 mitigation is required.

25 f. The following mitigation measures are necessary to reduce impacts  
26 to “less-than-significant” for Phase 2 - 2019 with Clinton Keith  
27 Road extension traffic conditions (EAP).

28 4.5.1-4 Margarita Road / Murrieta Hot Springs Road

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- Mitigation Measure 4.5.1-1 shall apply. No additional mitigation is required.

4.5.2-4 Winchester Road (SR-79) / Thompson Road

- Mitigation Measure 4.5.2-1 shall apply. No additional mitigation is required.

4.5.3-4 Winchester Road (SR-79) / Auld Road

- Mitigation Measure 4.5.3-3 shall apply. No additional mitigation is required.

4.5.4-4 Winchester Road (SR-79) / Murrieta Hot Springs Road

- Mitigation Measure 4.5.4-3 shall apply. No additional mitigation is required.

4.5.5-4 Winchester Road (SR-79) / Nicolas Road

- Mitigation Measure 4.5.5-3 shall apply. No additional mitigation is required.

4.5.6-4 Winchester Road (SR-79) / Margarita Road

- Mitigation Measure 4.5.6-3 shall apply. No additional mitigation is required. Please note, a portion of the improvements required under Measure 4.5.6-3 to reduce impacts to a less than significant level may not be feasible due to right-of-way constraints.

4.5.7-4 Winchester Road (SR-79) / Ynez Road

- Mitigation Measure 4.5.7-1 shall apply. No additional mitigation is required.

4.5.8-4 Winchester Road (SR-79) / I-15 Northbound Ramps

- Mitigation Measure 4.5.8-1 shall apply. No additional mitigation is required.

4.5.9-4 Briggs Road / Auld Road

- Mitigation Measure 4.5.9-3 shall apply. No additional

mitigation is required.

4.5.10-4 Industry Way / Auld Road

- Mitigation Measure 4.5.10-1 shall apply. No additional mitigation is required.

4.5.11-4 Pourroy Road-West / Auld Road

- Mitigation Measure 4.5.11-1 shall apply. No additional mitigation is required.

4.5.12-2 Winchester Road (SR-79) / Benton Road

- Mitigation Measure 4.5.12-1 shall apply. No additional mitigation is required.

4.5.13-2 Winchester Road (SR-79) / Willows Avenue

- Mitigation Measure 4.5.13-1 shall apply. No additional mitigation is required.

4.5.14-2 Pourroy Road-East / Auld Road

- Mitigation Measure 4.5.14-1 shall apply. No additional mitigation is required.

Q. Utilities and Service Systems

1. Impacts.

*Would the Project require or result in the construction of new water treatment facilities or expansion of existing facilities, the construction of which would cause significant environmental effects; or, have sufficient water supplies available to serve the Project from existing entitlements and resources, or are new or expanded entitlements needed?*

According to the Plan of Service Summary compiled by the Project engineer and Eastern Municipal Water District, the proposed Project is forecast to create a demand for up to 167,200 gallons of water per day at buildout. This includes both the proposed land use and the site landscaping. According to EMWD, this volume of water supply is available and the

1 demand will be primarily supplied by imported water, supplemented by  
2 local groundwater resources. A review of the EMWD 2010 Urban Water  
3 Management Plan (2011) documents the water availability for this Project  
4 and the whole EMWD service area, when the water shortage contingency  
5 plan and demand management measure are taken into account. Based on  
6 these substantiating data, provision of domestic water supply can be  
7 accomplished without causing significant impacts on the existing water  
8 system or existing entitlements.

9 The mitigation measures identified are feasible and would avoid or  
10 substantially lessen the potentially significant impacts associated with  
11 future Project-related water consumption impacts to a level of less than  
12 significant and no unavoidable adverse impacts would occur.

13 *Would the Project require or result in the construction of new wastewater*  
14 *treatment facilities, including septic systems, or expansion of existing*  
15 *facilities, the construction of which would cause significant environmental*  
16 *effects; or, result in a determination by the wastewater treatment provider*  
17 *that serves or may service the project that it has adequate capacity to serve*  
18 *the Project's projected demand in addition to the provider's existing*  
19 *commitments?*

20 According to the Plan of Service Summary compiled by the Project  
21 engineer and Eastern Municipal Water District, the proposed Project is  
22 forecast to generate up to 102,000 gallons of wastewater per day (gpd) at  
23 buildout. This wastewater will be delivered to the Temecula Valley  
24 Regional Water Reclamation Facility. According to the EMWD website  
25 this facility currently has typical daily wastewater flows of 12 million  
26 gallons per day. The plant's capacity is 18 mgd. The Project's 0.12 mgd  
27 generation of wastewater would consume two percent of the remaining  
28 capacity. This consumption of capacity will not cause the construction of

1 new wastewater treatment facilities. In addition, the EMWD (the  
2 wastewater treatment provider) indicates in the Plan of Service Summary in  
3 Appendix 2 that the Temecula Valley Facility has adequate capacity to  
4 serve the Project in addition to the provider's existing commitments. Thus,  
5 the proposed Project will consume some capacity of the existing Water  
6 Reclamation Facility, but the level of adverse impact is considered less than  
7 significant.

8 Is the Project served by a landfill with sufficient permitted capacity to  
9 accommodate the Project's solid waste disposal needs; or, does the Project  
10 comply with federal, state, and local statutes and regulations related to solid  
11 wastes (including the CIWMP (County Integrated Waste Management  
12 Plan)? The Project site is located about 20 miles south of two County  
13 regional municipal landfills, El Sobrante and Lamb Canyon. The Lamb  
14 Canyon Landfill is located between the City of Beaumont and City of San  
15 Jacinto at 16411 Lamb Canyon Road (State Route 79). The landfill  
16 property encompasses approximately 1,189 acres, of which 580.5 acres  
17 encompass the current landfill permit area. Of the 580.5-acre landfill  
18 permit area, approximately 144.6 acres are permitted for waste disposal.  
19 The landfill is currently permitted to receive about 5,000 tons of refuse per  
20 day and had an estimated total disposal capacity of approximately 15.646  
21 million tons as of June 30, 2009. As of January 2011, the landfill had a  
22 total remaining capacity of approximately 8.647 million tons. The current  
23 landfill remaining disposal capacity is estimated to last, at a minimum, until  
24 approximately 2021. During 2010 the Lamb Canyon Landfill accepted  
25 daily average volume of 1,703 tons and a period total of approximately  
26 529,744 tons. Landfill expansion potential exists at this landfill site.

27 The El Sobrante Landfill is located east of Interstate 15 and Temescal  
28 Canyon Road to the south of the City of Corona and Cajalco Road at 1910



1 Dawson Canyon Road. The landfill is owned and operated by USA Waste  
2 of California, a subsidiary of Waste Management, Inc. It encompasses  
3 1,322 acres, of which 645 acres are permitted for landfill operations.  
4 According to the El Sobrante operating permit, the Landfill has a total  
5 disposal capacity of approximately 209.91 million cubic yards and can  
6 receive up to 70,000 tons per week of refuse. The operating permit allows a  
7 maximum of 16,054 tons per day of waste to be accepted at the landfill, due  
8 to limitations on the number of vehicle trips per day. As of January 2011,  
9 the landfill had a remaining in-County disposal capacity of approximately  
10 38.506 million tons. In 2010, the El Sobrante Landfill accepted a total of  
11 694,963 tons, or approximately 0.695 million tons of waste generated  
12 within Riverside County. The daily average for in-County waste was 2,235  
13 tons during 2010. The landfill is expected to reach capacity in  
14 approximately 2045.

15 The County evaluates solid waste generation based on a per capita  
16 generation rate. Therefore, a review of solid waste generation rates  
17 published by CalRecycle was performed to obtain a reasonable rate of waste  
18 generation for the mixed business park/industrial Project. An industrial  
19 waste generation rate of 5 lbs./1,000 square feet per day is identified in the  
20 CalRecycle Waste Characterization sheets for the Industrial uses. For  
21 comparison, an office generation rate of 6 lbs./1,000 square feet per day is  
22 also identified in the CalRecycle Waste Characterization sheets for the  
23 Commercial sector. Therefore, an average 5.5 lbs./1,000 square feet per day  
24 will be used for the French Valley Airport Center. Based on the aggregate  
25 755,000 square feet of business park and industrial space, 4,252.5 lbs. of  
26 solid waste will be generated per day by the Center. Assuming a mandatory  
27 50% recycling rate, daily solid waste generation is forecast to be 2,126  
28 lbs/day for disposal at either the El Sobrante Landfill or the Lambs Canyon

1 Landfill. This is approximately one ton per day or an increase in solid  
2 waste disposal of about 0.05% at either landfill. Thus, the proposed Project  
3 will consume some capacity of the existing landfills, but the level of  
4 adverse impact is considered less than significant. There is adequate  
5 capacity at the area landfills to accommodate the solid waste generated by  
6 the proposed Project, and the Project will comply with all laws and  
7 regulations in managing solid waste.

8 *Would the Project impact the following facilities requiring or resulting in*  
9 *the construction of new facilities or the expansion of existing facilities; the*  
10 *construction of which could cause significant environmental effects to*  
11 *electricity?*

12 The proposed Project will consume electricity. A daily and annual  
13 electricity consumption rate for the total 754,411 square feet of business  
14 park and industrial park has been calculated. The total estimated daily  
15 electricity consumption has been estimated at 22 megawatts (MW). The  
16 annual estimated annual electricity consumption has been estimated at  
17 8,218 MW.

18 *Would the Project impact the following facilities requiring or resulting in*  
19 *the construction of new facilities or the expansion of existing facilities; the*  
20 *construction of which could cause significant environmental effects to*  
21 *natural gas?* The proposed Project will not be connected to the natural gas  
22 distribution system. Therefore, no requirement to install additional natural  
23 gas infrastructure will result from implementing the proposed Project.

24 *Would the Project impact the following facilities requiring or resulting in*  
25 *the construction of new facilities or the expansion of existing facilities; the*  
26 *construction of which could cause significant environmental effects to*  
27 *communication systems?*

28 The communication system is provided by Verizon. Verizon is a private

1 company that provides connection to the communication system on an as  
2 needed basis. No expansion of facilities will be necessary to connect the  
3 Project to the communication system.

4 *Would the Project impact the following facilities requiring or resulting in*  
5 *the construction of new facilities or the expansion of existing facilities; the*  
6 *construction of which could cause significant environmental effects to storm*  
7 *water drainage?*

8 The drainage system consists of the collection system within the developed  
9 area; a water quality basin that will limit flows to the existing natural  
10 channel to historic levels; and the discharge culverts to the natural stream  
11 channel retained on the Project site. This system will require maintenance  
12 by the property owner, but this Project will not place a substantial demand  
13 on the regional storm water drainage system.

14 *Would the Project impact the following facilities requiring or resulting in*  
15 *the construction of new facilities or the expansion of existing facilities; the*  
16 *construction of which could cause significant environmental effects to street*  
17 *lighting?*

18 New street lights will be installed by the proposed Project in accordance  
19 with standard requirements and County Ordinance No. 655. The  
20 installation of these lighting improvements are part of the proposed Project  
21 and with compliance with Ordinance No. 655, the installation and future  
22 operation of these street lights can be accomplished without causing  
23 significant adverse environmental impact.

24 *Would the Project impact the following facilities requiring or resulting in*  
25 *the construction of new facilities or the expansion of existing facilities; the*  
26 *construction of which could cause significant environmental effects to*  
27 *maintenance of public facilities, including roads?*

28 The Project will add new roads and circulation system improvements to the

1 County's circulation system. Other Project features, such as street lights,  
2 will also require future maintenance by the County. Ongoing maintenance  
3 costs will be covered by annual property taxes of the proposed Project and  
4 the future maintenance of public facilities will not cause significant adverse  
5 environmental impacts in the future.

6 *Would the Project impact the following facilities requiring or resulting in*  
7 *the construction of new facilities or the expansion of existing facilities; the*  
8 *construction of which could cause significant environmental effects to other*  
9 *governmental services?*

10 No demand for any other specific governmental services has been  
11 identified. However, as is the case with all large business parks or  
12 industrial areas, there may be random demand for emergency services or  
13 inspections by fire personnel in the future. No follow-on construction or  
14 permanent demand for any other governmental services has been identified.

15 *Would the Project impact the following facilities requiring or resulting in*  
16 *the construction of new facilities or the expansion of existing facilities; the*  
17 *construction of which could cause significant environmental effects to*  
18 *conflict with adopted energy conservation plans?*

19 The proposed Project must incorporate all of the current energy  
20 conservation design measures established by State law under Title 24.  
21 These requirements will be met for the new structures that will be installed  
22 if the proposed Project is approved. Therefore, the proposed Project will  
23 not have any conflict with energy conservation plans.

24 2. Mitigation.

25 The Project has been modified to mitigate or avoid the potentially  
26 significant impacts by the following mitigation measure:

- 27 a. Native and ornamental drought resistant plants shall be used in the  
28 common landscaped area and no invasive plant species listed in

1 Table 6-2 of the MSHCP shall be planted within the landscaped  
2 areas.

- 3 b. The Project landscape areas shall be plumbed with purple pipe. If  
4 and when reclaimed water becomes available at the Project site, the  
5 site landscape shall be watered with reclaimed water.
- 6 c. Low water consuming plumbing fixtures (toilets, etc.) shall be  
7 installed in the Project residences.
- 8 d. The applicant shall provide evidence to Building and Safety during  
9 the final inspection of all residential structures that demonstrates that  
10 low water consuming plumbing fixtures (toilets, etc.) were installed  
11 in the Project.

12 **BE IT FURTHER RESOLVED** by the Board of Supervisors that all applicable regulatory  
13 requirements and feasible mitigation measures to reduce environmental impacts have been considered and  
14 are applied as conditions of the Project approval, yet the following impacts potentially resulting from the  
15 approval of Plot Plan No. 25183 and Tentative Parcel Map No. 33691 (Revision No. 1) cannot be fully  
16 mitigated and will be only partially avoided or lessened by the mitigation measures hereinafter specified:  
17 a statement of overriding findings is therefore included herein:

18 A. Air Quality

19 1. Impacts.

20 *Would the Project result in a cumulatively considerable net increase of any*  
21 *criteria pollutant for which the project region is non-attainment under an*  
22 *applicable federal or state ambient air quality standard (including*  
23 *releasing emissions which exceed quantitative thresholds for ozone*  
24 *precursors)?*

25 The proposed Project operational-source emissions would exceed  
26 SCAQMD regional significance thresholds for VOCs during operation of  
27 Phase 1 (2016) and for VOCs and NOx during operation of Phase 2 (2019).  
28 Operational air emissions associated with the proposed Project are not

1 anticipated to exceed applicable air quality standards for any other criteria  
2 pollutant.

3 2. Mitigation.

4 The proposed Project cannot be fully mitigated below a level of significance  
5 for this issue area. The following mitigation measures can reduce, but not  
6 eliminate, air pollution emissions generated during long-term occupancy of  
7 the Project. Nonetheless, the following mitigation measures are provided to  
8 reduce operational emissions to the extent feasible.

9 a. MM4.2-5: Prior to the issuance of building permits, the Project  
10 proponent shall submit energy usage calculations to the Planning  
11 Division showing that the Project is designed to achieve 20%  
12 efficiency beyond the 2008 California Building Code Title 24  
13 requirements (in the aggregate). Example of measures that reduce  
14 energy consumption include, but are not limited to, the following (it  
15 being understood that the items listed below are not all required and  
16 merely present examples; the list is not all-inclusive and other  
17 features that reduce energy consumption also are acceptable):

- 18 • Increase in insulation such that heat transfer and thermal  
19 bridging is minimized;
- 20 • Limit air leakage through the structure and/or within the heating  
21 and cooling distribution system;
- 22 • Use of energy-efficient space heating and cooling equipment;
- 23 • Installation of electrical hook-ups at loading dock areas;
- 24 • Installation of dual-paned or other energy efficient windows;
- 25 • Use of interior and exterior energy efficient lighting that exceeds  
26 the 2008 California Title 24 Energy Efficiency performance  
27 standards;
- 28 • Installation of automatic devices to turn off lights where they are

1 not needed;

- 2 • Application of a paint and surface color palette that emphasizes
- 3 light and off-white colors that reflect heat away from buildings;
- 4 • Design of buildings with “cool roofs” using products certified by
- 5 the Cool Roof Rating Council, and/or exposed roof surfaces
- 6 using light and off-white colors; and
- 7 • Design of buildings to accommodate photo-voltaic solar
- 8 electricity systems or the installation of photo-voltaic solar
- 9 electricity systems.

10 b. MM4.2-6: To reduce energy consumption, the Project shall install

11 Energy Star-rated appliances.

12 c. MM4.2-7: To reduce energy demand associated with potable water

13 conveyance, the Project shall implement U.S. EPA Certified

14 WaterSense labeled or equivalent faucets, high-efficiency toilets

15 (HETs), and water-conserving shower heads.

16 d. MM4.2-8: In order to reduce vehicle reliance for short trips, the

17 Project shall include a master-planned design that creates an urban

18 center setting, enhancing walkability and connectivity as well as

19 incorporating bicycle lanes and paths, and improving the on-site

20 pedestrian network and connecting off-site.

21 e. MM4.2-9: The Project will reduce vehicle miles traveled and

22 emissions associated with trucks and vehicles by implementing the

23 following measure: Inform future building owners and recommend

24 that they implement a trip reduction program, for which all

25 employees shall be eligible to participate.

26 f. MM4.2-10: The Project will designate one parking space per

27 building for a future EV charging station and provide an EV

28 charging circuit conduit to this space.

- 1 g. MM4.2-11: The Project will provide natural gas lines in the public  
2 streets inside PM33691R1 to facilitate installation of future natural  
3 gas fueling stations at individual buildings.
- 4 h. MM4.2-12: The developer will strengthen the roofs of all structures  
5 to support installation of future solar panels by future building  
6 owners.

7 B. Traffic/Transportation

8 1. Impacts.

9 *Would the Project conflict with an applicable plan, ordinance or policy*  
10 *establishing a measure of effectiveness for the performance of the*  
11 *circulation system, taking into account all modes of transportation,*

12 *including mass transit and non-motorized travel and relevant components*  
13 *of the circulation system, including but not limited to intersections, streets,*  
14 *highways and freeways, pedestrian and bicycle paths, and mass transit?*

15 The proposed Project would not result in any Project specific significant  
16 circulation system effects during construction with implementation of the  
17 identified construction mitigation measures. The Project's contribution to  
18 the TUMF program as a fair share contribution is considered sufficient  
19 (refer to Section 15130(a)(3) to address the Project's fair share toward a  
20 mitigation measure or measures designed to alleviate any potential  
21 cumulative impacts; however, with implementation of the identified  
22 operational mitigation measures, the proposed Project can be implemented  
23 without causing any unavoidable adverse circulation system effects over the  
24 long-term with the following caveats and exceptions. This finding may not  
25 apply to the operation of the proposed Project. Three intersections on the  
26 affected circulation system cannot be fully mitigated to a less than  
27 significant impact, or the proposed mitigation would itself result in a  
28 significant adverse impact, even though the Project's contribution does not



1 cause the cumulatively considerable adverse impact.

2 2. Mitigation.

3 The proposed Project cannot be fully mitigated below a level of significance  
4 for this issue area. The following mitigation measures can reduce, but not  
5 eliminate, traffic/transportation impacts generated during long-term  
6 occupancy of the Project. Nonetheless, the following mitigation measures  
7 are provided to reduce operational impacts to the extent feasible.

8 a. The following mitigation measures are necessary to reduce impacts  
9 to “less-than-significant” for Phase 1-2016 without Clinton Keith  
10 Road extension traffic conditions (EAP):

11 4.5.1-1 Margarita Road / Murrieta Hot Springs Road

- 12 • Construct a 3rd eastbound through lane.
- 13 • Remove the southbound (west leg) cross walk.

14 4.5.2-1 Winchester Road (SR-79) / Thompson Road

- 15 • Construct a 2nd northbound left turn lane.
- 16 • Modify the traffic signal and implement overlap  
17 phasing on the eastbound right turn lane.
- 18 • Construct a 2nd westbound left turn lane.

19 4.5.3-1 Winchester Road (SR-79) / Auld Road

- 20 • Modify the traffic signal and implement overlap  
21 phasing on the northbound right turn lane.
- 22 • Construct a 2nd westbound left turn lane and a right  
23 turn lane.

24 4.5.4-1 Winchester Road (SR-79) / Murrieta Hot Springs Road

- 25 • Modify the traffic signal and implement overlap  
26 phasing on the southbound right turn lane.
- 27 • Remove the southbound (west leg) cross walk.

28 4.5.5-1 Winchester Road (SR-79) / Nicolas Road

- Modify the traffic signal and implement overlap phasing on the northbound right turn lane.

4.5.6-1 Winchester Road (SR-79) / Margarita Road

- Stripe a dedicated southbound right turn lane and modify the traffic signal to implement overlap phasing on the southbound right turn lane.

4.5.7-1 Winchester Road (SR-79) / Ynez Road

- Modify the traffic signal and implement overlap phasing on the eastbound right turn lane.

4.5.8-1 Winchester Road (SR-79) / I-15 Northbound Ramps

- Construct a southbound free-right turn lane.

4.5.9-1 Briggs Road / Auld Road

- Install a traffic signal.
- Modify the northbound free-right turn lane as a dedicated northbound right turn lane.
- Construct a southbound left turn lane.
- The existing intersection is skewed, and as such, improvements to this intersection should also include improving its alignment.

4.5.10-1 Industry Way / Auld Road

- Install a traffic signal.

4.5.11-1 Pourroy Road-West / Auld Road

- Install a traffic signal.

b. The following mitigation measures are necessary to reduce impacts to “less-than-significant” for Phase 1-2016 with Clinton Keith Road extension traffic conditions (EAP).

4.5.1-1 Winchester Road (SR-79) / Benton Road

- Construct a northbound left turn lane, 3rd through lane

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and modify the traffic signal to implement overlap phasing on the right turn lane.

- Construct a 3rd southbound shared through-right turn lane.
- Construct an eastbound left turn lane, two through lanes and right turn lane.
- Construct 2 westbound through lanes and modify the traffic signal to implement overlap phasing on the right turn lane.

4.5.1-2 Margarita Road / Murrieta Hot Springs Road

- Mitigation Measure 4.5.1-1 shall apply. No additional mitigation is required.

4.5.2-2 Winchester Road (SR-79) / Thompson Road

- Construct a 2nd northbound left turn lane.
- Modify the traffic signal and implement overlap phasing on the eastbound right turn lane.

4.5.3-2 Winchester Road (SR-79) / Auld Road

- Mitigation Measure 4.5.3-1 shall apply. No additional mitigation is required.

4.5.4-2 Winchester Road (SR-79) / Murrieta Hot Springs Road

- Mitigation Measure 4.5.4-1 shall apply. No additional mitigation is required.

4.5.5-2 Winchester Road (SR-79) / Nicolas Road

- Mitigation Measure 4.5.5-1 shall apply. No additional mitigation is required.

4.5.6-2 Winchester Road (SR-79) / Margarita Road

- Mitigation Measure 4.5.6-1 shall apply. No additional mitigation is required.

1 4.5.7-2 Winchester Road (SR-79) / Ynez Road

- 2 • Mitigation Measure 4.5.7-1 shall apply. No additional  
3 mitigation is required.

4 4.5.8-2 Winchester Road (SR-79) / I-15 Northbound Ramps

- 5 • Mitigation Measure 4.5.8-1 shall apply. No additional  
6 mitigation is required.

7 4.5.9-2 Briggs Road / Auld Road

- 8 • Mitigation Measure 4.5.9-1 shall apply.  
9 • Modify the traffic signal and implement overlap  
10 phasing on the northbound right turn lane.

11 4.5.10-2 Industry Way / Auld Road

- 12 • Mitigation Measure 4.5.10-1 shall apply. No additional  
13 mitigation is required.

14 4.5.11-2 Pourroy Road-West / Auld Road

- 15 • Mitigation Measure 4.5.11-1 shall apply. No additional  
16 mitigation is required.

17 c. The following mitigation measures are necessary to reduce impacts  
18 to “less-than-significant” for Phase 2-2019 without Clinton Keith  
19 Road extension traffic conditions (EAP).

20 4.5.13-1 Winchester Road (SR-79) / Willows Avenue

- 21 • Stripe a northbound right turn lane and modify the  
22 traffic signal to implement overlap phasing on the  
23 northbound right turn lane.

24 4.5.14-1 Pourroy Road-East / Auld Road

- 25 • Install a traffic signal.  
26 • Construct an eastbound left turn lane.

27 4.5.1-3 Margarita Road / Murrieta Hot Springs Road

- 28 • Mitigation Measure 4.5.1-1 shall apply. No additional

mitigation is required.

4.5.2-3 Winchester Road (SR-79) / Thompson Road

- Mitigation Measure 4.5.2-1 shall apply.
- Construct a 3rd southbound through lane.

4.5.3-3 Winchester Road (SR-79) / Auld Road

- Mitigation Measure 4.5.3-1 shall apply.
- Construct a 3rd northbound through lane.
- Construct a 2nd southbound left turn lane and 3rd shared through-right turn lane.

4.5.4-3 Winchester Road (SR-79) / Murrieta Hot Springs Road

- Mitigation measure 4.5.4-1 shall apply.
- Construct a 4th northbound through lane.
- Construct a 2nd southbound right turn lane and modify the traffic signal to implement overlap phasing.

4.5.5-3 Winchester Road (SR-79) / Nicolas Road

- Mitigation measure 4.5.5-1 shall apply.
- Construct a 2nd southbound right turn lane.

4.5.6-3 Winchester Road (SR-79) / Margarita Road

- Mitigation Measure 4.5.6-1 shall apply.
- The PM peak hour intersection delay is anticipated to exceed the City of Temecula's significance threshold of 2.0 seconds over pre-Project conditions with the implementation of Mitigation Measure 4.5.6-1. In order to achieve acceptable peak hour intersection operations: Construct a 2nd westbound right turn lane with overlap phasing. This improvement may not be feasible due to right-of-way constraints.

4.5.7-3 Winchester Road (SR-79) / Ynez Road

- Mitigation Measure 4.5.7-1 shall apply. No additional mitigation is required.

4.5.8-3 Winchester Road (SR-79) / I-15 Northbound Ramps

- Mitigation Measure 4.5.8-1 shall apply. No additional mitigation is required.

4.5.9-3 Briggs Road / Auld Road

- Mitigation measure 4.5.9-1 shall apply.
- Construct a 2nd northbound right turn lane and modify the traffic signal to implement overlap phasing on the northbound right turn lanes.

4.5.10-3 Industry Way / Auld Road

- Mitigation Measure 4.5.10-1 shall apply. No additional mitigation is required.

4.5.11-3 Pourroy Road-West / Auld Road

- Mitigation Measure 4.5.11-1 shall apply. No additional mitigation is required.

d. The following mitigation measures are necessary to reduce impacts to “less-than-significant” for Phase 2 - 2019 with Clinton Keith Road extension traffic conditions (EAP).

4.5.1-4 Margarita Road / Murrieta Hot Springs Road

- Mitigation Measure 4.5.1-1 shall apply. No additional mitigation is required.

4.5.2-4 Winchester Road (SR-79) / Thompson Road

- Mitigation Measure 4.5.2-1 shall apply. No additional mitigation is required.

4.5.3-4 Winchester Road (SR-79) / Auld Road

- Mitigation Measure 4.5.3-3 shall apply. No additional mitigation is required.

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- 4.5.4-4 Winchester Road (SR-79) / Murrieta Hot Springs Road
  - Mitigation Measure 4.5.4-3 shall apply. No additional mitigation is required.
- 4.5.5-4 Winchester Road (SR-79) / Nicolas Road
  - Mitigation Measure 4.5.5-3 shall apply. No additional mitigation is required.
- 4.5.6-4 Winchester Road (SR-79) / Margarita Road
  - Mitigation Measure 4.5.6-3 shall apply. No additional mitigation is required. Please note, a portion of the improvements required under Measure 4.5.6-3 to reduce impacts to a less than significant level may not be feasible due to right-of-way constraints.
- 4.5.7-4 Winchester Road (SR-79) / Ynez Road
  - Mitigation Measure 4.5.7-1 shall apply. No additional mitigation is required.
- 4.5.8-4 Winchester Road (SR-79) / I-15 Northbound Ramps
  - Mitigation Measure 4.5.8-1 shall apply. No additional mitigation is required.
- 4.5.9-4 Briggs Road / Auld Road
  - Mitigation Measure 4.5.9-3 shall apply. No additional mitigation is required.
- 4.5.10-4 Industry Way / Auld Road
  - Mitigation Measure 4.5.10-1 shall apply. No additional mitigation is required.
- 4.5.11-4 Pourroy Road-West / Auld Road
  - Mitigation Measure 4.5.11-1 shall apply. No additional mitigation is required.
- 4.5.12-2 Winchester Road (SR-79) / Benton Road

- Mitigation Measure 4.5.12-1 shall apply. No additional mitigation is required.

4.5.13-2 Winchester Road (SR-79) / Willows Avenue

- Mitigation Measure 4.5.13-1 shall apply. No additional mitigation is required.

4.5.14-2 Pourroy Road-East / Auld Road

- Mitigation Measure 4.5.14-1 shall apply. No additional mitigation is required.

**BE IT FURTHER RESOLVED** by the Board of Supervisors that State CEQA Guidelines (Section 15126, (g)), requires an EIR to discuss how a proposed Project could directly or indirectly lead to economic, population, or housing growth. The following growth-inducing impacts were considered in relation to the proposed Project:

A. The proposed Project would provide business park and light industrial development in a manner consistent with the land use and circulation objectives contained in the County's planning documents. The development would not require a significant extension of infrastructure to support the proposed Project. Infrastructure would be extended from adjacent properties to serve the site. Only the immediate access roads to the site would require expansion to serve the site. The Project would not provide infrastructure or service capacity that accommodates growth beyond the levels currently permitted by local or regional land use plans.

The proposed Project is not in an isolated area and would not extend substantial urban infrastructure into a new area that might cause new or additional development pressure on the intervening and surrounding land. Plot Plan No. 21163 was originally approved in 2008. As originally approved, the proposed Project would have resulted in the construction of approximately 121,520 square feet of business park development and 632,891 square feet of industrial park development on approximately 63 acres in two separate phases. A primary reason for the request to consider the proposed Plot Plan No. 25183 is that the French Valley Airport Master Plan deleted a previously planned (but



1 never built) secondary runway which led to a revision to the French Valley Airport Land  
2 Use Compatibility Plan (ALUCP). The change in the ALUCP allows for development on  
3 a portion of the previously approved Plot Plan (Parcel 2) where development had not been  
4 allowed due to potential runway incompatibilities.

5 If Plot Plan No. 25183 is approved as proposed, the only changes to the development of  
6 structures proposed by Plot Plan No. 21163 would occur on Parcels 2, 4 and 5. The  
7 arrangement of the structures on Parcels 4 and 5 proposed as Plot Plan No. 25183 are  
8 slightly altered from that which was approved as Plot Plan No. 21163. Within Parcel 2,  
9 which is located on the west side of the proposed Project, the plot plan is proposing 15  
10 single story light industrial structures between 3,000 and 30,000 square feet, consisting of  
11 33 individual units, 6 basins, parking, trash enclosures and access drive isles. Within  
12 Parcels 4 and 5, which are located on the east side of the of the proposed Project, the  
13 applicant is proposing 10 single-story light-industrial structures between 3,000 and 30,000  
14 square feet, consisting of 26 individual units, 4 basins, parking, trash enclosures and access  
15 drive isles. Most of the development of the Project site has already been approved, and  
16 the modified development proposed by the Project would expand development to  
17 encompass additional area, but it would not add square footage to the total development.

18 B. The proposed Project is not a large-scale project, relative to the surrounding area, that  
19 would have the potential of producing a “multiplier effect” resulting in substantial indirect  
20 community growth. Existing development within the Project vicinity includes the  
21 Southwest Justice Center located north of the Project site, the French Valley Airport  
22 located west of the site and single-family small agricultural uses located east of the site.  
23 While there is some vacant agricultural land to the south of the Project site, the proposed  
24 Project does not include any changes to the underlying land use designations. Thus, any  
25 future development proposed on adjacent or nearby lands would be required to be  
26 consistent with the land use designations for the location or would require approvals to  
27 alter land use designations that would require future environmental review. No growth  
28 beyond that which is provided for in the County and/or City land use policies and plans

1 could occur without subsequent review, including a separate environmental analysis, of  
2 land use policy. To reiterate, any future development that might be proposed for the land  
3 adjacent to the proposed Project would require subsequent environmental review,  
4 including review for consistency with the general plan. Similarly, any change in land use  
5 designations that might be proposed for land adjacent to the proposed Project would  
6 require subsequent environmental review.

7 C. Due to the nature and scale of the proposed Project, it will not induce substantial  
8 population growth in an area, either directly (for example, by proposing a substantial  
9 number of new homes and businesses) or indirectly (for example, through extension of  
10 roads or other infrastructure.)

11 **BE IT FURTHER RESOLVED** by the Board of Supervisors that it has considered the following  
12 alternatives identified in EIR No. 539 in light of the environmental impacts which cannot be fully  
13 mitigated or substantially lessened and has rejected those alternatives as infeasible for the reasons  
14 described below:

15 A. Alternative 1 – No Project Alternative

- 16 1. The No Project Alternative would also allow the Project site to be developed with  
17 the same overall land uses and density. The actual Project submitted to the County  
18 for approval does not include any change in the net square footage of the approved  
19 Project, only a slight change in the total acreage within the Project area and a  
20 modification of some conditions of approval related to the manner in which the  
21 Project contributes to area circulation system improvements.
- 22 2. Denial of the proposed Project (Plot Plan No. 25183 and TTM No. 33691) would  
23 not alter the potential development impacts of the Project on the circulation system  
24 or on the air quality environment affected by the proposed Project.
- 25 3. The significant adverse impacts of implementing the No Project Alternative will be  
26 exactly the same as approval of the proposed Project.
- 27 4. The No-Project Alternative, while feasible, is less than desirable than the proposed  
28 Project due to the accommodation of the proposed Project to the elimination of the

1 second runway at the French Valley Airport.

2 B. Alternative 2 – Big Box Warehouse Alternative

- 3 1. Based on the area available at the Project site, approximately 63 acres, it is assumed  
4 that a total of 1,100,000 square feet of high cube warehouse(s) could be constructed  
5 at the existing site adjacent to French Valley Airport.
- 6 2. This would substantially reduce the number of persons employed at the site from  
7 between 1,500-3,772 for the proposed Project to about 200-300 for the warehouse  
8 alternative.
- 9 3. Approximately 1.1 million square feet of large box warehouse would reduce the  
10 VOC emissions to a less than significant impact level, but NOx emissions would  
11 still exceed the SCAQMD threshold, primarily due to emissions from large trucks.
- 12 4. Vehicle trips would be substantially reduced on the area circulation system under  
13 the large box warehouse alternative; however, due to cumulative traffic increases in  
14 the Project area traffic impacts on the circulation system would remain unavoidably  
15 significant under this alternative.
- 16 5. Based on the general reduction in air emissions, except for NOx, the large box  
17 warehouse would be environmentally superior to the proposed Project.
- 18 6. Large box warehouses are typically located near major regional transportation  
19 corridors (motor vehicle and rail) to ensure that material can be easily delivered to  
20 and from the warehouse. The French Valley Airport is not located in a major  
21 regional transportation corridor.
- 22 7. The primary objective of the proposed Project (French Valley Airport Center) is to  
23 create a major employment center. A large box warehouse or warehouses is not a  
24 major employment center. It generate jobs, but substantially less than envisioned  
25 by the Center, 200-300 jobs versus the estimated 1,500-3,772 jobs forecast to be  
26 generated by the Center.
- 27 8. A large box warehouse project, although a plausible alternative on the property,  
28 cannot feasibly meet the overall Project objective.

1           **BE IT FURTHER RESOLVED** by the Board of Supervisors that the Project will implement  
2 applicable elements of the Riverside County General Plan as follows:

3           A.     Land Use Element

4                     1.     Analysis of applicable policies of the Land Use Element is presented  
5                             throughout EIR No. 539 and concludes that the Project would not conflict  
6                             with any applicable policy of the General Plan Land Use Element.  
7                             Furthermore the proposed Project complies with all design standards for the  
8                             various land use designation and considers the unique characteristics and  
9                             features of the Project site and surrounding community. The proposed  
10                            Project is consistent with the General Plan Land Use Element, and is  
11                            therefore consistent with the General Plan.

12           B.     Circulation Element

13                    1.     The Project will construct or contribute its fair share of the costs associated  
14                            with the improvement of roadways and certain intersections. The Project  
15                            will implement mitigation measures that address Project-specific and  
16                            cumulative transportation and traffic impacts, and based thereon, the Board  
17                            of Supervisors finds that the Project is consistent with the General Plan  
18                            Circulation Element. All required improvements that are directly  
19                            attributable to the Project would be constructed as part of the Project and  
20                            fair share costs would be contributed for improvements to affected off-site  
21                            roadways through payment of the TUMF, and County's Development  
22                            Impact Fee. There is no bus service adjacent to the Project. The proposed  
23                            Project is consistent with the General Plan Circulation Element, and is  
24                            therefore consistent with the General Plan.

25           C.     Multipurpose Open Space Element

26                    1.     The Multipurpose Open Space Element of the General Plan describes an  
27                            open space system which includes methods for the acquisition,  
28                            maintenance, and operation of a variety of open spaces. The County's open

1 spaces are utilized for visual relief, natural resources protection, habitat  
2 protection, recreational uses, and protection from natural hazards for public  
3 health and safety. The Project site is located within Criteria Cell No. 5879  
4 of the Multiple Species Habitat Conservation Plan (MSHCP). Accordingly,  
5 per Section 6.2 of the MSHCP, the proposed Project underwent the Habitat  
6 Acquisition Negotiation Strategy (HANS) process and Joint Project Review  
7 (JPR) review process. As a result of these review procedures, the property  
8 owner was required to conserve 8.3 acres of the Project site. This acreage  
9 has been set aside and the property was dedicated to the County under the  
10 MSHCP process.

- 11 2. Under the 2008 approval, the Project site has been mass graded and there  
12 are no remaining natural habitat values on the property. Thus, under the  
13 current site conditions the proposed Project cannot have any conflicts with  
14 the MSHCP or any other habitat or natural community conservation plan.
- 15 3. Implementation of the Project will not result in cultural resource impacts  
16 (including paleontological resources), that will exceed the established  
17 thresholds of significance. Nonetheless, as part of mitigation for potential  
18 impacts to unknown cultural resources, all ground-disturbing activities  
19 would be monitored.
- 20 4. The proposed Project does not include any recreational facilities nor will it  
21 directly include the use of existing parks which might experience  
22 substantial physical deterioration of such facilities. No adverse impact to  
23 recreation resources will result from Project implementation. The Project  
24 site is not located within a CSA or park district with Community Parks and  
25 Recreation Plan fees. Therefore, no adverse effect on such a district will  
26 result from Project implementation. No County designated trails are shown  
27 on the Project site. Therefore, the proposed Project has no potential to  
28 adversely impact such trails. The proposed Project is consistent with the

1 General Plan's Multipurpose Open Space Element, and is therefore  
2 consistent with the General Plan.

3 D. Safety Element

4 1. The Project complies with all applicable building codes, County  
5 Ordinances, and State and Federal laws. The Project complies with all  
6 applicable provisions of the Alquist-Priolo Earthquake Fault Zoning Act,  
7 and as concluded by the Project geotechnical study, the Project site is not  
8 subject to significant hazards associated with earthquake induced  
9 liquefaction, land sliding, or settlement (assuming the implementation of  
10 mitigation). In addition, the proposed Project would not be subject to  
11 significant flood or dam inundation. The Project also would comply with  
12 all applicable standards for fire safety and be consistent with the Riverside  
13 County Fire Protection Master Plan. Furthermore, Project impacts  
14 associated with hazardous waste and materials on the Project site would be  
15 mitigated below a level of significance, and the proposed Project would not  
16 conflict with any disaster preparedness plans nor subject individuals to  
17 significant risk of loss, injury, or death involving wildland fires, erosion,  
18 seismic activity, blow sand, or flooding. The proposed Project is consistent  
19 with the General Plan Safety Element, and is therefore consistent with the  
20 General Plan.

21 E. Noise Element

22 1. Project construction and operational noise impacts would be less than  
23 significant with mitigation incorporated. Mitigation is provided to reduce  
24 noise impacts to below a level of significance. With implementation of the  
25 recommendations provided in the noise impact analysis and the required  
26 mitigation measures, the Project would be consistent with the General Plan  
27 Noise Element, and is therefore consistent with the General Plan.

28 F. Air Quality Element

1                   1.       The Project-specific evaluation of emissions presented in the FEIR analysis  
2 demonstrates that after implementation of the recommended mitigation  
3 measures, operational of the proposed Project would result in exceedances  
4 of regional air quality thresholds. Construction activity is not projected to  
5 result in unavoidable significant adverse impacts. Once construction is  
6 completed the site will be occupied by future businesses. Implementation of  
7 the mitigation measures enhancement of the area's jobs/housing balance,  
8 and recommendations provided in Section 4.2 of EIR No. 539, and in the air  
9 quality technical study, would ensure that the proposed Project would be  
10 consistent with the Air Quality Element and General Plan.

11       G.       Housing Element

12                   1.       The purpose of the General Plan Housing Element is to meet the needs of  
13 existing and future residents in Riverside County through the establishment  
14 of policies to guide County decision-making and to establish an action plan  
15 to meet the County's housing goals in the next seven years. The Project is  
16 industrial and does not include any housing. Therefore, the Project would  
17 not impede the goals of the General Plan Housing Element. Accordingly,  
18 the proposed Project would be consistent with the General Plan Housing  
19 Element and General Plan.

20       H.       Administration Element

21                   1.       The Administration Element contains information regarding the structure of  
22 the General Plan as well as general planning principles and a statement  
23 regarding the vision for Riverside County. The General Plan Amendment  
24 proposed by the Project would be consistent with the Administration  
25 Element policies governing Foundation Amendments, as the proposed  
26 Project would help to achieve the purposes of the General Plan through  
27 compliance with applicable General Plan policies.

28       I.       Healthy Communities Element

1                   1.     The proposed project is consistent with the Healthy Communities  
2                   element. More specifically the project includes trails to encourage walking,  
3                   as prescribed for in policy HC 3.2. These trails are designed to carry  
4                   pedestrians through the site and beyond, connecting to existing trail in  
5                   existing residential communities surrounding the site as prescribed for in  
6                   policies HC 5.4 and 6.4. These include bike trails and pedestrian  
7                   trails. Additionally, the project is within a Specific Plan that places  
8                   residential uses close to large job centers including business parks, office,  
9                   and retail uses intended to foster walking between retail, jobs, and  
10                  residential uses which is specifically prescribed for in policies HC 6.5, HC  
11                  2.2 and HC 4.2.

12                  **BE IT FURTHER RESOLVED** by the Board of Supervisors that the Project would not conflict  
13 with the conservation requirements of the Western Riverside Multiple Species Habitat Conservation Plan  
14 (MSHCP) in that:

- 15                  A.     The Project site is located within Criteria Cell No. 5879 of the Multiple Species Habitat  
16 Conservation Plan (MSHCP). Accordingly, per Section 6.2 of the MSHCP, the proposed  
17 Project underwent the Habitat Acquisition Negotiation Strategy (HANS) process and Joint  
18 Project Review (JPR) review process. As a result of these review procedures, the property  
19 owner was required to conserve 8.3 acres of the Project site. This acreage has been set  
20 aside and the property was dedicated to the County under the MSHCP process.
- 21                  B.     Under the 2008 approval, the Project site has been mass graded and there are no remaining  
22 natural habitat values on the property. Thus, under the current site conditions the proposed  
23 Project cannot have any conflicts with the MSHCP or any other habitat or natural  
24 community conservation plan.
- 25                  C.     No special status species, endangered or threatened or otherwise protected, were identified  
26 on the Project site prior to the original project approval in 2008. Since the 2008 approval,  
27 the site has been mass graded and the sensitive habitat (a riparian stream through the  
28 property) has been preserved for conservation purposes. The area proposed for



1 development has no natural habitat and therefore cannot support special status species. No  
2 potential exists to adversely impact special status species.

3 D. The sensitive habitat within the Project site has been preserved and transferred to the  
4 County for long-term management. The remainder of the site has been mass graded and  
5 contains no habitat that could support any sensitive species.

6 E. The stream channel on the Project site that could support wildlife movement has been  
7 preserved. The remainder of the site has been mass graded and does not support wildlife  
8 movement. Thus, approval of the proposed Project has no potential to adversely impact  
9 wildlife movement through the Project area.

10 F. The riparian habitat onsite was preserved through the HANS process. No other riparian  
11 habitat exists on this mass graded site. Therefore, the proposed Project has no potential to  
12 adversely impact any riparian habitat or other sensitive natural community.

13 G. All wetlands subject to jurisdiction have been preserved on the property, and the remainder  
14 of the site has been mass graded. The proposed Project has no potential exists to adversely  
15 impact such resources.

16 H. With the exception of the stream channel preserved onsite, the site has been mass graded  
17 and no biological resources subject to local policies or ordinances exist onsite. Therefore,  
18 no potential for conflict with such policies can occur through approval of the proposed  
19 Project.

20 **BE IT FURTHER RESOLVED** by the Board of Supervisors that it has balanced the economic,  
21 legal, social, technological, and other benefits of the Project, against the unavoidable adverse  
22 environmental effects thereof, and has determined that the following benefits outweigh and render  
23 acceptable those environmental effects:

24 A. The proposed Project will provide an estimated 1,500-3,772 new jobs within the Southwest  
25 Area Plan region and 400 man-years of interim construction jobs.

26 B. The proposed Project will enhance the region's jobs/housing balance a major objective of  
27 the County Board of Supervisors.

28 C. The proposed Project contributes to regional infrastructure without in and itself causing

1 any adverse impacts to the area circulation system.

2 D. The proposed Project is expected to increase the property tax base exceeding \$100 million.

3 **BE IT FURTHER RESOLVED** by the Board of Supervisors that Plot Plan No. 25183 and  
4 Tentative Parcel Map No. 33691 (Revision No. 1) is consistent with the Riverside County General Plan.

5 **BE IT FURTHER RESOLVED** by the Board of Supervisors that it has reviewed and considered  
6 EIR No. 539 in evaluating Plot Plan No. 25183 and Tentative Parcel Map No. 33691 (Revision No. 1),  
7 that EIR No. 539 is an accurate and objective statement that complies with the California Environmental  
8 Quality Act and reflects the County's independent judgment, and that EIR No. 539 is incorporated herein  
9 by this reference.

10 **BE IT FURTHER RESOLVED** by the Board of Supervisors that it **CERTIFIES** EIR No. 539  
11 and **ADOPTS** the Mitigation Monitoring and Reporting Plan specified therein. In the event of any  
12 inconsistencies between the mitigation measures as set forth herein and the Mitigation Monitoring and  
13 Reporting Program, the Mitigation Monitoring and Reporting Program shall control.

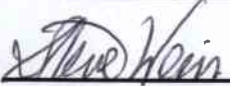
14 **BE IT FURTHER RESOLVED** by the Board of Supervisors that it **APPROVES** Plot Plan No.  
15 25183 and Tentative Parcel Map No. 33691 (Revision No. 1), on file with the Clerk of the Board, subject  
16 to the final conditions of approval and exhibits.

17 **BE IT FURTHER RESOLVED** by the Board of Supervisors that copies of Plot Plan No. 25183  
18 and Tentative Parcel Map No. 33691 (Revision No. 1) shall be placed on file in the Clerk of the Board, in  
19 the Office of the Planning Director, and in the Office of the Building and Safety Director, and that no  
20 applications for other development approvals shall be accepted for real property described and shown in  
21 the Project, unless such applications are substantially in accordance herewith.

22 **BE IT FURTHER RESOLVED** by the Board of Supervisors that the custodians of the  
23 documents upon which this decision is based are the Clerk of the Board of Supervisors and the County  
24 Planning Department and that such documents are located at 4080 Lemon Street, Riverside, California.

**Agenda Item No.:**  
**Area Plan: Southwest Area Plan**  
**Zoning Area: Rancho California**  
**Supervisorial District: Third**  
**Project Planner: Matt Straite**  
**Board of Supervisors: November 3, 2015**

**PLOT PLAN NO. 25183**  
**TENTATIVE PARCEL MAP NO. 33691R1**  
**ENVIRONMENTAL IMPACT REPORT NO. 539**  
**Applicant: French Valley Airport Center, LLC**  
**Engineer/Representative: Temecula**  
**Engineering Consultants**



Steve Weiss, AICP  
Planning Director

## **COUNTY OF RIVERSIDE PLANNING DEPARTMENT STAFF REPORT**

### **PROJECT DESCRIPTION AND LOCATION:**

**PLOT PLAN NO. 25183** proposes the development of a business/industrial park for single-story light industrial office buildings, comprised of 57 units and ranging from 3,000 to 30,000 square feet and with a combined gross floor area of 331,003 square feet. As shown in Exhibit A, parcel 2 is proposing 15 single-story light industrial structures between 3,000 and 30,000 square feet, consisting of 33 individual units, 6 basins, parking, trash enclosures and access drive isles. On Parcels 4 and 5 (the east side of the of the project), the applicant is proposing 10 single-story light industrial structures between 3,000 and 30,000 square feet, consisting of 26 individual units, 4 basins, parking, trash enclosures and access drive isles.

**TENTATIVE PARCEL MAP NO. 33691R1** proposes a Schedule E subdivision to divide 82.74 acres into 11 parcels for office/business and resides within Planning Area (PA) No. 2 of the Borel Airpark Center Specific Plan (SP265A1). The map is divided into two phases, phase 1 consists of parcels 2, 3, 5 and 6, phase two are parcels 1, 4, 7, 8, 9, 10, 11, and 12. NOTE: Phase 1 is proposed to be for condominium purposes, phase 2 is intended to be fee simple.

The project is located northerly of Jolyn Road, southerly of Auld Road, easterly of Sky Canyon Road, and westerly of Leon Avenue.

### **ISSUES OF POTENTIAL CONCERN:**

There are two primary reasons for the request to consider the new Plot Plan. First, the French Valley Airport Land Use Compatibility Plan (ALUCP) has been revised due to the elimination of the second runway at this airport. The consequence of this change in the ALUCP is that additional property owned by the developer of the French Valley Airport Center is available for development because the boundaries of the Airport Compatibility Zones have been modified to allow development on Parcel 2 of Planning Area 2, which was not previously available for development. The second reason is to obtain modifications to the conditions of approval regarding offsite circulation system improvements. The Developer seeks to pay for all required offsite intersection improvements instead of requiring the applicant to contract said improvements directly.

### **SUMMARY OF FINDINGS:**

1. Existing General Plan Land Use (Ex. #5): Community Development: Light Industrial (LI), as

reflected on the land use plan for SP265A1.

2. Surrounding General Plan Land Use (Ex. #5): Business Park (BP) and Public Facilities (PF) to the north, Open Space Conservation (OS-C) and Light Industrial (LI) to the south, Open Space Conservation (OS-C) and Business Park (BP) to the east, Public Facilities (PF) to the west as reflected on the land use plan for SP265A1.
3. Existing Zoning (Ex. #2): SP No. 265 A1.
4. Surrounding Zoning (Ex. #2): Manufacturing Service Commercial (MS-C) and Light Agriculture - 10 acre minimum (A-1-10) to the north, Specific Plan to the south, Light Agriculture - 5 acre minimum (A-1-5) and Light Agriculture - 10 acre minimum (A-1-10) to the east and Manufacturing Service Commercial (MS-C) to the west.
5. Existing Land Use (Ex. #1): Existing graded and vacant property
6. Surrounding Land Use (Ex. #1): Southwest Justice Center to the north, vacant open space and graded land utilized for dryland farming to the south, rural residences to the east, and French Valley Airport to the west.
7. Project Data:  
Tract Map No. 33691R1:  
Total Acreage: 82.74 acres  
Total Proposed Lots: 11  
Schedule: E
8. Environmental Concerns: See attached environmental assessment

**RECOMMENDATIONS:**

**CERTIFY ENVIRONMENTAL IMPACT REPORT NO. 539**, based on the findings and conclusions in Environmental Assessment No. 42533;

**ADOPT RESOLUTION NO. 539** certifying Environmental Impact Report No. 539 based upon the findings and conclusions in the resolution and incorporated in the staff report; and,

**APPROVE PLOT PLAN NO. 25183**, subject to the attached conditions of approval, and based upon the findings and conclusions incorporated in the staff report; and,

**APPROVE TENTATIVE PARCEL MAP NO. 33691R1**, subject to the attached conditions of approval, and based upon the findings and conclusions incorporated in the staff report.

**FINDINGS:** The following findings are in addition to those incorporated in the summary of findings, and in the Environmental Impact Report which is incorporated herein by reference.

1. The project site is designated Community Development: Light Industrial (LI) within the Southwest Area Plan, as reflected on the Land Use Plan for Specific Plan No.265 Amendment No.1 (SP265A1).
2. The proposed use, the development of a business/industrial park for single-story light industrial office buildings, comprised of 57 units and ranging from 3,000 to 30,000 square feet and with a combined gross floor area of 331,003 square feet, as well as, a Schedule E subdivision to divide 82.74 acres into 11 parcels for office/business, is consistent with the Community Development: Light Industrial (LI), as reflected on the land use plan for SP265A1 designation, because this industrial use implements all aspects of the General Plan Land Use designation.
3. Plot Plan No. 25183, and Tentative Parcel Map No. 33691R1 are located within Specific Plan No. 265, Amendment No. 1 (SP00265A1) Planning Area Number 2 which is located within the Highway 79 Policy Area. SP00265A1 analyzed this policy area and was determined to be consistent with the requirements of Highway 79 Policy Area. The proposed project is in conformance with SP00265A1 and is therefore also in conformance with the Highway 79 Policy Area. The Policy area is specific to restrictions for residential projects. This project proposes no residential uses.
4. Based on the review of the project by staff and other County departments the project is consistent with all the requirements of Ordinance No 460, Schedule E because it conforms to all the standards and requirements.
5. The project site is surrounded by properties which are designated Business Park (BP) and Public Facilities (PF) to the north, Open Space Conservation (OS-C) and Light Industrial (LI) to the south, Open Space Conservation (OS-C) and Business Park (BP) to the east, Public Facilities (PF) to the west, as reflected on the Conditional Use Permit for SP265A1.
6. The zoning for the subject site is Specific Plan No. 265 A1 (SP265A1).
7. The proposed use, a business/industrial park for single-story light industrial office building, is permitted in the Specific Plan Zoning Ordinance for Planning Area No. 2 as permitted with a Plot Plan.
8. The project site is surrounded by properties which are zoned Manufacturing Service Commercial (MS-C) and Light Agriculture - 10 acre minimum (A-1-10) to the north, Specific Plan to the south, Light Agriculture - 5 acre minimum (A-1-5) and Light Agriculture - 10 acre minimum (A-1-10) to the east and Manufacturing Service Commercial (MS-C) to the west.
9. Similar uses have been constructed and are operating in the project vicinity along the southern boundary of the Specific Plan.

10. This project is located within Criteria Cell Number 5879 of the Western Riverside County Multiple Species Habitat Conservation Plan, and as such will be required to dedicate 8.2 acres of conservation to the Western Riverside County Regional Conservation Authority (RCA). The area identified for conservation shall be offered in a fee title donation or in a conservation easement, of which will be decided upon by RCA . This project therefore fulfills those requirements.
11. This project is within the City Sphere of Influence of Temecula.
12. The proposed project is not located within a CAL FIRE state responsibility area or a very high fire hazard severity zone.
13. It was concluded that the proposed project could result in significant impacts to the following environmental issues: Air Quality and Traffic/Transportation (discussed below). All other potential impacts were determined to be less than significant without mitigation or can be reduced to a less than significant level with implementation of the mitigation measures identified in the DEIR.

Air Quality:

The project-specific evaluation of emissions and analysis presented in the DEIR demonstrated that after implementation of the recommended mitigation measures, construction of the proposed project would not result in exceedances of regional air quality thresholds. Thus, construction activity is not projected to result in unavoidable significant adverse impacts. After construction of the proposed project is completed, construction related air emissions will cease. Thus, no significant irreversible air quality impacts would occur as a result of project construction.

Transportation/Traffic:

Based on the evaluation presented in the DEIR, the proposed project would not result in any project specific significant circulation system effects during construction with implementation of the identified construction mitigation measures. With implementation of the identified operational mitigation measures, the proposed project can be implemented without causing any unavoidable adverse circulation system effects over the long-term with three exceptions. These exceptions involve three intersections where the proposed mitigation is either infeasible or where the recommended mitigation itself would result in an adverse impact.

**CONCLUSIONS:**

1. The proposed project is in conformance with the Community Development: Light Industrial (LI) Land Use Designation, and with all other elements of the Riverside County General Plan.
2. The proposed project is consistent with the Specific Plan 265A1 zoning classification of Ordinance No. 348, and with all other applicable provisions of Ordinance No. 348.
3. The proposed project is consistent with the Schedule E map requirements of Ordinance No. 460, and with other applicable provisions of Ordinance No. 460.
4. The public's health, safety, and general welfare are protected through project design.

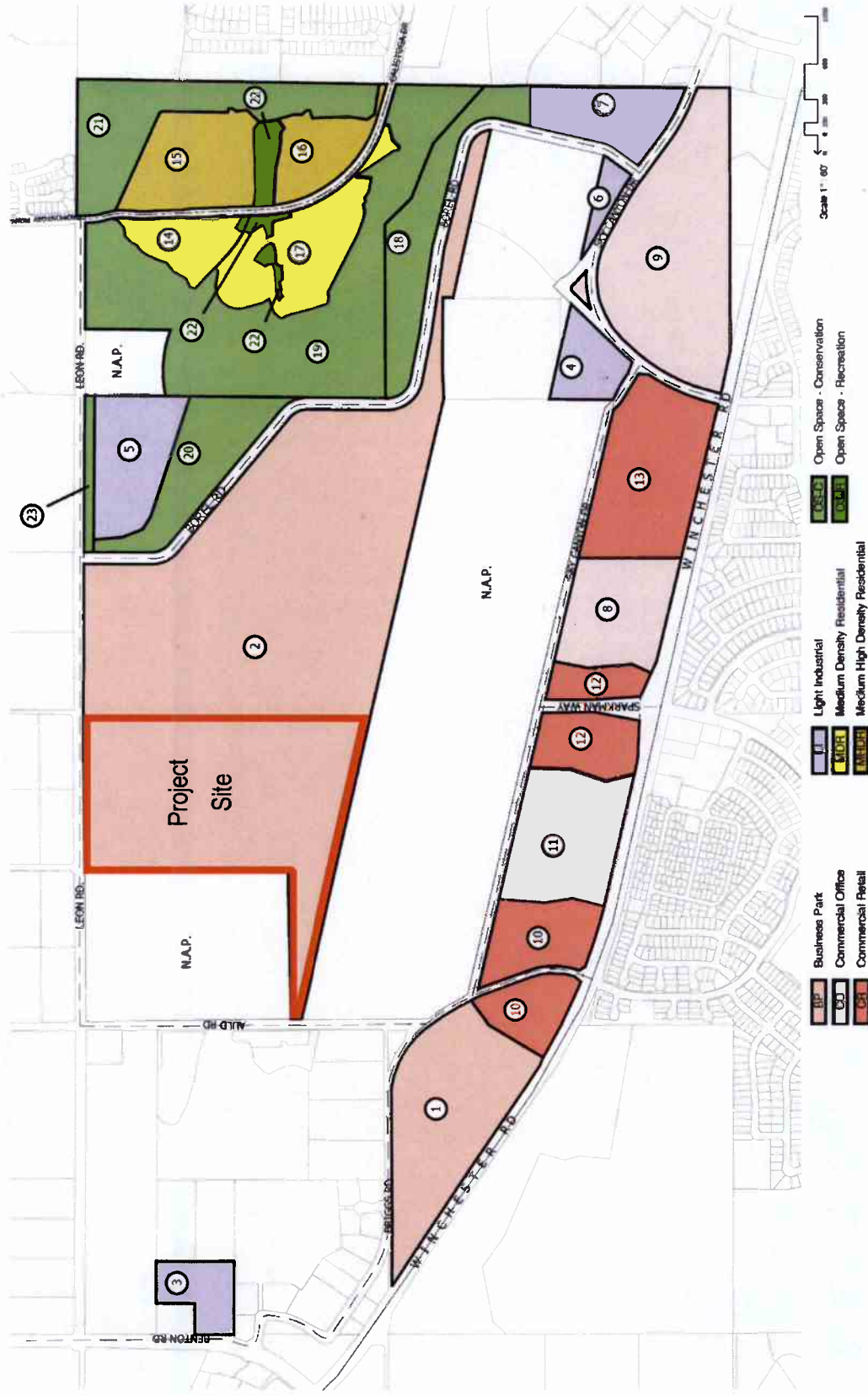
5. The proposed project is conditionally compatible with the present and future logical development of the area.
6. The proposed project will have a significant effect on the environment.
7. The proposed project will not preclude reserve design for the Western Riverside County Multiple Species Habitat Conservation Plan (WRCMSHCP).

**INFORMATIONAL ITEMS:**

1. As of this writing, no letters, in support or opposition have been received.
2. The project site is not located within:
  - a. County Service Area;
  - b. A Fault zone;
  - c. State responsibility or high fire hazard severity zone;
  - d. A 100-year flood plain, an area drainage plan, or dam inundation area; or,
  - e. An Agriculture Preserve.
3. The project site is located within:
  - a. The boundaries of the Southwest Area Plan;
  - b. City of Temecula Sphere of Influence;
  - c. An area of high paleontological sensitivity;
  - d. Stephens Kangaroo Rat Fee Area;
  - e. An area of low liquefaction;
  - f. A low potential for liquefaction;
  - g. An area of susceptibility for subsidence; and,
  - h. The Valley Wide Parks and Recreation District.

The subject site is currently designated as Assessor's Parcel Numbers 963-080-002.

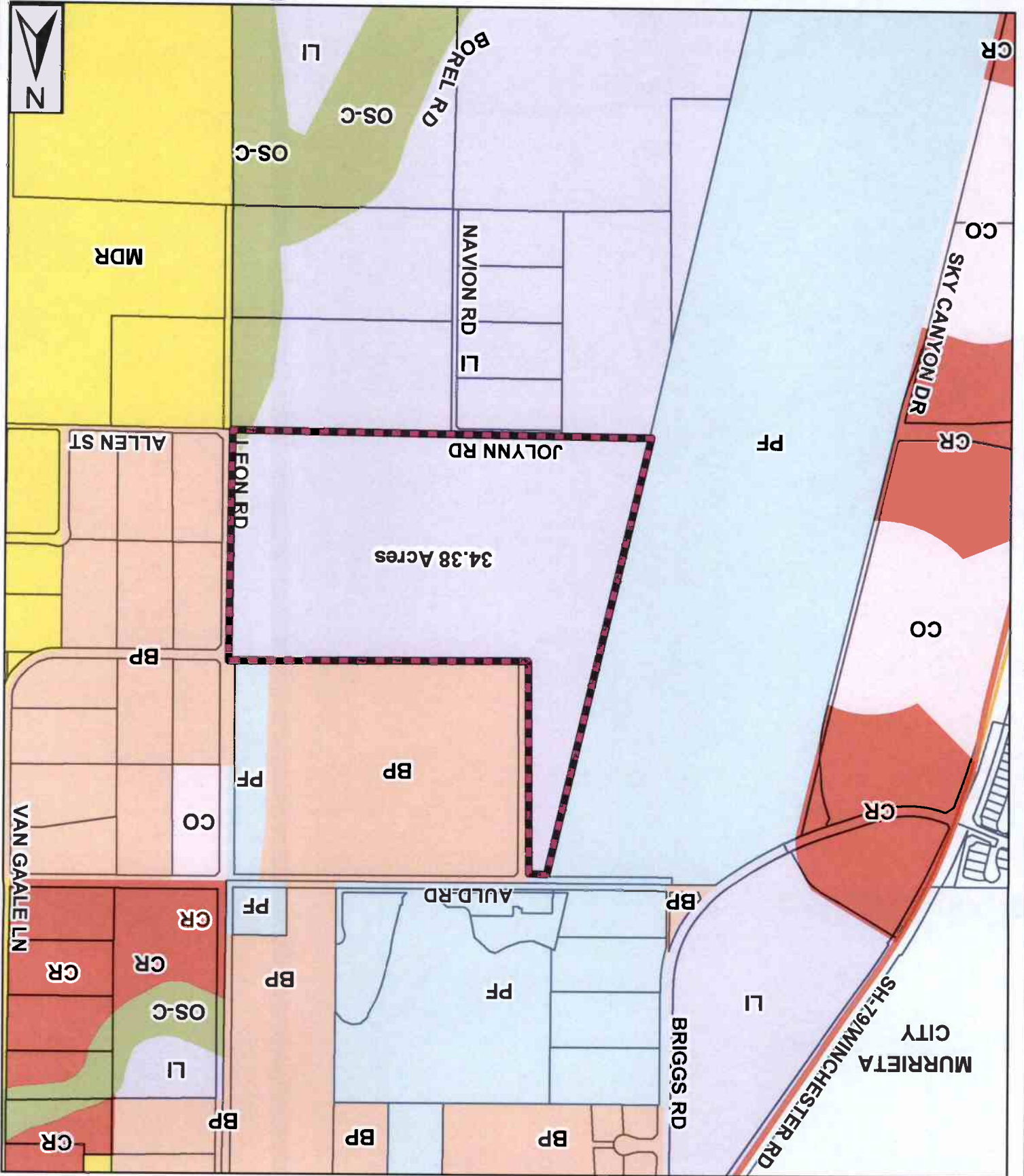
Section I - Executive Summary





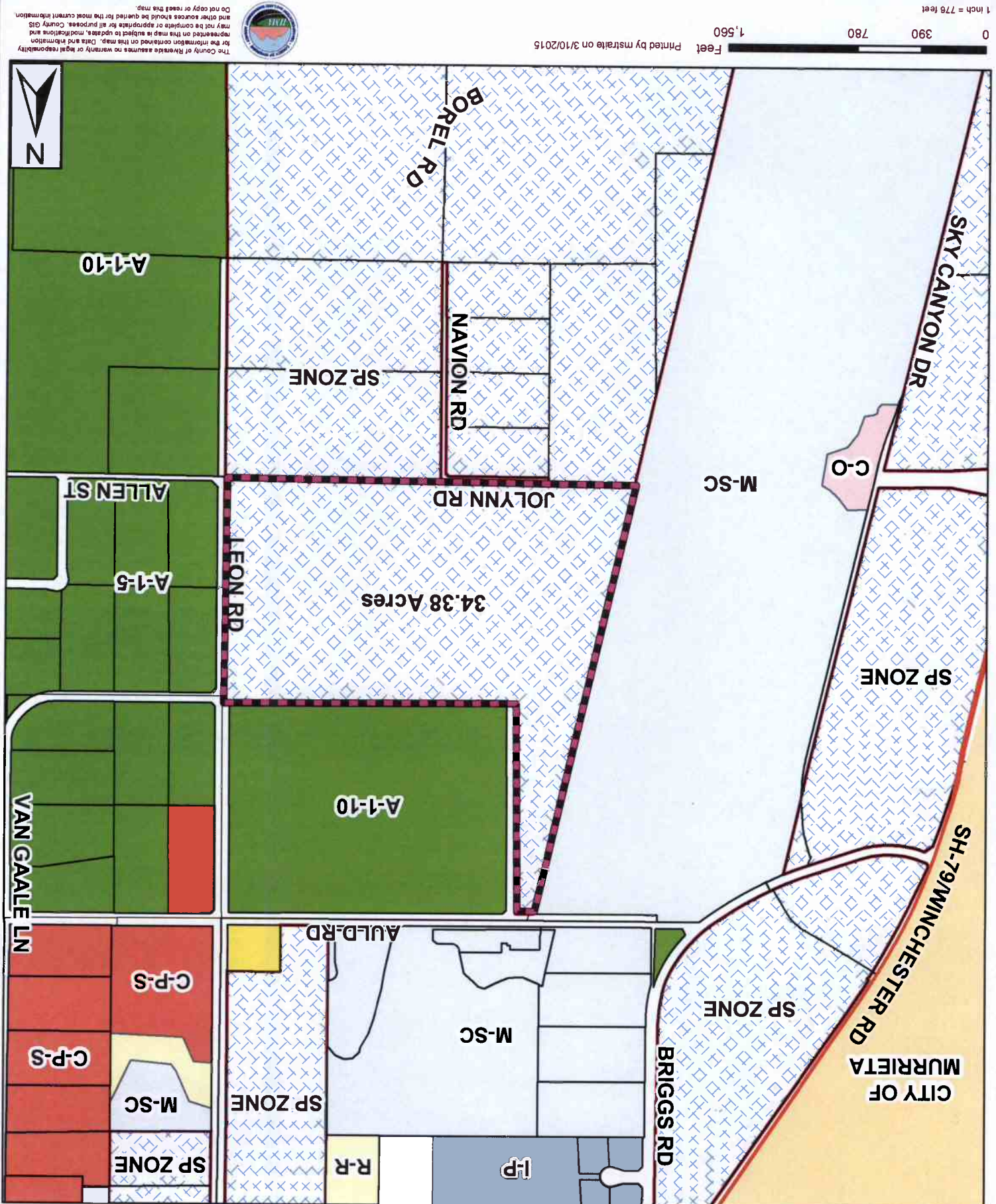
**RIVERSIDE COUNTY PLANNING DEPARTMENT**  
**PP25183 / PM33691R1**  
**EXISTING GENERAL PLAN**

Supervisor Stone  
 District 3



**RIVERSIDE COUNTY PLANNING DEPARTMENT**  
**PP25183 / PM33691R1**  
**EXISTING ZONING**

Supervisor Stone  
 District 3



The County of Riverside assumes no warranty or legal responsibility for the information contained on this map. Data and information represented on this map is subject to updates, modifications and other sources should be queried for the most current information. Do not copy or reuse this map.

Printed by mstrato on 3/10/2015

Supervisor Stone  
District 3

# RIVERSIDE COUNTY PLANNING DEPARTMENT

## PP25183 / PM33691R1

### LANDUSE



0 275 550 1,100 Feet Orthophotos Flown 2/11 (MR, CV) or 4/07 (REMAP, Blythe)  
Printed by nstrale on 3/5/2015  
1 inch = 542 feet

The County of Riverside assumes no liability or legal responsibility for the information contained on this map. Data and information represented on this map is subject to updates, modifications and may not be complete or appropriate for all purposes. County GIS has queried for the most current information. Do not copy or reuse this map.

# RIVERSIDE COUNTY PLANNING DEPARTMENT

## PP25183 / PM33691R1

### VICINITY / POLICY AREAS

Supervisor Stone  
District 3



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Feet  
0 700 1,400 2,800  
1 inch = 1,417 feet

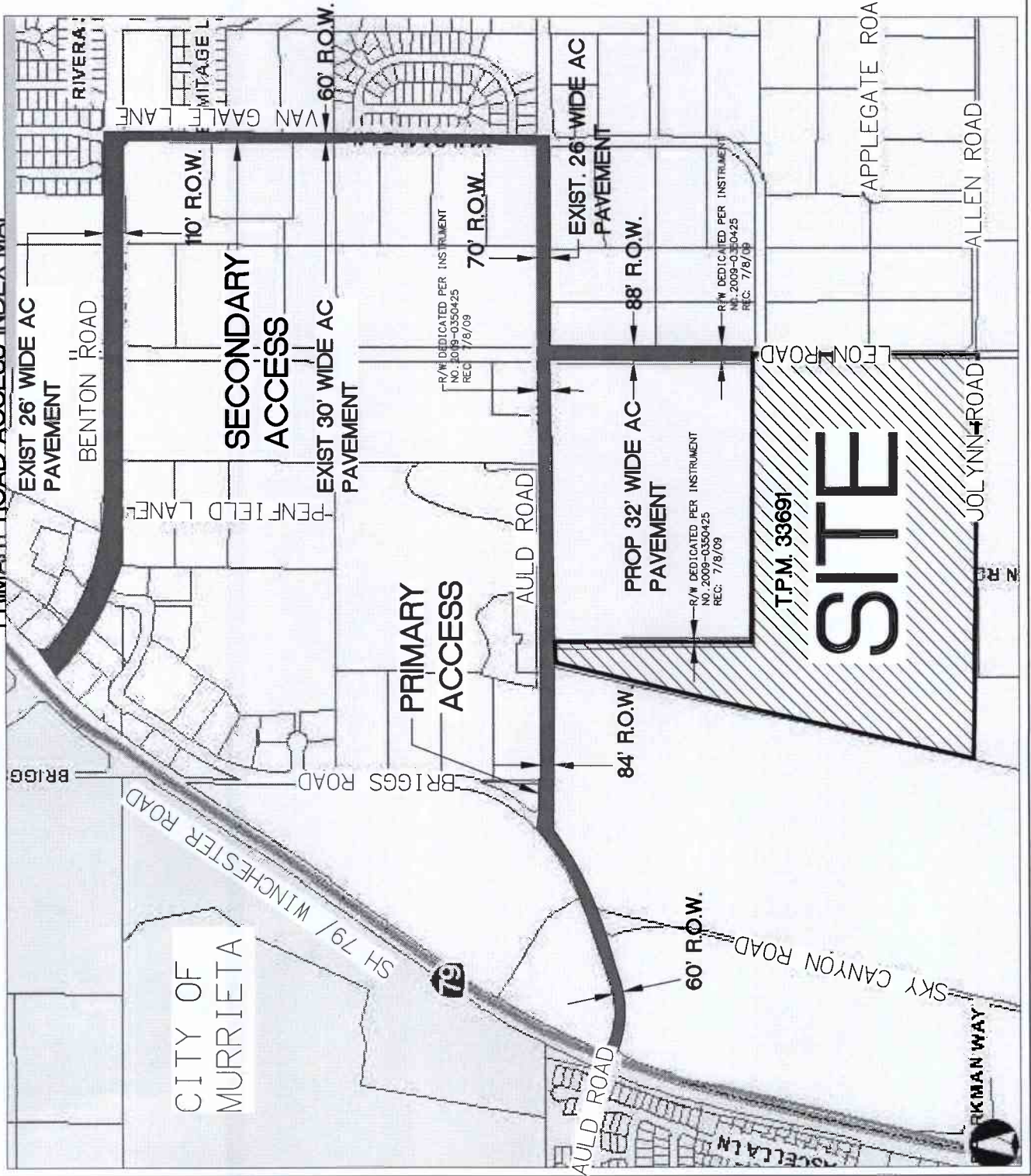
Orthophotos Flown 2111 (WR, CV) or 4107 (REMAP, Blythe)  
Printed by mstraitte on 3/10/2015



# TENTATIVE PARCEL MAP NO. 33691- REVISED NO. 1

## PRIMARY ROAD ACCESS INDEX MAP

**ROAD WIDTH NOTES:**  
ROAD WIDTHS SHOWN REPRESENT THE NARROWEST WIDTH OF A.C. PAVING WITHIN THE RESPECTIVE ROAD.



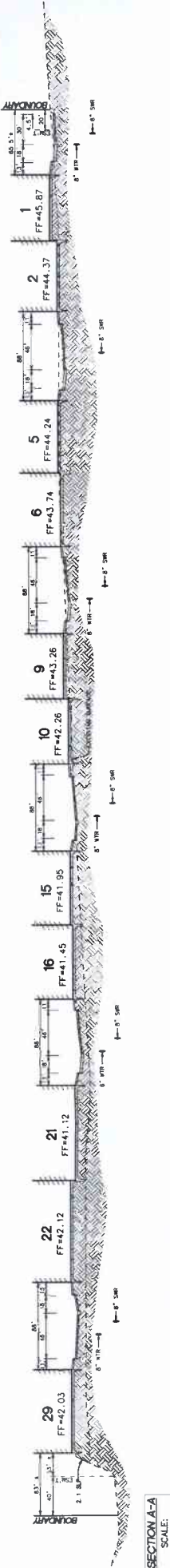
SCHEDULE 'E'  
BOREL AIRPARK CENTER  
SECTION PLAN NO. 290  
ADOPTED OCTOBER 4, 1984

<b>TTC TEMECULA ENGINEERING CONSULTANTS INC.</b>	
LAND PLANNING, CIVIL ENGINEERING, CONSTRUCTION CONSULTANTS	
10000 JOLYNN ROAD, SUITE 100, TEMECULA, CALIFORNIA 92590-1000	
TELEPHONE 951-976-1018 FAX 951-976-2214	
DATE: 10/07/14	
PROJECT NO.	33691
DATE	10/07/14
SCALE	AS SHOWN
DATE	10/07/14
BY	W. J. ...
CHECKED BY	...
DATE	...
SCALE	...
DATE	...

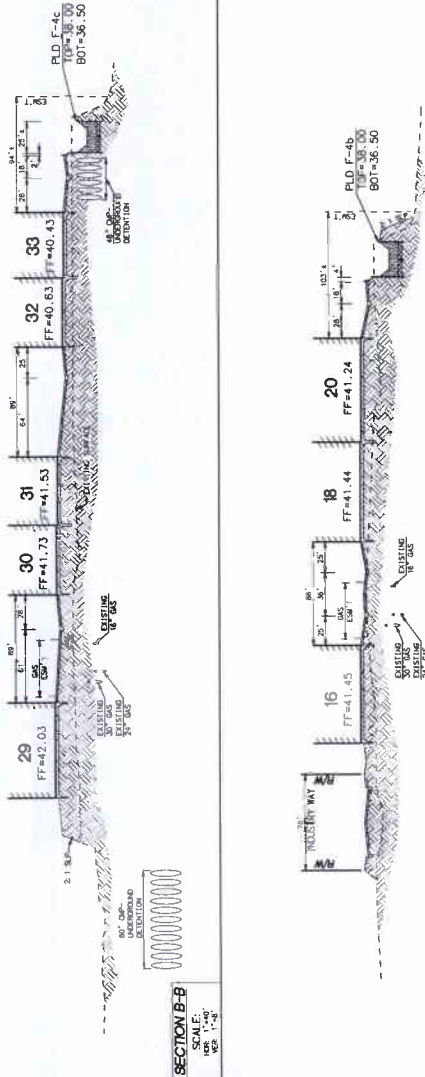




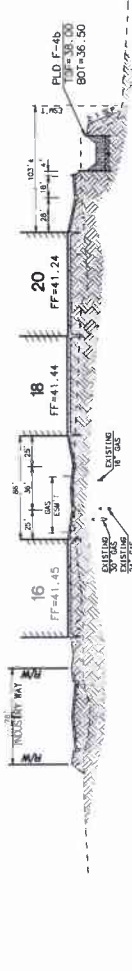




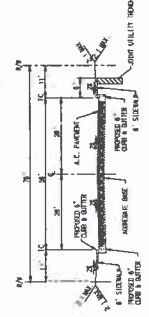
**SECTION A-A**  
 SCALE:  
 HORIZ. 1"=40'  
 VERT. 1"=4'



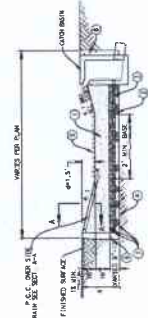
**SECTION B-B**  
 SCALE:  
 HORIZ. 1"=40'  
 VERT. 1"=4'



**SECTION C-C**  
 SCALE:  
 HORIZ. 1"=40'  
 VERT. 1"=4'

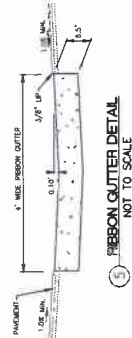


**INDUSTRY WAY TRUCK STOP TO SCALE**  
 NOT TO SCALE  
 PER REV. CD. JOB NO. 111

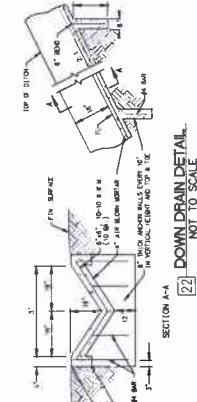


1. ALL CONCRETE SHALL BE CLASS 'C' CONCRETE.
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50. ALL CONCRETE SHALL BE FINISHED TO MATCH.

**POROUS LANDSCAPE DETENTION DETAIL**  
 NOT TO SCALE



**RIBBON GUTTER DETAIL**  
 NOT TO SCALE



**DOWN DRAIN DETAIL**  
 NOT TO SCALE



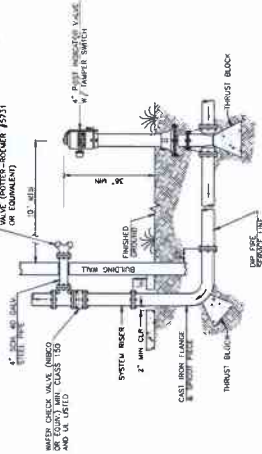
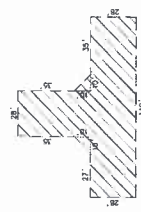
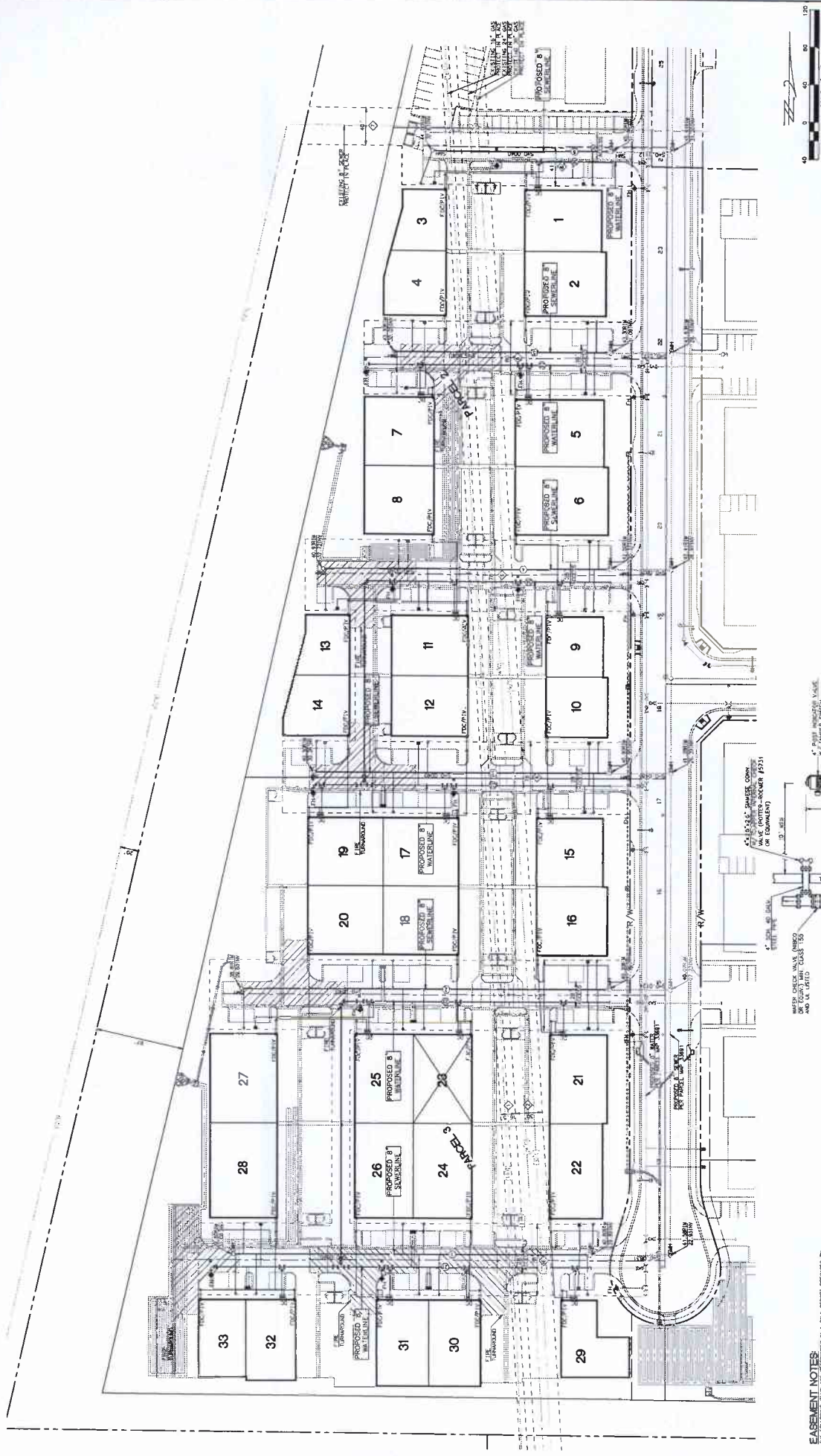
**CONCRETE GUTTER**  
 NOT TO SCALE

**TEC**  
TECHNICAL  
ENGINEERING  
CONSULTANTS, INC.

1100 PARKWAY ONE, FORTYSEVEN, FORTYSEVEN, FORTYSEVEN, FORTYSEVEN, FORTYSEVEN  
2077 RANCHO CALABAZA RD., ST. 202, TEMECULA, CA 92591  
DATE: 10/26/14 1-916-10 8 FASCIMILE 951-26-7311-4  
CELL: 951-26-7311-4

PROJECT NO.	14-00000
DATE	10/26/14
SCALE	AS SHOWN
DESIGNED BY	TEC
CHECKED BY	TEC
APPROVED BY	TEC
PROJECT NO.	14-00000
DATE	10/26/14
SCALE	AS SHOWN
DESIGNED BY	TEC
CHECKED BY	TEC
APPROVED BY	TEC

COUNTY OF RIVERSIDE  
CONCEPTUAL FIRE DEPARTMENT TURNAROUND (PART 2 AND 3)  
PLOT PLAN 25183, PAR 2, 3, 5 AND 6  
PK233691



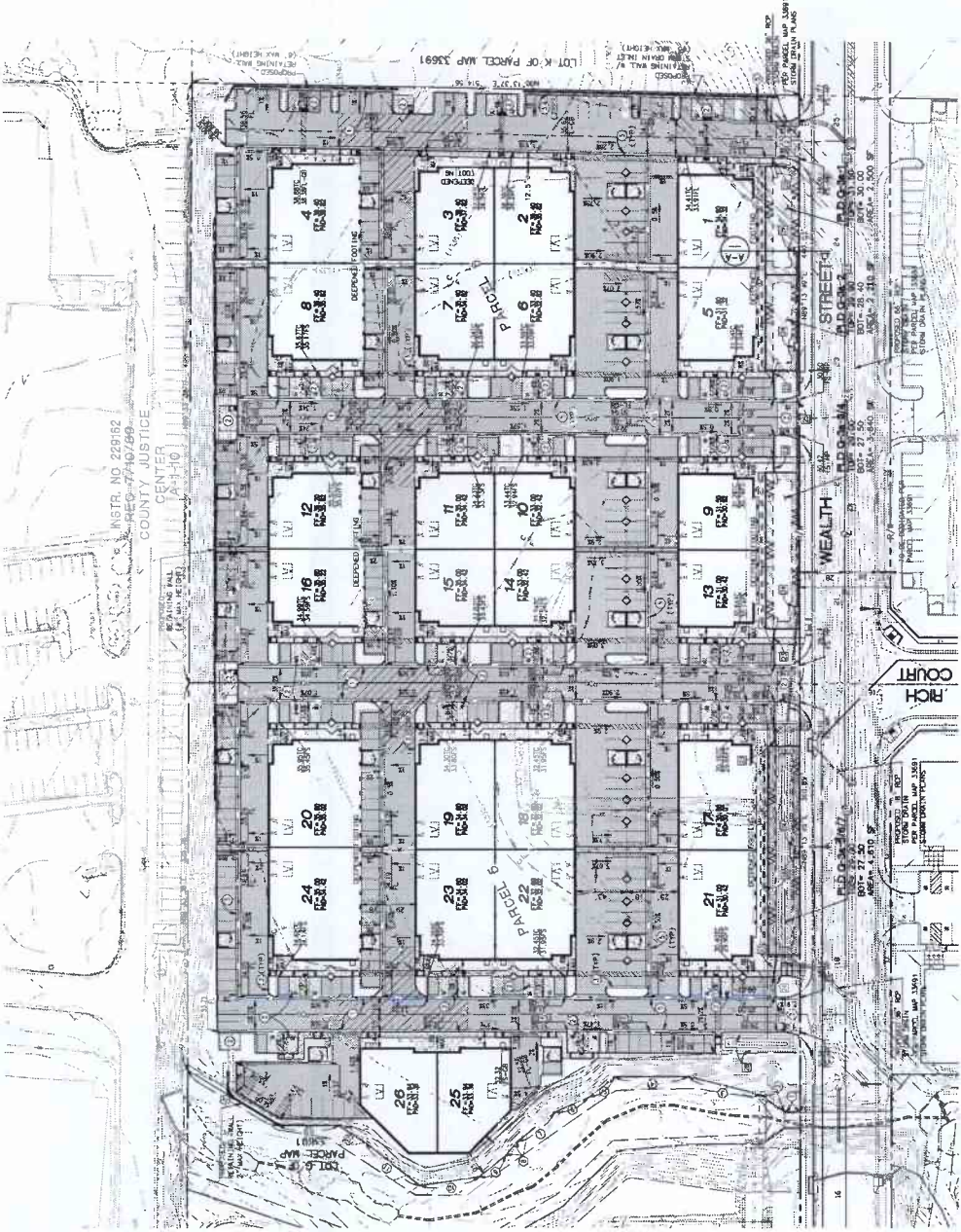
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  - 2. THE TURNAROUND SHALL BE MAINTAINED AS A FULL REPORT PREPARED BY THE FIRE DEPARTMENT (PART 2 AND 3) (PROJECT NUMBER 14-00000) AS SHOWN ON SHEET 14-00000-01.
  - 3. THE TURNAROUND SHALL BE MAINTAINED AS A FULL REPORT PREPARED BY THE FIRE DEPARTMENT (PART 2 AND 3) (PROJECT NUMBER 14-00000) AS SHOWN ON SHEET 14-00000-01.
  - 4. THE TURNAROUND SHALL BE MAINTAINED AS A FULL REPORT PREPARED BY THE FIRE DEPARTMENT (PART 2 AND 3) (PROJECT NUMBER 14-00000) AS SHOWN ON SHEET 14-00000-01.
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  - 10. THE TURNAROUND SHALL BE MAINTAINED AS A FULL REPORT PREPARED BY THE FIRE DEPARTMENT (PART 2 AND 3) (PROJECT NUMBER 14-00000) AS SHOWN ON SHEET 14-00000-01.

**TEC**  
TEMECULA  
ENGINEERING  
CONSULTANTS INC.

LAND PLANNING, CIVIL ENGINEERING, CONSTRUCTION CONSULTANTS  
1000 W. TEMECULA AVENUE, SUITE 200  
TEMECULA, CALIFORNIA 92592  
TELEPHONE 951-696-1010 FAX 951-696-2254  
DATE: 10/20/14 PROJECT NO: 208-167-77-6

DESIGNED BY	DATE	SCALE
CHECKED BY	DATE	SCALE
APPROVED BY	DATE	SCALE

COUNTY OF RIVERSIDE  
CONCEPTUAL GRADING PLAN (PAR 5+6)  
APPROVED FOR  
PROJECT NO. 208-167-77-6  
PLOT PLAN 258-167-77-6  
5 OF 6



SECTION A-A  
NOT TO SCALE

**CONSTRUCTION NOTES AND ESTIMATED QUANTITIES:**

**CONCEPTUAL GRADING NOTES:**

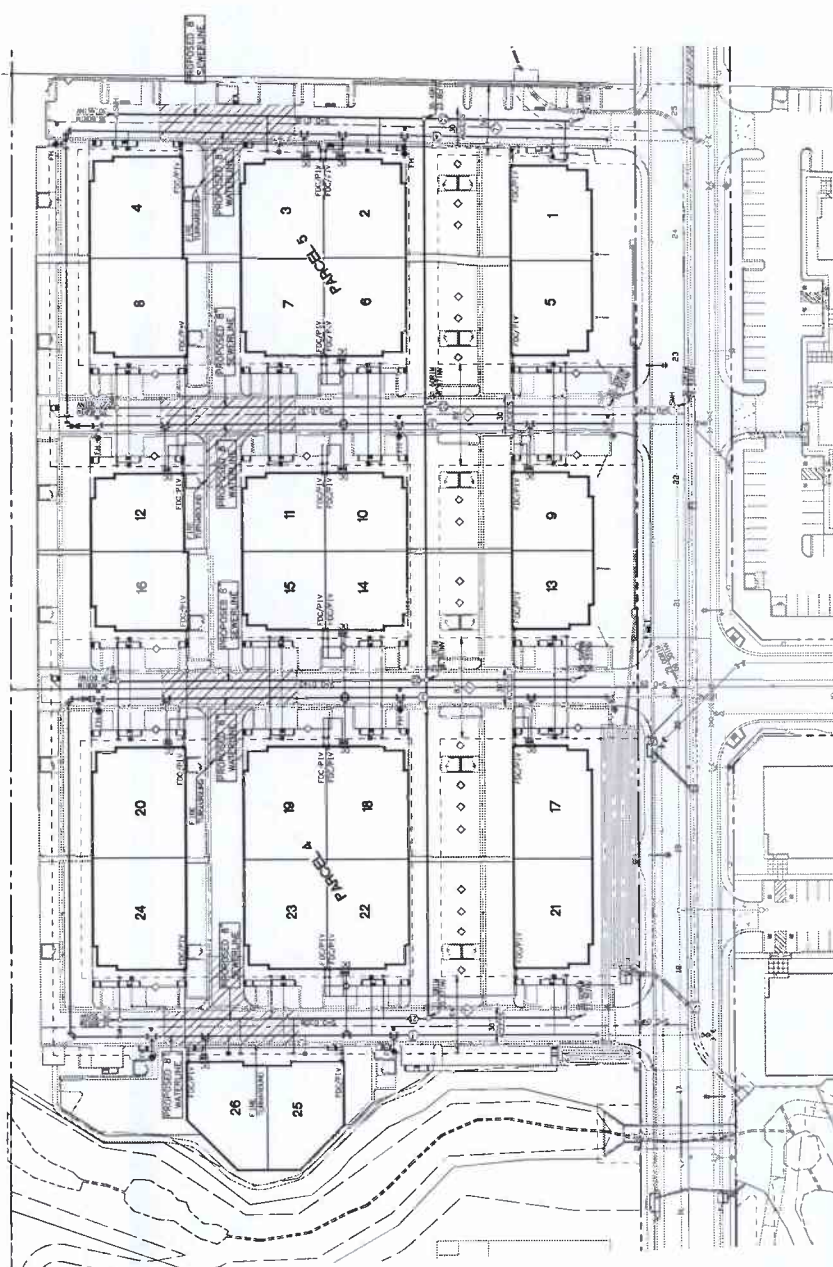
- 1) PLACE 3/4" #4 PAVING OVER 11" CLASS 3 AGGREGATE BASE
- 2) PLACE 6" CONCRETE PAVING
- 3) CONSTRUCT 12" DIA. CONCRETE CURB PER DETAIL ON SHEET 3
- 4) CONSTRUCT 12" DIA. CONCRETE GUTTER PER DETAIL ON SHEET 3
- 5) CONSTRUCT 12" DIA. CONCRETE GUTTER PER DETAIL ON SHEET 3
- 6) CONSTRUCT 12" DIA. CONCRETE GUTTER PER DETAIL ON SHEET 3
- 7) CONSTRUCT 12" DIA. CONCRETE GUTTER PER DETAIL ON SHEET 3
- 8) CONSTRUCT 12" DIA. CONCRETE GUTTER PER DETAIL ON SHEET 3
- 9) CONSTRUCT 12" DIA. CONCRETE GUTTER PER DETAIL ON SHEET 3
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- 25) CONSTRUCT 12" DIA. CONCRETE GUTTER PER DETAIL ON SHEET 3
- 26) CONSTRUCT 12" DIA. CONCRETE GUTTER PER DETAIL ON SHEET 3

**ESTIMATED QUANTITIES:**

ITEM	QUANTITY	UNIT
1	222,800	SF
2	27,800	SF
3	30,000	LF
4	30,000	LF
5	3,300	LF
6	3,300	LF
7	3,300	LF
8	3,300	LF
9	3,300	LF
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20	3,300	LF
21	3,300	LF
22	3,300	LF
23	3,300	LF
24	3,300	LF
25	3,300	LF
26	3,300	LF

**EASEMENT NOTES:**

- 1) ALL EASEMENTS ARE TO BE SET OUT IN A TITLE REPORT PREPARED BY THE ENGINEER AND RECORDED IN THE PUBLIC RECORDS OF THE COUNTY OF RIVERSIDE.
- 2) ALL EASEMENTS ARE TO BE SET OUT IN A TITLE REPORT PREPARED BY THE ENGINEER AND RECORDED IN THE PUBLIC RECORDS OF THE COUNTY OF RIVERSIDE.
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- 26) ALL EASEMENTS ARE TO BE SET OUT IN A TITLE REPORT PREPARED BY THE ENGINEER AND RECORDED IN THE PUBLIC RECORDS OF THE COUNTY OF RIVERSIDE.



- EASEMENT NOTES:**
- 1. THIS EASEMENT AGREEMENT IS PART OF THE RECORD DRAWING FOR THE PROJECT AND SHALL BE CONSIDERED AS PART OF THE RECORD DRAWING.
  - 2. THE EASEMENT GRANTOR HAS REPRESENTED AND WARRANTED THAT THE EASEMENT GRANTOR IS THE OWNER OF THE EASEMENT GRANTED AND THAT THE EASEMENT GRANTOR HAS THE AUTHORITY TO GRANT THE EASEMENT.
  - 3. THE EASEMENT GRANTOR HAS REPRESENTED AND WARRANTED THAT THE EASEMENT GRANTOR HAS THE AUTHORITY TO GRANT THE EASEMENT.
  - 4. THE EASEMENT GRANTOR HAS REPRESENTED AND WARRANTED THAT THE EASEMENT GRANTOR HAS THE AUTHORITY TO GRANT THE EASEMENT.
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  - 24. THE EASEMENT GRANTOR HAS REPRESENTED AND WARRANTED THAT THE EASEMENT GRANTOR HAS THE AUTHORITY TO GRANT THE EASEMENT.
  - 25. THE EASEMENT GRANTOR HAS REPRESENTED AND WARRANTED THAT THE EASEMENT GRANTOR HAS THE AUTHORITY TO GRANT THE EASEMENT.
  - 26. THE EASEMENT GRANTOR HAS REPRESENTED AND WARRANTED THAT THE EASEMENT GRANTOR HAS THE AUTHORITY TO GRANT THE EASEMENT.

**SEE SHEET 3 FOR POST INDICATOR VALVE AND FIRE DEPARTMENT CONNECTION DETAIL AND TYPICAL FIRE DEPARTMENT TURNAROUND DETAIL**

WATER MAIN DATA	DATE	DEPTH	DIAMETER	MATERIAL	LOCATION
1	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
2	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13
3	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 25
4	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
5	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13
6	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 25
7	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
8	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13
9	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 25
10	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
11	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13
12	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 25
13	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
14	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13
15	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 25
16	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
17	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13
18	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 25
19	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
20	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13
21	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 25
22	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
23	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13
24	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 25
25	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 1
26	12/15/11	10'	12"	DUCTILE IRON	100' W. OF UNIT 13

**TEC** THERMUL A ENGINEERING CONSULTANTS INC.  
 LAND PLANNING CIVIL ENGINEERING CONSTRUCTION CONSULTANTS  
 2937 HANCOCK CALIFORNIA, 95741 TEL: (916) 486-1018 FAX: (916) 486-1019  
 DATE: 07/20/11 PROJECT NO: 11-0001 SHEET NO: 6 OF 6  
 COUNTY OF FRESNO CONCEPTUAL MASTER PLAN FOR PAR 4+5 PREPARED FOR PLOT PLAN 25183 PAR 2, 3, 5 AND 6 PN:030691

# COUNTY OF RIVERSIDE

## FRENCH VALLEY AIRPORT CENTER - PARCEL MAP 33691

### LEON ROAD / INTERIOR STREETS

### LANDSCAPE PLANS

### PROJECT DIRECTORY

FRENCH VALLEY AIRPORT CENTER, LLC  
 515 SOUTH CALIFORNIA STREET, SUITE 1008  
 LOS ANGELES, CALIFORNIA 90071  
 TEL: (213) 881-1928  
 FAX: (213) 881-8029

R. DALE HADFIELD, LANDSCAPE ARCHITECT  
 17351 WILSON AVENUE, SUITE 50  
 JARDINA WOODS, CA 92577  
 TEL: (949) 367-9273  
 FAX: (949) 367-9371

TEMECLA ENGINEERING CONSULTANTS, INC.  
 29377 RANCHO CALIFORNIA RD., SUITE 202  
 TEMECULA, CALIFORNIA 92591  
 TEL: (951) 678-1015  
 FAX: (951) 678-2294

### MAINTENANCE QUANTITIES

- LANDSCAPED PLANT AREAS 45,038 S.F.
- LANDSCAPING: TREES 188 U.L.
- CONCRETE PAVING 7,946 U.L.

### INDEX OF SHEETS

- TITLE SHEET
- IRRIGATION PLAN
- IRRIGATION PLAN
- IRRIGATION PLAN
- IRRIGATION PLAN
- IRRIGATION LEGEND AND NOTES
- IRRIGATION DETAILS
- IRRIGATION DETAILS
- IRRIGATION DETAILS
- IRRIGATION DETAILS
- PLANTING PLAN
- PLANTING PLAN
- PLANTING PLAN
- PLANTING DETAILS
- LANDSCAPE SPECIFICATIONS
- LANDSCAPE SPECIFICATIONS

CONTINUED LANDSCAPE MAINTENANCE PROVIDED BY COUNTY OF RIVERSIDE LIGHTING AND LANDSCAPE MAINTENANCE DISTRICT 88-1-L.

COUNTY OF RIVERSIDE  
 LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
 NO. 88-1-CONSOLIDATED

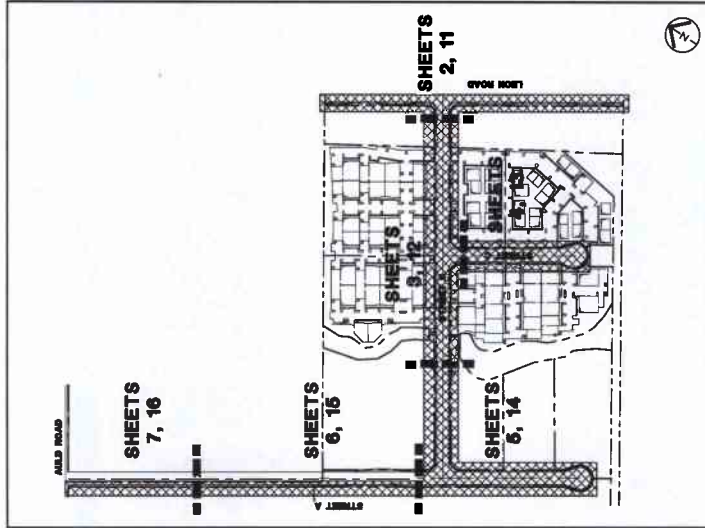
APPROVED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_

On-Site Recycled Water Approved By  
 EASTERN MUNICIPAL WATER DISTRICT

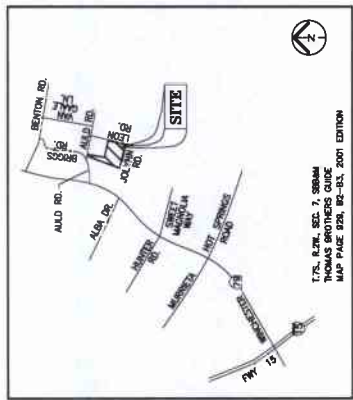
ROLE	NAME OF INDIVIDUAL	DATE
PROJECT OWNER		
CREATOR		
FILED		
DATE		

P.M. 33691 P.P. 21163 I.P. No. 0820238 SHEET NO.

COUNTY OF RIVERSIDE SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE	1
TITLE SHEET	OF 22 SHEETS
FOR: FRENCH VALLEY AIRPORT CENTER	W.O. COUNTY FILE NO.



SHEET INDEX MAP



VICINITY MAP

T.S., R.T.L., SEC. 7, 380M  
 THOMAS BROTHERS GUIDE  
 MAP PAGE 326, 80-83, 2001 EDITION

### COUNTY OF RIVERSIDE NOTES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CLEANING OF THE PROPOSED WORK AREA, AND RELOCATING COSTS OF ALL UTILITIES. PLANTING SHALL BE COMPLETED 14-15 MONTHS PRIOR TO COMMENCEMENT OF CONSTRUCTION (88-9).

TOTAL LLMD AREA:  
 45,038 S.F. (1,084 ACRES)

### PROJECT DATA:

CITY OF TEMECULA	1900
S.F. OF SPRINK AREA	45,038 S.F.
CITY OF WATER METERS	1
CITY OF PHONE LINES	1
S.F. OF MELON AT 2' DEPTH	45,038 S.F.

**NOTE:**  
 CONTACT COUNTY REPRESENTATIVE MARK NUMBERS AT (951) 88-6767 TO START 90-DAY PLANT ESTABLISHMENT PERIOD.

- RECYCLED WATER NOTE - EASTERN MUNICIPAL WATER DISTRICT: THE RECYCLED WATER SYSTEM SHALL BE CONSIDERED AS A SEPARATE AND INDEPENDENT WATER SYSTEM FROM THE TRADITIONAL WATER SUPPLY SYSTEMS. THE RECYCLED WATER SHALL BE USED FOR IRRIGATION AND LANDSCAPING PURPOSES ONLY. THE RECYCLED WATER SHALL NOT BE USED FOR DRINKING OR OTHER DOMESTIC PURPOSES.
- IRIGATION PLAN: THE IRRIGATION PLAN SHALL BE DESIGNED TO PROVIDE SUFFICIENT WATER TO MAINTAIN THE HEALTH OF ALL PLANTS AND TREES. THE IRRIGATION PLAN SHALL BE DESIGNED TO PROVIDE SUFFICIENT WATER TO MAINTAIN THE HEALTH OF ALL PLANTS AND TREES.
- PLANTING PLAN: THE PLANTING PLAN SHALL BE DESIGNED TO PROVIDE SUFFICIENT WATER TO MAINTAIN THE HEALTH OF ALL PLANTS AND TREES. THE PLANTING PLAN SHALL BE DESIGNED TO PROVIDE SUFFICIENT WATER TO MAINTAIN THE HEALTH OF ALL PLANTS AND TREES.
- LANDSCAPE SPECIFICATIONS: THE LANDSCAPE SPECIFICATIONS SHALL BE DESIGNED TO PROVIDE SUFFICIENT WATER TO MAINTAIN THE HEALTH OF ALL PLANTS AND TREES. THE LANDSCAPE SPECIFICATIONS SHALL BE DESIGNED TO PROVIDE SUFFICIENT WATER TO MAINTAIN THE HEALTH OF ALL PLANTS AND TREES.
- CONSTRUCTION: THE CONSTRUCTION SHALL BE COMPLETED 14-15 MONTHS PRIOR TO COMMENCEMENT OF CONSTRUCTION (88-9).
- MAINTENANCE: THE MAINTENANCE SHALL BE COMPLETED 14-15 MONTHS PRIOR TO COMMENCEMENT OF CONSTRUCTION (88-9).
- UTILITIES: THE UTILITIES SHALL BE RELOCATED AND PROTECTED PRIOR TO COMMENCEMENT OF CONSTRUCTION (88-9).
- ENVIRONMENTAL: THE ENVIRONMENTAL IMPACTS SHALL BE MINIMIZED AND RESTORED TO ORIGINAL CONDITIONS (88-9).
- PERMITS: ALL NECESSARY PERMITS SHALL BE OBTAINED PRIOR TO COMMENCEMENT OF CONSTRUCTION (88-9).
- INSURANCE: THE CONTRACTOR SHALL MAINTAIN SUFFICIENT LIABILITY AND WORKERS COMPENSATION INSURANCE (88-9).
- SAFETY: ALL SAFETY MEASURES SHALL BE MAINTAINED THROUGHOUT THE PROJECT (88-9).
- QUALITY CONTROL: THE CONTRACTOR SHALL MAINTAIN STRICT QUALITY CONTROL THROUGHOUT THE PROJECT (88-9).
- PROGRESS REPORTS: THE CONTRACTOR SHALL SUBMIT REGULAR PROGRESS REPORTS TO THE COUNTY (88-9).
- FINAL INSPECTION: THE CONTRACTOR SHALL PARTICIPATE IN A FINAL INSPECTION WITH THE COUNTY (88-9).
- AS-BUILT: THE CONTRACTOR SHALL PROVIDE AS-BUILT DRAWINGS TO THE COUNTY (88-9).
- TRAINING: THE CONTRACTOR SHALL PROVIDE TRAINING TO THE COUNTY (88-9).
- COMMUNITY: THE CONTRACTOR SHALL MAINTAIN GOOD COMMUNITY RELATIONS THROUGHOUT THE PROJECT (88-9).
- TRANSPORTATION: THE TRANSPORTATION DEPARTMENT SHALL BE NOTIFIED OF ANY ROAD CLOSURES OR DELAYS (88-9).
- CONTRACTOR'S OBLIGATIONS: THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS OF THE PROJECT (88-9).
- FORCE MAJEURE: THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS OF FORCE MAJEURE (88-9).
- ASSIGNMENT: THE CONTRACTOR SHALL NOT ASSIGN THE PROJECT TO ANY OTHER PARTY (88-9).
- INDEMNIFICATION: THE CONTRACTOR SHALL INDEMNIFY THE COUNTY (88-9).
- ENTIRE AGREEMENT: THIS CONTRACT SHALL BE THE ENTIRE AGREEMENT BETWEEN THE PARTIES (88-9).
- FORCE MAJEURE: THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS OF FORCE MAJEURE (88-9).
- ASSIGNMENT: THE CONTRACTOR SHALL NOT ASSIGN THE PROJECT TO ANY OTHER PARTY (88-9).
- INDEMNIFICATION: THE CONTRACTOR SHALL INDEMNIFY THE COUNTY (88-9).
- ENTIRE AGREEMENT: THIS CONTRACT SHALL BE THE ENTIRE AGREEMENT BETWEEN THE PARTIES (88-9).

NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.

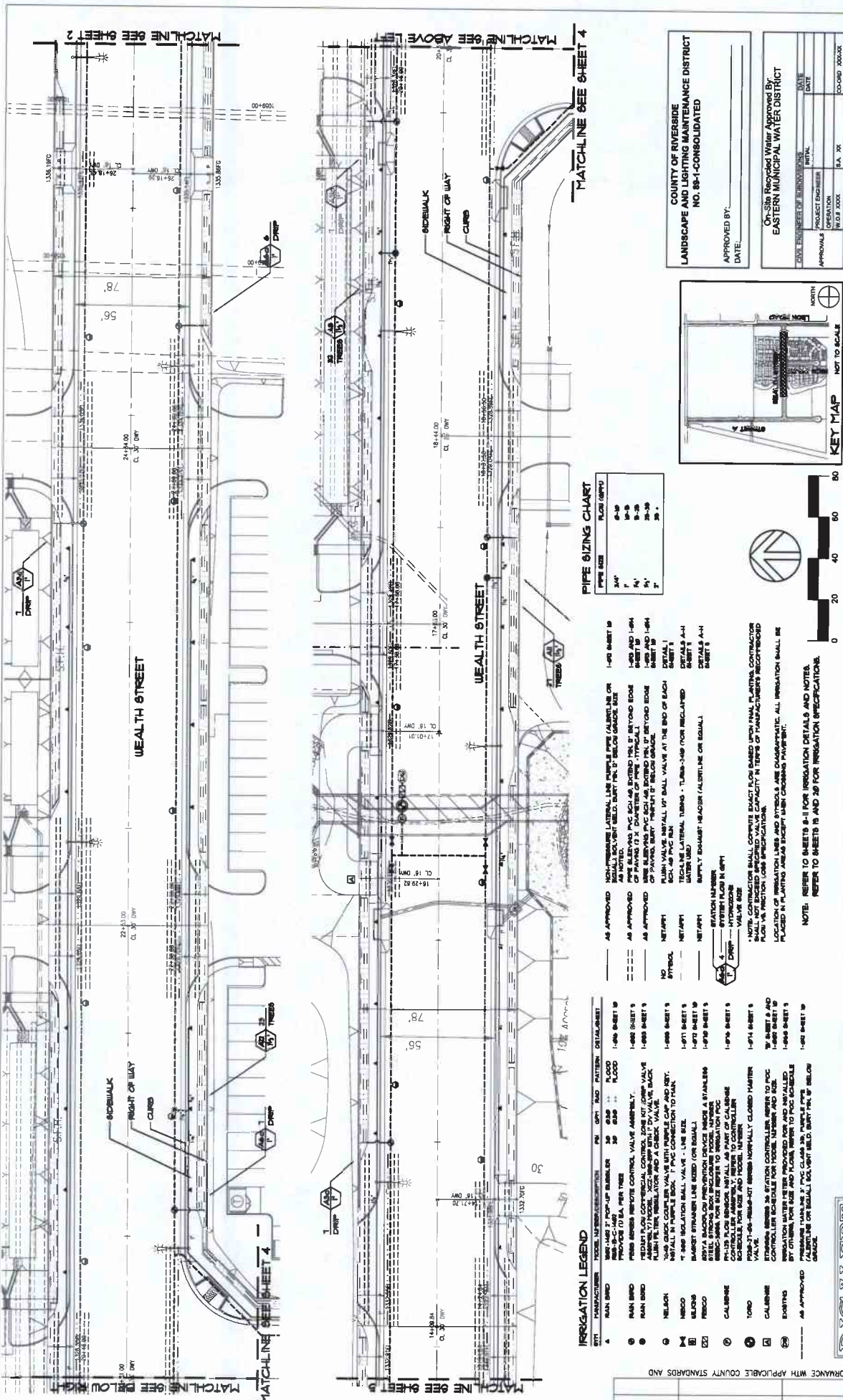
NOTE:  
 WORK COMPLETED WITHIN THESE PLANS  
 SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.  
 ENFORCEMENT PERMIT AND/OR A  
 GRADING PERMIT MUST BE OBTAINED.

DIG AHEAD  
 BEFORE YOU DIG  
 1-800-277-2900  
 TOLL FREE A SAFER SERVICE ALERT  
 UNDERGROUND SERVICE ALERT

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES

PLAN CHECK OVERSIGHT ENGINEER	REGISTRATION NUMBER	DATE SIGNED





**PIPE SIZING CHART**

PIPE SIZE	FLOW (GPM)
3/4"	0-30
1"	30-50
1 1/4"	50-75
1 1/2"	75-100

1-4" SHEET 10  
 1-4" SHEET 11  
 1-4" SHEET 12

1-2" SHEET 13  
 1-2" SHEET 14

1-1 1/2" SHEET 15  
 1-1 1/2" SHEET 16

1-1 1/4" SHEET 17  
 1-1 1/4" SHEET 18

1-1 1/8" SHEET 19  
 1-1 1/8" SHEET 20

1-1 1/4" SHEET 21  
 1-1 1/4" SHEET 22

1-1 1/2" SHEET 23  
 1-1 1/2" SHEET 24

1-1 1/4" SHEET 25  
 1-1 1/4" SHEET 26

1-1 1/2" SHEET 27  
 1-1 1/2" SHEET 28

1-1 1/4" SHEET 29  
 1-1 1/4" SHEET 30

1-1 1/2" SHEET 31  
 1-1 1/2" SHEET 32

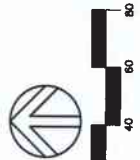
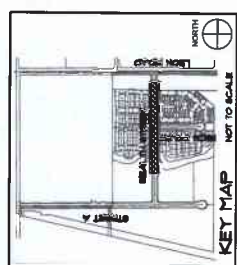
COUNTY OF RIVERSIDE  
 LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
 NO. 88-1-CONSOLIDATED

APPROVED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_

On-Site Checked Water Approved By:  
 EASTERN MUNICIPAL WATER DISTRICT

DATE	DATE
PROJECT ENGINEER	DATE
OPERATION	DATE
NO. 88-1	NO. 88-1
NO. 88-1	NO. 88-1

COULD XXXXX



P.M. 3.1691 P.L. 21163 I.P. No. 080038 SHEET NO.  
 COUNTY OF RIVERSIDE SCHEDULE "E"  
 STREET IMPROVEMENT PLAN & PROFILE  
 FOR: FRENCH VALLEY I.R.O. COUNTY OF RIVERSIDE  
 AIRPORT CENTER 04-777.03 FILE NO.

BENCHMARK: COUNTY OF RIVERSIDE  
 DISSEMINATION 1-88  
 CHISELED BOX IN  
 NORTHEAST CORNER OF  
 SOUTHERLY OF OLD ROAD +  
 AND 87 FEET WESTERLY OF  
 ROAD. ELEVATION: 133.05  
 SCALE: 1" = 40'

PREPARED BY:  
  
 R. DALE HADFIELD LANDSCAPING, INC.  
 2701 FORBES ROAD  
 LAGUNA HILLS, CA 92653  
 PHONE (949) 397-3821  
 FAX (949) 397-3821



DATE: \_\_\_\_\_

WORK BY: \_\_\_\_\_ DATE: \_\_\_\_\_

APPR. DATE: \_\_\_\_\_ COUNTY: \_\_\_\_\_

REVISIONS: \_\_\_\_\_

ENGINEER: \_\_\_\_\_

**IRRIGATION LEGEND**

SYM	DESCRIPTION	PIPE DIA.	MATERIAL	DETAIL/SHEET
1	MAN BIRD	20	HDPE	1-640 SHEET 19
2	MAN BIRD	20	HDPE	1-640 SHEET 19
3	FRIS SERIES METRIC CONTROL VALVE ASSEMBLY.	20	BRASS	1-640 SHEET 19
4	ALUMINUM PEX WITH CRIMP VALVE	1/2"	PEX	1-689 SHEET 5
5	ALUMINUM PEX WITH CRIMP VALVE	3/4"	PEX	1-689 SHEET 5
6	ALUMINUM PEX WITH CRIMP VALVE	1"	PEX	1-689 SHEET 5
7	ALUMINUM PEX WITH CRIMP VALVE	1 1/4"	PEX	1-689 SHEET 5
8	ALUMINUM PEX WITH CRIMP VALVE	1 1/2"	PEX	1-689 SHEET 5
9	T 400 ISOLATION BALL VALVE - LINE SIZE.	1"	BRASS	1-671 SHEET 8
10	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
11	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
12	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
13	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
14	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
15	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
16	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
17	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
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28	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
29	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8
30	20" x 1/4" BRASS STRAINER (OR SMALL)	20"	BRASS	1-671 SHEET 8

NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.

SCALE: AS SHOWN

NOTE: CONTRACTOR SHALL VERIFY EXISTING AND PROPOSED LOCATIONS OF ALL UTILITY LINES AND STRUCTURES. ALL UTILITIES SHALL BE MARKED IN PLANTING AREAS EXCEPT UNDER CROSSING PAVERS.

NOTE: REFER TO SHEETS 8-11 FOR IRRIGATION DETAILS AND NOTES.

NOTE: REFER TO SHEETS 18 AND 19 FOR IRRIGATION SPECIFICATIONS.



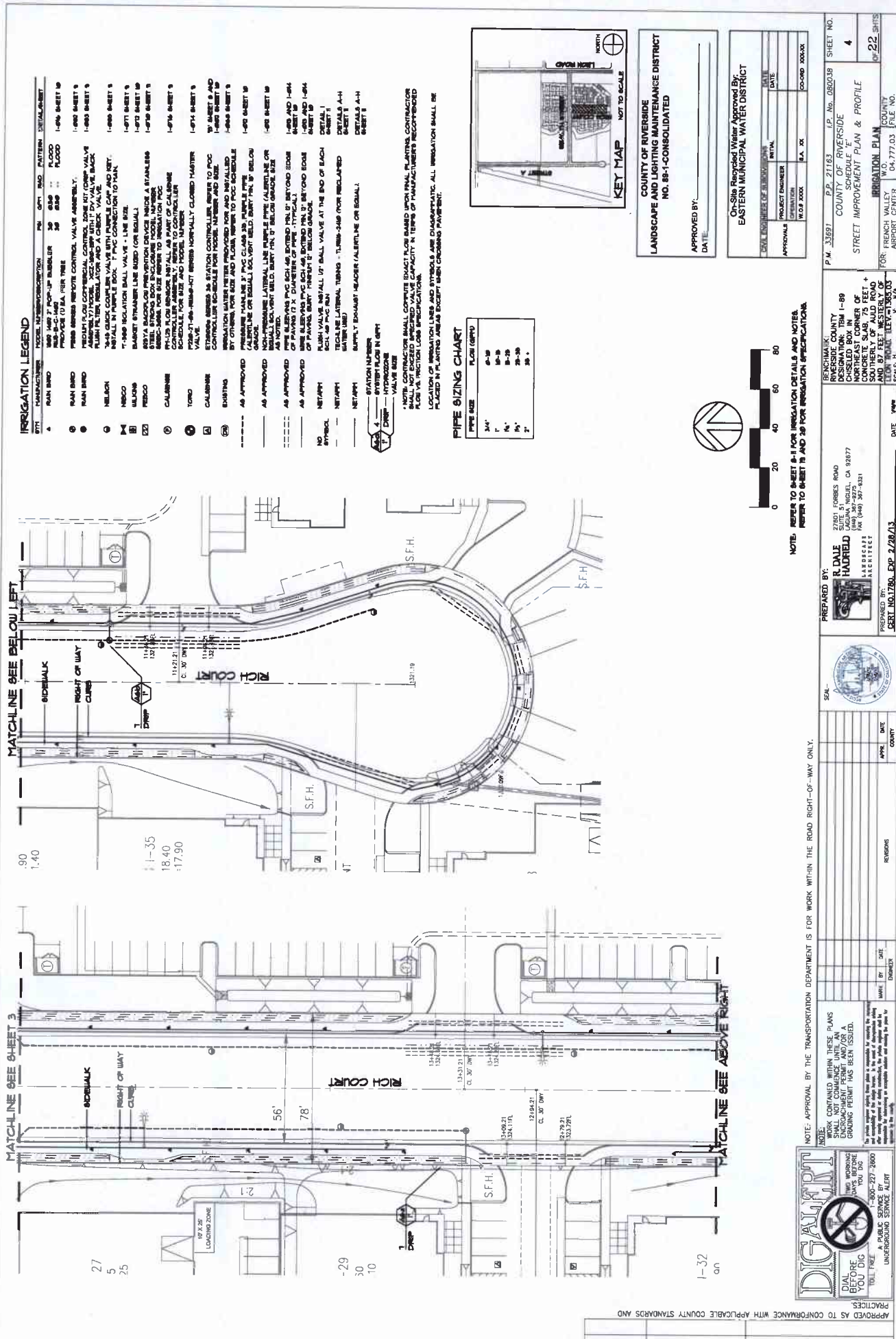
UTILITY WORK  
 NO UTILITY WORK  
 CALL FREE 1-800-237-8800  
 A PUBLIC SERVICE BY  
 UNDERGROUND SERVICE ALERT

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES

PLAN CHECK OVERSIGHT ENGINEER: \_\_\_\_\_ DATE: \_\_\_\_\_

REGISTRATION NUMBER: \_\_\_\_\_

REGISTRATION NUMBER: \_\_\_\_\_

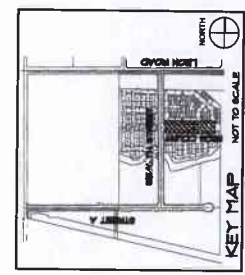


**IRRIGATION LEGEND**

- 1. RAIN BIRD
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- 99. RAIN BIRD
- 100. RAIN BIRD

**PIPE SIZING CHART**

PIPE SIZE	FLOW (GPM)
4"	4-10
6"	10-20
8"	20-35
10"	35-50
12"	50+

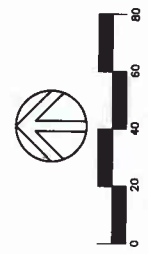


APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

On-Site Proposed Water Approval By:  
EASTERN MUNICIPAL WATER DISTRICT

APPROVALS

PROJECT ENGINEER: \_\_\_\_\_  
OPERATOR: \_\_\_\_\_  
MAIL ROOM: \_\_\_\_\_  
DATE: \_\_\_\_\_



NOTE: REFER TO SHEET 8-11 FOR IRRIGATION DETAILS AND NOTES.  
REFER TO SHEET 18 AND 28 FOR IRRIGATION SPECIFICATIONS.

P.M. 3.6591 P.P. 21163 I.P. No. 080038 SHEET NO. 4

COUNTY OF RIVERSIDE  
SCHEDULE 'E'  
STREET IMPROVEMENT PLAN & PROFILE

IRRIGATION PLAN  
NO. 22 SHEETS

FOR: FRENCH VALLEY W.D. COUNTY FILE NO.  
AIRPORT CENTER 04-777.03

SEAL: [Professional Seal]

PREPARED BY:  
R. DALE HAARDREDD  
REGISTERED LANDSCAPE ARCHITECT

12 X 26 LOADING ZONE

12 X 26 LOADING ZONE

12 X 26 LOADING ZONE

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES

PLAN CHECK OVERSIGHT ENGINEER: \_\_\_\_\_ DATE SIGNED: \_\_\_\_\_

REGISTERATION NUMBER: \_\_\_\_\_

DATE SIGNED: \_\_\_\_\_

REGISTERATION NUMBER: \_\_\_\_\_

DATE SIGNED: \_\_\_\_\_







PLAN CHECK OVERSIGHT ENGINEER	REGISTRATION NUMBER	DATE SIGNED
APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES.		

**DOG MOUNT**  
 DIAL BEFORE YOU DIG  
 1-800-277-2000  
 TOLL FREE  
 A PUBLIC SERVICE ALERT  
 UNDERGROUND SERVICE ALERT

NOTE: WORK CONTAINED WITHIN THESE PLANS IS TO BE CONSIDERED AN ENFORCEMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.  
 The public agency doing these plans is responsible for marking the location and condition of the underground utility lines in the field of proposed utility work. It is the responsibility of the contractor to locate and mark the utility lines prior to beginning work.

REVISIONS	DATE	BY	DATE	COUNTY

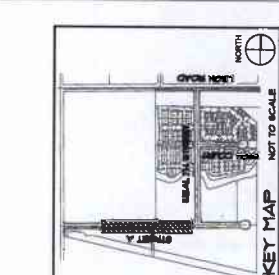
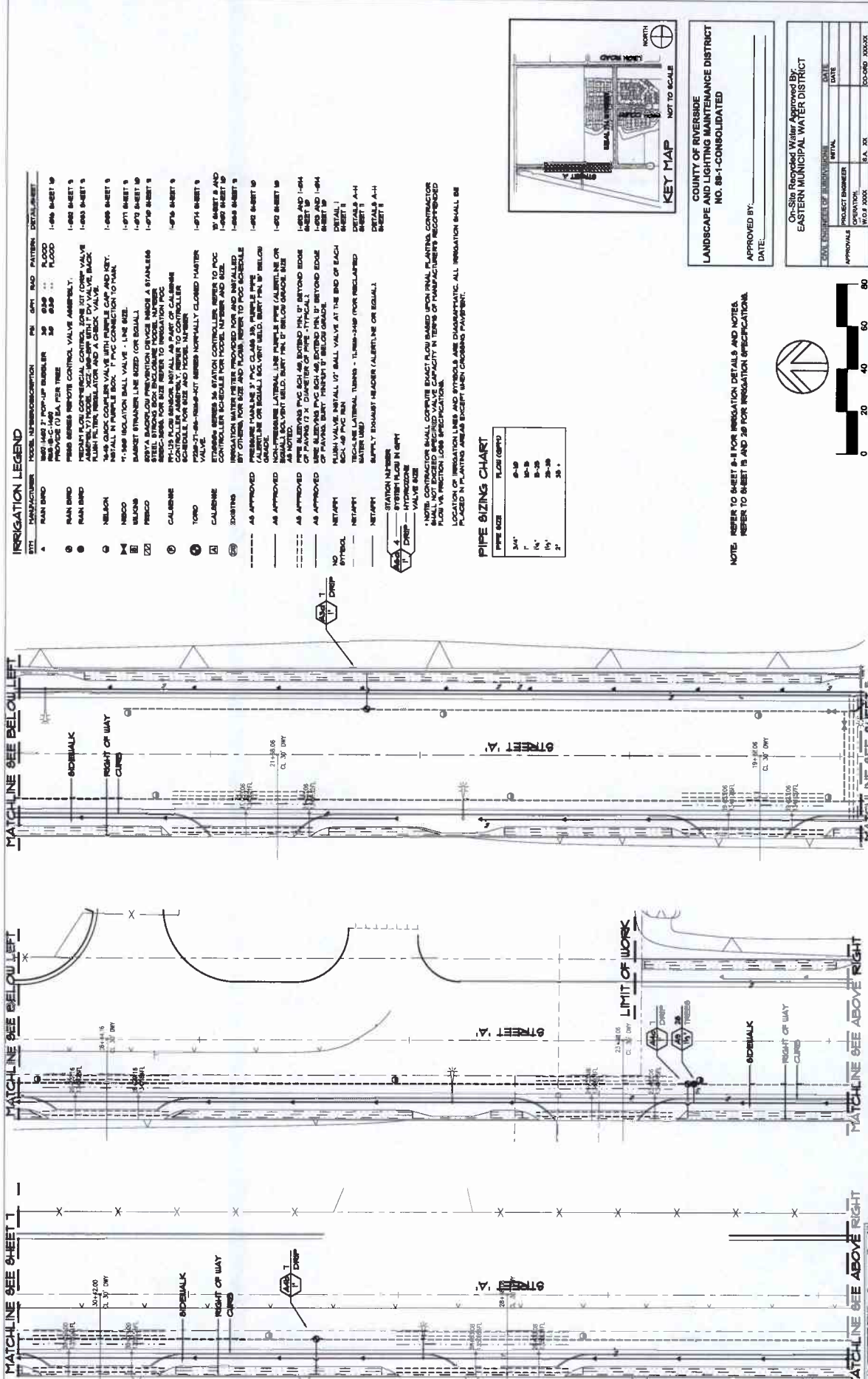


PREPARED BY:  
**R. DALE HADFIELD**  
 LICENSED PROFESSIONAL ENGINEER  
 LICENSE NO. 41587  
 27401 FORBES ROAD  
 DUBLIN, CALIF. 94568  
 (916) 963-2675  
 FAX (916) 963-2631

BENCHMARK  
 RIVERSIDE COUNTY  
 CHESLED BOX  
 NORTHWEST CORNER OF  
 CONCRETE SLAB, 75 FEET  
 AND 87 FEET WESTERLY OF  
 LEON ROAD WESTERLY OF  
 154.2.1 N. V. MA.

COUNTY OF RIVERSIDE  
 SCHEDULE 'E'  
**STREET IMPROVEMENT PLAN & PROFILE**  
**IRRIGATION PLAN**  
 COUNTY FILE NO.  
 04-77-03

P.P. 21163 I.P. No. 082018 SHEET NO. **6**  
 APPROVED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 On-Site Reviewed Water Approved By:  
 EASTERN MUNICIPAL WATER DISTRICT  
 OPERATOR: \_\_\_\_\_  
 PROJECT ENGINEER: \_\_\_\_\_  
 APPROVALS: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 COUNTY FILE NO.



**PIPE SIZING CHART**

PIPE SIZE	FLOW (GPM)
3/4"	6-10
1"	10-15
1 1/4"	15-20
1 1/2"	20-30
2"	30-50

- IRRIGATION LEGEND**
- 1 1" MANUFACTURER SYMBOL DESCRIPTION
  - 2 1" MANUFACTURER SYMBOL DESCRIPTION
  - 3 1" MANUFACTURER SYMBOL DESCRIPTION
  - 4 1" MANUFACTURER SYMBOL DESCRIPTION
  - 5 1" MANUFACTURER SYMBOL DESCRIPTION
  - 6 1" MANUFACTURER SYMBOL DESCRIPTION
  - 7 1" MANUFACTURER SYMBOL DESCRIPTION
  - 8 1" MANUFACTURER SYMBOL DESCRIPTION
  - 9 1" MANUFACTURER SYMBOL DESCRIPTION
  - 10 1" MANUFACTURER SYMBOL DESCRIPTION
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  - 49 1" MANUFACTURER SYMBOL DESCRIPTION
  - 50 1" MANUFACTURER SYMBOL DESCRIPTION

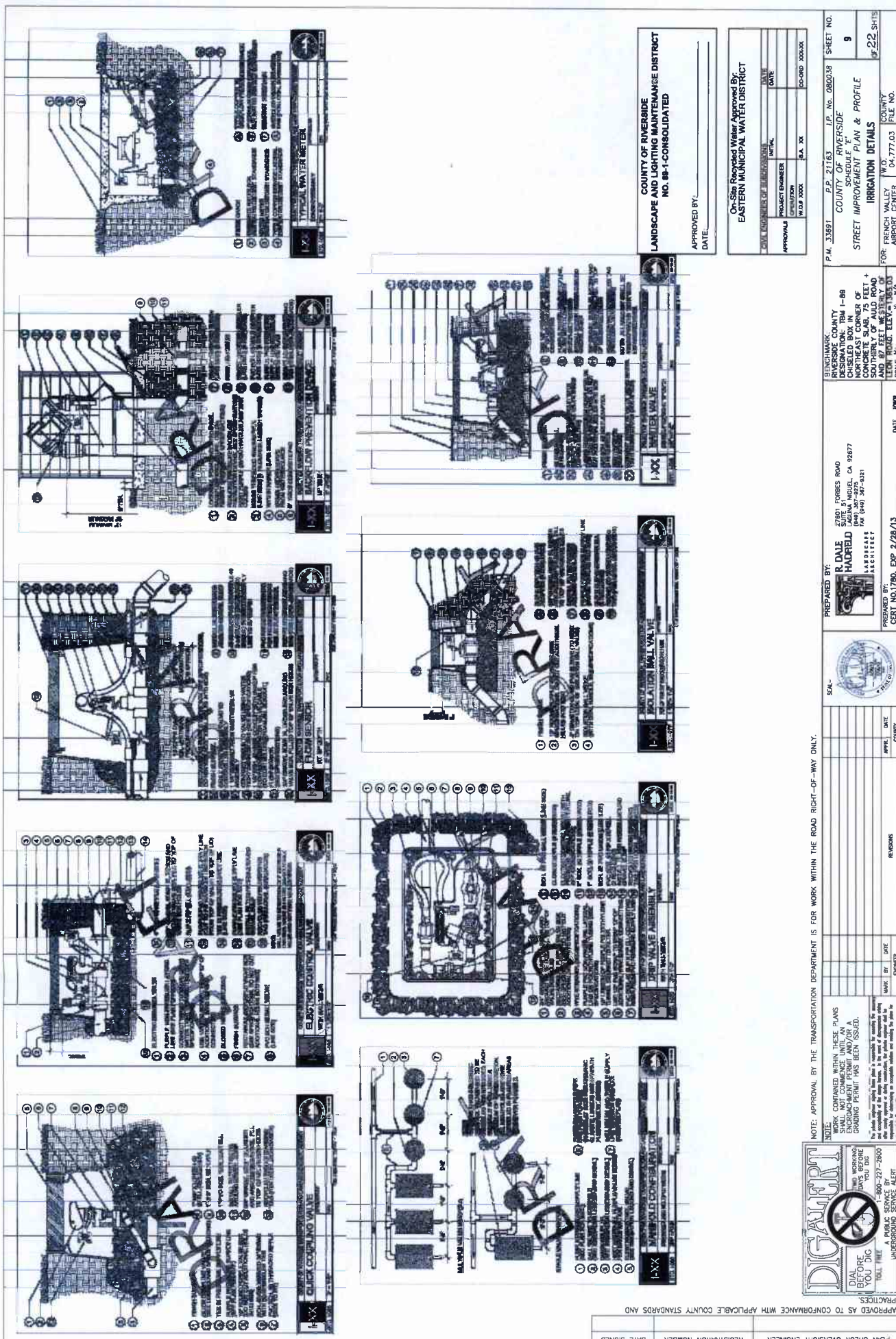
NOTE: CONTRACTOR SHALL COVER EXACT FLOW BASED UPON FINAL PLANTING CONTRACTOR FLOW RATE RECOMMENDATION. LOCATION OF IRRIGATION LINES AND SYMBOLS ARE DIAGNOSTIC. ALL IRRIGATION SHALL BE PLACED IN PLANTING AREAS EXCEPT WHEN CROSSING PAVEMENT.

NOTE: REFER TO SHEET 8-11 FOR IRRIGATION DETAILS AND NOTES. REFER TO SHEET 18 AND 26 FOR IRRIGATION SPECIFICATIONS.

BENCH MARK  
 RIVERSIDE COUNTY  
 CHESLED BOX  
 NORTHWEST CORNER OF  
 CONCRETE SLAB, 75 FEET  
 AND 87 FEET WESTERLY OF  
 LEON ROAD WESTERLY OF  
 154.2.1 N. V. MA.







APPROVED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_

COUNTY OF RIVERSIDE  
 LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
 NO. 88-1-CONSOLIDATED

On-Site Proposed Water Approved By:  
 EASTERN MUNICIPAL WATER DISTRICT

DATE: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 DATE: \_\_\_\_\_

PROJECT ENGINEER: \_\_\_\_\_  
 DATE: \_\_\_\_\_

APPROVALS: \_\_\_\_\_  
 W.M.D. XXXXX S.A. XX SO-ORD XXXXX

P.M. 33891 P.P. 21163 I.P. No. 080038 SHEET NO. 9

COUNTY OF RIVERSIDE  
 SCHEDULE "E"  
 STREET IMPROVEMENT PLAN & PROFILE  
 IRRIGATION DETAILS

FOR: FRENCH VALLEY W.C. COUNTY  
 AIRPORT CENTER 04.777.03 FILE NO.

BENCHMARK: COUNTY OF RIVERSIDE  
 PROJECT NO. 1-88  
 CHELSEA BOX IN  
 SOUTHERLY OF AULD ROAD  
 AND 87 FEET WESTERLY OF  
 ROAD ELEVATION 11.84

SCALE: \_\_\_\_\_

DATE: \_\_\_\_\_



PREPARED BY:  
**R. DALE HARDFIELD**  
 ARCHITECT

27801 FORBES ROAD  
 JAGUNA HIGHLAND, CA 92677  
 PHONE (949) 367-3321  
 FAX (949) 367-3321

SCHEMATIC DESIGN

DATE: \_\_\_\_\_  
 COUNTY: \_\_\_\_\_

NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.

WORK CONTAINED WITHIN THESE PLANS  
 IS SUBJECT TO THE CITY OF RIVERSIDE  
 ENFORCEMENT PERMIT AND/OR A  
 GRADING PERMIT HAS BEEN ISSUED.

The reader should verify that the work is to be done by the contractor and that the contractor is responsible for obtaining all necessary permits and approvals for the work.



APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES.

PLAN CHECK OVERSIGHT ENGINEER \_\_\_\_\_ DATE SIGNED \_\_\_\_\_

REGISTRATION NUMBER \_\_\_\_\_

### WELL & SUMP

SEE SHEET FOR DETAILS

1. CONCRETE WELLS SHALL BE 18" DIA.
2. WELLS SHALL BE 4' DEEP MIN.
3. WELLS SHALL BE FINISHED TO THE TOP OF THE CURB.
4. WELLS SHALL BE FINISHED TO THE TOP OF THE CURB.
5. WELLS SHALL BE FINISHED TO THE TOP OF THE CURB.
6. WELLS SHALL BE FINISHED TO THE TOP OF THE CURB.

### TRENCH IN LANDSCAPE

SEE SHEET FOR DETAILS

1. TRENCHES SHALL BE 18" WIDE.
2. TRENCHES SHALL BE 4' DEEP MIN.
3. TRENCHES SHALL BE FINISHED TO THE TOP OF THE CURB.
4. TRENCHES SHALL BE FINISHED TO THE TOP OF THE CURB.
5. TRENCHES SHALL BE FINISHED TO THE TOP OF THE CURB.
6. TRENCHES SHALL BE FINISHED TO THE TOP OF THE CURB.

### TRUNK

SEE SHEET FOR DETAILS

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5. TRUNKS SHALL BE FINISHED TO THE TOP OF THE CURB.
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### TRUNK

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### TRUNK

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### TRUNK

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5. TRUNKS SHALL BE FINISHED TO THE TOP OF THE CURB.
6. TRUNKS SHALL BE FINISHED TO THE TOP OF THE CURB.

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 COUNTY OF RIVERSIDE LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT NO. 88-1-CONSOLIDATED

DATE PREPARED: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 PROJECT ENGINEER: \_\_\_\_\_  
 OPERATOR: \_\_\_\_\_  
 W.O.P. NO.: \_\_\_\_\_

P.P. 21163 I.P. No. 080038 SHEET NO. 10 OF 22 SHEETS  
 COUNTY OF RIVERSIDE SCHEDULE 'E'  
 STREET IMPROVEMENT PLAN & PROFILE  
 IRRIGATION DETAILS  
 COUNTY OF RIVERSIDE W.G. COUNTY OF RIVERSIDE  
 FOR: FRENCH VALLEY W.G. COUNTY OF RIVERSIDE  
 AIRPORT CENTER FILE NO. \_\_\_\_\_

BENCHMARK: \_\_\_\_\_  
 RIVERSIDE COUNTY I-89  
 CHESTNUT BOX IN  
 NORTHEAST CORNER OF  
 SOUTHWEST CORNER OF  
 AND 87 FEET WESTERLY OF  
 LEON ROAD 125' X 130' X 135'

PREPARED BY:  
  
 R. DALE HALDRUP ARCHITECTS  
 2760 FORBES ROAD  
 LAGUNA WOODS, CA 92677  
 TEL: (949) 367-4821  
 FAX: (949) 367-4821  
 DATE: \_\_\_\_\_ DATE: \_\_\_\_\_  
 CERT. NO. 1780, EXP. 2/28/13

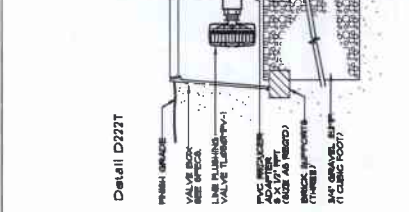
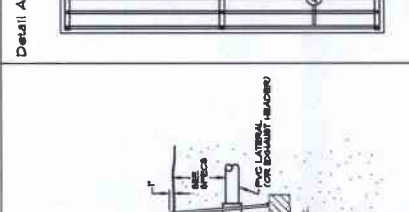
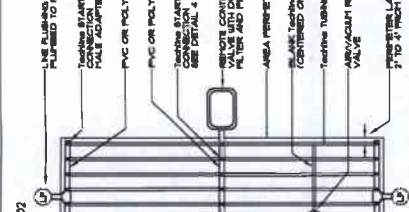
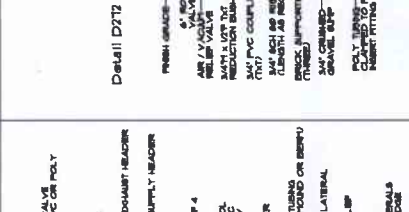
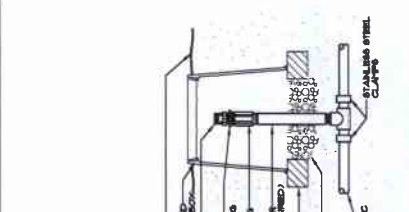
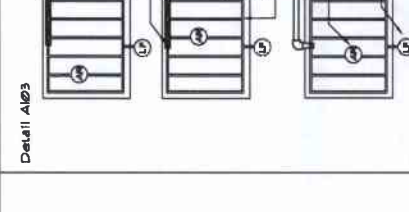
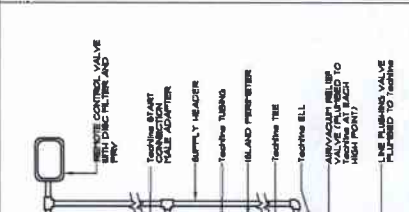
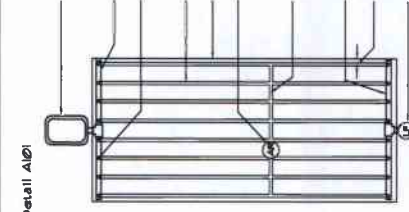
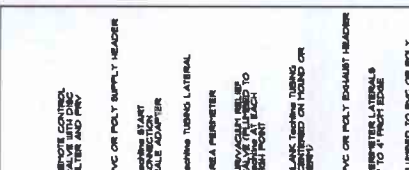


NO.	REVISIONS	DATE	COUNTY

NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.

NOTE: CONTRACTORS WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.  
 The plans, specifications, and drawings have been prepared by the undersigned and are hereby approved as being correct, to the best of my knowledge and belief, for the purposes intended and in accordance with the laws and regulations governing the practice of civil engineering in the State of California.

DIAL BEFORE YOU DIG  
 CALL 800-4-A-DIG  
 TOLL FREE - A PUBLIC SERVICE ALERT  
 UNDERGROUND SERVICE ALERT



**1** Techline Line Flushing Valve (Plumbed to PVC)

**G** Techline Center Feed Layout

**C** Techline Island Layout

**D** Techline Irregular Areas: Triangular

**E** Techline Air/Vacuum Relief (Plumbed to Poly)

**A** Techline End Feed Layout

**A301** Techline Sub-Header Installation

**A302** Techline Sub-Header Installation

**A303** Techline Sub-Header Installation

NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.

WORK CONTAINED WITHIN THESE PLANS IS SUBJECT TO THE SAN DIEGO COUNTY ENGINEER'S REVIEW AND APPROVAL. A GRADING PERMIT AND/OR A SIGNING PERMIT MUST BE OBTAINED FROM THE SAN DIEGO COUNTY ENGINEER'S OFFICE BEFORE WORK BEGINS. THE USER OF THESE PLANS SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR THE CORRECTNESS OF THE INFORMATION PROVIDED BY THE USER.

**DISCLAIMER**  
DO NOT REMOVE THIS DISCLAIMER BEFORE YOU BEGIN WORK.  
TOLL FREE 1-800-227-2800  
PO BOX 1013  
SAN DIEGO, CA 92168

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES: \_\_\_\_\_

REGISTRATION NUMBER \_\_\_\_\_ DATE SIGNED \_\_\_\_\_

PLANNING ENGINEER \_\_\_\_\_

DATE \_\_\_\_\_

PROJECT: LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT NO. 88-1-CONSOLIDATED

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PREPARED BY: R. DALE HADFIELD  
DATE: 2/28/13  
SCALE: 1/8" = 1'-0"

BENCHMARK: RIVERSIDE COUNTY SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE

SHEET NO. 11 OF 22 SHEETS

COUNTY: SAN DIEGO  
PROJECT NUMBER: 04-777.03  
FILE NO. AIRPORT CENTER

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REGISTRATION NUMBER \_\_\_\_\_ DATE SIGNED \_\_\_\_\_

PLANNING ENGINEER \_\_\_\_\_

DATE \_\_\_\_\_

PROJECT: LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT NO. 88-1-CONSOLIDATED

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PREPARED BY: R. DALE HADFIELD  
DATE: 2/28/13  
SCALE: 1/8" = 1'-0"

BENCHMARK: RIVERSIDE COUNTY SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
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SHEET NO. 11 OF 22 SHEETS

COUNTY: SAN DIEGO  
PROJECT NUMBER: 04-777.03  
FILE NO. AIRPORT CENTER

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SAN DIEGO, CA 92168

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES: \_\_\_\_\_

REGISTRATION NUMBER \_\_\_\_\_ DATE SIGNED \_\_\_\_\_

PLANNING ENGINEER \_\_\_\_\_

DATE \_\_\_\_\_

PROJECT: LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT NO. 88-1-CONSOLIDATED

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PREPARED BY: R. DALE HADFIELD  
DATE: 2/28/13  
SCALE: 1/8" = 1'-0"

BENCHMARK: RIVERSIDE COUNTY SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
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SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE

SHEET NO. 11 OF 22 SHEETS

COUNTY: SAN DIEGO  
PROJECT NUMBER: 04-777.03  
FILE NO. AIRPORT CENTER

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PO BOX 1013  
SAN DIEGO, CA 92168

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES: \_\_\_\_\_

REGISTRATION NUMBER \_\_\_\_\_ DATE SIGNED \_\_\_\_\_

PLANNING ENGINEER \_\_\_\_\_

DATE \_\_\_\_\_

PROJECT: LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT NO. 88-1-CONSOLIDATED

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PREPARED BY: R. DALE HADFIELD  
DATE: 2/28/13  
SCALE: 1/8" = 1'-0"

BENCHMARK: RIVERSIDE COUNTY SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
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SHEET NO. 11 OF 22 SHEETS

COUNTY: SAN DIEGO  
PROJECT NUMBER: 04-777.03  
FILE NO. AIRPORT CENTER

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REGISTRATION NUMBER \_\_\_\_\_ DATE SIGNED \_\_\_\_\_

PLANNING ENGINEER \_\_\_\_\_

DATE \_\_\_\_\_

PROJECT: LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT NO. 88-1-CONSOLIDATED

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PREPARED BY: R. DALE HADFIELD  
DATE: 2/28/13  
SCALE: 1/8" = 1'-0"

BENCHMARK: RIVERSIDE COUNTY SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE  
SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE OF 22' SHIRAZ AVENUE

SHEET NO. 11 OF 22 SHEETS

COUNTY: SAN DIEGO  
PROJECT NUMBER: 04-777.03  
FILE NO. AIRPORT CENTER

PLAN CHECK OVERSIGHT ENGINEER	REGISTRATION NUMBER	DATE SIGNED

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES

**DIAL BEFORE YOU DIG**  
 1-800-227-2800  
 TOLL FREE A PUBLIC SERVICE ALERT  
 UNDERGROUND SERVICE ALERT

NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.  
 WORK CONTAINED WITHIN THESE PLANS IS TO BE CONSIDERED A PROPOSED PROJECT. A PERMIT FROM THE CALIFORNIA DEPARTMENT OF TRANSPORTATION AND HIGHWAYS IS REQUIRED FOR ALL WORK WITHIN THE ROAD RIGHT-OF-WAY. THE USER OF THESE PLANS SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR VERIFYING THE ACCURACY OF ALL INFORMATION PROVIDED HEREON.

DATE	
BY	
REVISIONS	



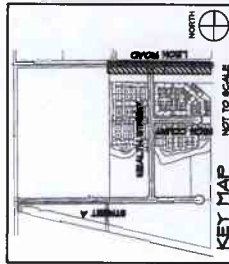
PREPARED BY:  
**R. DALE HADFIELD**  
 ARCHITECT  
 27801 FORBES ROAD  
 LAKELAND, CALIFORNIA 92577  
 TEL: (949) 367-8321  
 FAX: (949) 367-8321

BENCHMARK:  
 COUNTY OF RIVERSIDE  
 SCHEDULE 'E'  
 STREET IMPROVEMENT PLAN & PROFILE  
 AND 87 FEET WESTERLY OF  
 AND 87 FEET WESTERLY OF  
 SOUTHERLY OF AULD ROAD  
 SCALE: 1" = 10' MAX

P.M. 23891 P.P. 21163 I.P. No. 080038 SHEET NO. **12**  
 COUNTY OF RIVERSIDE  
 STREET IMPROVEMENT PLAN & PROFILE  
 OF 22 SHEETS  
 FOR FRENCH VALLEY W.O. COUNTY  
 AIRPORT CENTER 04-777.03 FILE NO.

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
 NO. 88-1-CONSOLIDATED

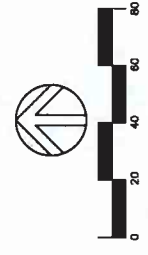
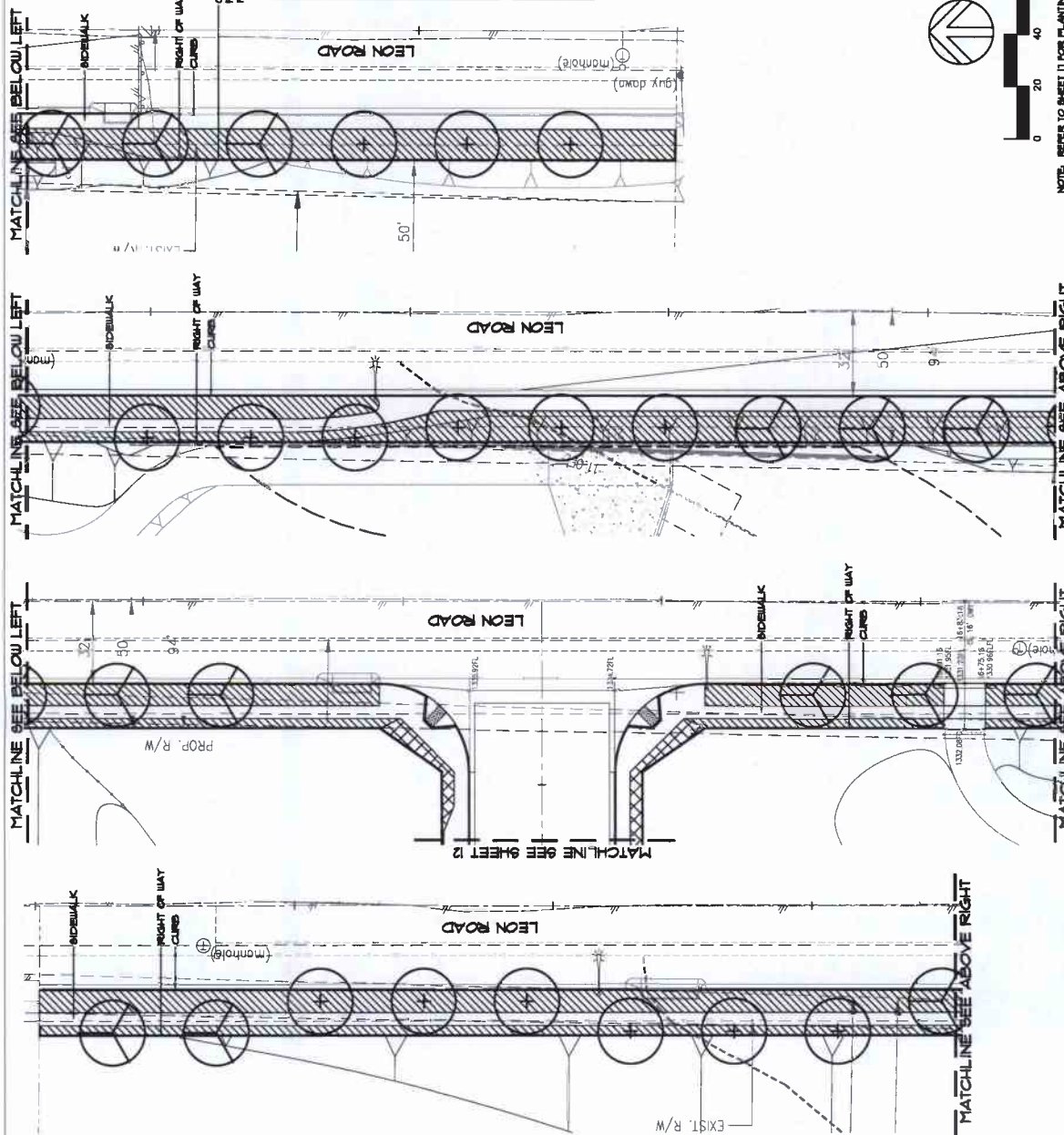
On-Site Respected Water Approved By:  
 EASTERN MUNICIPAL WATER DISTRICT  
 CIVIL ENGINEER OF SUBDIVISIONS  
 PROJECT ENGINEER: \_\_\_\_\_ DATE: \_\_\_\_\_  
 APPROVALS: \_\_\_\_\_ INITIAL: \_\_\_\_\_ DATE: \_\_\_\_\_  
 W.O.S. 2000 S.A. XX S.O.ORD. 2000XX



**PLANT LEGEND**

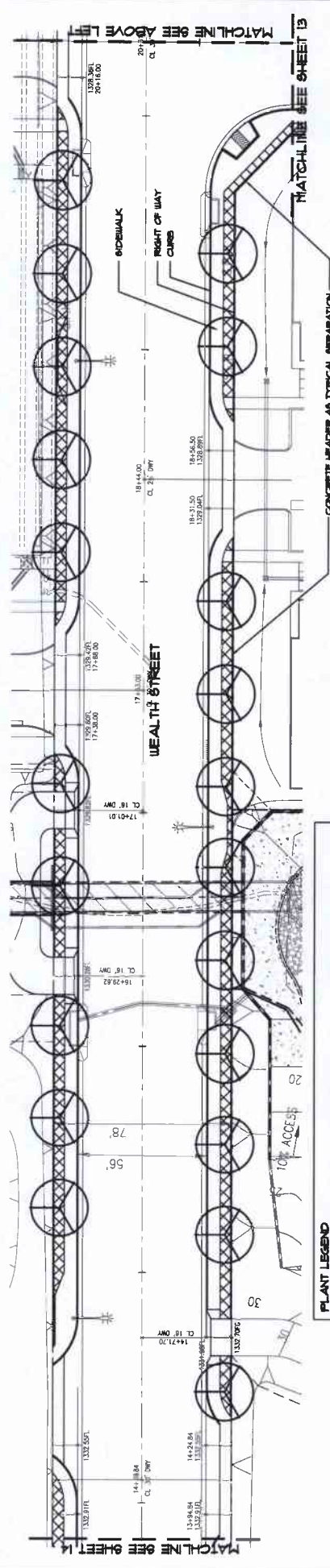
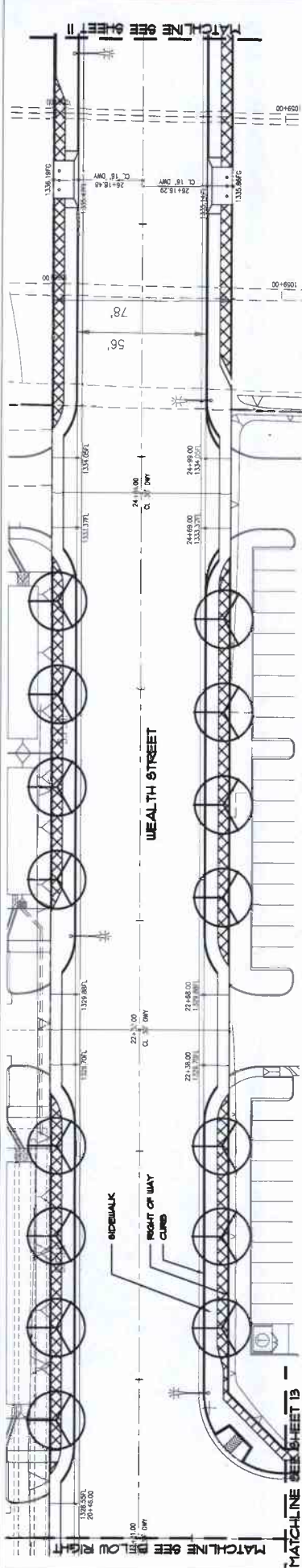
SYM	BOTANICAL NAME	COTTON NAME	QTY.	SIZE/SPACING	WHEELS	DET. NO.
+	CECROPIUM DISCRETE HEMISPAT	PAID VIBRZE	6	34" BOX	LOW	P-68984T.N
○	NOELANIBERIA PANICULATA	GOLDENRUM TREE	18	24" BOX	MED	P-68984T.N
○	CLEOME AMPHOLA	COAST LIVE OAK	8	24" BOX	LOW	P-68984T.N
○	SHADES	CHAME COYOTE BUSH	18, 24, 48	1.5 GAL	LOW	DET. 11 P-689
○	BACCHARIS TRIN REARX	REVERBLET WEDGE	36, 448	3" O.C.	LOW	DET. 11 P-689
○	CAREX DIVELLA	ROSEMARY	180	3" O.C.	LOW	DET. 11 P-689
○	ROSEMARYS OFFICINALIS	TULCAN BLUE	180	3" O.C.	LOW	DET. 11 P-689

**NOTES**  
 ALL PLANTER AREAS SHALL RECEIVE 3" DEPTH MULCH. REFER TO FOR MULCH DETAIL. REFER TO DETAIL P-684 ON SHEET 11.  
 ALL TREES WITHIN 6 FEET OF ANY HARDSCAPE SHALL BE INSTALLED WITH ROOT BARRIERS. REFER TO DETAIL P-675 ON SHEET 11.



CONCRETE LEADERS AS TYPICAL SEPARATION FOR LINED MAINTENANCE. REFER TO DETAIL P-675, SHEET 11.





APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES

PLAN CHECK OVERSIGHT - ENGINEER	REGISTRATION NUMBER	DATE SIGNED
---------------------------------	---------------------	-------------

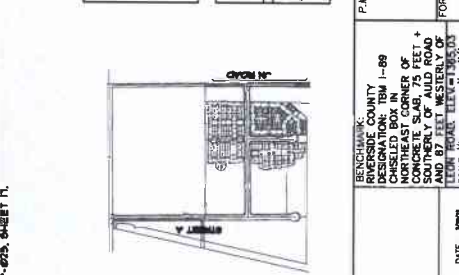
  

APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT NO. 88-1-CONSOLIDATED

SEAL: \_\_\_\_\_  
DATE: \_\_\_\_\_  
PROJECT ENGINEER: \_\_\_\_\_  
OPERATION NO. 8-8-2008

CONTRACT: \_\_\_\_\_  
DATE: \_\_\_\_\_  
OPERATION NO. 8-8-2008

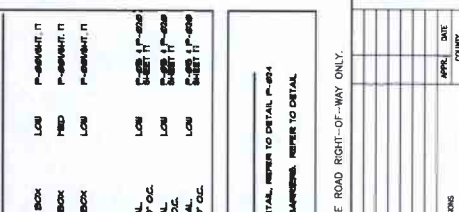


P.M. 33681 P.P. 21163 I.P. No. 062038 SHEET NO. 12 OF 22 SITS

COUNTY OF RIVERSIDE  
SCHEDULE 'E'  
STREET IMPROVEMENT PLAN & PROFILE  
PLANTING PLAN

FOR FERRY VALLEY, COUNTY OF RIVERSIDE  
APPROPRI CENTER 04-777.03 FILE NO.

CONCRETE HEADERS AS TYPICAL SEPARATION FOR LIQUID MAINTENANCE. REFER TO DETAIL P-605, SHEET 11.



NOTE: REFER TO SHEET 11 FOR PLANTING DETAILS. REFER TO SHEET 10 & 13 FOR PLANTING SPECIFICATIONS.

PLANT LEGEND

SYM	BOTANICAL NAME	COMMON NAME	CITY	SIZE/REMARKS	WECOLS DET. NO.
+	CRONQUIST TERNSTROM TERNSTROM PALM VERDE	34" BOX	LOW	P-668117	
⊕	COLENEURUS PANDURATA	34" BOX	MED	P-668117	
⊕	QUERCUS ASPERIFOLIA	COMF LIVE OAK	LOW	P-668117	
⊕	SHRUBS				
⊕	COCCORDIA TWIN FLORA	COYOTE BUSH	1 GAL	P-668117	
⊕	CAREX DYNALIA	REBELLY BUSH	36" 6.0	P-668117	
⊕	BOERHAAVE OFFICINALE	ROSEMARY	100	P-668117	
⊕	MULCAH BLUE		3" 6"	P-668117	

NOTES  
ALL PLANTING AREAS SHALL RECEIVE 3" DEPTH MULCH. REFER TO HOLE DETAIL, REFER TO DETAIL P-604 ON SHEET 11.  
ALL TREES WITHIN 6 FEET OF ANY LANDSCAPE SHALL BE INSTALLED WITH ROOT BARRIERS. REFER TO DETAIL P-604 ON SHEET 11.

NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.

APPROVED	DATE
REVISIONS	DATE

MAKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_ COUNTY: \_\_\_\_\_

NOTE: CONTRACTORS WITH THESE PLANS SHALL NOT CONSIDERED UNLESS AN ENFORCEMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

THE PUBLIC WORKS DIVISION DOES NOT GUARANTEE THE ACCURACY OF THE INFORMATION PROVIDED HEREON. CONTRACTORS ARE RESPONSIBLE FOR VERIFYING ALL INFORMATION PRIOR TO CONSTRUCTION.

**DIG-AHEAD**

DIAL BEFORE YOU DIG  
TOLL FREE 1-800-4-A-DIG (4264)  
UNDEGROUND SERVICE ALERT

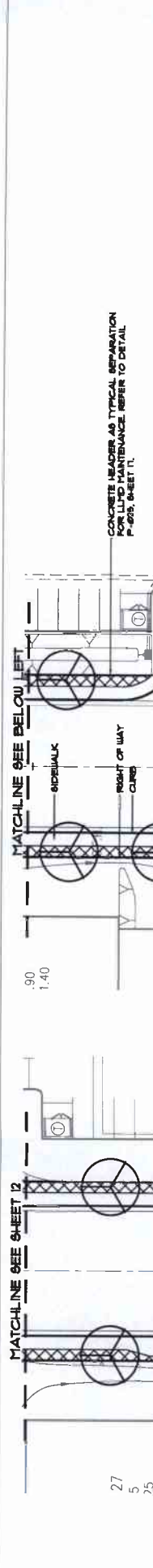
BENCHMARKING:  
RIVERSIDE COUNTY  
CHESLEIGH BOX 11-89  
NORTHEAST CORNER OF  
SOUTHWEST CORNER OF  
AND 87 FEET WESTERLY OF  
LEON ROAD ELEVATION 1070.0

PREPARED BY:  
R. DALE  
HARDFIELD  
REGISTERED CIVIL ENGINEER  
CERT. NO. 17860, EXP. 2/28/13

DATE: 04/28/13

DATE: 04/28/13

DATE: 04/28/13



**PLANT LEGEND**

SYMBOL	BOTANICAL NAME	COPPER NAME	CITY	SIZE/SPACING	WHEELS DET. NO.
<b>TREES</b>					
	CERCOIDIA TORRENTI	HUMBLY	PALO VERDE	24" BOX	LOW
	ACELIMBICHA PANICULATA	GOLDENRAIN TREE	BIRCH	24" BOX	LOW
	QUERCUS AEMULATA	COAST LIVE OAK	BIRCH	24" BOX	LOW
<b>SHRUBS</b>					
	DIACYLIUM TIBETANUM	FLAME ACACIA	1.5" CAL.	3' x 3' O.C.	LOW
	CAMEXIA DYNALIA	BERKLEY MOORE	3/4" CAL.	1' x 1' O.C.	LOW
	ROSEMARY	ROSEMARY	1/2" CAL.	5' x 5' O.C.	LOW
	ROSEMARY	ROSEMARY	1/2" CAL.	5' x 5' O.C.	LOW

**NOTES**

ALL PLANTING AREAS SHALL RECEIVE 3" DEPTH MULCH. REFER TO PUNCH DETAIL. REFER TO DETAIL P-284 ON SHEET 11.

ALL TREES WITHIN 5' FEET OF ANY LANDSCAPE SHALL BE INSTALLED WITH ROOT BARRIERS. REFER TO DETAIL P-285 ON SHEET 11.



COUNTY OF RIVERSIDE  
LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
NO. 88-4-CONSOLIDATED

APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

ENGINEER	DATE
PROJECT ENGINEER	DATE
OPERATION	DATE
W.P. 2000	DATE
NO. 88-4-CONSOLIDATED	DATE

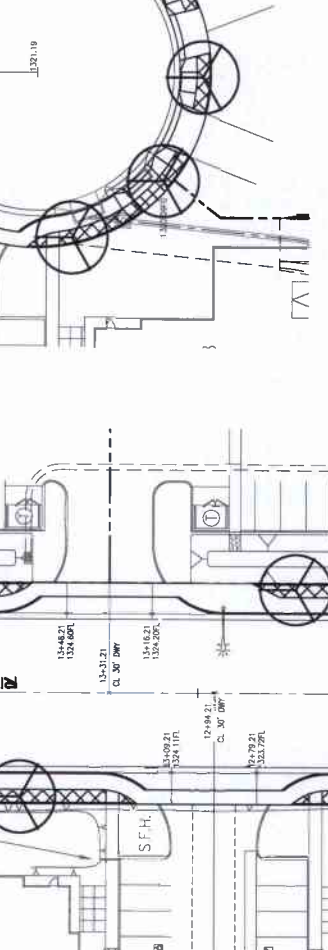
On-Site Reviewed and Approved By:  
EASTERN MUNICIPAL WATER DISTRICT

P.P. 3.1891 P.P. 2.1163 I.P. No. 080238 SHEET NO. 13

COUNTY OF RIVERSIDE  
SCHEDULE E  
STREET IMPROVEMENT PLAN & PROFILE

PLANTING PLAN  
OF 22 SHEETS

FOR: FRENCH VALLEY W.D. 04-777 03 COUNTY FILE NO.  
AIRPORT CENTER



**NOTES**

REFER TO SHEET 11 FOR PLANTING DETAILS.

REFER TO SHEET 12 FOR PLANTING SPECIFICATIONS.



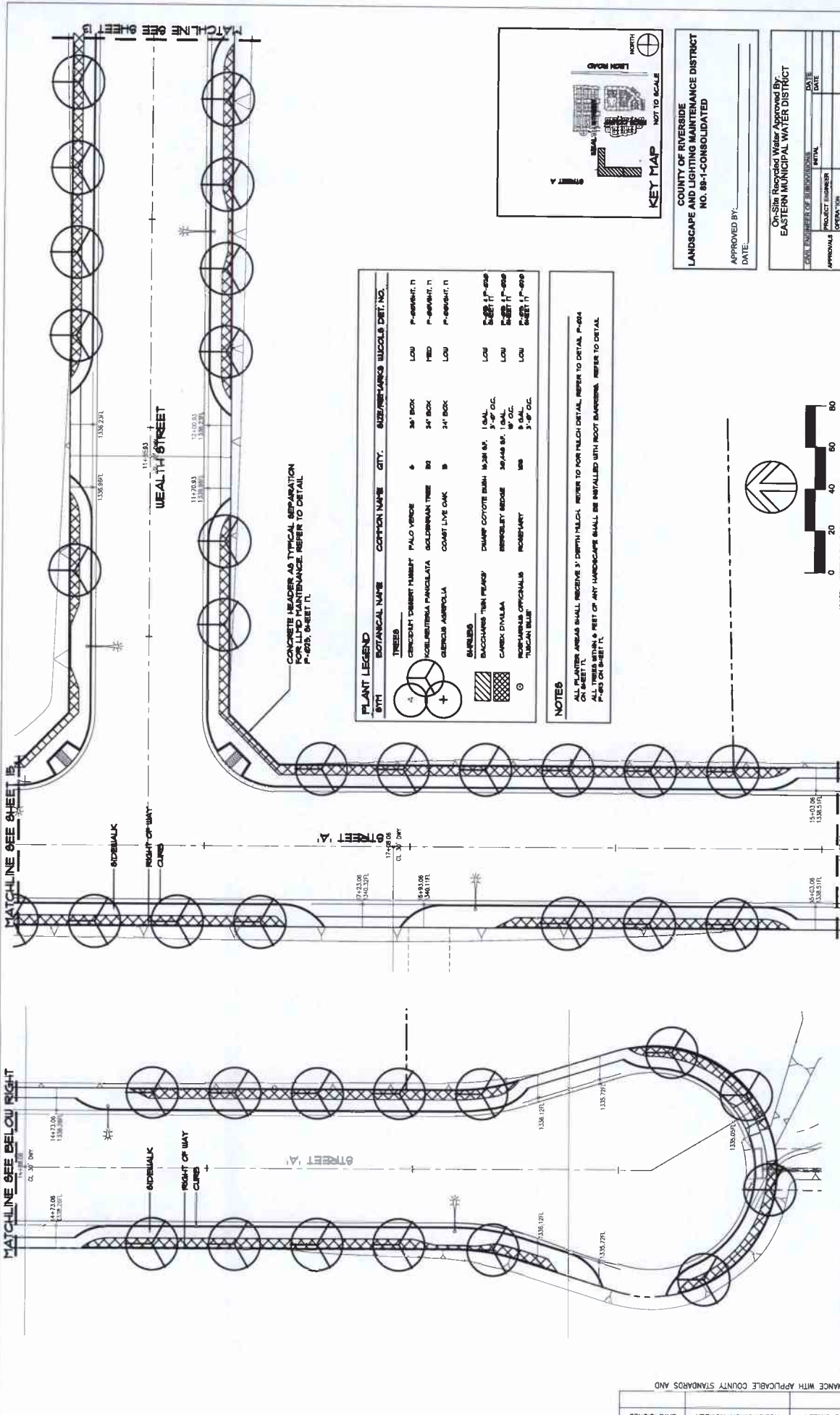
PREPARED BY:  
**R. DALE HADFIELD**  
REGISTERED PROFESSIONAL ENGINEER  
NO. 48827  
7201 FORBES ROAD  
LAGUNA HILLS, CA 92657  
(949) 367-4321

DATE: 2/28/13

NO.	REVISIONS	DATE	ENGINEER	COUNTY

**DIG ALERT**  
CALL BEFORE YOU DIG  
TOLL FREE 1-800-297-2800  
A PUBLIC SERVICE BY UNDERGROUND SERVICE ALERT

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES.  
PLAN CHECK OPERSHIFT ENGINEER  
REGISTRATION NUMBER  
DATE SIGNED



CONCRETE FINISH AS TYPICAL SEPARATION FOR LAND MAINTENANCE REFER TO DETAIL P-623, SHEET 11.

SYM	BOTANICAL NAME	COTTON NAME	CITY	SIZE/REPLACEMENT	WUCOLS DET. NO.
T	CERISEAU	SHERRY HUBBART	PALO VERDE	36" BOX	LOW
	ROSELINDA	PANCOLATA	GOLDENMAN TREE	24" BOX	MED
	GENICUS	ASPIROLA	COURT LIVE OAK	24" BOX	LOW
S	SHARLES	CHAMP	COTYOTE BARK	10.25" Ø	LOW
	SHARLES	TRIN PEARL	REINOLLY REDGE	36.45" Ø	LOW
	SHARLES	TRIN PEARL	REINOLLY REDGE	36.45" Ø	LOW
	SHARLES	TRIN PEARL	REINOLLY REDGE	36.45" Ø	LOW
O	SHARLES	TRIN PEARL	REINOLLY REDGE	36.45" Ø	LOW
	SHARLES	TRIN PEARL	REINOLLY REDGE	36.45" Ø	LOW
	SHARLES	TRIN PEARL	REINOLLY REDGE	36.45" Ø	LOW

**NOTES**  
 ALL PLANTING AREAS SHALL RECEIVE 2" DEPTH MULCH. REFER TO FOR MULCH DETAIL. REFER TO DETAIL P-624 ON SHEET 11.  
 ALL TREES WITHIN 6 FEET OF ANY LANDSCAPE SHALL BE INSTALLED WITH ROOT BARRIERS. REFER TO DETAIL P-624 ON SHEET 11.

APPROVED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_

COUNTY OF RIVERSIDE  
 LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
 NO. 88-1-CONSOLIDATED

ON SITE RIVERSIDE WATER APPROVED BY  
 EASTERN MUNICIPAL WATER DISTRICT

LOCAL ENGINEER OF RECORD/PROFESSIONAL SEAL  
 PROJECT ENGINEER: \_\_\_\_\_  
 OPERATOR: \_\_\_\_\_  
 DATE: \_\_\_\_\_

P.P. 21163 J.P. No. 080238 SHEET NO. 14  
 COUNTY OF RIVERSIDE SCHEDULE "E"  
 STREET IMPROVEMENT PLAN & PROFILE  
 PLANTING PLAN  
 FOR: FRENCH VALLEY V.M.D. COUNTY OF RIVERSIDE  
 AIRPORT CENTER 04-777.03 FILE NO.

PREPARED BY:  
 R. DALE HADFIELD ARCHITECTS  
 2701 FORBES ROAD  
 UCIANA, INDIAN CA 92877  
 PHONE (949) 367-4321  
 FAX (949) 367-4321

DATE: \_\_\_\_\_



NO.	REVISIONS	DATE	COUNTY

NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.

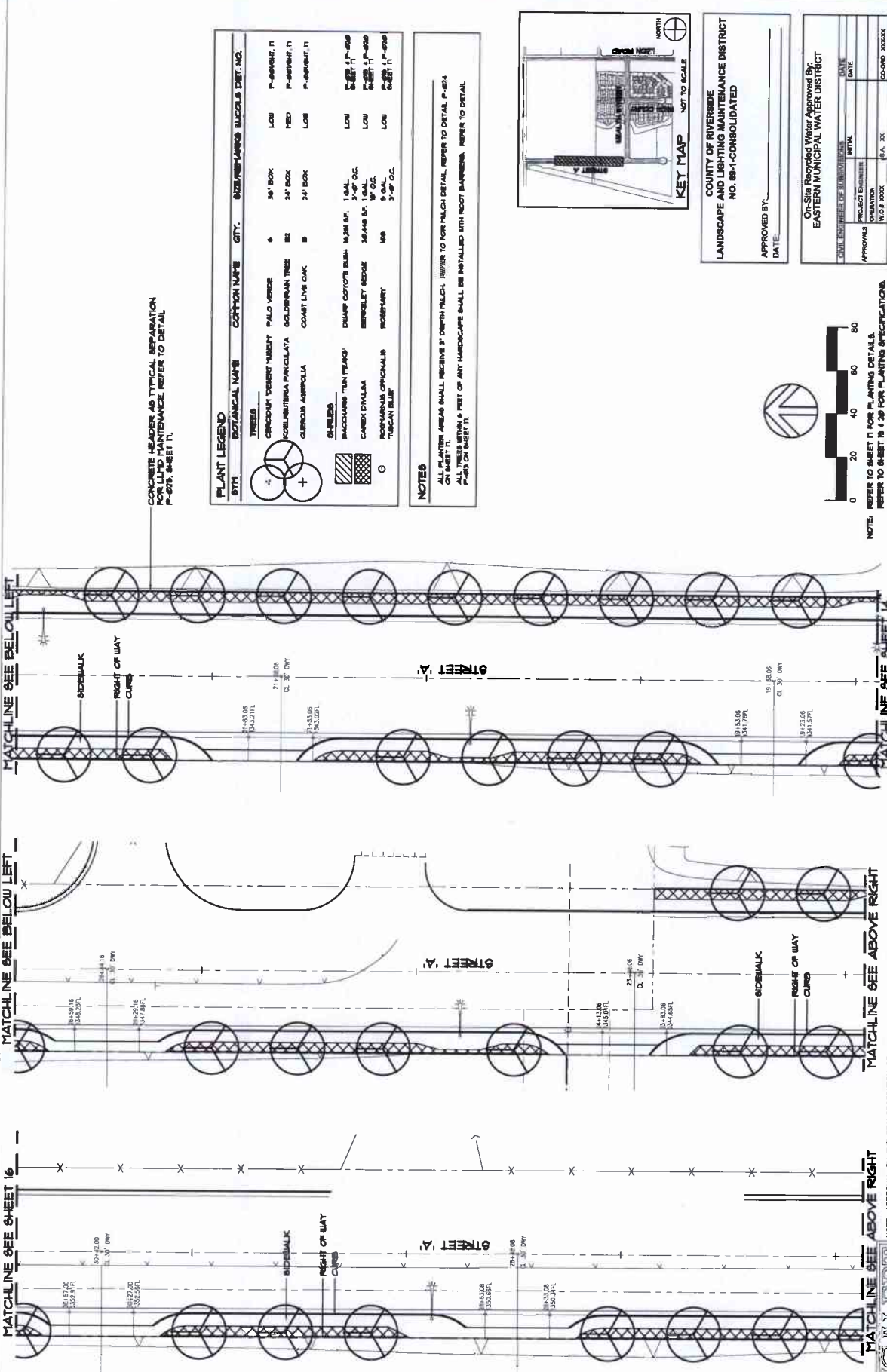
NOTES CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ALL NECESSARY PERMITS AND/OR GRADING PERMIT HAS BEEN ISSUED.

The public safety of the community is the paramount concern of the contractor. The contractor shall be responsible for the safety of the public and the community during the construction of the project.

**DIG ALERT**  
 DIAL BEFORE YOU DIG  
 TOLL FREE 1-800-227-2600  
 A PUBLIC SERVICE BY UNDERGROUND SERVICE ALERT

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES

PLAN CHECK OVERSIGHT ENGINEER \_\_\_\_\_ DATE SIGNED \_\_\_\_\_  
 REGISTRATION NUMBER \_\_\_\_\_



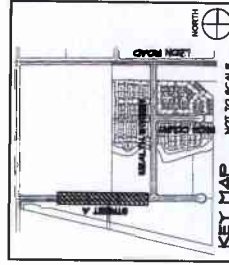
CONCRETE HEADER AS TYPICAL SEPARATION FOR LAND MAINTENANCE REFER TO DETAIL P-675, SHEET 11.

SYM	BOTANICAL NAME	COTTON NAME	CITY	SIZE/SPACING	MUCOSUS DIST. NO.
TREES	CRUCIATA TRIDENT FERNET	PALO VERDE	8	36" BOX	LOW P-684/11, P-684/12
	KOELBUTERIA PANICOLATA	SOLDANERIAN TREE	8B	24" BOX	MED P-684/11, P-684/12
	GENIBUS ASPEROLLA	COAST LIVE OAK	8	24" BOX	LOW P-684/11, P-684/12
SHRUBS	BACCHARIS TRIN FRANGI	DWARF COTONNE BUSH	16, 28 SF.	1 GAL.	3'-6" O.C. P-684/11, P-684/12
	CAREX DYNALIA	BIRCHLEY SCRUB	36x48 SF.	1 GAL.	3'-6" O.C. P-684/11, P-684/12
	RODRIGUESIA OFFICINALIS	ROSEMARY	108	9 GAL.	3'-6" O.C. P-684/11, P-684/12
	TURCAN BLUE			3'-6" O.C.	

**NOTES**

ALL PLANTER AREAS SHALL RECEIVE 3" DEPTH MULCH. REFER TO FOR MULCH DETAIL. REFER TO DETAIL P-674 ON SHEET 11.

ALL PLANTING SHALL BE INSTALLED WITH ROOT BARRIERS. REFER TO DETAIL P-674 ON SHEET 11.



COUNTY OF RIVERSIDE  
LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
NO. 88-1-CONSOLIDATED

APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

On-Site Prepared Under Approval By: EASTERN MUNICIPAL WATER DISTRICT			
DATE	DATE	DATE	DATE
PROJECT ENGINEER	OPERATOR	PROJECT ENGINEER	OPERATOR
10/21/2008	10/21/2008	10/21/2008	10/21/2008

P.P. 33891	P.P. 21163	L.P. No. 082038	SHEET NO.
			15
COUNTY OF RIVERSIDE SCHEDULE 'E' STREET IMPROVEMENT PLAN & PROFILE			OF 22 SHEETS
PLANTING PLAN			
FOR: FRENCH VALLEY AIRPORT CENTER			FILE NO. 04-1777 03

PREPARED BY:  
**R. DALE HADREDD**  
ARCHITECT  
CERT. NO. 1290, EXP. 3/28/13

2700 FORBES ROAD  
LAJUNIA, CALIF. 92037  
(949) 367-9321

BENCHMARK: COUNTY OF RIVERSIDE  
DESIGNATION: TRM 1-88  
CORNER OF NORTHEAST CORNER OF SOUTHERLY OF AULD ROAD AND 97 FEET WESTERLY OF 10th STREET  
SCALE: 1" = 100'

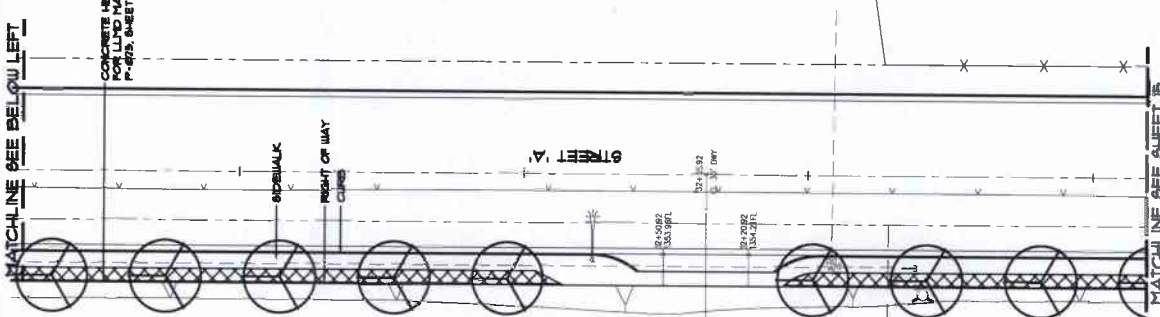


APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES:	PLANNING CHECK	OVERSIGHT ENGINEER	DATE SIGNED
APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES:	PLANNING CHECK	OVERSIGHT ENGINEER	DATE SIGNED
APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES:	PLANNING CHECK	OVERSIGHT ENGINEER	DATE SIGNED

WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENGINEERING GRADING PERMIT HAS BEEN ISSUED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR VERIFYING THE ACCURACY OF ALL INFORMATION PROVIDED TO THE ENGINEER.

**DOG MOUNTAIN**  
DIAZ ENGINEERING  
YOU DID IT ALL  
A PUBLIC SERVICE BY UNDERGROUND SERVICE ALERT

1-800-227-2800



CONCRETE HEADER AS TYPICAL SEPARATION FOR LIND MAINTENANCE REFER TO DETAIL P-025, SHEET 11.

PLANT LEGEND	BOTANICAL NAME	COMMON NAME	CITY	SIZE/STEM/ANNUALS	WILCOX'S DET. NO.
TREES	CELESTINA TREMBLANT	PALE YEW	0	34" BOX	P-000/01, 11
	ACELIBUTERA PANDOLATA	GOLDBERRY TREE	0	24" BOX	P-000/01, 11
	QUERCUS AGROBOLIA	COAST LIVE OAK	0	24" BOX	P-000/01, 11
SHRUBS	QUERCUS TINIFOLIA	CHAMP COYOTE BURN	1/2 3/8 5/8	1 GAL. 1" OC.	P-000/01, 11
	CAREX OVALIS	BENNETT'S SEDGE	3/8 1/4 1/2	1 GAL. 1" OC.	P-000/01, 11
	ROSEMARY	ROSEMARY	1/2 1/2	5 GAL. 3" OC.	P-000/01, 11
	ROSEMARY	ROSEMARY	1/2 1/2	5 GAL. 3" OC.	P-000/01, 11

**NOTES**  
 ALL PLANTING AREAS SHALL RECEIVE 3" DEPTH MULCH. REFER TO FOR MULCH DETAIL. REFER TO DETAIL P-024 ON SHEET 11.  
 ALL TREES WITHIN 6' FEET OF ANY HANDICAP SHALL BE INSTALLED WITH ROOT BARRIERS. REFER TO DETAIL P-020 ON SHEET 11.

PLUM CHECK OVERSIGHT ENGINEER  
 REGISTRATION NUMBER  
 DATE SIGNED



NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.  
 WORK CONTAINED WITHIN THESE PLANS IS SUBJECT TO THE REQUIREMENTS OF THE CALIFORNIA GRADING PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.  
 THE USER SHALL BE RESPONSIBLE FOR VERIFYING THE ACCURACY OF THE DATA AND THE USER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.

NO.	DATE	BY	FOR



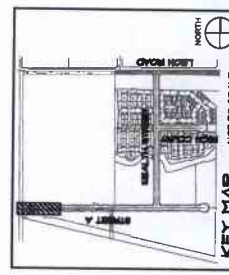
PREPARED BY:  
**DALE HADDOCK**  
 LICENSED PROFESSIONAL ARCHITECT  
 27801 FORBES ROAD  
 SUITE 51782  
 SAN JOSE, CA 95077  
 (408) 307-8275  
 FAX: (408) 307-8301

REVISIONS

NO.	DATE	BY	DESCRIPTION

PROJECT INFORMATION  
 PROJECT NUMBER: 080038  
 PROJECT ENGINEER: [Blank]  
 PROJECT DATE: [Blank]  
 COUNTY: RIVERSIDE  
 SHEET NO.: 16  
 SHEET TOTAL: 22

PLANTING PLAN & PROFILE  
 COUNTY: RIVERSIDE  
 W.O. NO.: 04-777.03  
 AIRPORT CENTER



COUNTY OF RIVERSIDE  
 LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
 NO. 88-1-CONSOLIDATED

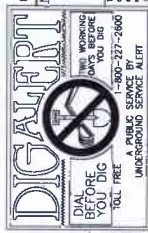
APPROVED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_

On-Site Recycled Water Approved By:  
 EASTERN MUNICIPAL WATER DISTRICT



NOTE: REFER TO SHEET 11 FOR PLANTING DETAILS.  
 REFER TO SHEET 16 FOR PLANTING SPECIFICATIONS.

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES



NOTE: APPROVAL BY THE TRANSPORTATION DEPARTMENT IS FOR WORK WITHIN THE ROAD RIGHT-OF-WAY ONLY.

WORK CONTAINED WITHIN THESE PLANS SHALL BE IN ACCORDANCE WITH THE CALIFORNIA TRANSPORTATION DEPARTMENT'S STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, LATEST EDITION, AND THE STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGE CONSTRUCTION, LATEST EDITION, AND THE STANDARD SPECIFICATIONS FOR HIGHWAY STRUCTURES, LATEST EDITION, AND THE STANDARD SPECIFICATIONS FOR HIGHWAY MATERIALS, LATEST EDITION.

THE USER OF THESE PLANS SHALL BE RESPONSIBLE FOR VERIFYING THE ACCURACY OF ALL DATA, FIELD SURVEYING, AND THE USE OF APPROPRIATE SAFETY MEASURES. THE USER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FOR THE WORK DESCRIBED IN THESE PLANS.

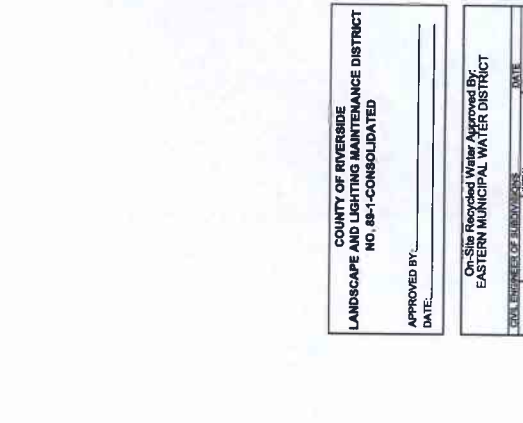
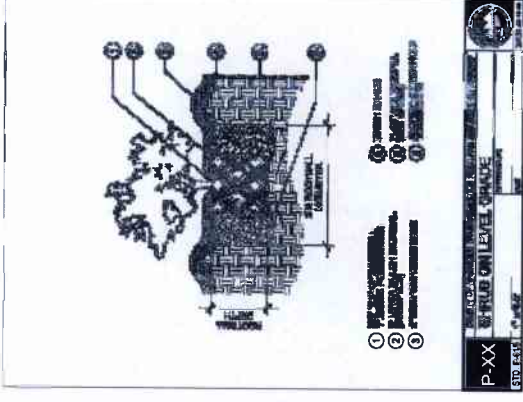
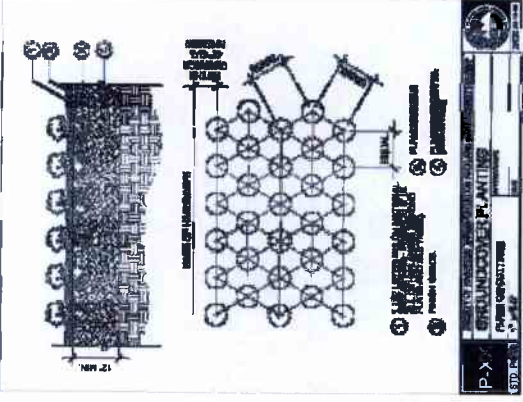
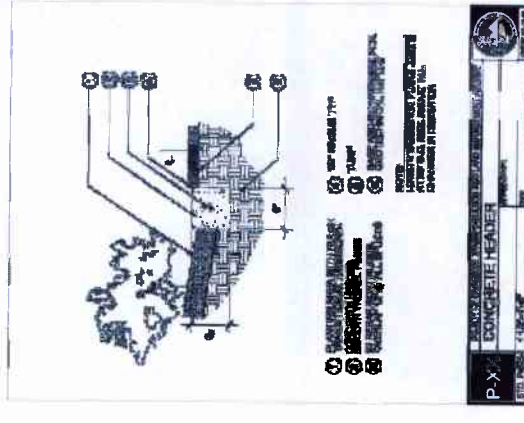
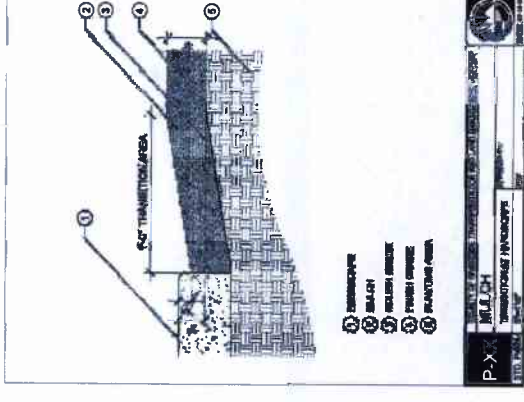
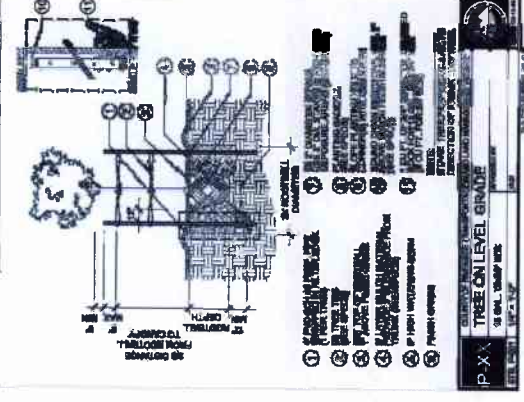
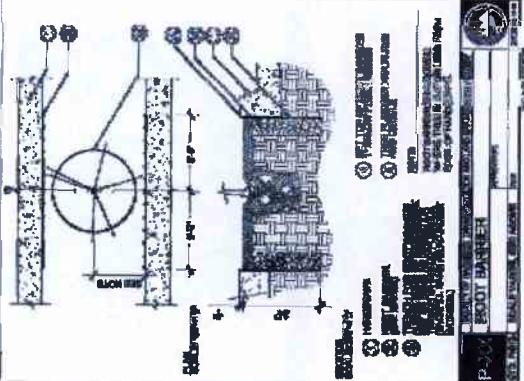
NO.	DATE	BY	REVISIONS



PREPARED BY: **R. DALE FAURIED**  
 2780 FORBES ROAD  
 SUITE 210  
 SAN JOSE, CALIF. 95077  
 (408) 267-3077  
 FAX (408) 267-8281

LABORATORY: **LABORATORY**  
 ADDRESS: **ADDRESS**  
 CITY: **CITY**  
 STATE: **STATE**  
 SCALE: **SCALE**

P.M. 31691 P.P. 21163 I.P. No. 0802.88 SHEET NO. 17  
 COUNTY OF RIVERSIDE  
 SCHEDULE 'E'  
 STREET IMPROVEMENT PLAN & PROFILE  
 PLANTING DETAILS  
 OF 2/2 SHEETS  
 FOR: FRENCH VALLEY W.C. COUNTY  
 AIRPORT CENTER FILE NO.



COUNTY OF RIVERSIDE  
 LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT  
 NO. 88-1-CONSOLIDATED

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

On-Site Recycled Water Approved By:  
 EASTERN MUNICIPAL WATER DISTRICT

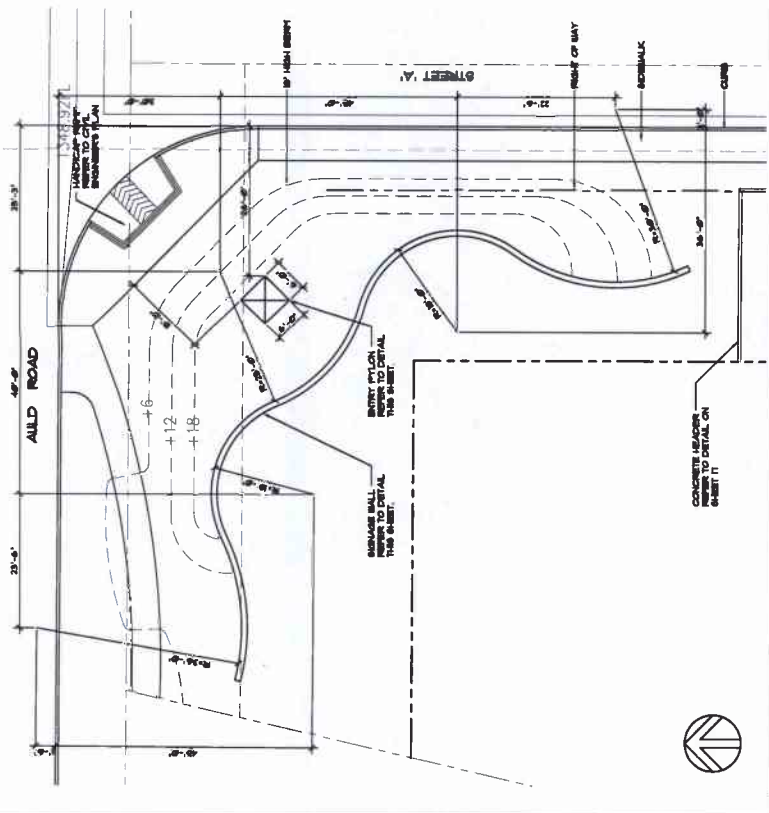
DATE: \_\_\_\_\_  
 PROJECT ENGINEER: \_\_\_\_\_  
 M.O.A. NO. \_\_\_\_\_  
 COUNTY: \_\_\_\_\_  
 SCALE: \_\_\_\_\_



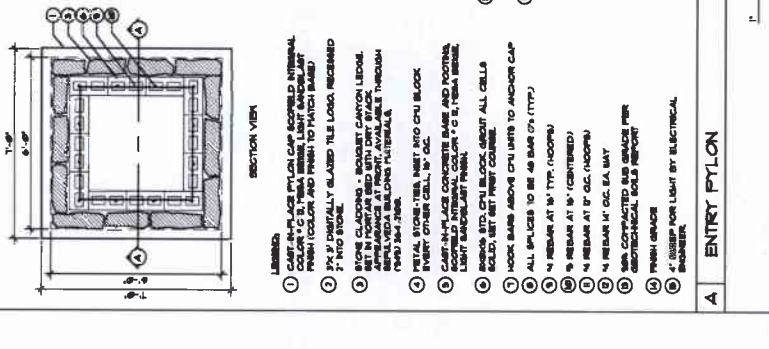




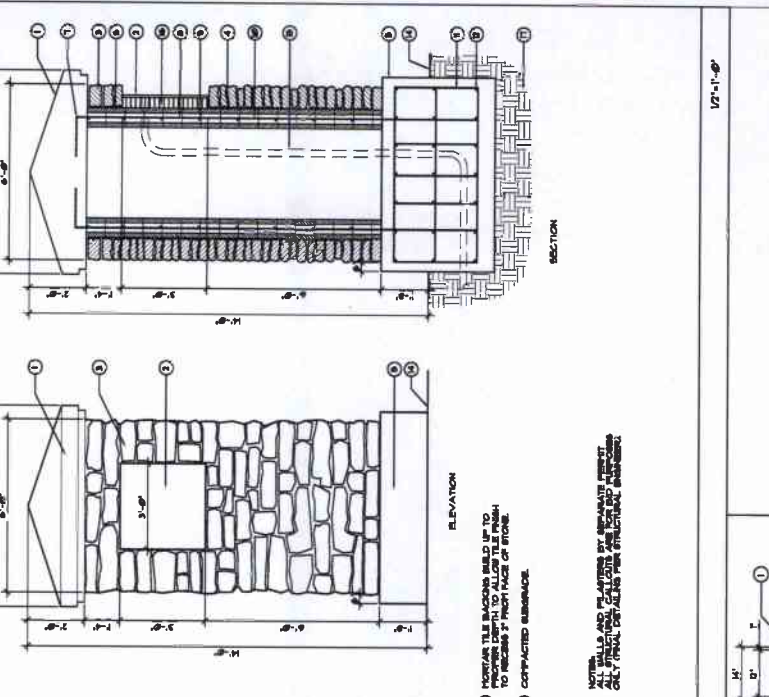




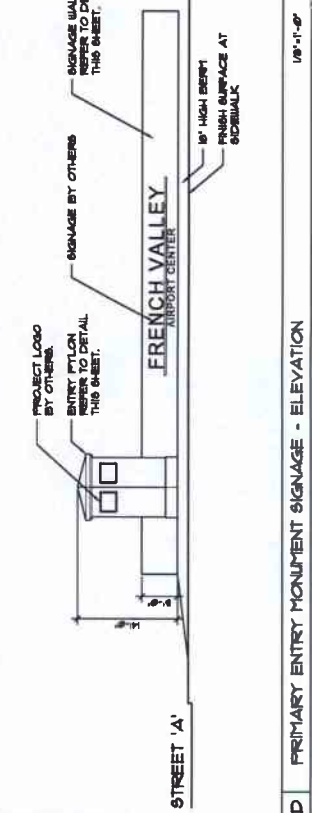
C PRIMARY ENTRY MONUMENT SIGNAGE - PLAN VIEW



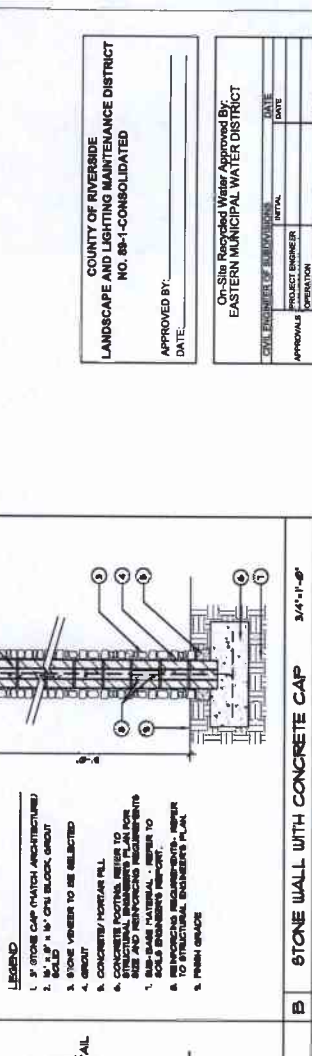
B STONE WALL WITH CONCRETE CAP



A ENTRY PYLON



D PRIMARY ENTRY MONUMENT SIGNAGE - ELEVATION



E ENTRY PYLON

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES:

PLAN CHECK OVERSIGHT ENGINEER	REGISTRATION NUMBER	DATE SIGNED
-------------------------------	---------------------	-------------

LEGEND:

1. CAP: IN-PLACE PYLON CAP SCORED INTERNAL COLOR C-2, FRESH BRUSH LIGHT SANDY GRAY FINISH (COLOR AND FINISH TO MATCH BUILT)
2. 2" x 2" POLYALLYL GLAZED TILE LOGS, RECEIVED
3. STONE: CALIFORNIA - SANDSTONE CANTON, LEISLE. SET IN PORTLAND BED WITH DIRT BRACK. APPLIED TO FRONT, AVAILABLE THROUGH FINISH AND JOINTS.
4. METAL STONE-TIE: NET INTO CPU BLOCK EVERY OTHER CELL, 1/2" O.C.
5. CAP: IN-PLACE CONCRETE BASE AND FOOTING LIGHT SANDY GRAY FINISH (COLOR C-2, FRESH BRUSH).
6. REBAR: 1/2" O.C. (HORIZONTAL)
7. REBAR: 1/2" O.C. (VERTICAL)
8. ALL SPLICES TO BE AS SHOWN ON (TYP.)
9. REBAR AT 18" (HORIZONTAL)
10. REBAR AT 18" (VERTICAL)
11. REBAR AT 12" O.C. (HORIZONTAL)
12. REBAR AT 12" O.C. (VERTICAL)
13. FINISH GRADE REFER TO DETAIL ON SHEET 11
14. FINISH GRADE REFER TO DETAIL ON SHEET 11

NOTES:

1. HORIZONAL TILE BACKINGS BUILT UP TO PROVIDE DEPTH TO ALLOW TILE FINISH TO RESIST 2" FRONT FACE OF STONE.
2. CONTRACTED SUBGRADE.

NOTES:

1. HORIZONAL TILE BACKINGS BUILT UP TO PROVIDE DEPTH TO ALLOW TILE FINISH TO RESIST 2" FRONT FACE OF STONE.
2. CONTRACTED SUBGRADE.

17'-1'-0"

17'-1'-0"

SECTION VIEW

SECTION

SECTION VIEW

SECTION

SECTION VIEW

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PLAN CHECK OVERSIGHT ENGINEER	REGISTRATION NUMBER	DATE SIGNED
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APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES:

PLAN CHECK OVERSIGHT ENGINEER	REGISTRATION NUMBER	DATE SIGNED
-------------------------------	---------------------	-------------

# Landscape Architecture Plot Plans for FRENCH VALLEY AIRPORT CENTER

WEALTH STREET  
TEMECULA, CA

COUNTY OF RIVERSIDE, CA



FRENCH VALLEY  
AIRPORT CENTER  
THE CENTER AT  
TEMECULA, CA

INDUSTRIAL  
OFFICE PARK  
#17 THROUGH 28

FRENCH VALLEY AIRPORT  
CENTER, LLC  
1105 ANGLEBO  
LOS ANGELES, CA



COVER SHEET

APW 983-080-002  
IP No. 080038  
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DATE	BY	FOR

SHEET  
L-1

**PROJECT DIRECTORY:**

**OWNER:**  
Edwards Properties, Inc  
dba French Valley Airport Center, LLC  
Manulife Plaza  
135 South Figueroa Street, Ste 8029  
Los Angeles, CA 90079  
(310) 661-1329  
FAX: (310) 661-5073  
Attn: Joe Poon

**LANDSCAPE ARCHITECT:**  
R. Dale Hadfield / L.L.A.  
27661 Fontaine Road, Suite 80  
Laguna Hills, CA 92653  
Phone: (714) 361-7030  
FAX: (714) 361-7030  
E-Mail: rhadfield@hadfield.com  
Attn: R. Dale Hadfield

**ARCHITECT:**  
Architects Orange  
144 North Orange Street  
Orange, CA 92666-1424  
Phone: (714) 951-7000  
FAX: (714) 635-9286  
Attn: Hugh Rowe

I AGREE TO COMPLY WITH THE CRITERIA OF  
COMPLIANCE AND MAINTENANCE PROVIDED IN THE  
IRRI-GATION DESIGN PLAN. THE IRRIGATION DESIGN  
CRITERIA FOR EFFICIENT USE OF WATER IN THE  
IRRI-GATION DESIGN PLAN.

R. DALE HADFIELD, LANDSCAPE ARCHITECT

**DATA**

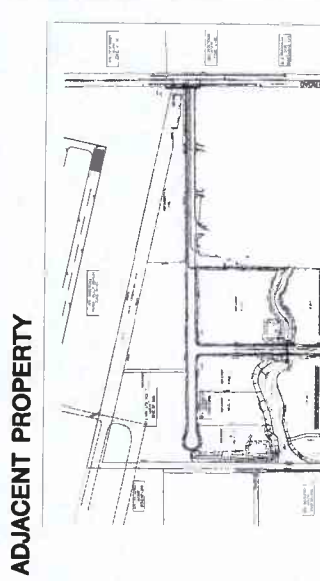
**PHASE I**  
9.86 ACRES (205,418 SF)  
APPROX. DIMENSION: 500' x 814'

**PHASE II**  
84 ACRES (336,678 SF)  
APPROX. DIMENSION: 465' x 930'

**PARKING LOT SHADING REQUIREMENT:**  
TOTAL PARKING STALL AREA:  
40,000 SF  
SHADING PROVIDED:  
3,000 SF

511 STALLS  
82 SF PER STALL  
AREA BY:  
30,000 SF  
3,000 SF

SHADING IS BASED ON 8 YEAR GROWTH OF TREES.  
50' PARKING SPACES + 50% TREY REQUIREMENT



**LEGAL DESCRIPTION**  
THE NORTHEAST QUARTER OF SECTION 1, TOWNSHIP 1 SOUTH, RANGE 2 SOUTH, COUNTY OF RIVERSIDE, CALIFORNIA, AND THE SOUTHWEST QUARTER OF SECTION 1, TOWNSHIP 1 SOUTH, RANGE 2 SOUTH, COUNTY OF RIVERSIDE, CALIFORNIA, AS SHOWN ON THE ORIGINAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, MAP NO. 20162, OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

**BASIS OF BEARINGS**  
REFERENCES TO QUADRANTS OR COMPLIANCE NO. 0485, RECORDED APRIL 28, 1988 AS INSTRUMENT NO. 20187, OFFICIAL RECORDS.

**BASES OF BEARINGS**  
BASIS OF BEARING CENTERLINE LEON RD. - SEC. 1 EASTLINE BETWEEN E14 AND SECTION CORNER 6, 1 1/8

**APN# 963-080-002**

**IP. No. 080038**

**PROJECT DIRECTORY:**

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dba French Valley Airport Center, LLC  
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144 North Orange Street  
Orange, CA 92666-1424  
Phone: (714) 951-7000  
FAX: (714) 635-9286  
Attn: Hugh Rowe



**GENERAL NOTES:**

1. THE CONTRACTOR OR DEVELOPER SHALL SUBMIT A SOIL MANAGEMENT PLAN TO THE COUNTY OF RIVERSIDE FOR REVIEW AND APPROVAL PRIOR TO THE BEGINNING OF CONSTRUCTION. THE LANDSCAPE INSTALLATION SPECIFICATION SHALL BE IN ACCORDANCE WITH THE LANDSCAPE INSTALLATION SPECIFICATION FOR THE COUNTY OF RIVERSIDE, CALIFORNIA. THIS INFORMATION CAN BE FOUND IN THE COUNTY OF RIVERSIDE GUIDE TO CALIFORNIA RESIDENTIAL LANDSCAPING PAGE 16, 17.

**GENERAL MAINTENANCE:**

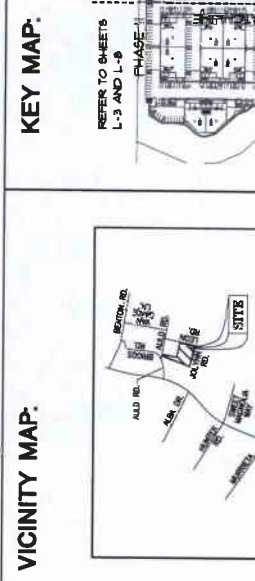
- CONTRACTOR SHALL MAINTAIN ALL EXISTING UTILITIES AS SHOWN ON THE RECORD DRAWINGS.
- CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES AND STRUCTURES FROM DAMAGE DURING CONSTRUCTION.
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**SHEET INDEX:**

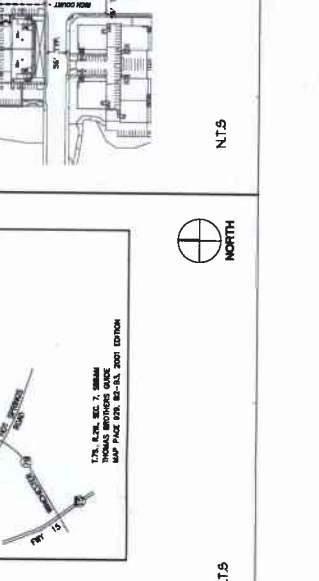
L-1 COVER SHEET  
L-2 IRRIGATION PLAN  
L-3 IRRIGATION PLAN  
L-4 IRRIGATION DETAILS AND SPECIFICATIONS  
L-5 IRRIGATION DETAILS AND SPECIFICATIONS  
L-6 IRRIGATION DETAILS AND SPECIFICATIONS  
L-7 PLANTING PLAN  
L-8 PLANTING PLAN  
L-9 PLANTING / PILASTER DETAILS AND SPECIFICATIONS

**SUBMITTAL DATES:**  
FIRST SUBMITTAL DATE  
Second Submittal Date  
Third Submittal Date  
Revision Submittal Date

**UNDERGROUND SERVICE ALERT**  
VALIDITY PERIOD  
437-641-3333  
THIS WARNING SIGN INDICATES YOU MAY ENCOUNTER UTILITY LINES. BEFORE BEGINNING WORK, YOU MUST CONTACT THE UTILITY LOCATOR AT THE NUMBER ABOVE TO IDENTIFY THE LOCATION AND DEPTH OF ALL UTILITIES IN THE WORK AREA. THE UTILITY LOCATOR PROVIDES A LIST OF UTILITIES THAT WILL BE LOCATED.



**LANDSCAPE MAINTENANCE ENTITY:**  
CONTINUED LANDSCAPE MAINTENANCE PROVIDED BY THE OWNER



**LANDSCAPE MAINTENANCE ENTITY:**  
CONTINUED LANDSCAPE MAINTENANCE PROVIDED BY THE OWNER



FRENCH VALLEY  
AIRPORT CENTER  
THEIRDALE, CA

INDUSTRIAL  
BUSINESS PARK  
HT THROUGH 128

FRENCH VALLEY AIRPORT  
CENTER, LLC  
100 AMBERLEA, CA



IRRIGATION PLAN

APW 05-00-02  
17th Edition  
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DATE  
BY  
CHECKED BY  
DESIGNED BY  
SCALE: 1" = 20'-0"  
SHEET L-2

PIPE SIZING CHART  
FLOOR (FT)

PIPE SIZE	1/2"	3/4"	1"	1 1/2"	2"
34'					
F					
N					
S					
38'					
42'					

- IRRIGATION LEGEND**
1. 1/2" PIPING SHALL BE USED FOR ALL FLOOD WATERS AND 1/2" PIPING FOR OTHER WATERS.
2. 3/4" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
3. 1" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
4. 1 1/2" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
5. 2" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
6. 3" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
7. 4" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
8. 6" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
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10. 10" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
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14. 18" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
15. 20" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
16. 22" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
17. 24" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
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41. 72" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
42. 74" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
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45. 80" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
46. 82" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
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48. 86" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
49. 88" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
50. 90" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
51. 92" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
52. 94" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
53. 96" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
54. 98" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.
55. 100" PIPING SHALL BE USED FOR ALL POP-UP SPRINKLER HEADS.

**IRRIGATION NOTES**

1. ALL MAIN LINES SHALL BE 12" MINIMUM AND SHALL BE 12" MINIMUM.

2. ALL MAIN LINES SHALL BE 12" MINIMUM AND SHALL BE 12" MINIMUM.

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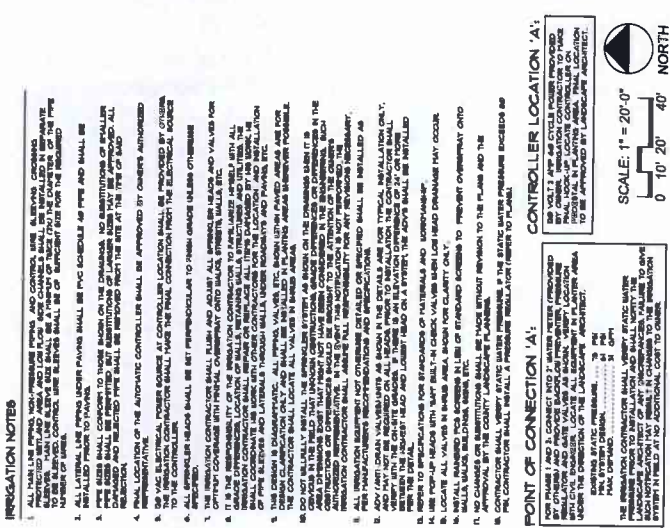
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50. ALL MAIN LINES SHALL BE 12" MINIMUM AND SHALL BE 12" MINIMUM.



**CONTROLLER LOCATION 'A'**  
 1. CONTROLLER SHALL BE INSTALLED AT THE LOCATION INDICATED BY THE NOTE.  
 2. CONTROLLER SHALL BE INSTALLED AT THE LOCATION INDICATED BY THE NOTE.  
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**POINT OF CONNECTION 'A'**  
 1. POINT OF CONNECTION SHALL BE AT THE LOCATION INDICATED BY THE NOTE.  
 2. POINT OF CONNECTION SHALL BE AT THE LOCATION INDICATED BY THE NOTE.  
 3. POINT OF CONNECTION SHALL BE AT THE LOCATION INDICATED BY THE NOTE.  
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**FRENCH VALLEY AIRPORT CENTER**  
 15411 N. STREET  
 FRENCH VALLEY, CA

**INDUSTRIAL BUSINESS PARK**  
 I-17 THROUGH I-26

**FRENCH VALLEY AIRPORT**  
 815 S. PALMDALE  
 LOS ANGELES, CA



**PLANTING PLAN**

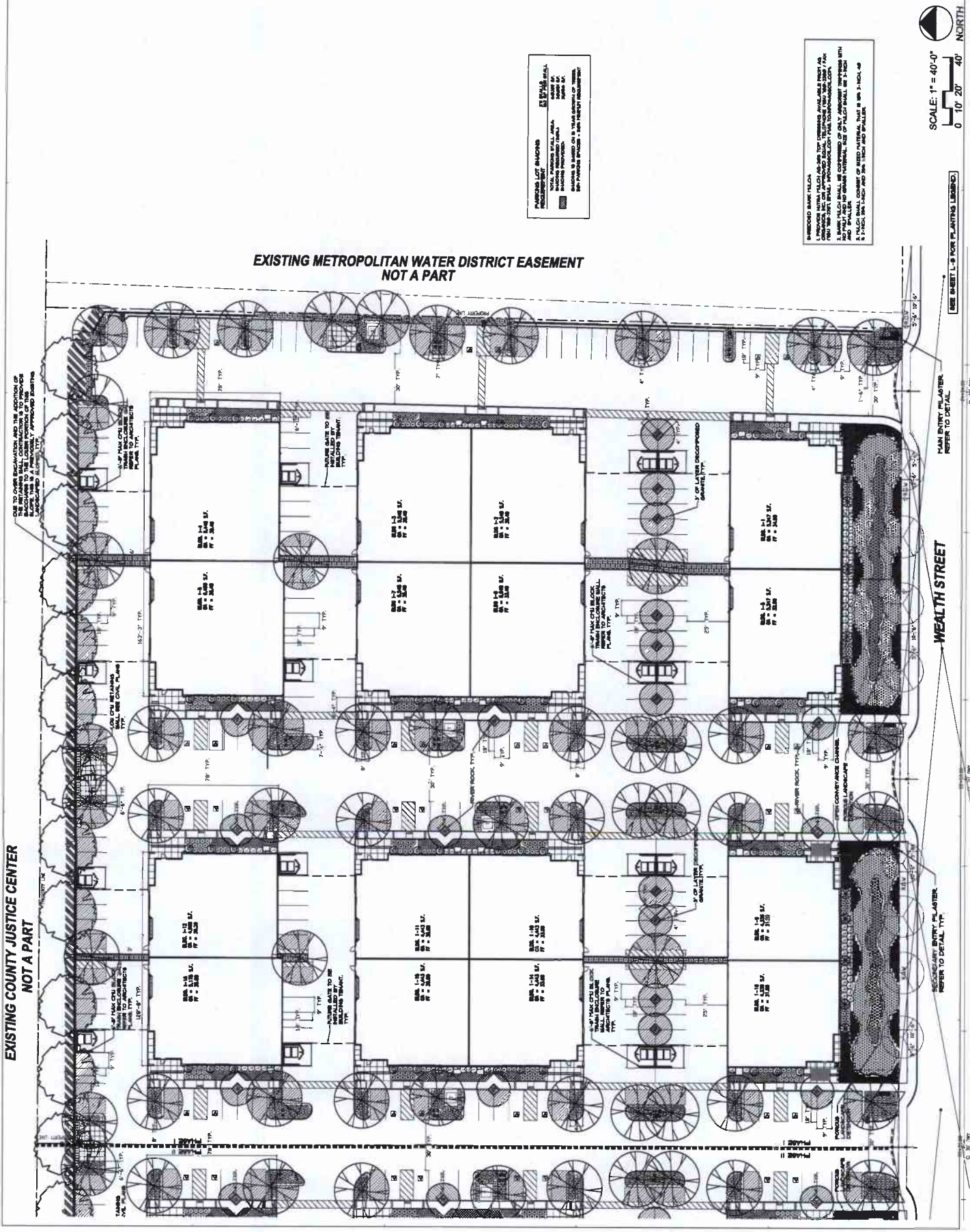
APR 2014 08:00Z  
 17' N. 00000

© 2014 Hubbard Group  
 10000 Hubbell Avenue, Suite 100  
 French Valley, CA 92534  
 (951) 770-1100

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 APPROVED BY: \_\_\_\_\_

SHEET

**L-7**



SCALE: 1" = 40'-0"

0 10' 20' 40'

USE SHEET L-8 FOR PLANTING LEGEND

MAIN ENTRY PLANTER REFER TO DETAIL

WEALTH STREET

SECONDARY ENTRY PLANTER REFER TO DETAIL

**PROPOSED BANK PLANTING**

1. PROVIDE BANK PLANTING AS SHOWN. PROVIDE PLANTING AS SHOWN FOR THE ENTIRE PERIMETER OF THE PLANTER WITH 12\"/>

**PARKING LOT SIGNAGE**

1. PROVIDE SIGNAGE AS SHOWN. PROVIDE SIGNAGE AS SHOWN FOR THE ENTIRE PERIMETER OF THE PLANTER WITH 12\"/>

SEE TO OTHER EASEMENTS AND THE LOCATION OF THE REMAINING WALL. CONTRACTORS TO PROVIDE BLOCK WALLS TO A MINIMUM APPROXIMATE FINISH FINISH TO MATCH EXISTING WALLS.

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PLANTING SYMBOL	COMMON NAME	SIZE	REMARKS	QTY.	SUCCLA #
	24" ROUND	24" ROUND	24" ROUND	14	1
	36" ROUND	36" ROUND	36" ROUND	20	2
	48" ROUND	48" ROUND	48" ROUND	10	3
	60" ROUND	60" ROUND	60" ROUND	10	4
	72" ROUND	72" ROUND	72" ROUND	10	5
	84" ROUND	84" ROUND	84" ROUND	10	6
	96" ROUND	96" ROUND	96" ROUND	10	7
	108" ROUND	108" ROUND	108" ROUND	10	8
	120" ROUND	120" ROUND	120" ROUND	10	9
	132" ROUND	132" ROUND	132" ROUND	10	10
	144" ROUND	144" ROUND	144" ROUND	10	11
	156" ROUND	156" ROUND	156" ROUND	10	12
	168" ROUND	168" ROUND	168" ROUND	10	13
	180" ROUND	180" ROUND	180" ROUND	10	14
	192" ROUND	192" ROUND	192" ROUND	10	15
	204" ROUND	204" ROUND	204" ROUND	10	16
	216" ROUND	216" ROUND	216" ROUND	10	17
	228" ROUND	228" ROUND	228" ROUND	10	18
	240" ROUND	240" ROUND	240" ROUND	10	19
	252" ROUND	252" ROUND	252" ROUND	10	20
	264" ROUND	264" ROUND	264" ROUND	10	21
	276" ROUND	276" ROUND	276" ROUND	10	22
	288" ROUND	288" ROUND	288" ROUND	10	23
	300" ROUND	300" ROUND	300" ROUND	10	24

- PLANTING SCHEDULE**
- PLANTING SYMBOL: PLANTING SYMBOL
  - COMMON NAME: COMMON NAME
  - SIZE: SIZE
  - REMARKS: REMARKS
  - QTY.: QTY.
  - SUCCLA #: SUCCLA #
- NOTES:**
- ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH ALL APPLICABLE REGULATIONS AND PERMITS.
  - ALL PLANTS SHALL BE SUPPLIED BY THE CONTRACTOR AND SHALL BE OF THE SPECIFIED QUALITY AND SIZE.
  - ALL PLANTS SHALL BE PLANTED IN ACCORDANCE WITH THE PLANTING PLAN AND THE SCHEDULE.
  - ALL PLANTS SHALL BE WATERED REGULARLY AND PROTECTED FROM DAMAGE BY ANIMALS AND WEATHER.
  - MAINTENANCE SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER.
  - FOR VERIFIED ALL CONTRACTORS.

**PARKING LOT GRASSING REQUIREMENTS**

SEED TYPE: SEED TYPE

SEED RATE: SEED RATE

SEEDING METHOD: SEEDING METHOD

SEEDING DATE: SEEDING DATE

SEEDING TIME: SEEDING TIME

SEEDING COST: SEEDING COST

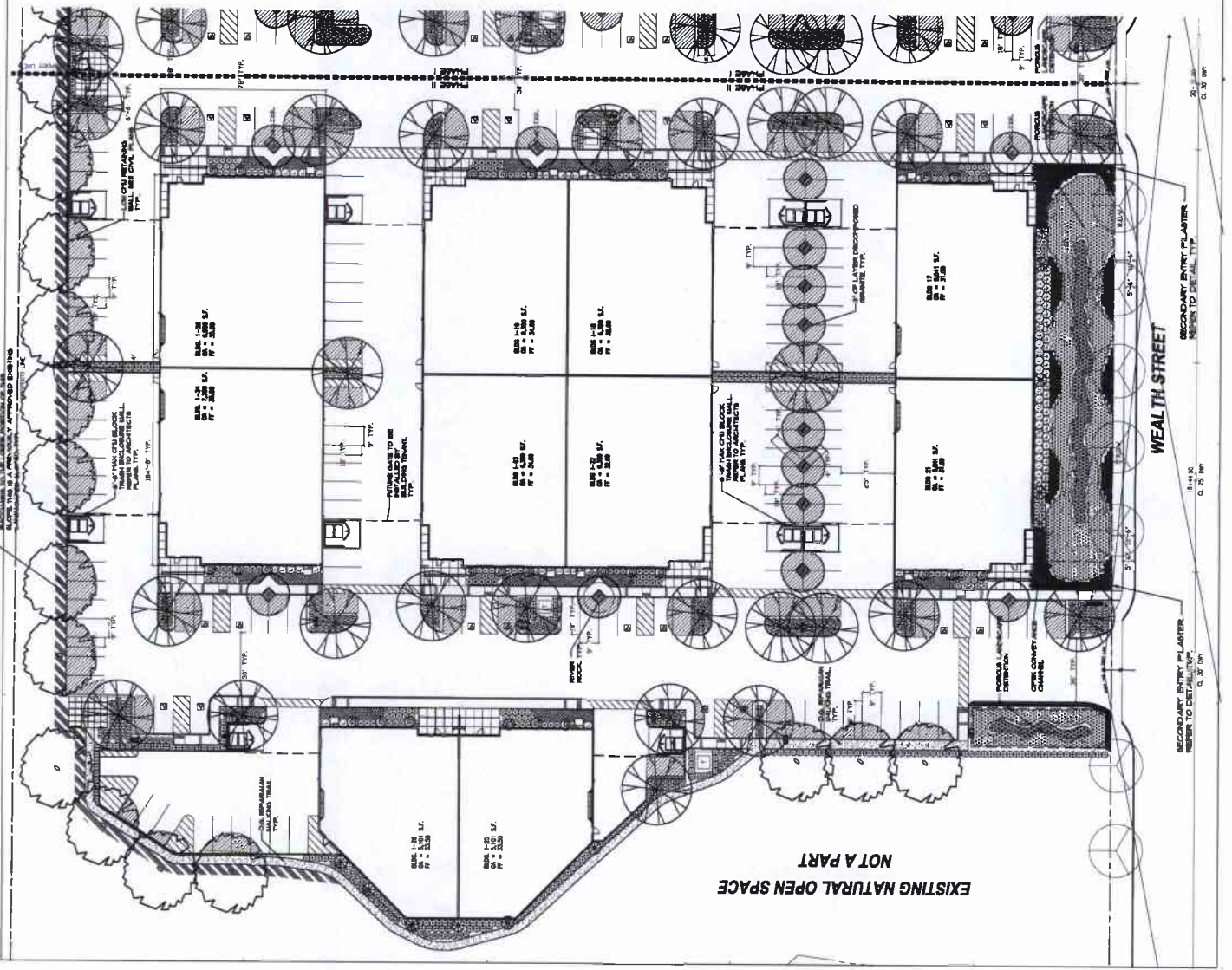
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SEEDING APPROVAL: SEEDING APPROVAL

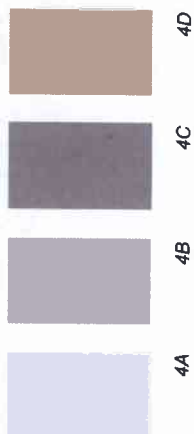
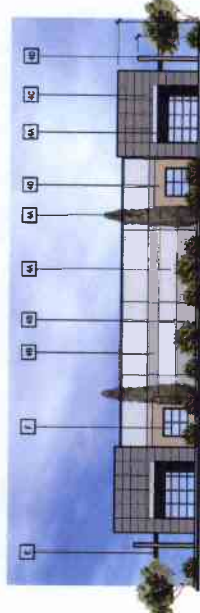
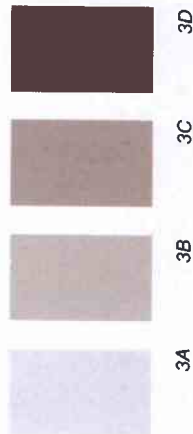
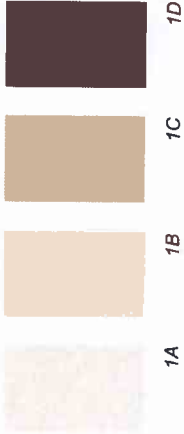
SEEDING INSPECTION: SEEDING INSPECTION

SEEDING REPORT: SEEDING REPORT

SEEDING RECORD: SEEDING RECORD







E



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**COLORS AND MATERIALS**

- 1A CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1800
- 1B CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1801
- 1C CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1803
- 1D CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1883
- 2A CONCRETE TILTUP PANEL - PAINT GLIDDEN # A0729
- 2B CONCRETE TILTUP PANEL - PAINT GLIDDEN # A0730
- 2C CONCRETE TILTUP PANEL - PAINT GLIDDEN # A0732
- 2D CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1945
- 3A CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1835
- 3B CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1836
- 3C CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1837
- 3D CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1848
- 4A CONCRETE TILTUP PANEL - PAINT GLIDDEN # A2010
- 4B CONCRETE TILTUP PANEL - PAINT GLIDDEN # A2011
- 4C CONCRETE TILTUP PANEL - PAINT GLIDDEN # A2012
- 4D CONCRETE TILTUP PANEL - PAINT GLIDDEN # A1823

E ALUMINUM AWNING

F ALUMINUM STOREFRONT AND GLAZING - PPG ATLANTICA, ARCADIA CLEAR



**ARCHITECTS ORANGE**  
 144 NORTH ORANGE ST., ORANGE, CALIFORNIA 92666 (714) 639-9960

**French Valley Airport Center**

**French Valley Airport Center, LLC**  
 515 South Figueroa Street, Suite 10256, Los Angeles, CA 90071