

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Federal Aviation Administration  
Air Traffic Airspace Branch, ASW-520  
2601 Meacham Blvd.  
Fort Worth, TX 76137-0520

Aeronautical Study No.  
2010-AWP-6302-OE

Issued Date: 11/23/2010

Phil Clayton  
Thermal Motorsports Track & Club  
49-499 Eisenhower Avenue  
La Quinta, CA 92253

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building Track Tower Pt 1 of 4
Location:	Thermal, CA
Latitude:	33-36-08.88N NAD 83
Longitude:	116-09-08.88W
Heights:	70 feet above ground level (AGL) -77 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be completed and returned to this office any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part I)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part II)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking and/or lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

Any height exceeding 70 feet above ground level (-77 feet above mean sea level), will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

This determination expires on 05/23/2012 unless:

- (a) extended, revised or terminated by the issuing office.
- (b) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within

6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2010-AWP-6302-OE.

**Signature Control No: 131395244-133534317**

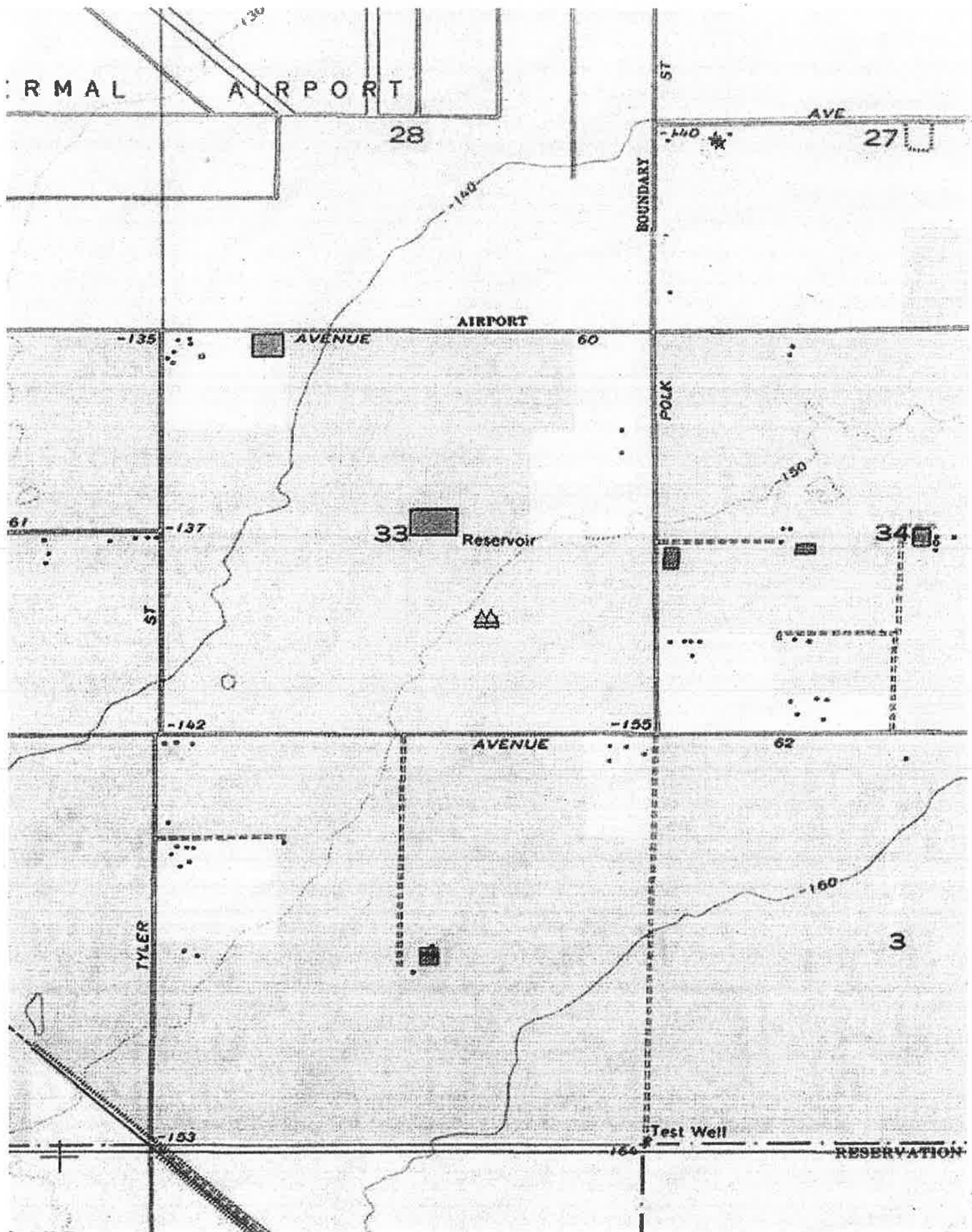
Karen McDonald  
Specialist

( DNE )

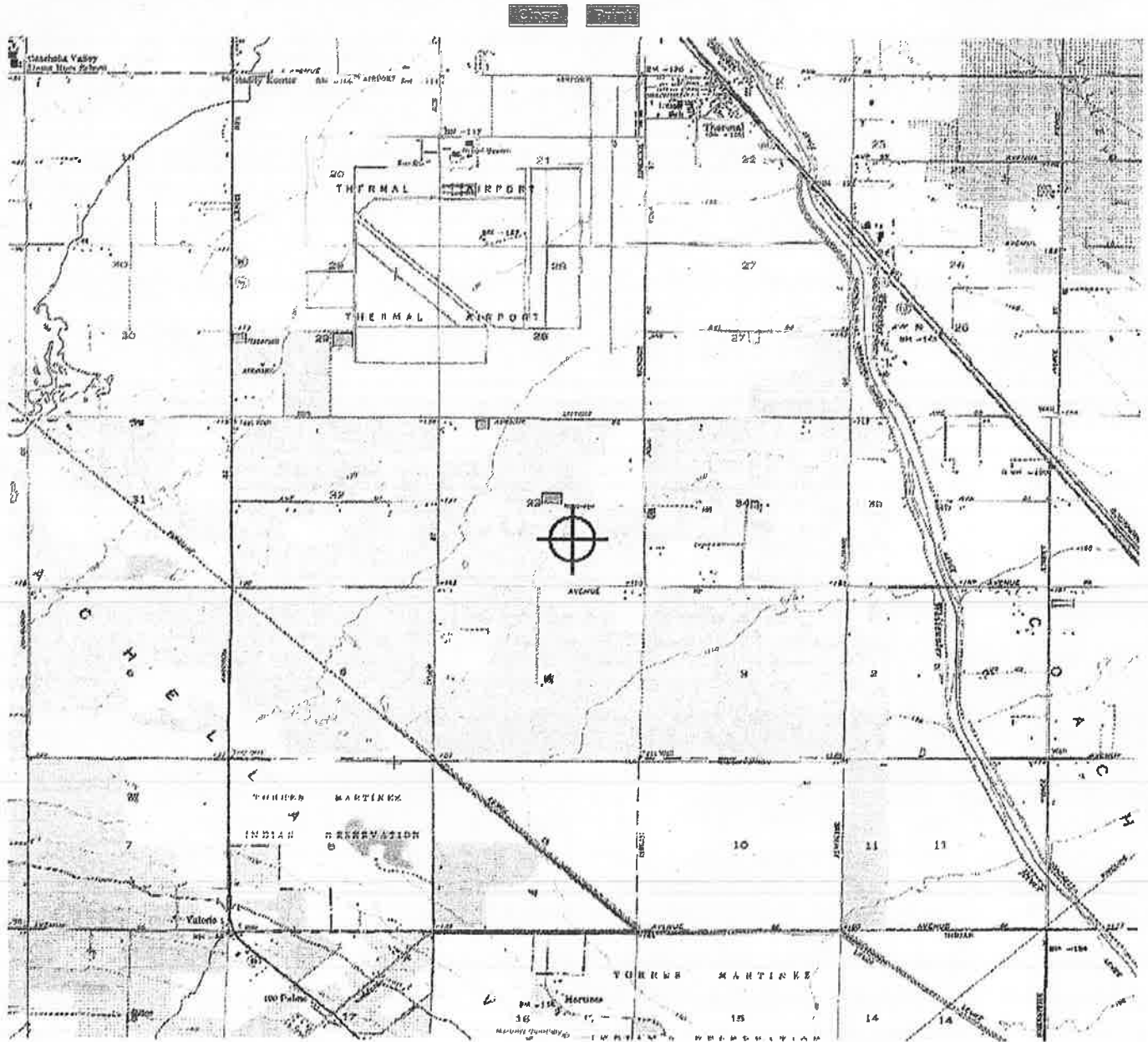
Attachment(s)  
Case Description  
Map(s)

Track Tower for new private motorsports park club track













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Structure: Building Track Tower Pt 2 of 4  
Location: Thermal, CA  
Latitude: 33-36-08.88N NAD 83  
Longitude: 116-09-07.66W  
Heights: 70 feet above ground level (AGL)  
-77 feet above mean sea level (AMSL)

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**Signature Control No: 131395246-133534320**

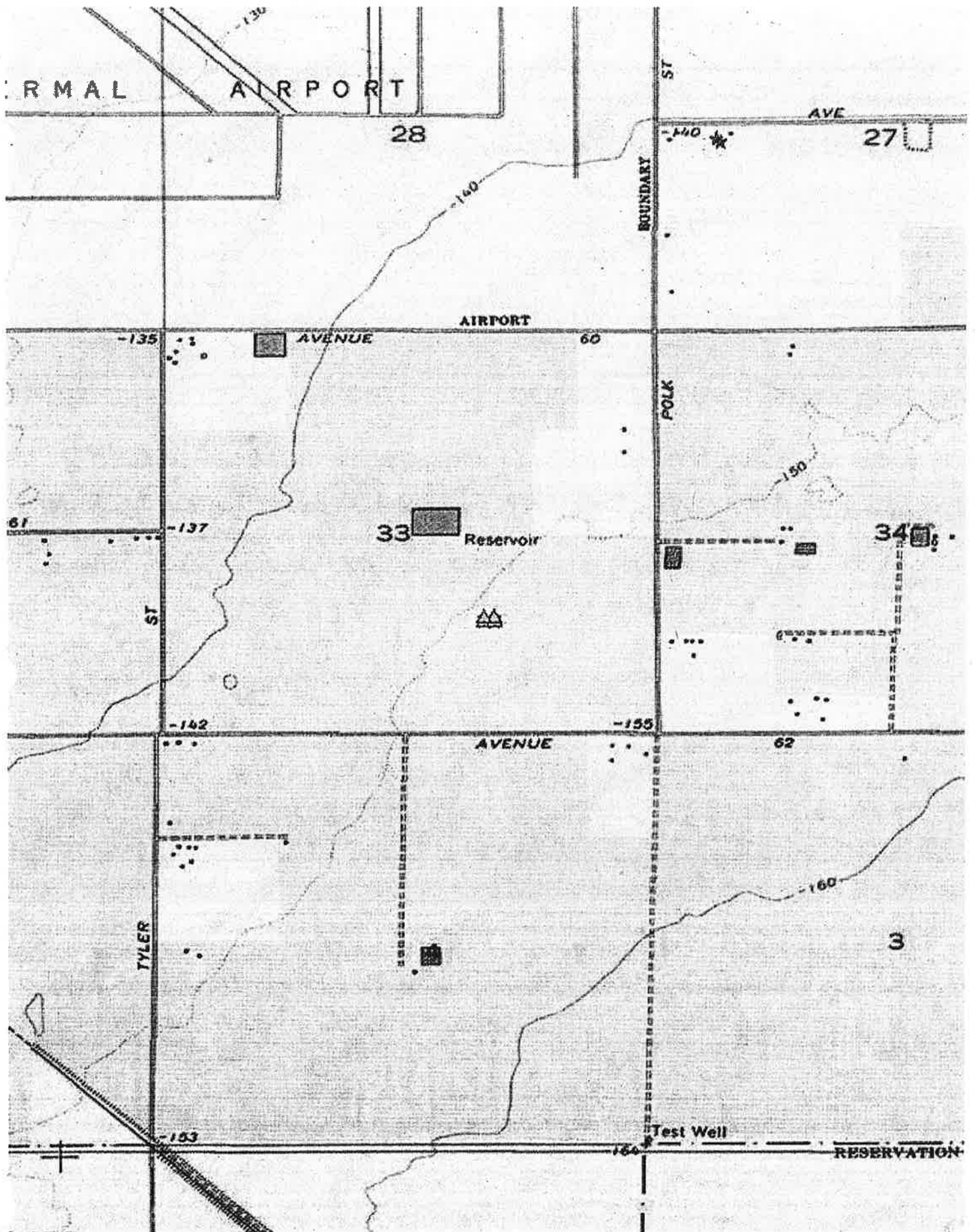
( DNE )

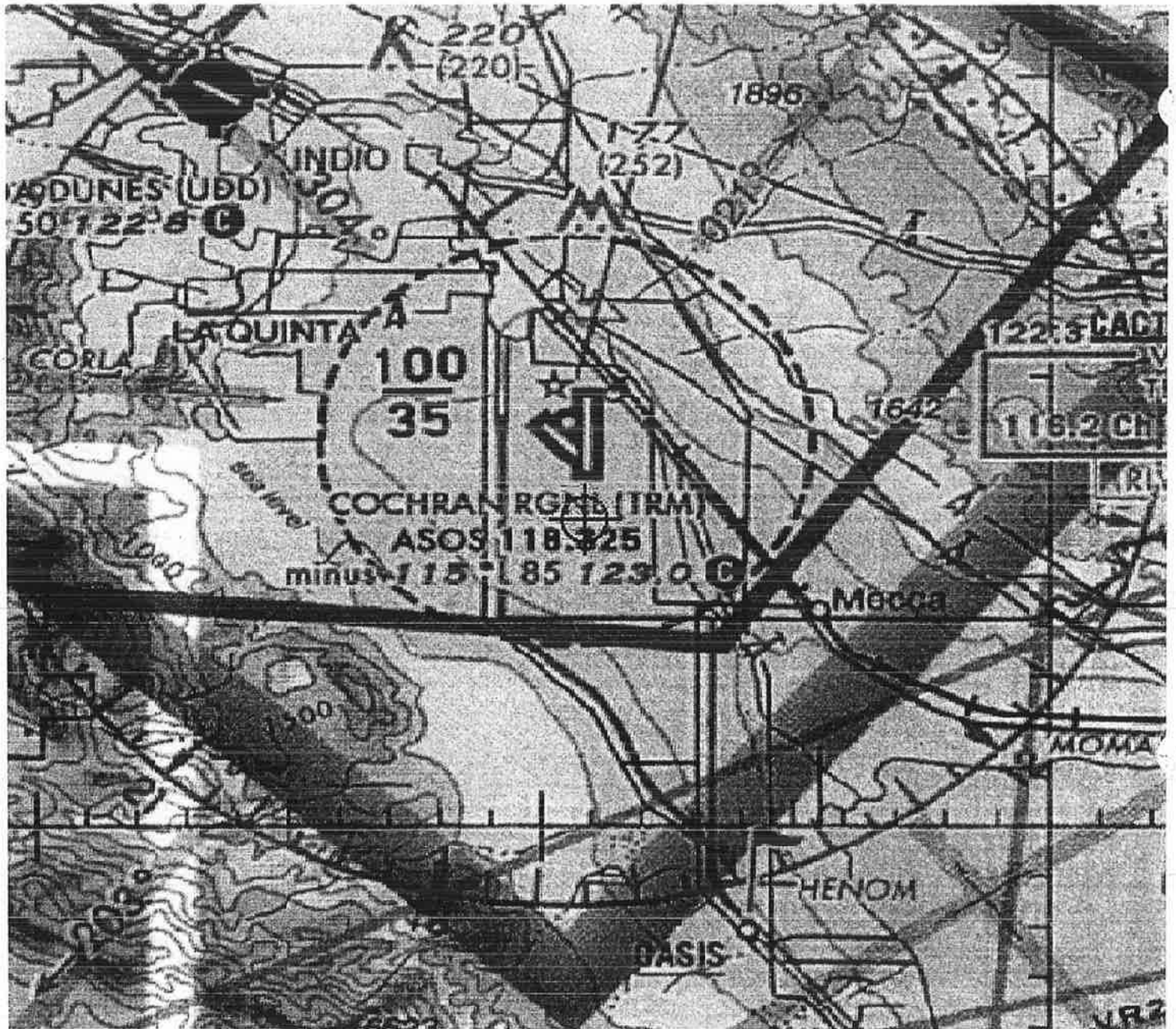
Karen McDonald  
Specialist

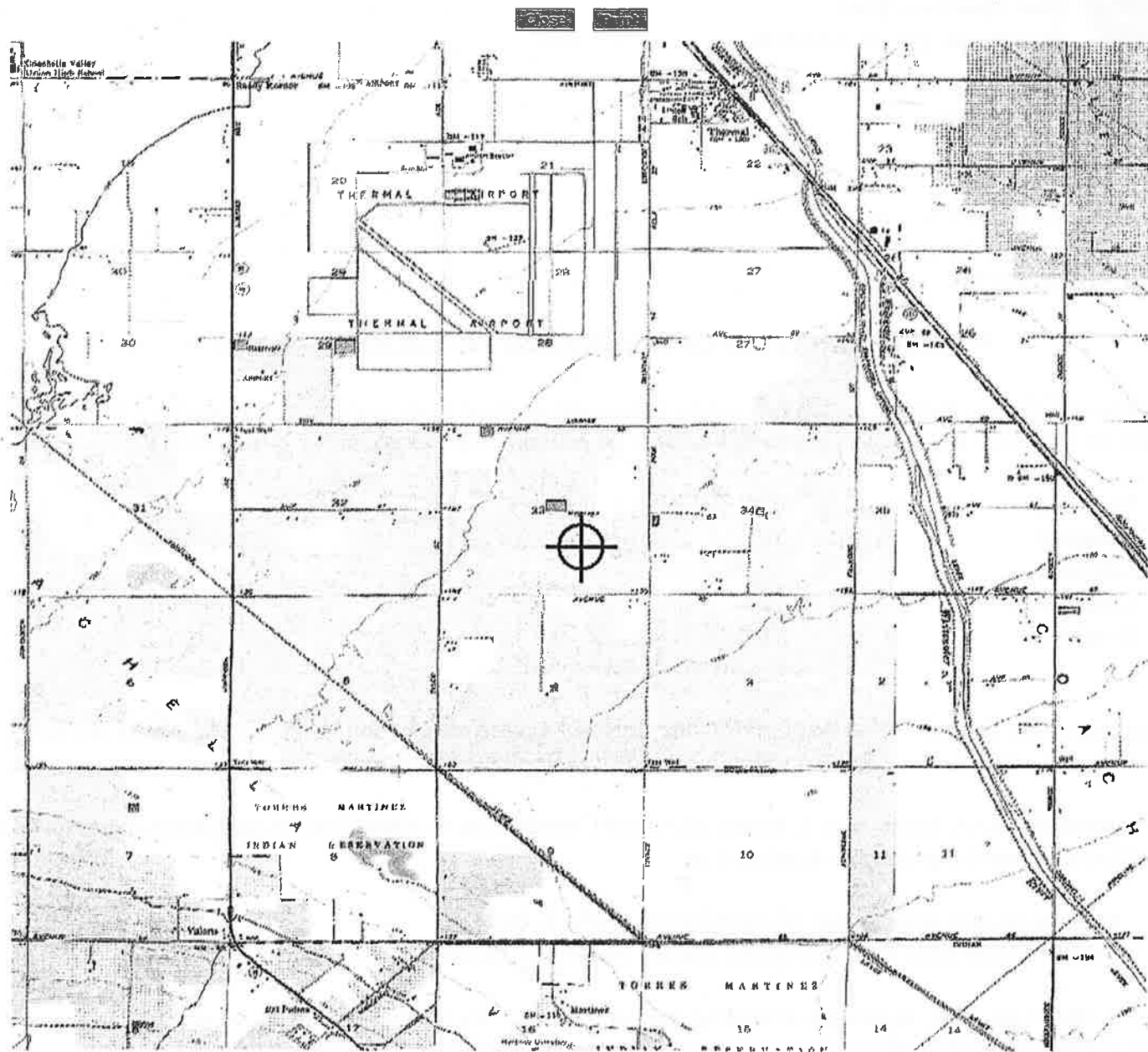
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Track Tower for new private motorsports park club track











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Structure: Building Track Tower Pt 3 of 4  
Location: Thermal, CA  
Latitude: 33-36-08.48N NAD 83  
Longitude: 116-09-07.62W  
Heights: 70 feet above ground level (AGL)  
-77 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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**Signature Control No: 131395248-133534319**

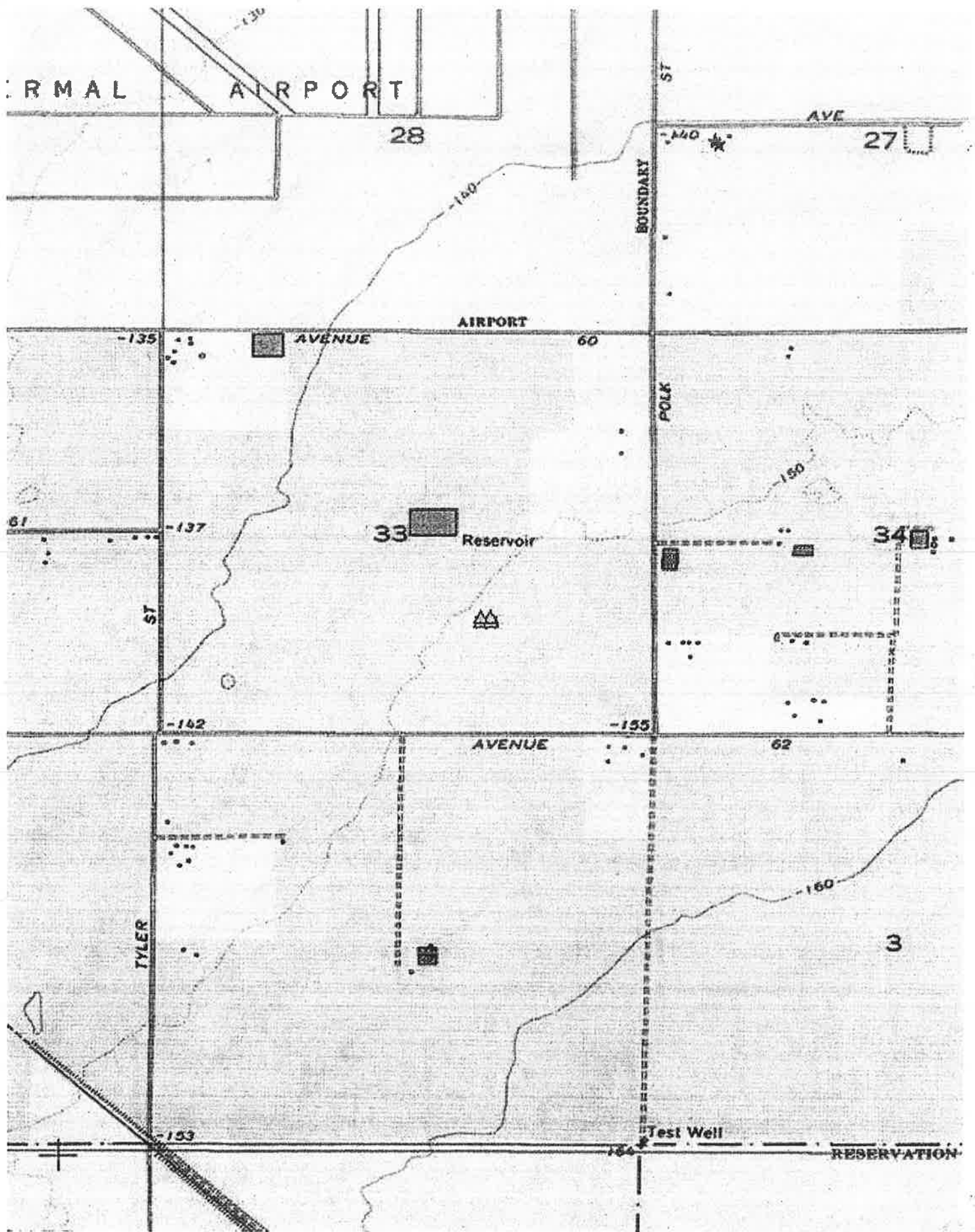
( DNE )

Karen McDonald  
Specialist

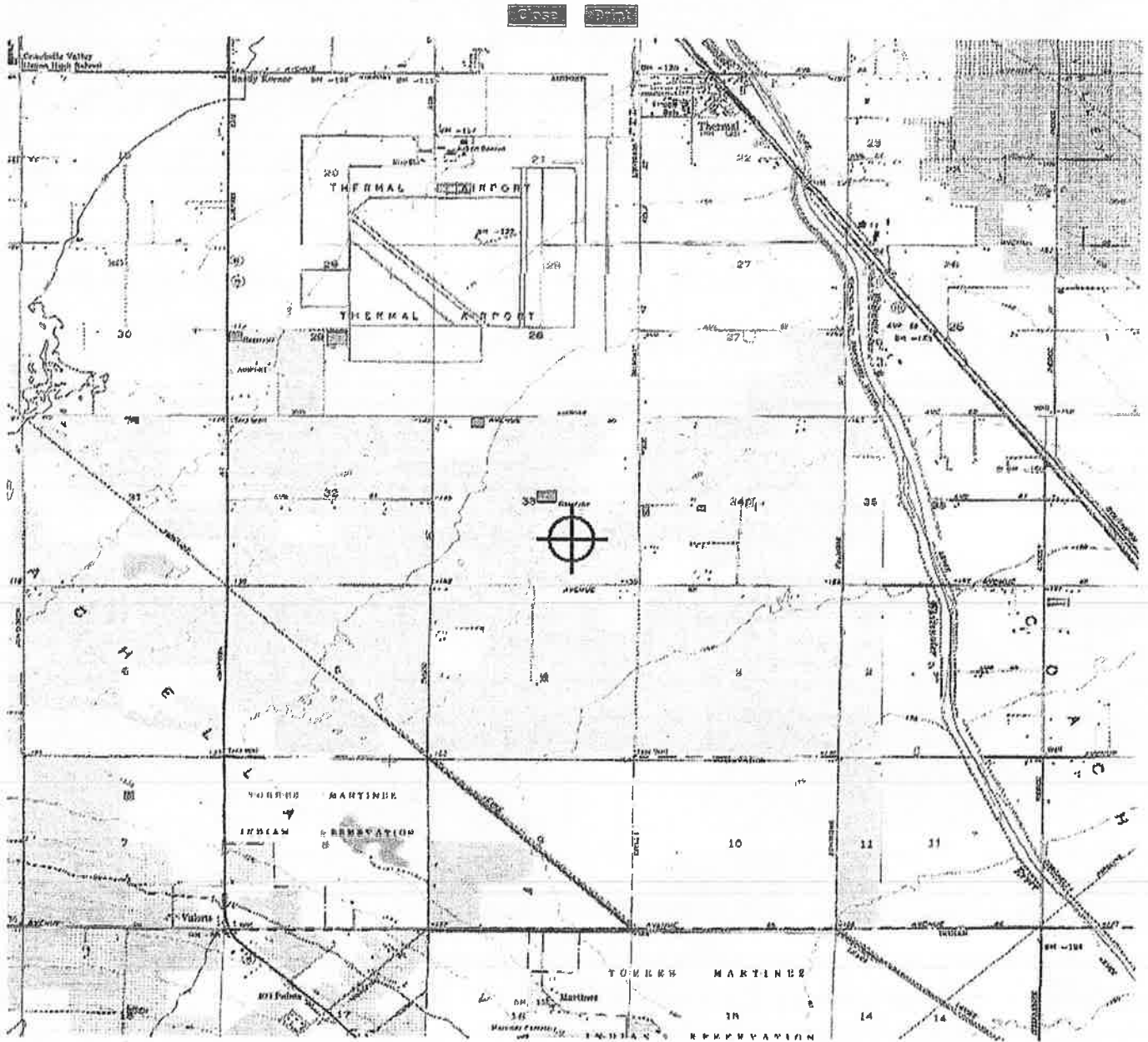
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Structure: Building Track Tower Pt 4 of 4  
 Location: Thermal, CA  
 Latitude: 33-36-08.45N NAD 83  
 Longitude: 116-09-08.85W  
 Heights: 70 feet above ground level (AGL)  
 -77 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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**Signature Control No: 131395250-133534318**

( DNE )

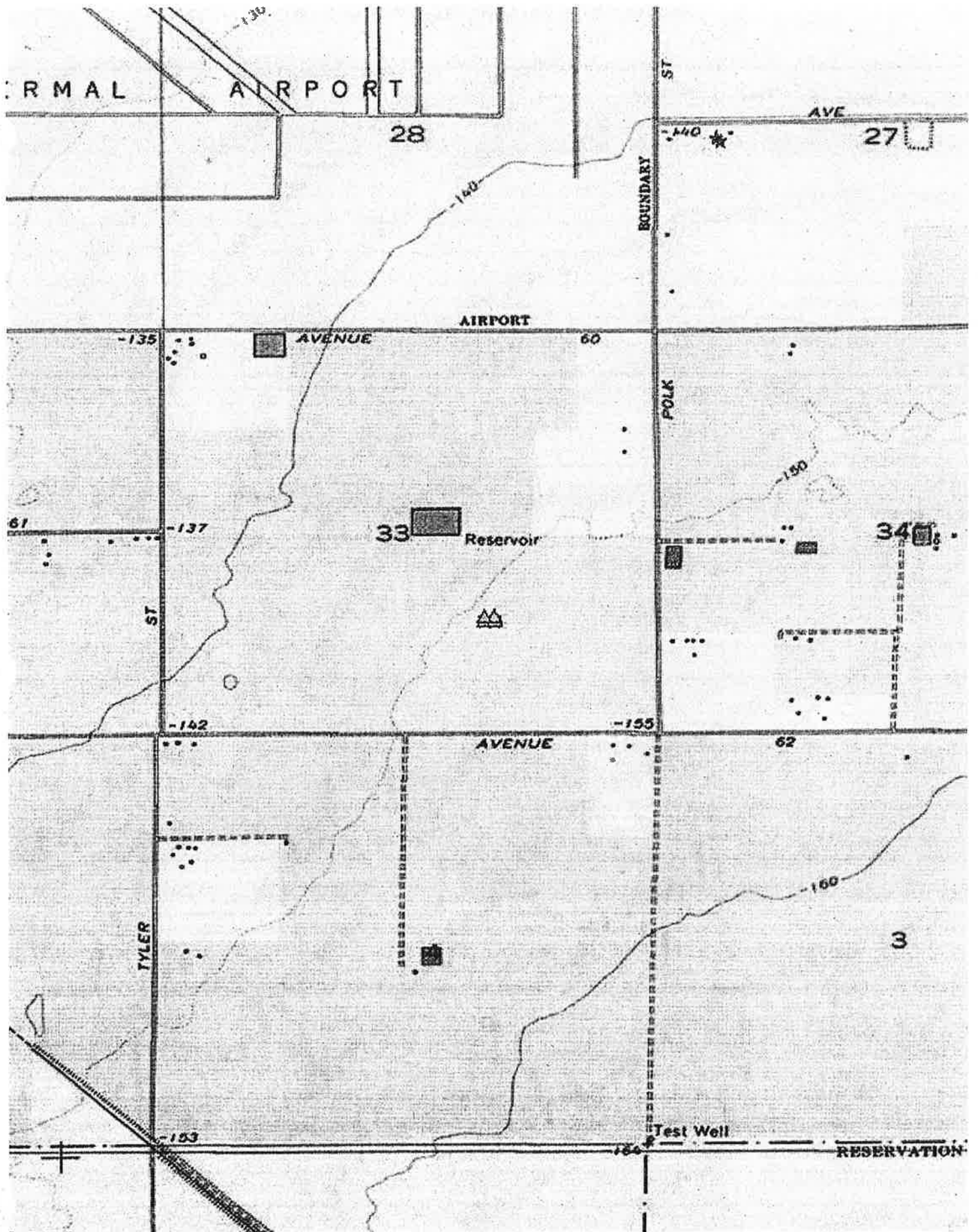
Karen McDonald  
Specialist

Attachment(s)  
Case Description  
Map(s)

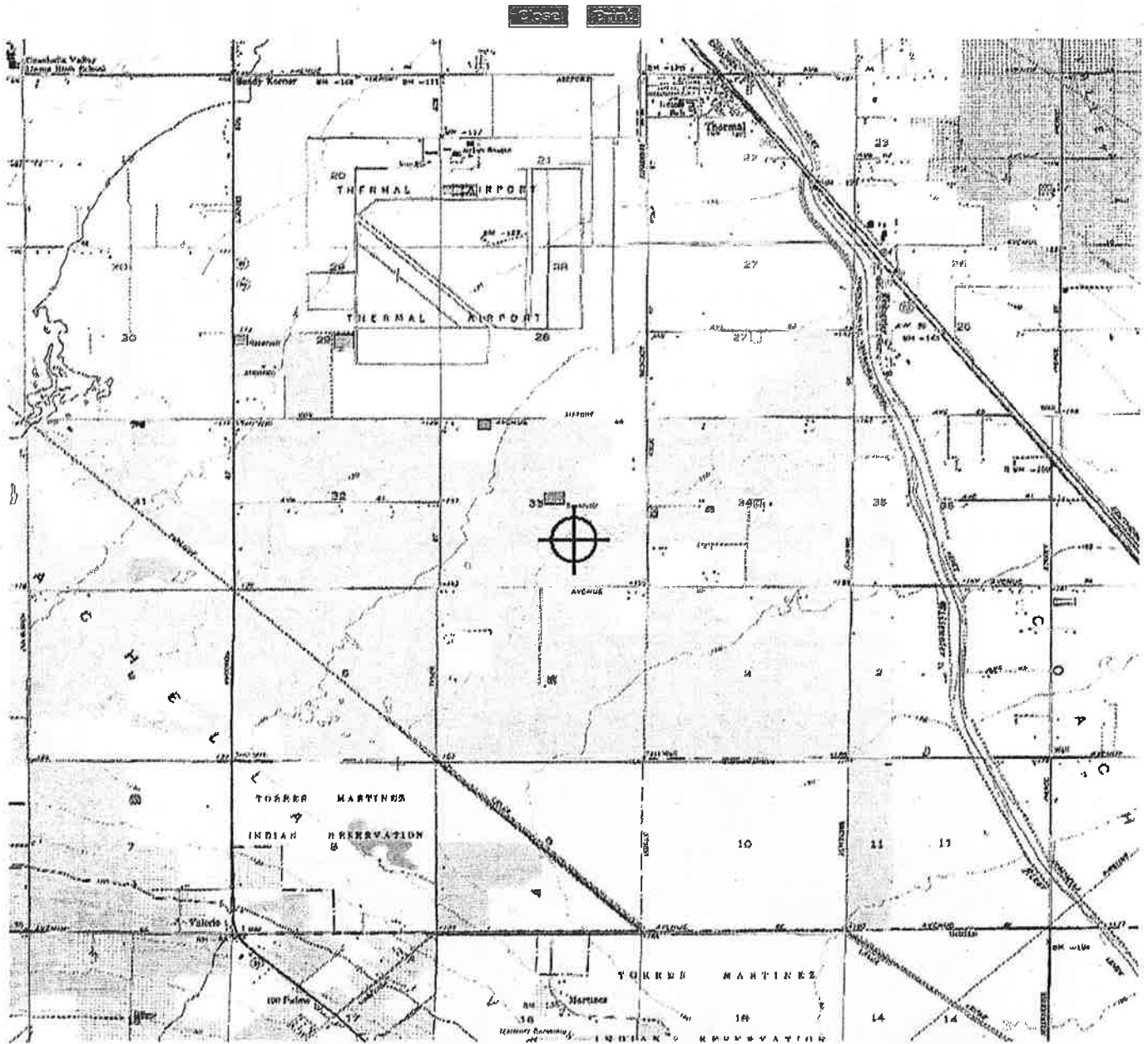


Track Tower for new private motorsports park club track









**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E

**Boundary Lines**

- Airport Property Line - Existing
- Airport Property Line - Planned
- City Limits

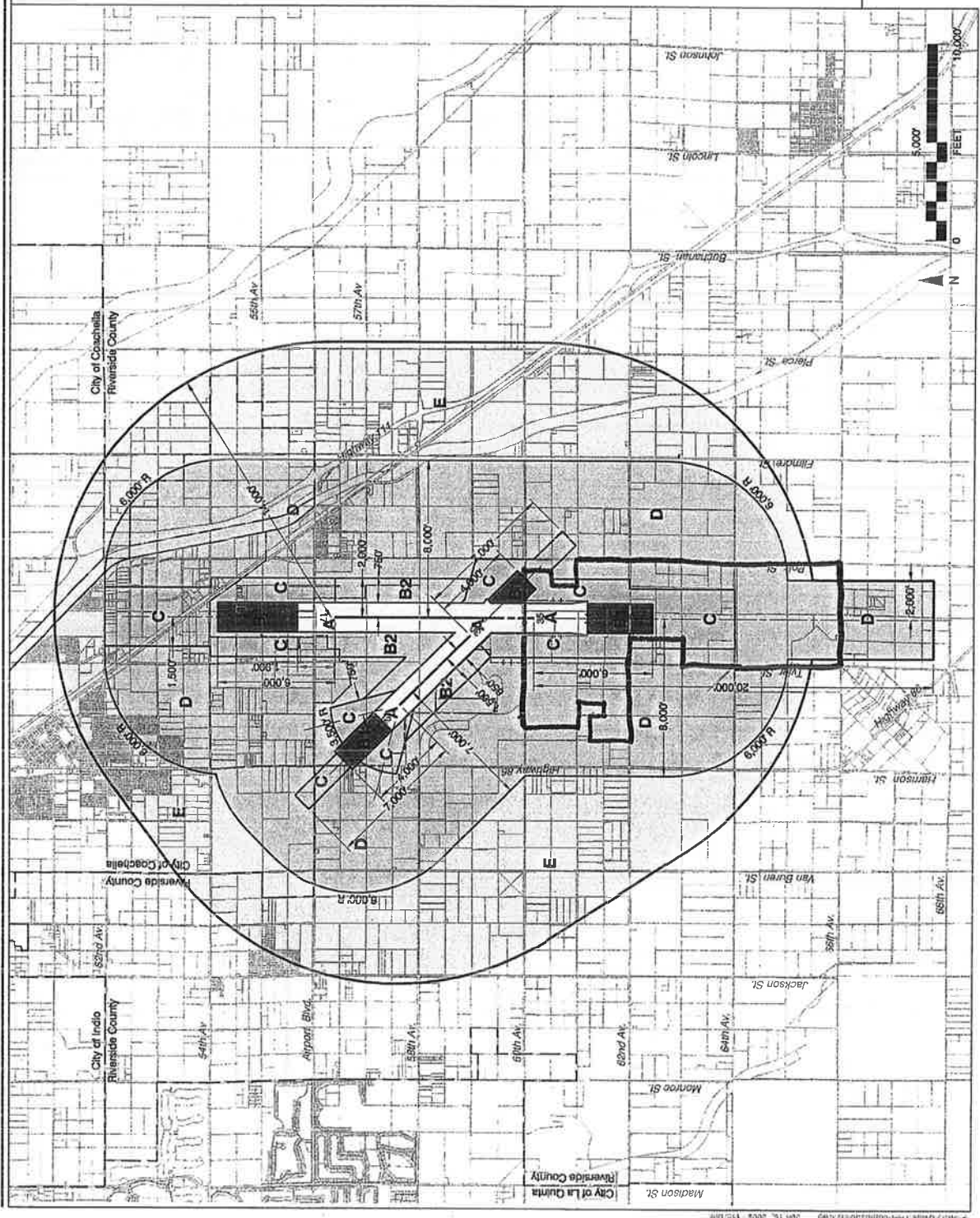
**Note**

Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAA Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

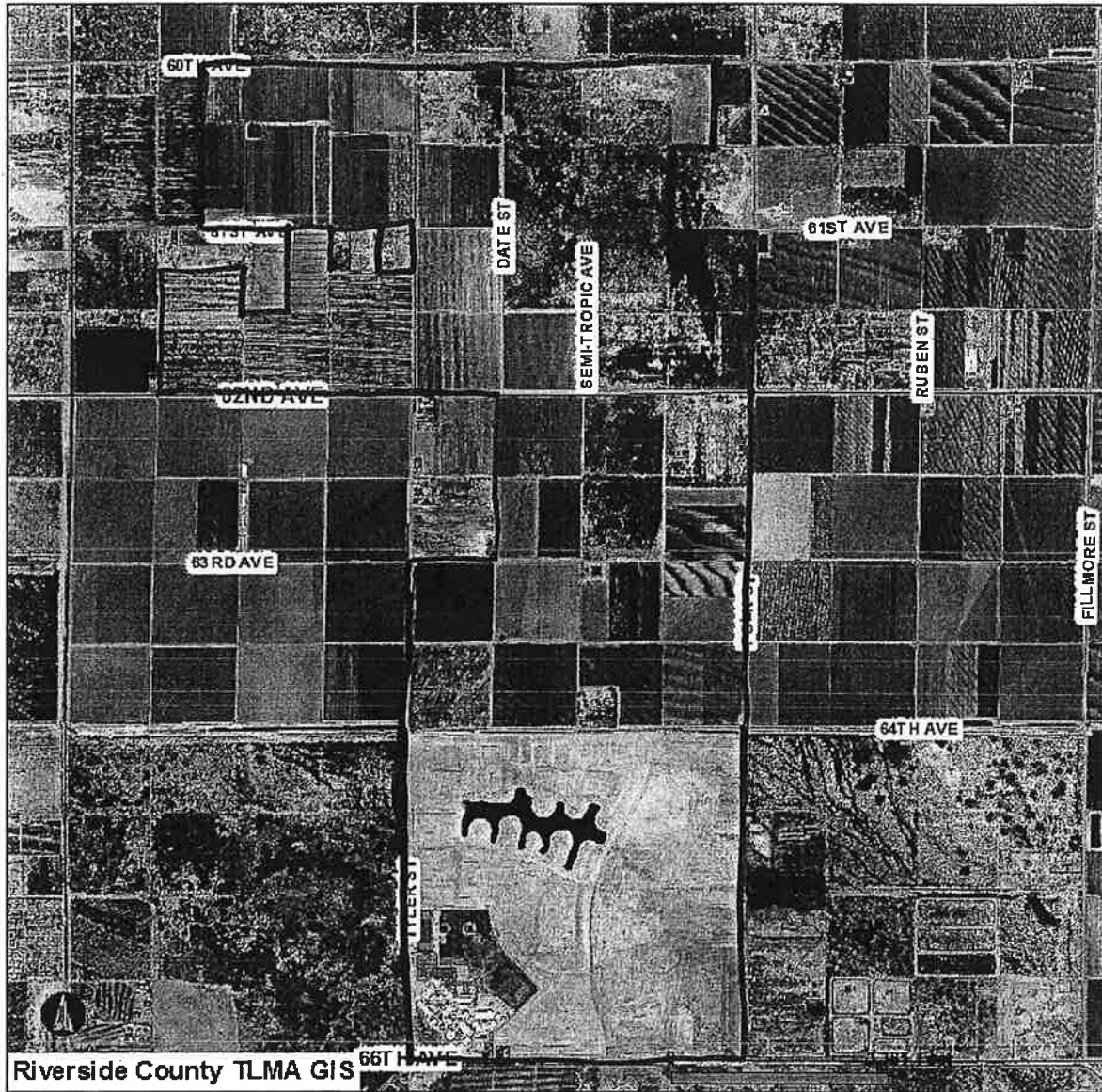
Riverside County  
 Airport Land Use Commission  
**Riverside County  
 Airport Land Use Compatibility Plan**  
 Policy Document  
 (Adopted June 2005)

Map JC-1  
**Compatibility Map**  
 Jacqueline Cochran Regional Airport



*Specific Plan*

RIVERSIDE COUNTY GIS



**Selected parcel(s):**  
 759-170-001 759-180-001 759-190-004

**LEGEND**

INTERSTATES

HIGHWAYS

STREETS

CITY

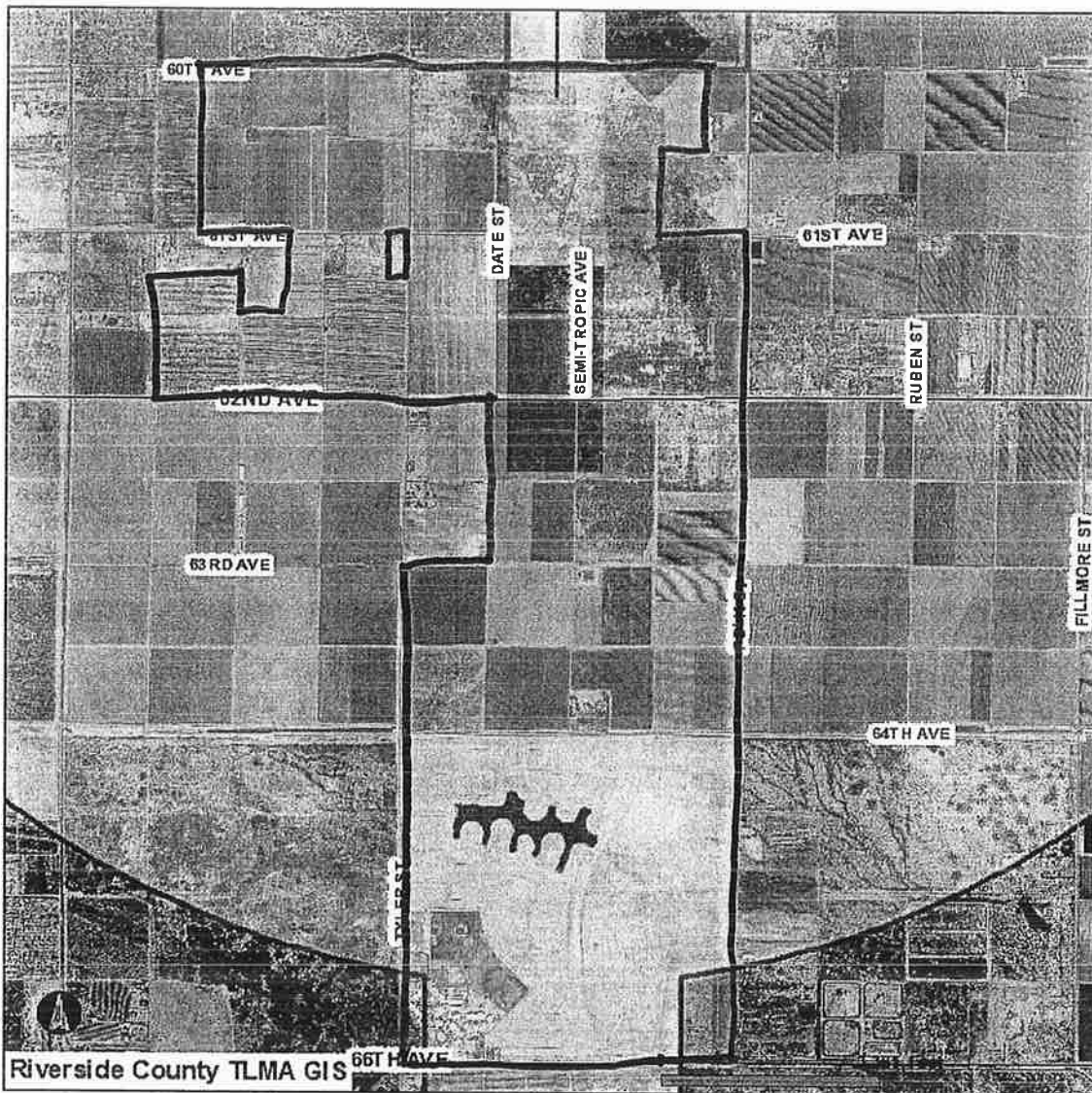
**\*IMPORTANT\***

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Version 100412











RIVERSIDE COUNTY GIS



Selected parcel(s):

759-170-001 759-180-001 759-190-004

AIRPORTS

-  INTERSTATES
-  HIGHWAYS
-  STREETS
-  AIRPORT RUNWAYS
-  AIRPORT INFLUENCE AREAS
-  AIRPORT BOUNDARIES
-  COMPATIBILITY ZONE A
-  COMPATIBILITY ZONE B1
-  COMPATIBILITY ZONE C
-  COMPATIBILITY ZONE D
-  COMPATIBILITY ZONE E

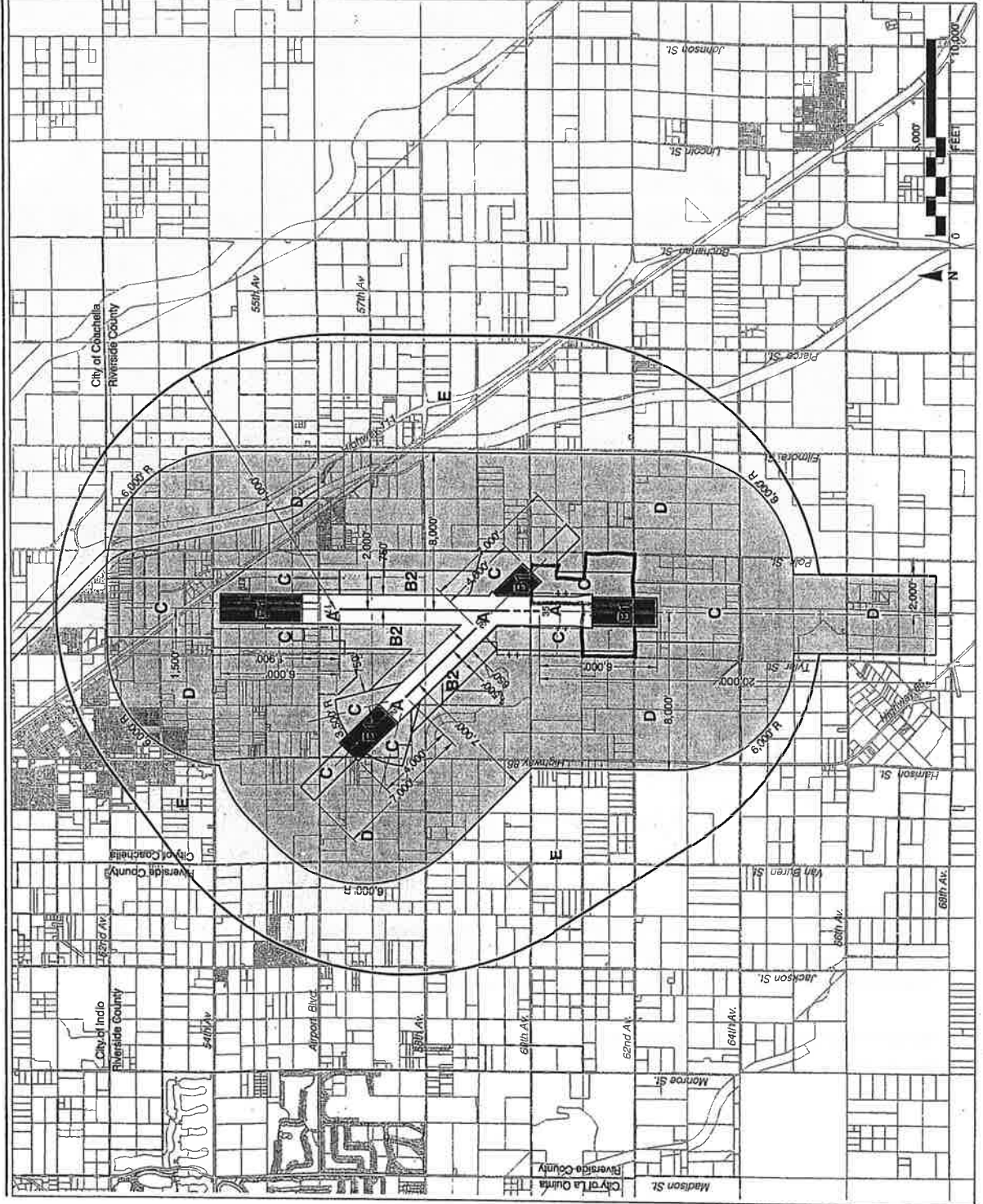
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**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

**Boundary Lines**

- Airport Property Line - Existing
- Airport Property Line - Planned
- City Limits

**Note**

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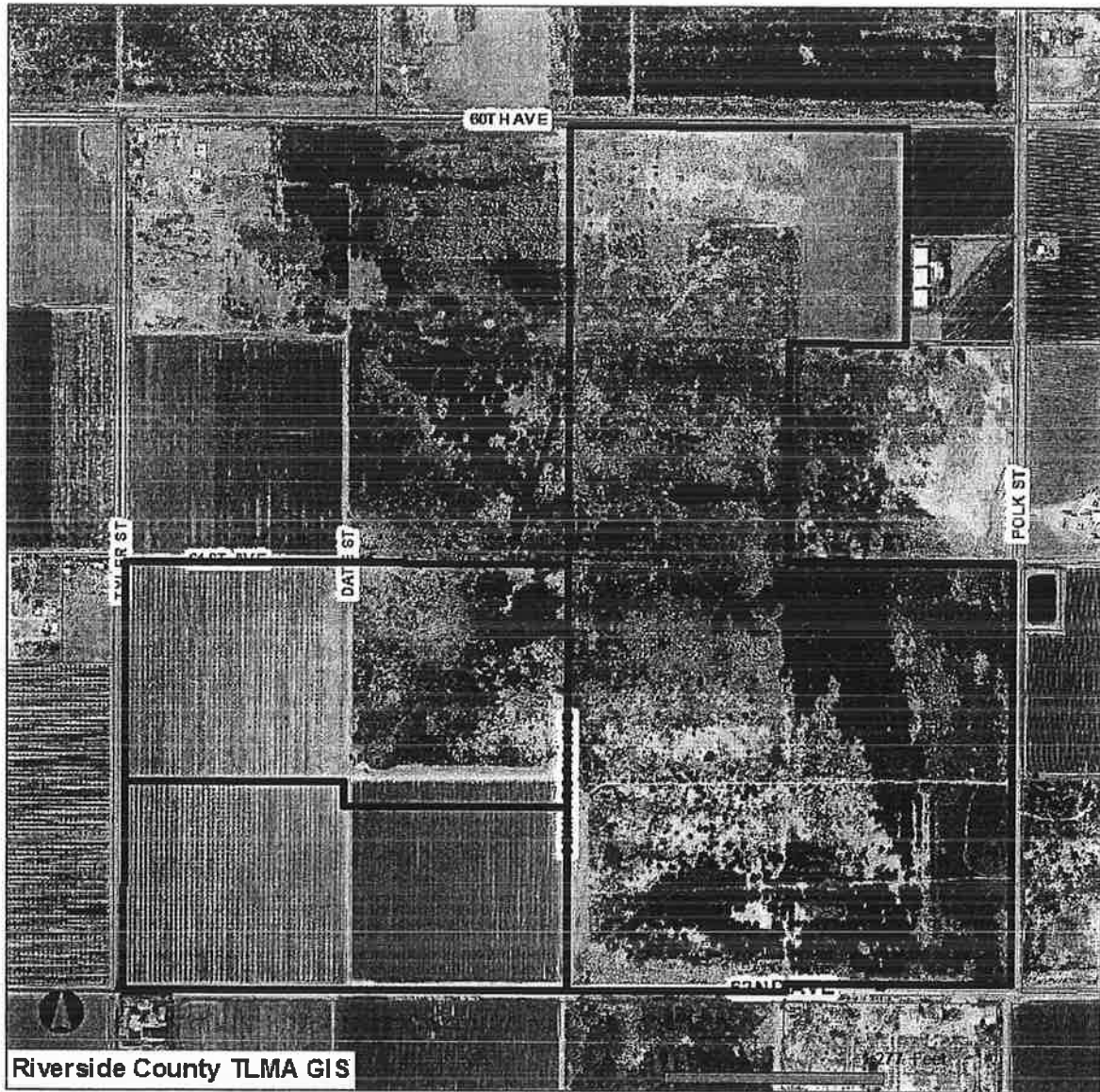
Riverside County  
 Airport Land Use Commission  
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 Policy Document  
 (Adopted June 2005)

Map JC-1

**Compatibility Map**  
 Jacqueline Cochran Regional Airport

Plot Plan

### RIVERSIDE COUNTY GIS



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#### LEGEND

- SELECTED PARCEL
- CITY
- INTERSTATES
- HIGHWAYS
- PARCELS

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RIVERSIDE COUNTY GIS



Riverside County TLMA GIS

Selected parcel(s):

759-170-001 759-180-001 759-190-004

AIRPORTS

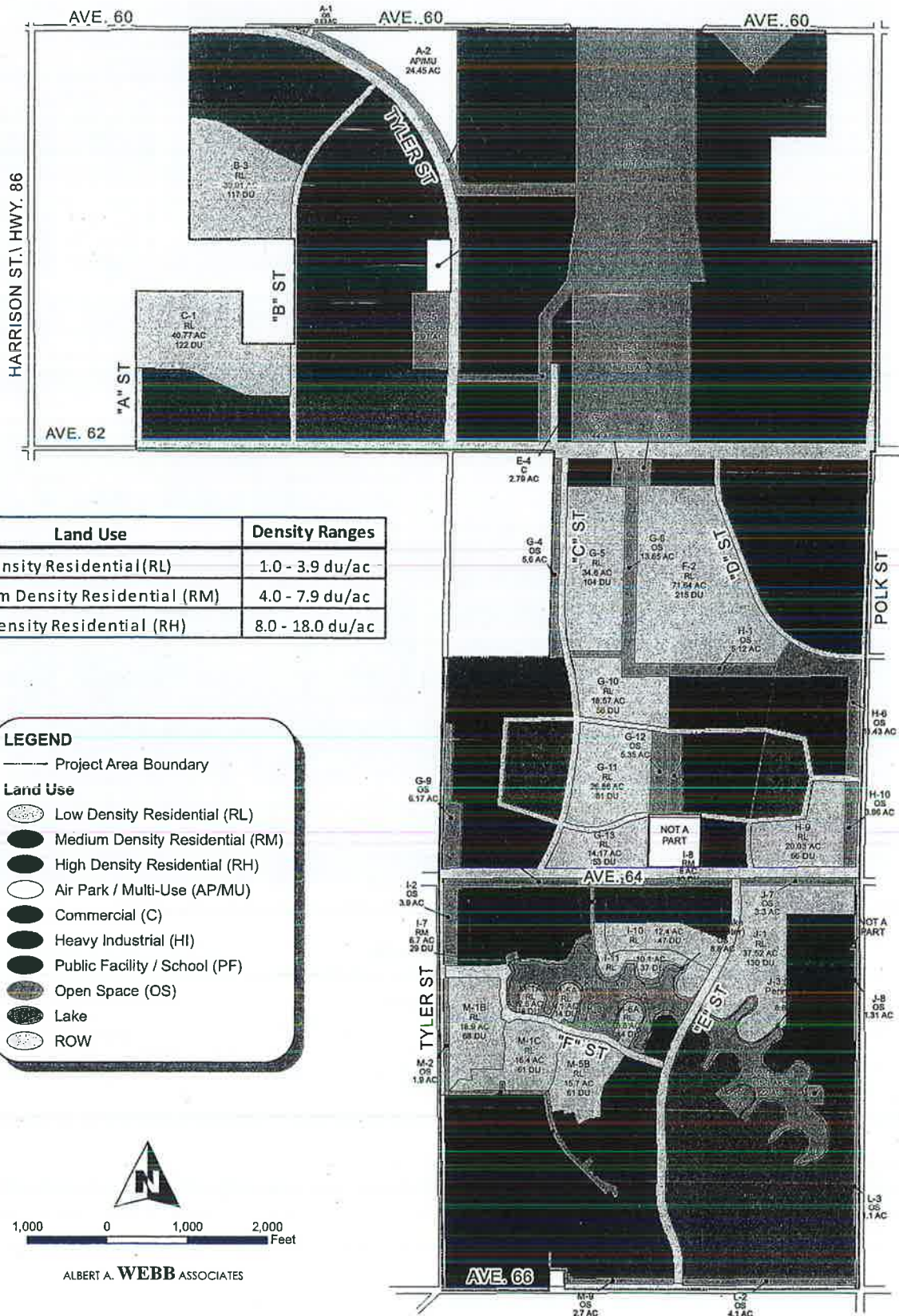
- SELECTED PARCEL
- INTERSTATES
- HIGHWAYS
- PARCELS
- AIRPORT RUNWAYS
- AIRPORT INFLUENCE AREAS
- AIRPORT BOUNDARIES
- COMPATIBILITY ZONE A
- COMPATIBILITY ZONE B1
- COMPATIBILITY ZONE C
- COMPATIBILITY ZONE D

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**THE KOHL RANCH**  
COACHELLA VALLEY, CALIFORNIA

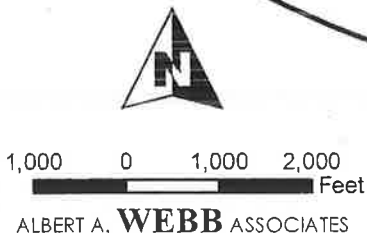
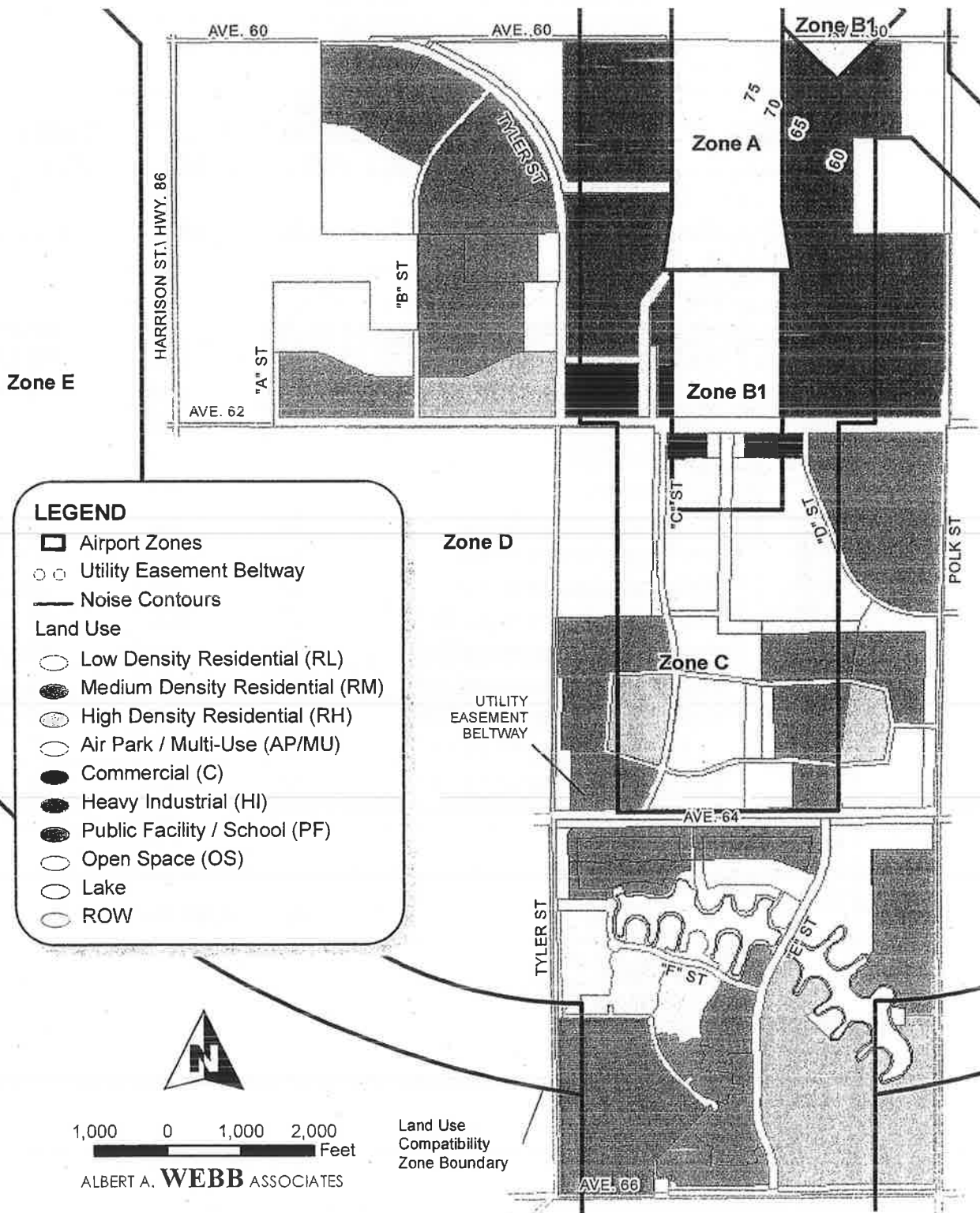
Figure II-5

<u>Planning Area</u>	<u>Land Use Code</u>	<u>Target Dwelling Units</u>	<u>Acres (Label)</u>	<u>Density Range</u>
A-1	Open Space (OS)	-	0.63	-
A-2	Air Park/Mixed Use (AP/MU)	-	24.45	-
A-3	Open Space (OS)	-	11.65	-
A-4	Heavy Industrial (HI)	-	65.39	-
A-5	Commercial (C)	-	17.61	-
A-6	Heavy Industrial (HI)	-	63.02	-
A-7	Open Space (OS)	-	9.53	-
A-8	Heavy Industrial (HI)	-	5.44	-
B-1	Medium Density Residential (RM)	210	30	4.0-7.9
B-2	Medium Density Residential (RM)	177	25.24	4.0-7.9
B-3	Low Density Residential (RL)	117	39.01	1.0-3.9
B-5	Medium Density Residential (RM)	229	32.70	4.0-7.9
B-6	Medium Density Residential (RM)	183	26.18	4.0-7.9
C-1	Low Density Residential (RL)	122	40.77	1.0-3.9
C-2	Medium Density Residential (RM)	218	31.16	4.0-7.9
C-4	Medium Density Residential (RM)	349	49.83	4.0-7.9
C-5	Open Space (OS)	-	9.97	-
C-6	High Density Residential (RH)	463	30.85	8.0-18.0
C-8	Medium Density Residential (RM)	105	14.96	4.0-7.9
D-1	Open Space (OS)	-	110.79	-
E-1	Open Space (OS)	-	5.82	-
E-2	Heavy Industrial (HI)	-	197.17	-
E-3	Open Space (OS)	-	69.33	-
E-4	Commercial (C)	-	2.79	-
F-4	Commercial (C)	-	6.23	-
F-2	Low Density Residential (RL)	215	71.64	1.0-3.9
F-3	Medium Density Residential (RM)	635	79.94	4.0-7.9
G-1	Commercial (C)	-	4.43	-
G-2	Open Space (OS)	-	1.44	-
G-3	Open Space (OS)	-	1.19	-
G-4	Open Space (OS)	-	5.6	-
G-5	Low Density Residential (RL)	104	34.6	1.0-3.9
G-6	Open Space (OS)	-	13.65	-
G-7	Medium Density Residential (RM)	432	60.38	4.0-7.9
G-8	High Density Residential	282	20.6	8.0-18.0

**Table II-1a  
LAND USE SUMMARY**

<u>Planning Area</u>	<u>Land Use Code</u>	<u>Target Dwelling Units</u>	<u>Acres (Label)</u>	<u>Density Range</u>
G-9	Open Space (OS)	-	6.17	-
G-10	Low Density Residential (RL)	56	18.57	1.0-3.9
G-11	Low Density Residential (RL)	81	26.86	1.0-3.9
G-12	Open Space (OS)	-	5.35	-
G-13	Low Density Residential (RL)	53	14.17	1.0-3.9
H-1	Open Space (OS)	-	5.12	-
H-2	Medium Density Residential (RM)	134	18.65	4.0-7.9
H-3	Open Space (OS)	-	3.79	-
H-4	Medium Density Residential (RM)	161	22.4	4.0-7.9
H-5	Medium Density Residential (RM)	45	7.83	4.0-7.9
H-6	Open Space (OS)	-	11.43	-
H-7	Medium Density Residential (RM)	151	20.99	4.0-7.9
H-8	High Density Residential	203	13.55	8.0-18.0
H-9	Low Density Residential (RL)	66	20.03	1.0-3.9
H-10	Open Space (OS)	-	3.96	-
I-1	Open Space (OS)	-	9.6	-
I-2	Open Space (OS)	-	3.9	-
I-3	Open Space (OS)	-	0.5	-
I-4	Medium Density Residential (RM)	35	7	4.0-7.9
I-5	Medium Density Residential (RM)	51	10.6	4.0-7.9
I-6	Medium Density Residential (RM)	42	9.6	4.0-7.9
I-7	Medium Density Residential (RM)	29	6.7	4.0-7.9
I-8	Medium Density Residential (RM)	40	8	4.0-7.9
I-9	Medium Density Residential (RM)	57	11.5	4.0-7.9
I-10	Low Density Residential (RL)	47	12.4	1.0-3.9
I-11	Low Density Residential (RL)	37	10.1	1.0-3.9
J-1	Low Density Residential (RL)	130	37.52	1.0-3.9
J-2	Open Space - Lake (OS)	-	22	-
J-3	Open Space - Lake (OS)	-	8.8	-
J-4	Medium Density Residential (RM)	189	43.36	4.0-7.9
J-5	Open Space (OS)	-	1	-
J-6	Open Space (OS)	-	2.2	-
J-7	Open Space (OS)	-	3.3	-
J-8	Open Space (OS)	-	1.3	-
K-1	Open Space - Lake (OS)	-	22.4	-
K-2	Open Space - Lake (OS)	-	8.8	-

<b>Table II-1a</b>				
<b>LAND USE SUMMARY</b>				
<u>Planning Area</u>	<u>Land Use Code</u>	<u>Target Dwelling Units</u>	<u>Acres (Label)</u>	<u>Density Range</u>
<u>K-3</u>	<u>Open Space (OS)</u>	-	<u>3.2</u>	-
<u>L-1</u>	<u>High Density Residential</u>	<u>1158</u>	<u>106.2</u>	<u>8.0-18.0</u>
<u>L-2</u>	<u>Open Space (OS)</u>	-	<u>4.1</u>	-
<u>L-3</u>	<u>Open Space (OS)</u>	-	<u>1.1</u>	-
<u>M-1A</u>	<u>Low Density Residential (RL)</u>	<u>8</u>	<u>2.5</u>	<u>1.0-3.9</u>
<u>M-1B</u>	<u>Low Density Residential (RL)</u>	<u>68</u>	<u>18.9</u>	<u>1.0-3.9</u>
<u>M-1C</u>	<u>Low Density Residential (RL)</u>	<u>61</u>	<u>16.4</u>	<u>1.0-3.9</u>
<u>M-2</u>	<u>Open Space (OS)</u>	-	<u>1.9</u>	-
<u>M-3</u>	<u>Open Space (OS)</u>	-	<u>1.6</u>	-
<u>M-4</u>	<u>Public Facility (PF)</u>	-	<u>84.3</u>	-
<u>M-5A</u>	<u>Low Density Residential (RL)</u>	<u>14</u>	<u>4.1</u>	<u>1.0-3.9</u>
<u>M-5B</u>	<u>Low Density Residential (RL)</u>	<u>61</u>	<u>15.7</u>	<u>1.0-3.9</u>
<u>M-6A</u>	<u>Low Density Residential (RL)</u>	<u>14</u>	<u>3.8</u>	<u>1.0-3.9</u>
<u>M-6B</u>	<u>Medium Density Residential (RM)</u>	<u>57</u>	<u>13.4</u>	<u>4.0-7.9</u>
<u>M-7A</u>	<u>Medium Density Residential (RM)</u>	<u>20</u>	<u>4.1</u>	<u>4.0-7.9</u>
<u>M-7B</u>	<u>Medium Density Residential (RM)</u>	<u>56</u>	<u>11</u>	<u>4.0-7.9</u>
<u>M-7C</u>	<u>Medium Density Residential (RM)</u>	<u>32</u>	<u>5.9</u>	<u>4.0-7.9</u>
<u>M-7D</u>	<u>Medium Density Residential (RM)</u>	<u>82</u>	<u>15.1</u>	<u>4.0-7.9</u>
<u>M-7E</u>	<u>Medium Density Residential (RM)</u>	<u>82</u>	<u>15.5</u>	<u>4.0-7.9</u>
<u>M-8</u>	<u>Open Space (OS)</u>	-	<u>2.9</u>	-
<u>M-9</u>	<u>Open Space (OS)</u>	-	<u>2.7</u>	-
-	<u>ROW</u>	-	<u>174.80</u>	-
<b><u>TOTAL</u></b>		<b><u>7,161</u></b>	<b><u>2,162.65</u></b>	



# THE KOHL RANCH

COACHELLA VALLEY, CALIFORNIA

Source: Jacqueline Cochran Regional Airport Compatibility Land Use Plan, 2004

Figure IV-20A



Current Roadway Name	Current	Proposed	Policy Document(s) Affected by Change
	Circulation Element Classification	Roadway Designations	
"A" Street (Between Avenue 60 and Polk Street)	None	Arterial highway (110' ROW) None	Specific Plan 303 Amendment 2
Tyler Street (Between Avenue 60 and Avenue 62)	None	Arterial highway (128' ROW)	Specific Plan 303 Amendment 2; General Plan Circulation Element
New "A" Street	None	Collector (74' ROW)	Specific Plan 303 Amendment 2
Avenue 62 (East of between Polk Street and SR-86S)	Secondary highway (88' ROW)	Arterial highway (110' ROW) Expressway (220')	Specific Plan 303 Amendment 2; General Plan Circulation Element
"B" Street	None Secondary highway (88' ROW)	Secondary highway (88' ROW) Collector street (74' ROW)	Specific Plan 303 Amendment 2
"C" Street	Secondary highway (88' ROW) None	Secondary highway (88' - 100' ROW)	Specific Plan 303 Amendment 2
"E" Street 64	Secondary highway (88' ROW) None	Secondary highway (88' - 100' ROW)	Specific Plan 303 Amendment 2
60th Avenue	Arterial highway (110' ROW)	Industrial Collector (78' ROW) (Delete as an Arterial Highway Classification)	
Polk Street	Major highway (100' ROW)	Arterial highway (128' ROW)	Specific Plan 303 Amendment 2
Tyler Street (north of Avenue 64)	Collector street (66' ROW)	Secondary highway (100' ROW)	Specific Plan 303 Amendment 2; General Plan Circulation Element
Tyler Street (south of Avenue 64)	Collector street (66' ROW)	Modified Secondary highway (88' ROW)	Specific Plan 303 Amendment 2; General Plan Circulation Element
Avenue 64	None	Major highway (118' ROW)	Specific Plan 303 Amendment 2
Avenue 66	Secondary highway (88' ROW)	Urban Arterial highway (152' ROW)	Specific Plan 303 Amendment 2

**Note:** There is no proposed roadway designation change for "D" Street. "F" Street is a newly named Collector street in SP 303 Amendment 2.

#### b. Circulation Development Standards

- 1) Any application for any subdivision within the specific plan boundary (excluding a Schedule I Parcel Map) shall cause the design and construction of the specific plan master planned infrastructure within the final map boundaries, with the exception of a division of land that has no parcel less than 40 acres or that is not less than a quarter of a

TENTATIVE PARCEL MAP NO. 36293 - SCHEDULE "E"

LEGAL DESCRIPTION:

PARCEL MAP NO. 36293 IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, BEING A SUBDIVISION OF PARCEL 1, OF PARCEL MAP NO. 33178, AS PER MAP RECORDS IN BOOK \_\_\_\_\_ PAGE(s) \_\_\_\_\_ OF PARCEL MAPS, RECORDS OF SAID COUNTY. APN 750-10-01, 750-10-01, 750-10-01 AND 750-10-01-01

SYMBOL PER MAP REPORT	DESCRIPTION
[Symbol]	ALL EASEMENTS FOR WATER PRESERVE
[Symbol]	BOOK NO. 1158 PAGE 49 OF OFFICIAL RECORDS.
[Symbol]	ALL EASEMENTS FOR PUBLIC USE
[Symbol]	RECORDED FEBRUARY 19, 1955
[Symbol]	ALL EASEMENTS FOR PUBLIC USE
[Symbol]	RECORDED MARCH 4, 1957
[Symbol]	ALL EASEMENTS FOR PUBLIC USE
[Symbol]	RECORDED JUNE 14, 1957
[Symbol]	ALL EASEMENTS FOR PUBLIC USE
[Symbol]	RECORDED SEPTEMBER 19, 1955
[Symbol]	INSTITUTION NO. 0070 OF OFFICIAL RECORDS.
[Symbol]	RECORDED SEPTEMBER 19, 1955
[Symbol]	ALL EASEMENTS FOR UNDERGROUND PIPELINE
[Symbol]	RECORDED FEBRUARY 19, 1955
[Symbol]	ALL EASEMENTS FOR UNDERGROUND PIPELINE
[Symbol]	RECORDED FEBRUARY 19, 1955
[Symbol]	ALL EASEMENTS FOR UNDERGROUND PIPELINE
[Symbol]	RECORDED FEBRUARY 19, 1955
[Symbol]	ALL EASEMENTS FOR UNDERGROUND PIPELINE
[Symbol]	RECORDED FEBRUARY 19, 1955



VICINITY MAP

APPLICANT	PRELIMINARY REPORT NO.	DATE OF PREPARATION
PHIL CLAYTON, INC.	750-10-01-01	AUGUST 2010
3525 WASHINGTON STREET, #234 LA QUIETA, CA 92531 (910) 488-9774	750-10-01-01	FEBRUARY 2011
ROSE PARK II, LLC	750-10-01-01	COORDINATOR
1220 BROADWAY DRIVE, SUITE 303 LA QUIETA, CA 92531 (910) 656-9800	750-10-01-01	CONTRACT NO.
LAWS DEVELOPMENT CORPORATION	750-10-01-01	TOTAL AREA
1220 BROADWAY DRIVE, SUITE 303 LA QUIETA, CA 92531 (910) 656-9800	750-10-01-01	354
		18.88 AC
		1.70 AC
		2.70 AC
		2.70 AC

LEGEND:

- PROJECT BOUNDARY
- ESTIMATE RIGHT-OF-WAY
- ULTIMATE RIGHT-OF-WAY
- EXISTING CENTERLINE
- PROPOSED LOT LINES
- EXISTING AVENUE & DRAIN LINE
- EXISTING SANITARY SEWER
- EXISTING EASEMENT
- PROPOSED SPEEDWAY BARRIERS
- PROPOSED STRUCTURE FINISHING
- STRUCTURES
- 70" WIDE BY 15" HIGH BERM (2:1 SIDE SLOPES, 10' FLAT TOP)
- PROPOSED 40' PAVEMENT / TRUCK
- PROPOSED DRIVEWAY WAY

BUILDING LEGEND:

- ADMINISTRATION / REGISTRATION
- TEAM GARAGE
- WOMEN'S STORAGE GARAGE
- CONTROL TOWER
- CORPORATE EVENT BLDG
- TRACK SIDE GARAGE / LUXURY SUITE
- TUNING SHOP
- MAINTENANCE BUILDING
- FUEL ISLAND
- MEMBER'S PRIVATE GARAGE
- CART ADMINISTRATION / RESTORATION

NOTES:

- EASEMENTS OF RECORDS ARE AS SHOWN.
- MAIN ACCESS SHALL BE ON 62ND AVENUE WITH MEMBER'S ACCESS ON POLK AVENUE.
- ALL SLOPES ARE 2:1 UNLESS OTHERWISE NOTED.
- PROVISIONAL PAVEMENT ESTIMATIONS ARE BASED ON THE PLAN.
- LAND IS IN HIGH LIQUEFACTION POTENTIAL AREA AND IS NOT WITHIN A FAULT FLOOD ZONE 0 AND X AREA AS SHOWN ON FEMA PANEL NO. 80665C03256.
- PROPOSED ON-SITE OPEN CHANNELS AS INDICATED ON MAP.
- PROJECT IS NOT ZONED AS RESTRICTED SINGLE FAMILY RES.
- THE PROJECT WILL COMPLY WITH ALL APPLICABLE REGULATIONS.

DATE OF MAP: AUGUST 2010

COUNTY OF RIVERSIDE

TENTATIVE PARCEL MAP NO. 36293 SCHEDULE "E"

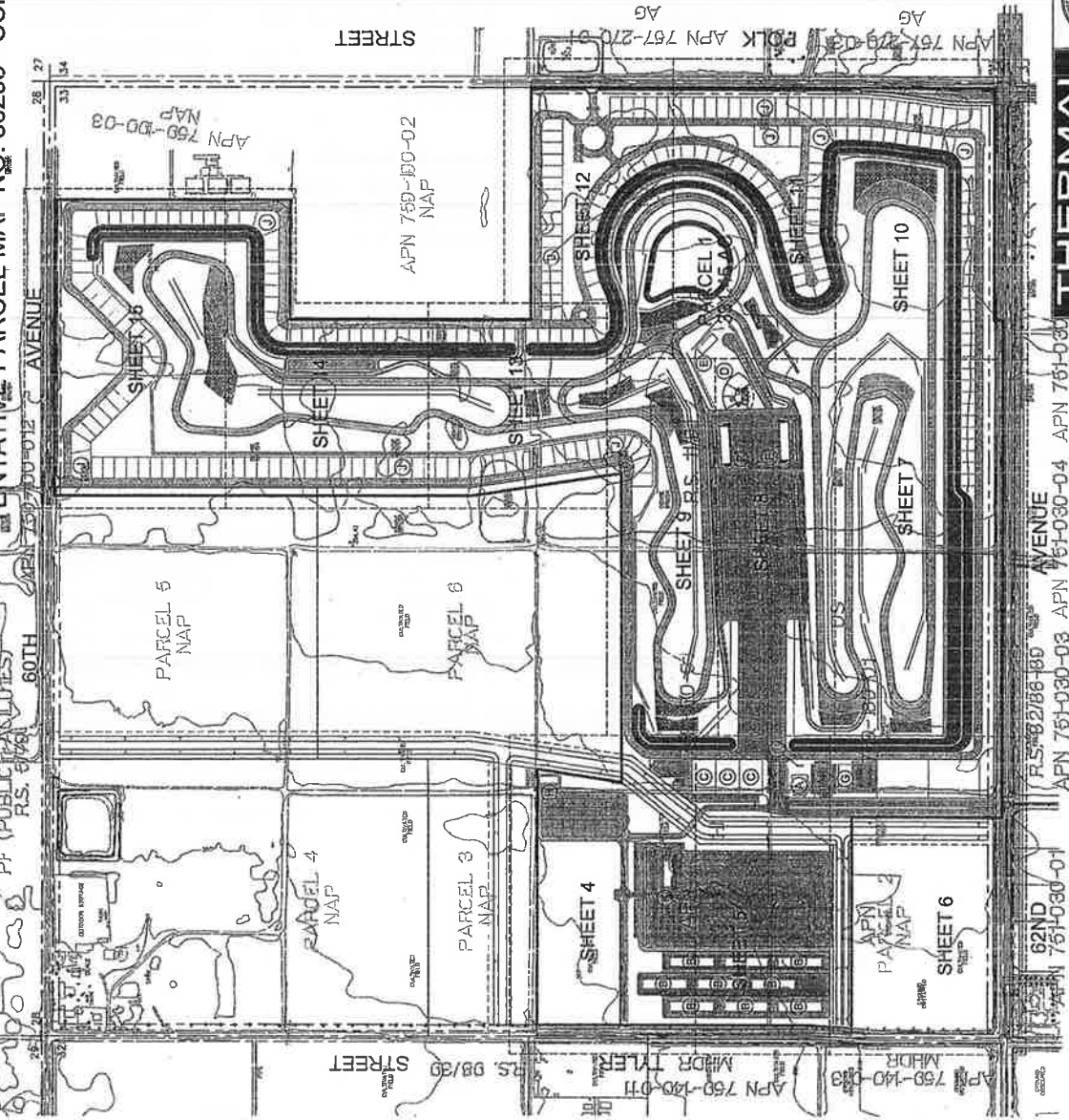
LAND PLANNERS

DEVELOPMENTS CONSULTANTS



THERMAL MOTORSPORTS TRACK & CLUB

MR. PHIL CLAYTON  
TRM 122, LLC #C234  
50-155 W. FIDUCIARY DRIVE  
LA QUIETA, CA 92551  
(910) 488-9774



62ND AVENUE  
APN 750-10-01-01  
APN 750-10-03-03  
APN 750-10-04-04  
APN 751-030-01

DATE	AMENDMENTS
	DESCRIPTION

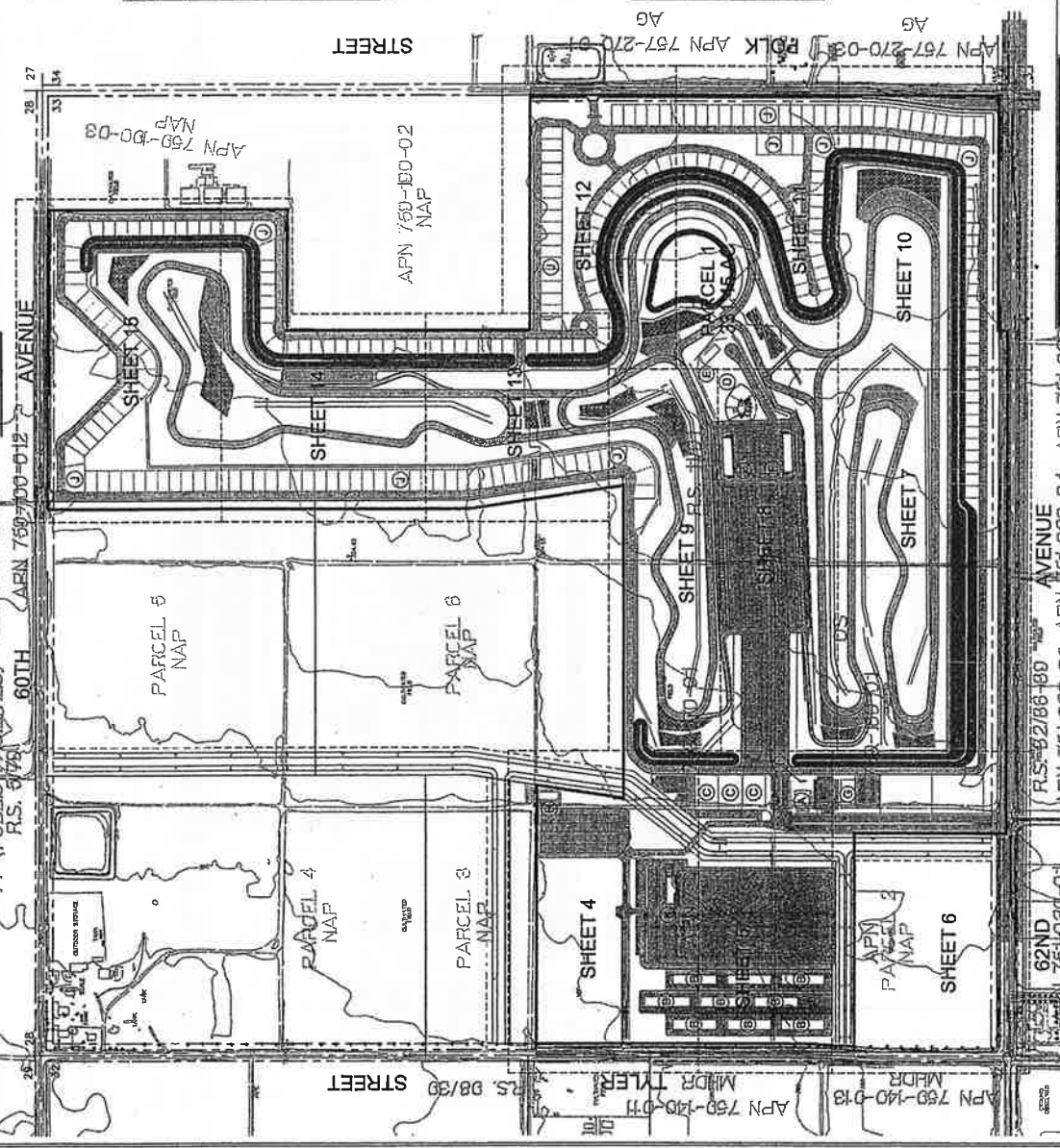
AERIAL INFO:  
OBTAINED BY PHOTOGRAPHY CORPORATION  
FROM AERIAL PHOTOGRAPHY DIVISION  
470 MAC ARTHUR DRIVE  
SAN JOSE, CA 95128  
SAN (408) 887-4263

BENCH MARK: CALTRANS BK NO. RM-144.1  
CORNER OF HIGHWAY 52 AND AVENUE 51, STAMPED CALIFORNIA DEPT OF TRANSPORTATION 1986 44-148 L, 0.7' BELOW ORIGINAL  
AND 3' S.E. OF PFP 840258  
ELEVATION = 276.53  
USDAE DATUM = 500' 1972 ADJUSTMENT.

DATE: 8/17/10  
DRAWN BY: JRM  
CHECKED BY: JRM  
DATE: 8/17/10  
APPROVED BY: JRM  
DATE: 8/17/10

1 OF 15  
JOB NO. 484

LEGAL DESCRIPTION: THERMAL MOTORSPORTS TRACK & CLUB



PARCEL MAP NO. 32033 IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, BEING A SUBDIVISION OF PARCEL 1 OF PARCEL MAP NO. 30315, AS PER MAP RECORDED IN BOOK \_\_\_\_\_ PAGE(S) \_\_\_\_\_ OF PARCEL MAPS, RECORDS OF SAID COUNTY.

APN 759-170-01, 759-180-01 AND 759-180-04

**EXISTING EASEMENTS TITLE COMPANY:**

SHEET	FILE NO.	DESCRIPTION
7	7	PROPERTY INTERESTS
8	8	INSTRUMENT NO. 1000000000, AUGUST 11, 2010
9	9	RECORDS OF OFFICIAL RECORDS
10	10	RECORDS OF OFFICIAL RECORDS
11	11	AN EASEMENT FOR UNDERGROUND UTILITIES
12	12	AN EASEMENT FOR UNDERGROUND UTILITIES
13	13	AN EASEMENT FOR UNDERGROUND UTILITIES
14	14	AN EASEMENT FOR UNDERGROUND UTILITIES
15	15	AN EASEMENT FOR UNDERGROUND UTILITIES
16	16	AN EASEMENT FOR UNDERGROUND UTILITIES
17	17	AN EASEMENT FOR UNDERGROUND UTILITIES
18	18	AN EASEMENT FOR UNDERGROUND UTILITIES

**PROJECT DESCRIPTION:**

THIS IS A PRIVATE FACILITY ONLY. VISITORS MUST BE ACCOMPANIED BY AN ADULT. VISITORS MUST WEAR SEATBELTS AND SAFETY BELTS AT ALL TIMES. VISITORS MUST WEAR HELMETS AND SAFETY GEAR AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE CLOTHING AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE FOOTWEAR AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE GLOVES AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE GOGGLES AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE MASKS AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE EARPLUGS AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE EYEGLASSES AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE HEADPHONES AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE EARPLUGS AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE EYEGLASSES AT ALL TIMES. VISITORS MUST WEAR PROTECTIVE HEADPHONES AT ALL TIMES.

**DENSITY CALCULATION**

TOTAL AREA	PAVED AREA	PERCENT PAVED	PERCENT PAVED	PERCENT PAVED
11,100 AC (280)	3,100 AC (80)	27.9%	27.9%	27.9%
11,100 AC (280)	3,100 AC (80)	27.9%	27.9%	27.9%
11,100 AC (280)	3,100 AC (80)	27.9%	27.9%	27.9%

- PROPOSED BUILDING LEGEND:**
- 1 ADMINISTRATION / REGISTRATION
  - 2 TEAM GARAGE
  - 3 MEMBER'S STORAGE GARAGE
  - 4 CONTROL TOWER
  - 5 CORPORATE EVENT TENT
  - 6 TRACK SIZE GARAGE / LUXURY SUITE
  - 7 TRUCKING SHED
  - 8 MAINTENANCE BUILDING
  - 9 FUEL ISLAND
  - 10 RACE ADMINISTRATION / REGISTRATION
  - 11 MEMBER'S PRIVATE GARAGE
  - 12 RACE ADMINISTRATION / REGISTRATION

**NOTES:**

- EASEMENTS OF RECORDS ARE AS SHOWN.
- ALL ACCESS SHALL BE ON ROAD SURFACE WITH MEMBER'S ACCESS ON POOL.
- ALL SLOPES ARE 2:1 RATIO, UNLESS OTHERWISE NOTED.
- NO SUBSURFACE UTILITIES ARE INDICATED ON THE PLAN.
- LAND IS IN HIGH LIQUEFACTION POTENTIAL AREA AND IS NOT WITHIN A FAULT.
- LAND IS NOT SUBJECT TO OVERFLOW, INUNDATION OR FLOOD HAZARD.
- THOSE AREAS NOT SHOWN AS SHOWN ON FEMA PANEL NO. 08052C225A.
- PROPOSED ON-SITE OPEN CHANNEL AS INDICATED ON MAP.
- PROJECT IS NOT ZONED R-2, RESTRICTED SINGLE FAMILY RES.
- THE PROJECT WILL COMPLY WITH ALL APPLICABLE REQUIREMENTS.



**PROJECT DATA**

APPLICANT: 194-122, LLC  
 50-555 WASHINGTON STREET, #214  
 LOS ANGELES, CA 90023  
 (310) 404-1774

OWNER: VORN BIRCH, LLC  
 8000 SURE SUITE 200  
 LOS ANGELES, CA 90019  
 (310) 858-1800

ENGINEER: LAND DEVELOPMENT CONSULTANTS, INC.  
 1540 BROOKHOLLOW DRIVE, SUITE 33  
 LOS ANGELES, CA 90048  
 TEL: (714) 537-7700  
 FAX: (714) 537-7700

AGENCY: 759-170-01, 759-180-01, 759-180-04

PROJECT NO: 759-170-01, 759-180-01, 759-180-04

DATE OF PREPARATION: FEBRUARY 2010

PROJECT LOCATION: 60TH AVENUE, TYLER STREET, 61ST AVENUE

**PROJECT DATA**

APPLICANT	OWNER	ENGINEER	AGENCY	PROJECT NO	DATE OF PREPARATION	PROJECT LOCATION
194-122, LLC	VORN BIRCH, LLC	LAND DEVELOPMENT CONSULTANTS, INC.	759-170-01, 759-180-01, 759-180-04	759-170-01, 759-180-01, 759-180-04	FEBRUARY 2010	60TH AVENUE, TYLER STREET, 61ST AVENUE

**NOTES:**

- EASEMENTS OF RECORDS ARE AS SHOWN.
- ALL ACCESS SHALL BE ON ROAD SURFACE WITH MEMBER'S ACCESS ON POOL.
- ALL SLOPES ARE 2:1 RATIO, UNLESS OTHERWISE NOTED.
- NO SUBSURFACE UTILITIES ARE INDICATED ON THE PLAN.
- LAND IS IN HIGH LIQUEFACTION POTENTIAL AREA AND IS NOT WITHIN A FAULT.
- LAND IS NOT SUBJECT TO OVERFLOW, INUNDATION OR FLOOD HAZARD.
- THOSE AREAS NOT SHOWN AS SHOWN ON FEMA PANEL NO. 08052C225A.
- PROPOSED ON-SITE OPEN CHANNEL AS INDICATED ON MAP.
- PROJECT IS NOT ZONED R-2, RESTRICTED SINGLE FAMILY RES.
- THE PROJECT WILL COMPLY WITH ALL APPLICABLE REQUIREMENTS.

**PROJECT DATA**

APPLICANT	OWNER	ENGINEER	AGENCY	PROJECT NO	DATE OF PREPARATION	PROJECT LOCATION
194-122, LLC	VORN BIRCH, LLC	LAND DEVELOPMENT CONSULTANTS, INC.	759-170-01, 759-180-01, 759-180-04	759-170-01, 759-180-01, 759-180-04	FEBRUARY 2010	60TH AVENUE, TYLER STREET, 61ST AVENUE

**NOTES:**

- EASEMENTS OF RECORDS ARE AS SHOWN.
- ALL ACCESS SHALL BE ON ROAD SURFACE WITH MEMBER'S ACCESS ON POOL.
- ALL SLOPES ARE 2:1 RATIO, UNLESS OTHERWISE NOTED.
- NO SUBSURFACE UTILITIES ARE INDICATED ON THE PLAN.
- LAND IS IN HIGH LIQUEFACTION POTENTIAL AREA AND IS NOT WITHIN A FAULT.
- LAND IS NOT SUBJECT TO OVERFLOW, INUNDATION OR FLOOD HAZARD.
- THOSE AREAS NOT SHOWN AS SHOWN ON FEMA PANEL NO. 08052C225A.
- PROPOSED ON-SITE OPEN CHANNEL AS INDICATED ON MAP.
- PROJECT IS NOT ZONED R-2, RESTRICTED SINGLE FAMILY RES.
- THE PROJECT WILL COMPLY WITH ALL APPLICABLE REQUIREMENTS.

**PROJECT DATA**

APPLICANT	OWNER	ENGINEER	AGENCY	PROJECT NO	DATE OF PREPARATION	PROJECT LOCATION
194-122, LLC	VORN BIRCH, LLC	LAND DEVELOPMENT CONSULTANTS, INC.	759-170-01, 759-180-01, 759-180-04	759-170-01, 759-180-01, 759-180-04	FEBRUARY 2010	60TH AVENUE, TYLER STREET, 61ST AVENUE

**NOTES:**

- EASEMENTS OF RECORDS ARE AS SHOWN.
- ALL ACCESS SHALL BE ON ROAD SURFACE WITH MEMBER'S ACCESS ON POOL.
- ALL SLOPES ARE 2:1 RATIO, UNLESS OTHERWISE NOTED.
- NO SUBSURFACE UTILITIES ARE INDICATED ON THE PLAN.
- LAND IS IN HIGH LIQUEFACTION POTENTIAL AREA AND IS NOT WITHIN A FAULT.
- LAND IS NOT SUBJECT TO OVERFLOW, INUNDATION OR FLOOD HAZARD.
- THOSE AREAS NOT SHOWN AS SHOWN ON FEMA PANEL NO. 08052C225A.
- PROPOSED ON-SITE OPEN CHANNEL AS INDICATED ON MAP.
- PROJECT IS NOT ZONED R-2, RESTRICTED SINGLE FAMILY RES.
- THE PROJECT WILL COMPLY WITH ALL APPLICABLE REQUIREMENTS.

**THERMAL MOTORSPORTS TRACK & CLUB**

PREPARED FOR: MR. PHIL CLAYTON  
 THERMAL, CALIFORNIA  
 50-855 WASHINGTON STREET, #234  
 LA QUINTA, CA 92553  
 (310) 488-4774

DATE: 8/17/10  
 SHEET: 1 OF 15  
 JOB NO. 454

**LAND DEVELOPERS ASSOCIATION**

LAND PLANNERS  
 SURVEYORS  
 CIVIL ENGINEERS

MEMBER NO. 1000000000, AUGUST 11, 2010

**LEGAL DESCRIPTION:**

PARCEL MAP NO. 32033 IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, BEING A SUBDIVISION OF PARCEL 1 OF PARCEL MAP NO. 30315, AS PER MAP RECORDED IN BOOK \_\_\_\_\_ PAGE(S) \_\_\_\_\_ OF PARCEL MAPS, RECORDS OF SAID COUNTY.

**AERIAL INFO:**

DATE: 8/17/10  
 SHEET: 1 OF 15  
 JOB NO. 454

**USGS DATUM: 500' 4972' ADJUSTMENT**

ELEVATION = 315.53

DATE: 8/17/10  
 SHEET: 1 OF 15  
 JOB NO. 454

# THERMAL MOTORSPORTS TRACK & CLUB

## LEGAL DESCRIPTION:

ALL LOTS SHOWN IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, BEING A SUBDIVISION OF PARCELS OF PARCEL MAP NO. 32253, AS PER MAP RECORDED IN BOOK \_\_\_\_\_ PAGE(S) \_\_\_\_\_ OF PARCEL MAPS, RECORDS OF SAID COUNTY.

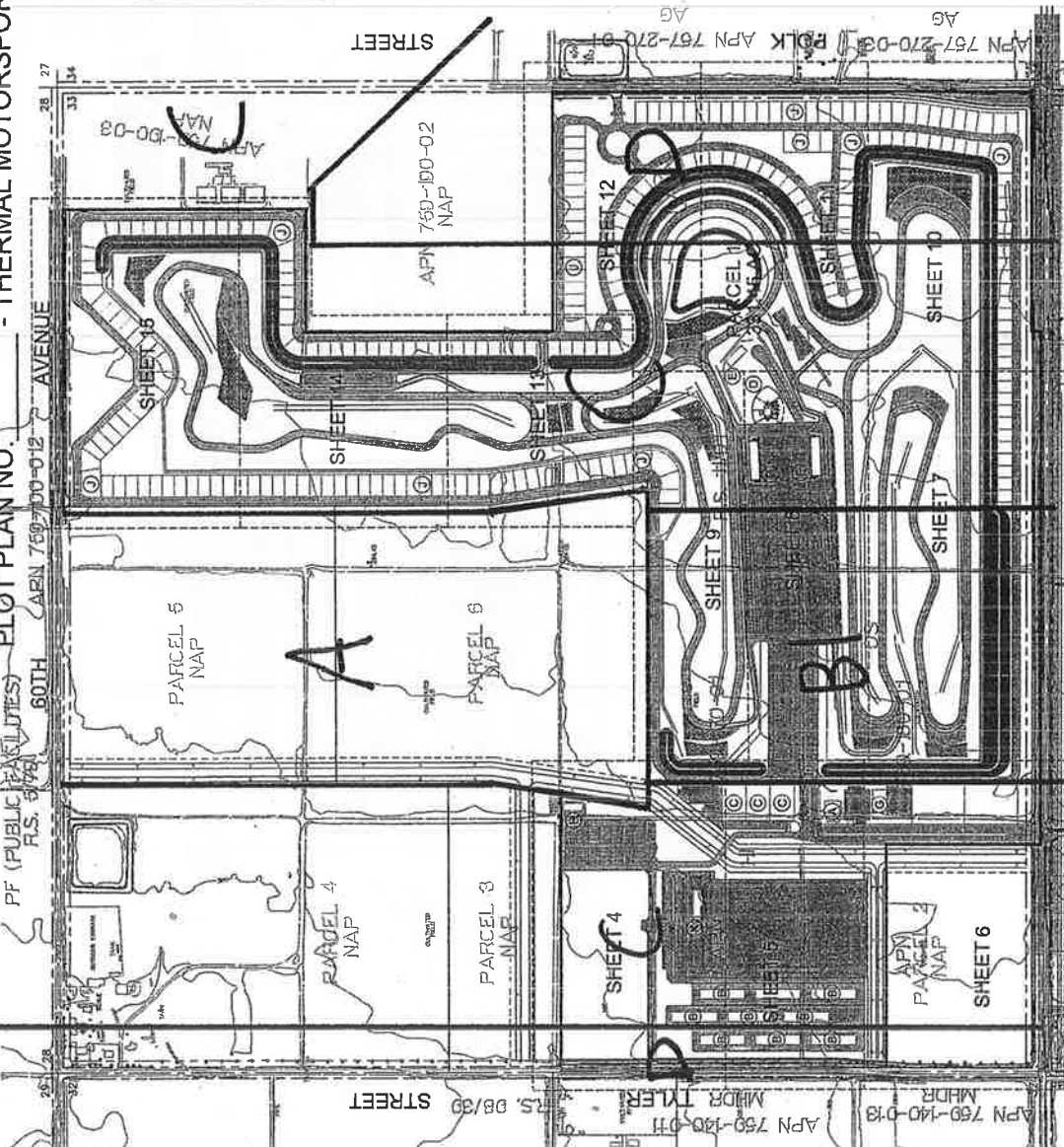
APN 750-170-01, 750-180-01, AND 750-180-04

SPRINKLER MAP REPORT	EXISTING EASEMENTS TITLE COMPANY, PER PRELIMINARY ORDER NO. 20000303-001, AUGUST 11, 2010
7	EXISTING EASEMENT FOR OFFICIAL RECORDS, INSTRUMENT NO. 1577 OF OFFICIAL RECORDS, RECORDED FEBRUARY 13, 1950.
8	EXISTING EASEMENT FOR OFFICIAL RECORDS, INSTRUMENT NO. 8115 OF OFFICIAL RECORDS, RECORDED FEBRUARY 13, 1950.
9	EXISTING EASEMENT FOR OFFICIAL RECORDS, INSTRUMENT NO. 8075 OF OFFICIAL RECORDS, RECORDED FEBRUARY 13, 1950.
10	EXISTING EASEMENT FOR OFFICIAL RECORDS, INSTRUMENT NO. 8077 OF OFFICIAL RECORDS, RECORDED FEBRUARY 13, 1950.
11	EXISTING EASEMENT FOR OFFICIAL RECORDS, INSTRUMENT NO. 8077 OF OFFICIAL RECORDS, RECORDED FEBRUARY 13, 1950.
11A	EXISTING EASEMENT FOR OFFICIAL RECORDS, INSTRUMENT NO. 8077 OF OFFICIAL RECORDS, RECORDED FEBRUARY 13, 1950.
12	EXISTING EASEMENT FOR OFFICIAL RECORDS, INSTRUMENT NO. 8077 OF OFFICIAL RECORDS, RECORDED FEBRUARY 13, 1950.

**PROJECT DESCRIPTION:**  
 THIS IS A PRIVATE FACILITY. ONLY MEMBERS, THEIR INVITED GUESTS, STAFF AND OTHERS WHO ARE AUTHORIZED BY THE CLUB WILL BE PERMITTED TO ENTER THE FACILITY. ACCESS TO ALL AREAS IS AT THE DISCRETION OF THE CLUB. THE CLUB WILL MAINTAIN AND OPERATE THE FACILITY. THE CLUB WILL MAINTAIN AND OPERATE THE FACILITY. THE CLUB WILL MAINTAIN AND OPERATE THE FACILITY.

DENSITY CALCULATION	AREA	PERMITTED USE	PERMITTED DENSITY
TOTAL AREA	114,454 SQ. FT.	RESIDENTIAL	1.0
ROADS	18,000 SQ. FT.	RESIDENTIAL	1.0
OPEN SPACE	21,147 SQ. FT.	RESIDENTIAL	1.0
PERMITTED USE	75,307 SQ. FT.	RESIDENTIAL	1.0

PROPOSED BUILDING LEGEND:	AREA	PERMITTED USE	PERMITTED DENSITY
(A) ADMINISTRATION / REGISTRATION	1,000 SQ. FT.	RESIDENTIAL	1.0
(B) TEAM GARAGE	1,000 SQ. FT.	RESIDENTIAL	1.0
(C) MEMBERS' STORAGE GARAGE	1,000 SQ. FT.	RESIDENTIAL	1.0
(D) CONTROL TOWER	1,000 SQ. FT.	RESIDENTIAL	1.0
(E) CORPORATE EVENT TENT	1,000 SQ. FT.	RESIDENTIAL	1.0
(F) THICK SOIL GARAGE / LUXURY SUITE	1,000 SQ. FT.	RESIDENTIAL	1.0
(G) TRUCK SHOP	1,000 SQ. FT.	RESIDENTIAL	1.0
(H) MAINTENANCE BUILDING	1,000 SQ. FT.	RESIDENTIAL	1.0
(I) FUEL ISLAND	1,000 SQ. FT.	RESIDENTIAL	1.0
(J) MEMBERS' PRIVATE GARAGE	1,000 SQ. FT.	RESIDENTIAL	1.0
(K) PARTY ADMINISTRATION / REGISTRATION	1,000 SQ. FT.	RESIDENTIAL	1.0



**THERMAL MOTORSPORTS TRACK & CLUB**  
 PREPARED FOR:  
**MR. PHIL CLAYTON**  
**TEM 122, LLC**  
 50-855 WASHINGTON STREET, #234  
 LA QUINTA, CA 92553  
 (310) 488-4774

**AERIAL INFO:**  
 COURTESY OF AERIAL PHOTOGRAPHY, DATED 10-22-09  
 FROM AERIAL PHOTOGRAPHY, DATED 10-22-09  
 SW 1/4 SEC. 37, T4S, R12E, S4E, 1/4  
 ELEVATION = 375.53  
 UTM ZONE = 11S  
 UTM EASTING = 375,530



PROJECT DATA	APPLICANT
750-170-01, 750-180-01, AND 750-180-04	TEM 122, LLC
50-855 WASHINGTON STREET, #234 LA QUINTA, CA 92553 (310) 488-4774	
750-170-01, 750-180-01, AND 750-180-04	
750-170-01, 750-180-01, AND 750-180-04	

APPLICANT	OWNER	ENGINEER
750-170-01, 750-180-01, AND 750-180-04	TEM 122, LLC	LAND DEVELOPMENT CONSULTANTS, INC. 1528 BROOKWOOD DRIVE, SUITE 31 LOS ANGELES, CA 90047 TEL: (714) 557-7700 FAX: (714) 557-7707
750-170-01, 750-180-01, AND 750-180-04		
750-170-01, 750-180-01, AND 750-180-04		

**NOTES:**  
 1. EASEMENTS OF RECORDS ARE AS SHOWN.  
 2. STREET AND EASEMENT ACCESS ON BOTH SIDES.  
 3. ALL SURFACES ARE 2:1 RATIO, UNLESS OTHERWISE NOTED.  
 4. ALL SURFACES ARE 2:1 RATIO, UNLESS OTHERWISE NOTED.  
 5. NO SUBSURFACE SEPTIC SEWAGE DISPOSAL IN DENIED.  
 6. LAND IS IN HIGH UNDEVELOPMENT POTENTIAL AREA AND IS NOT WITHIN A FAULT.  
 7. LAND IS NOT SUBJECT TO OVERFLOW, INUNDATION OR FLOOD HAZARD.  
 8. THERE ARE NO EXISTING WELLS ON THE PROPERTY.  
 9. NO EXISTING STRUCUTURES OR UTILITIES ON-SITE.  
 10. PROJECT IS NOT ZONED R-2, RESTRICTED SINGLE FAMILY RES.  
 11. THE PROJECT WILL COMPLY WITH ALL NECESSARY REGULATIONS.

**COUNTY OF RIVERSIDE**  
**PLOT PLAN NO. THERMAL MOTORSPORTS TRACK & CLUB**

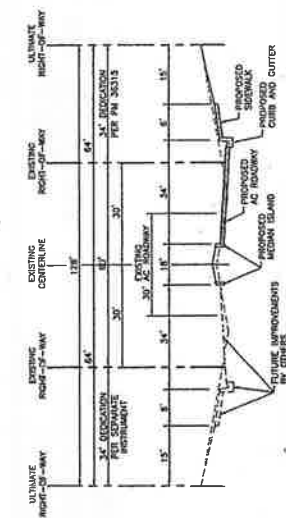
DATE OF MAP	APPROVED
AUGUST 2010	

DESIGNED BY	CHECKED BY	DATE
8/17/10	8/17/10	8/17/10

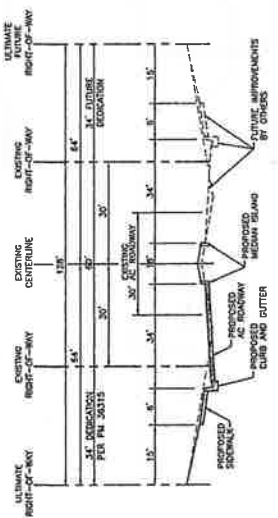
LAND PLANNERS	SURVEYORS
LAND DEVELOPMENT CONSULTANTS, INC.	CIVIL ENGINEERS

DATE	DESCRIPTION

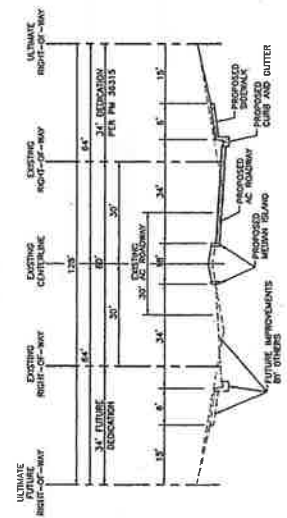
DATE	DESCRIPTION



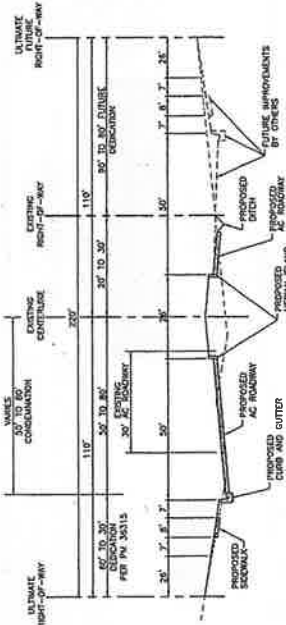
TYPICAL SECTION: TYLER STREET  
FROM 62ND AVENUE TO 61ST AVENUE  
NOT TO SCALE



TYPICAL SECTION: POLK STREET  
FROM 62ND AVENUE TO 61ST AVENUE  
NOT TO SCALE



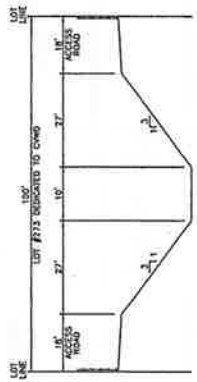
TYPICAL SECTION: 60TH AVENUE  
FROM 3000' EAST OF TYLER STREET TO  
TO 750' WEST OF POLK STREET  
NOT TO SCALE



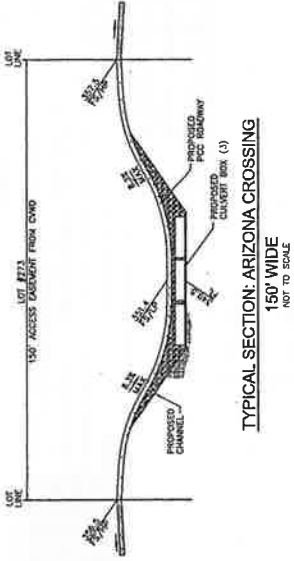
TYPICAL SECTION: 62ND AVENUE  
FROM TYLER STREET TO POLK STREET  
NOT TO SCALE



TYPICAL SECTION: OPEN CHANNEL  
150' WIDE  
NOT TO SCALE



TYPICAL SECTION: OPEN CHANNEL  
100' WIDE  
NOT TO SCALE



TYPICAL SECTION: ARIZONA CROSSING  
150' WIDE  
NOT TO SCALE

**BENCH MARK:** CALTRANS BN NO. RV-148 L  
CORNER OF 60TH AVENUE & 61ST AVENUE  
TRANSPORTATION 1986 N-14.8 L 0.5' BELOW GROUND  
AND 3' S.E. OF PVI 850758  
ELEVATION = 375.53  
USPCS DATUM + 300' 1972 ADJUSTMENT

**AERIAL INFO:**  
COURTESY BY AERIAL PHOTOGRAPHY DATED 10-22-08  
FROM AERIAL PHOTOGRAPHY DATED 10-22-08  
BY J. M. LEE, J. M. LEE, J. M. LEE  
SIN (909) 897-4320

DATE	AMENDMENTS	DESCRIPTION

PREPARED FOR  
**MR. PHIL CLAYTON**  
50-855 WASHINGTON STREET #C234  
LA QUINTA, CA 92553  
(310) 486-4774



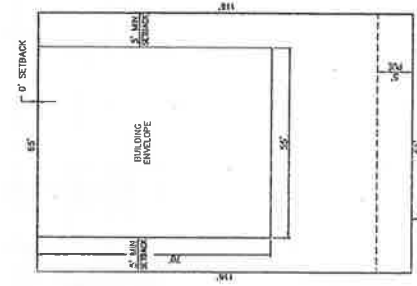
**LAND DEVELOPMENT CONSULTANTS**  
also MECHANICAL CONSULTANTS  
CIVIL ENGINEERS  
LAND PLANNERS  
SURVEYORS

DESIGNED BY: RIN  
DATE: 8/17/10  
CHECKED BY: RIN  
DATE: 8/17/10  
APPROVED BY: RIN  
DATE: 8/17/10

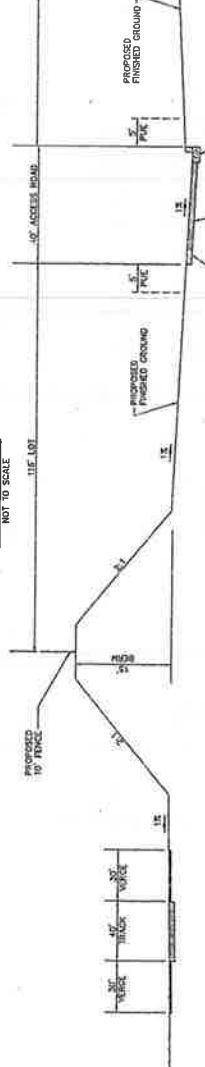
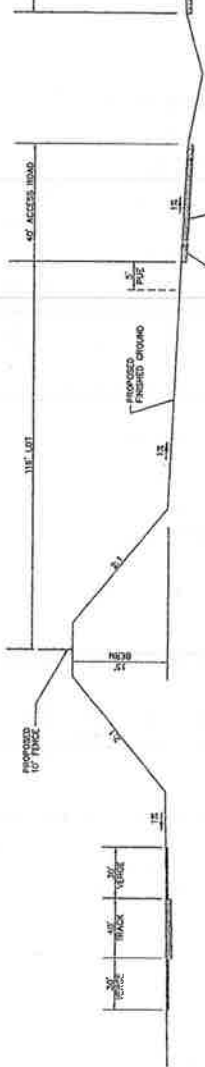
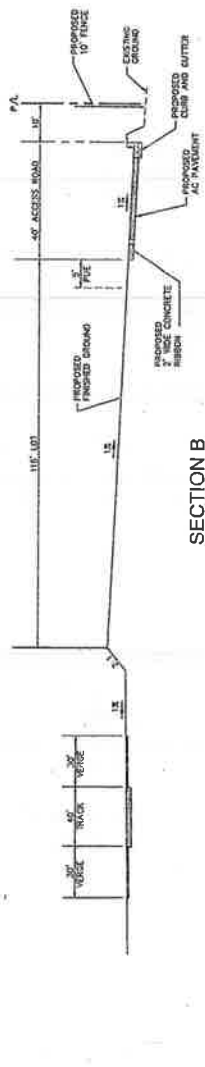
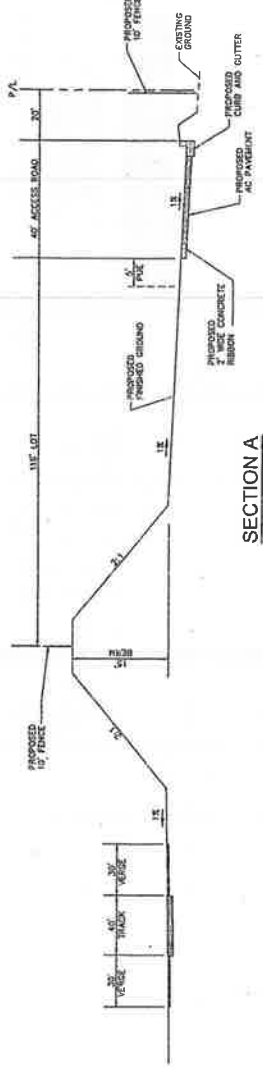
**COUNTY OF RIVERSIDE**  
PLOT PLAN NO.  
THERMAL MOTORSPORTS  
TRACK & CLUB  
SECTIONS

SHEET  
2 OF 15  
JOB NO. 464

MEMBER CAR STORAGE BUILDING  
 2 STORY BUILDING  
 1ST FLOOR 3578 SQ.FT.  
 2ND FLOOR 3575 SQ.FT.  
 HEIGHT=42'



TYPICAL FOUNDER LOT  
 NOT TO SCALE



COUNTY OF RIVERSIDE  
 PLOT PLAN NO.  
 THERMAL MOTORSPORTS  
 TRACK & CLUB  
 SECTIONS

LAND PLANNERS SURVEYORS  
**LD DEVELOPMENT CONSULTANTS**  
 CIVIL ENGINEERS  
 1920 PROCKHOLLOW DRIVE, SUITE 100  
 CHINA GLEN, CALIFORNIA 92520  
 (951) 697-7700 FAX (951) 697-7700

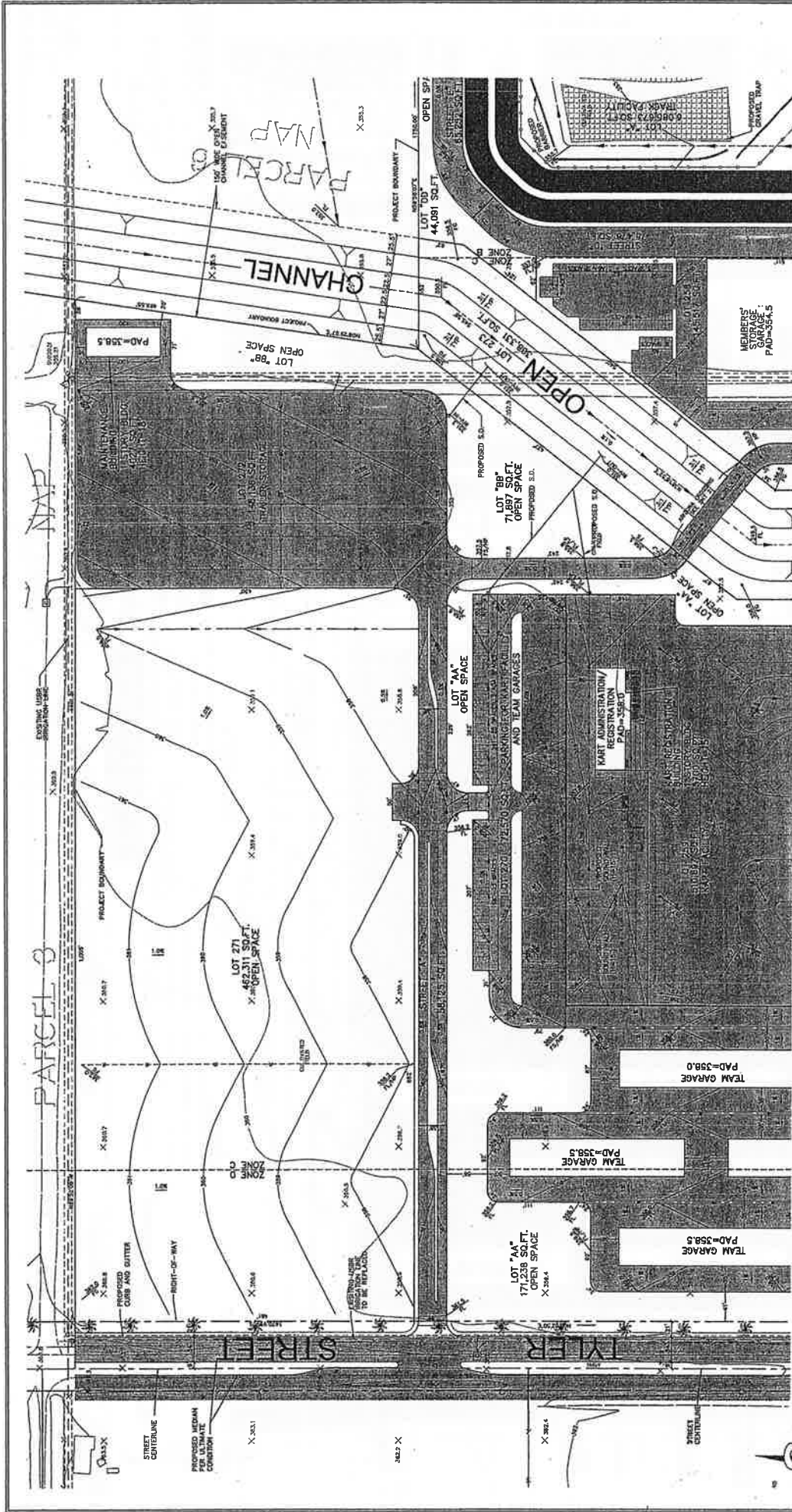
**THERMAL MOTORSPORTS TRACK & CLUB**  
 PREPARED FOR:  
**MR. PHIL CLAYTON**  
**TRM 192, LLC**  
 50-555 WASHINGTON STREET, #C234  
 LA QUINIA, CA 92253  
 (310) 486-4774

DATE	AMENDMENTS	DESCRIPTION

**AERIAL INFO:**  
 OBTAINED BY AERIAL PHOTOGRAPHY COOPERATION FROM AERIAL PHOTOGRAPHY DIVISION 10-22-09  
 437 MACKEY DRIVE  
 SAN JUAN CA 92078  
 (951) 881-5500

BENCH MARK: CALTRANS BM NO. 894-4.8 L  
 3 1/2" BRASS CROWN  
 CORNER OF INTERSECTION OF  
 C. HAYES AND PALM ST., STAMPED CALIFORNIA DEPT  
 AND 3 1/2" OF P.W. 800288  
 ELEVATION = 375.53  
 USGS DATUM + 500' 1972 ADJUSTMENT.

DESIGNED BY: RN  
 DATE: 8/17/10  
 DRAWN BY: RN  
 DATE: 8/17/10  
 CHECKED BY: RN  
 DATE: 8/17/10  
 SHEET  
 3 OF 15  
 JOB NO. 404



**LEGEND:**

- PROJECT BOUNDARY
- EXISTING RIGHT-OF-WAY
- ULTIMATE RIGHT-OF-WAY
- EXISTING CENTERLINE
- PROPOSED LOT LINES
- EXISTING 61' AVE 82' DRAIN LINE
- EXISTING SANITARY SEWER
- EXISTING EASEMENT
- PROPOSED SAFETY BARRIERS
- PROPOSED FOOTBALL FENCING
- 70' WIDE BY 15' HIGH BURN (2" S&C SURF, 10' FLAT TOP)
- PROPOSED AC PAVEMENT / TRACK
- PROPOSED GRAVEL TYP

**SEE SHEET 5**

**THERMAL MOTORSPORTS TRACK & CLUB**

PREPARED FOR:

**MR. PHIL CLAYTON**  
 50-855 WASHINGTON STREET, #0234  
 LA BUREAU, CA 92253  
 (310) 461-4774

**LAND DEVELOPMENT CONSULTANTS**  
 1050 BRIDGEHOLLOW DRIVE, SUITE 300  
 CHAS CITY, MO 64639  
 (314) 887-7700 / FAX (314) 887-7700

LAND PLANNERS SURVEYORS  
 CIVIL ENGINEERS  
 1050 BRIDGEHOLLOW DRIVE, SUITE 300  
 CHAS CITY, MO 64639  
 (314) 887-7700 / FAX (314) 887-7700

**COUNTY OF RIVERSIDE**

**PLOT PLAN NO. THERMAL MOTORSPORTS TRACK & CLUB**

ISSUED BY: 8/17/10  
 DATE: 8/17/10  
 SHEET: 4 OF 15  
 JOB NO. 404

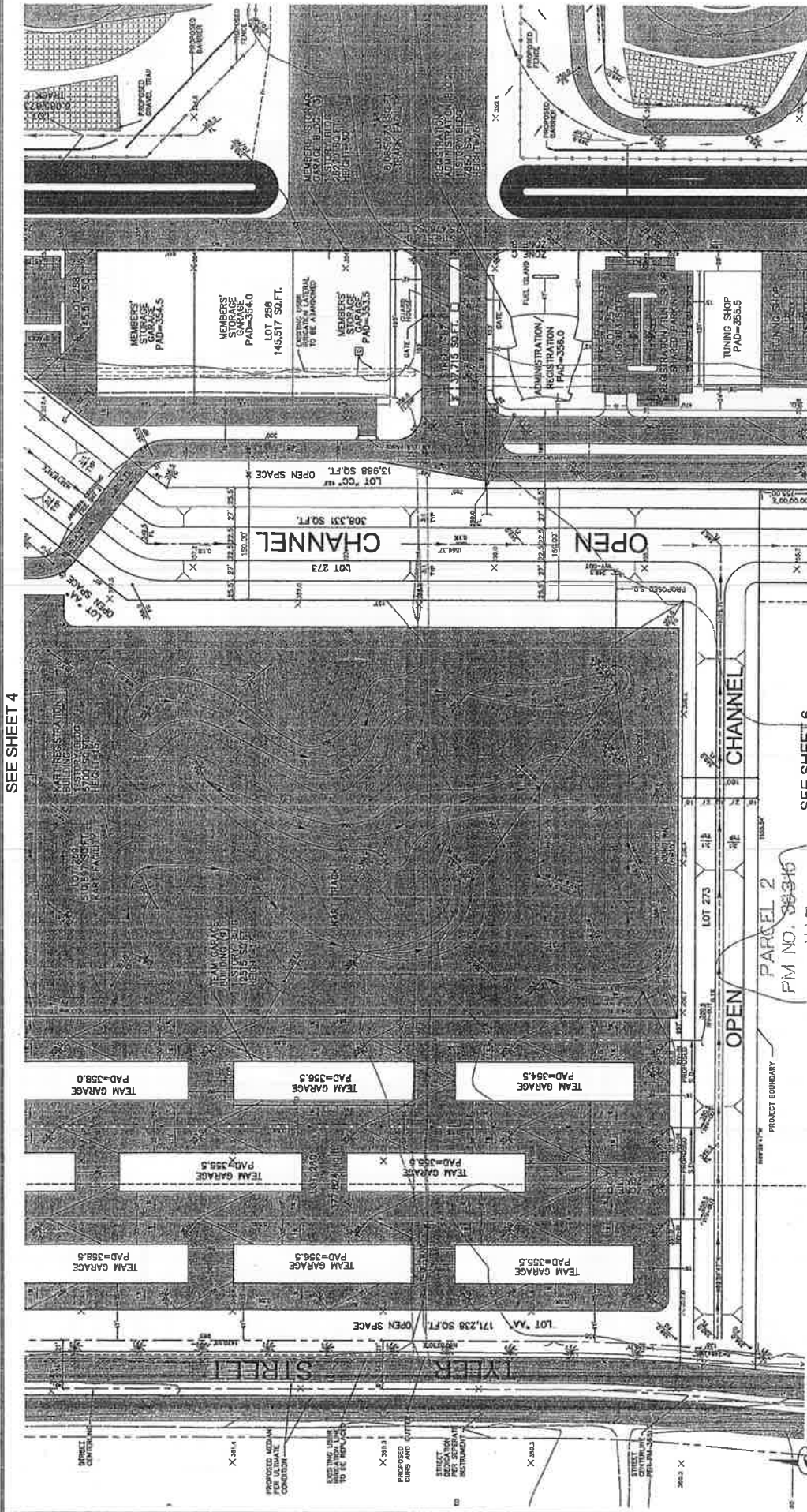
**GRAPHIC SCALE**  
 1" = 40' (1" = 13.33 METERS)

**AERIAL INFO:**  
 COURTESY OF MICHAEL MORGAN ARCHITECTS  
 FROM AERIAL PHOTOGRAPHY DATED 11-22-09  
 AT 1:30 PM  
 SAN FRANCISCO, CA 94108  
 (415) 398-7428

**BENCH MARK:** GALIHAM BN NO. RPV-148 L  
 2 1/2" BRASS DISK IN CONCRETE AT THE S.W. CORNER  
 TRANSPORTATION TRAIL # 14.0 L.O.S. BELOW GROUND  
 AND 3' S.E. OF PLY 805980  
 ELEVATION = 375.53  
 UTM ZONE DATUM = 5007 1972 ADJUSTMENT

SEE SHEET 4

SEE SHEET 10



**COUNTY OF RIVERSIDE**  
**PLOT PLAN NO.**  
**THERMAL MOTORSPORTS**  
**TRACK & CLUB**

DESIGNED BY: [Signature]  
 DATE: 8/17/10  
 CHECKED BY: [Signature]  
 DATE: 8/17/10  
 SCALE: AS SHOWN  
 SHEET: 5 OF 15  
 JOB NO. 484

**LAND DEVELOPMENT CONSULTANTS**  
**L & D CONSULTANTS**  
 CIVIL ENGINEERS  
 5000 WASHINGTON STREET, #234  
 LA QUINTA, CA 92553  
 (919) 888-4242

**LAND PLANNERS SURVEYORS**  
 CIVIL ENGINEERS  
 5000 WASHINGTON STREET, #234  
 LA QUINTA, CA 92553  
 (919) 888-4242

DATE: 8/17/10

**THERMAL MOTORSPORTS TRACK & CLUB**  
 PREPARED FOR:  
**MR. PHIL CLAYTON**  
**TRM 122 LLC**  
 50-1155 WASHINGTON STREET, #234  
 LA QUINTA, CA 92553  
 (310) 488-4774

**LEGEND:**

- PROJECT BOUNDARY
- EXISTING RIGHT-OF-WAY
- ULTRA WIDE RIGHT-OF-WAY
- EXISTING CENTERLINE
- PROPOSED LOT LINES
- EXISTING 4" E/A/C 6" DEEP DRAIN LINE
- EXISTING SANITARY SEWER
- EXISTING DRAINAGE
- PROPOSED SAFETY BARRIERS
- PROPOSED FERTILIZATION FENCING

**AMENDMENTS**

DATE	DESCRIPTION

**GRAPHIC SCALE**  
 1 inch = 60 ft.

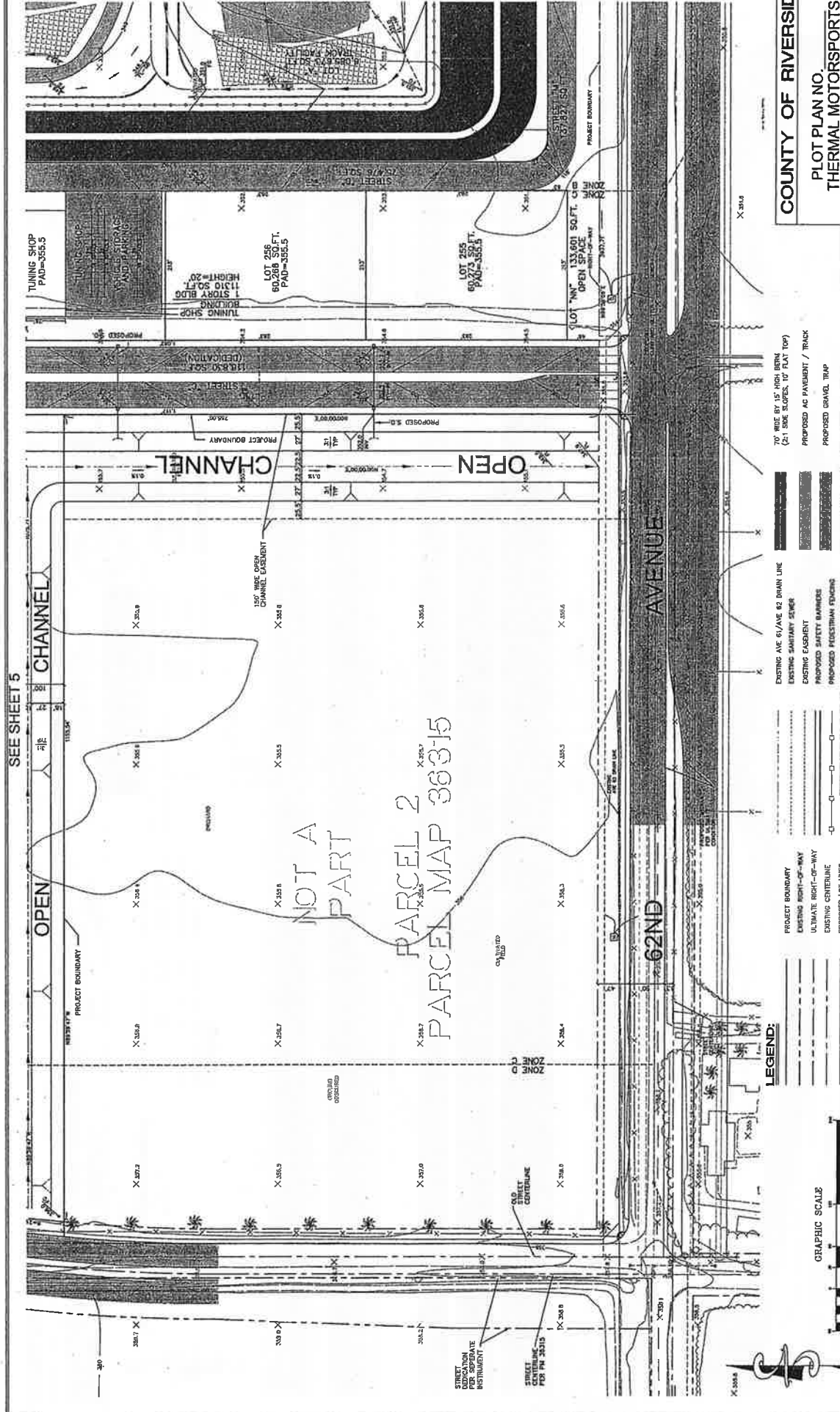
**BENCH MARK:** CALTRANS BM NO. RM-148 L  
 3772 BRASS ORN IN CONCRETE AT THE S.W. CORNER  
 OF HWY 98 AND AVE 61, STAFFED CALIFORNIA DEPT  
 AND 3' S.E. OF PM 800780  
 ELEVATION = 275.53

**AERIAL INFO:**  
 COMPILED BY AERIAL PHOTOGRAPHY DIVISION  
 FROM AERIAL PHOTOGRAPHY DATED: 10-22-09  
 SAN JUAN CALIFORNIA DIVISION  
 (919) 888-5420



SEE SHEET 5

SEE SHEET 7



**COUNTY OF RIVERSIDE**  
**PLOT PLAN NO.**  
**THERMAL MOTORSPORTS**  
**TRACK & CLUB**

70' WIDE BY 15' HIGH BERM (2' SUR. SLOPE, TO PLAN TOP)  
 PROPOSED AC PAVEMENT / TRACK  
 PROPOSED GRAVEL TRIP

EXISTING AVE 61/AVE 62 DRAIN LINE  
 EXISTING SANITARY SEWER  
 EXISTING EASEMENT  
 PROPOSED SAFETY BARRIERS  
 PROPOSED FORESTRAL FENCING

PROJECT BOUNDARY  
 EXISTING RIGHT-OF-WAY  
 ULTIMATE RIGHT-OF-WAY  
 EXISTING CENTERLINE  
 PROPOSED LOT LINES

**LEGEND:**

**GRAPHIC SCALE**  
 (1" = 60' N.)  
 1 inch = 60 ft.

**THERMAL MOTORSPORTS TRACK & CLUB**  
 PREPARED FOR:  
**MR. PHIL CLAYTON**  
**TRM 122, LLC**  
 50-855 WASHINGTON STREET, #0234  
 LA QUINTA, CA 92253  
 (310) 486-4774

**LAND DEVELOPMENT CONSULTANTS**  
 SURVEYORS  
 CIVIL ENGINEERS  
 1850 BRANCHCANYON DRIVE, SUITE 303  
 CHINA GLEN, CA 91701  
 (916) 987-7700 FAX  
 (916) 987-7700

**LAND PLANNERS**  
 SURVEYORS  
 CIVIL ENGINEERS  
 1850 BRANCHCANYON DRIVE, SUITE 303  
 CHINA GLEN, CA 91701  
 (916) 987-7700 FAX  
 (916) 987-7700

**AERIAL INFO:**  
 CALTRANS BN NO. RV-148 L  
 2 1/2" BRASS BORE IN CONCRETE AT THE S.W. QUAD  
 CORNER OF 62ND AVENUE AND 3RD STREET  
 TRANSPORTATION 1986 N-4148 L 0.3' BELOW GROUND  
 AND 3' S.E. OF PPH 8007880  
 ELEVATION = 375.33  
 USGAGES DATUM = 500' 897' ADJUSTMENT.

**DATE:**  
**DESCRIPTION:**

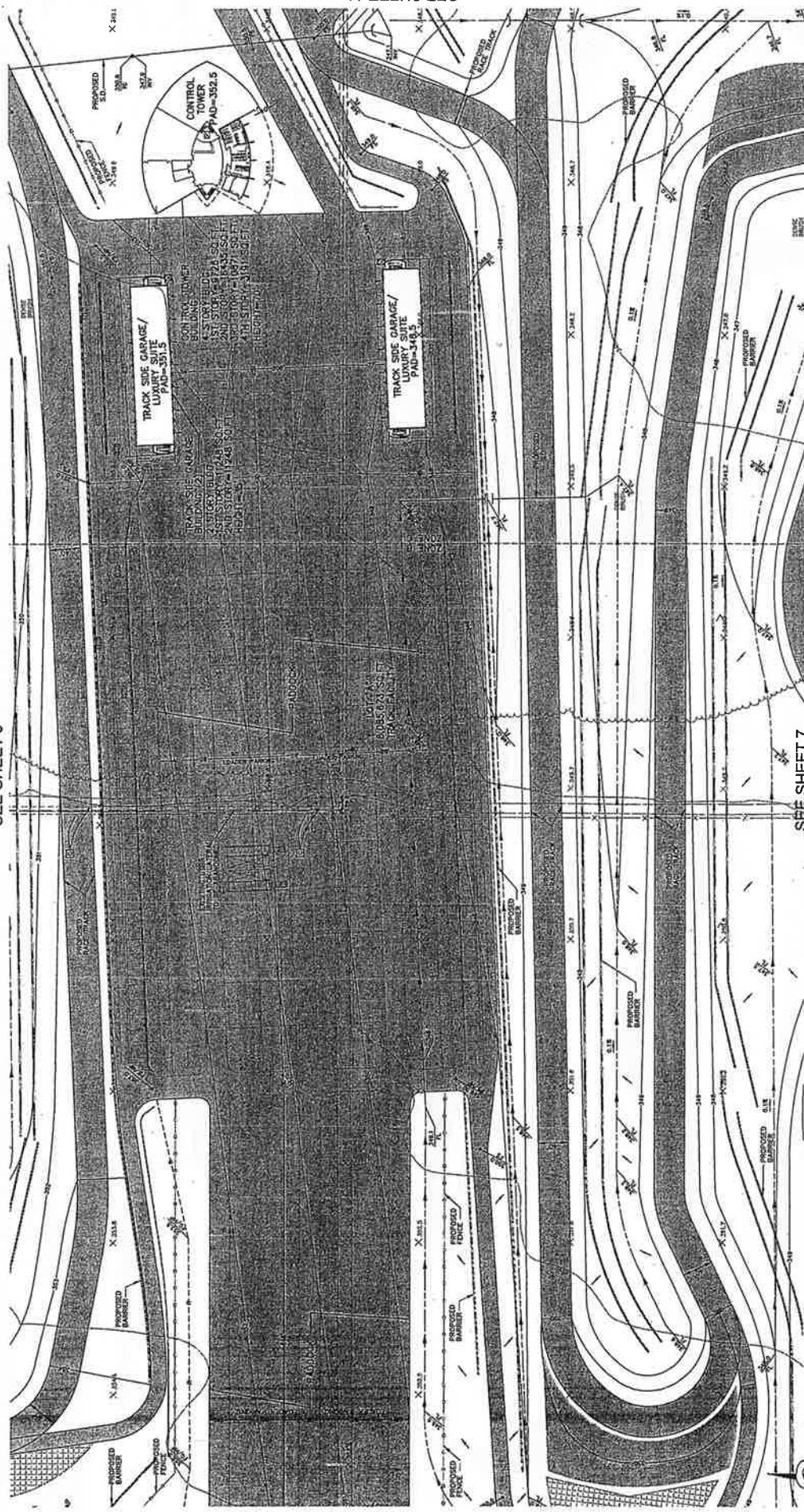
**SHEET**  
**6 OF 15**  
**JOB NO. 484**



SEE SHEET 9

SEE SHEET 11

SEE SHEET 5



**COUNTY OF RIVERSIDE**  
**PLOT PLAN NO.**  
**THERMAL MOTORSPORTS**  
**TRACK & CLUB**

DESIGNED BY: [Name]  
 DATE: 8/17/10  
 DRAWN BY: [Name]  
 DATE: 8/17/10  
 CHECKED BY: [Name]  
 DATE: 8/17/10  
 APPROVED BY: [Name]  
 DATE: 8/17/10

**LAND DEVELOPMENT CONSULTANTS**  
**LAND PLANNERS SURVEYORS**  
**CIVIL ENGINEERS**

1020 MIDCOURTLOW DRIVE, SUITE 303  
 TEMECULA, CA 92590  
 (760) 699-7760 FAX (760) 699-7700 FAX

STATE OF CALIFORNIA  
 PROFESSIONAL SEAL  
 CIVIL ENGINEER  
 PHIL CLAYTON  
 No. 10000  
 EXPIRES 12/31/10

PLANS PREPARED UNDER SUPERVISION OF:  
 PHIL CLAYTON, P.E.  
 E.C.E. #488 EXP. 7/31/2017

**THERMAL MOTORSPORTS TRACK & CLUB**

PREPARED FOR:  
**MR. PHIL CLAYTON**  
**TRM 122, LLC**  
 50-855 LA VERGNE STREET, #0234  
 LA QUINA, CA 92553  
 (910) 488-4774

DATE	AMENDMENTS	DESCRIPTION

**LEGEND:**

- PROJECT BOUNDARY
- EXISTING RIGHT-OF-WAY
- ULTIMATE RIGHT-OF-WAY
- EXISTING CENTERLINE
- PROPOSED LOT LINES
- EXISTING 5' HIGH BERM (E.I. SEE SUPP. TO PLAT 109)
- EXISTING SANITARY SEWER
- EXISTING EASEMENT
- PROPOSED SAFETY BARRIERS
- PROPOSED EASEMENT FENCING
- PROPOSED AC PAVEMENT / TRACK
- PROPOSED GRAVEL TRAP

**GRAPHIC SCALE**  
 1" = 50' (AS SHOWN)  
 1" = 100' (AS SHOWN)

**AERIAL INFO:**  
 COURTESY BY: CALTRANS (M.C. 814-148 L)  
 DATE: 10-22-08  
 FROM: AERIAL PHOTOGRAPHY DATED: 10-22-08  
 BY: M. J. KAY PHOTOGRAPHY  
 331 M. J. KAY DRIVE  
 SAN JUAN, CA 92078  
 (909) 888-2008

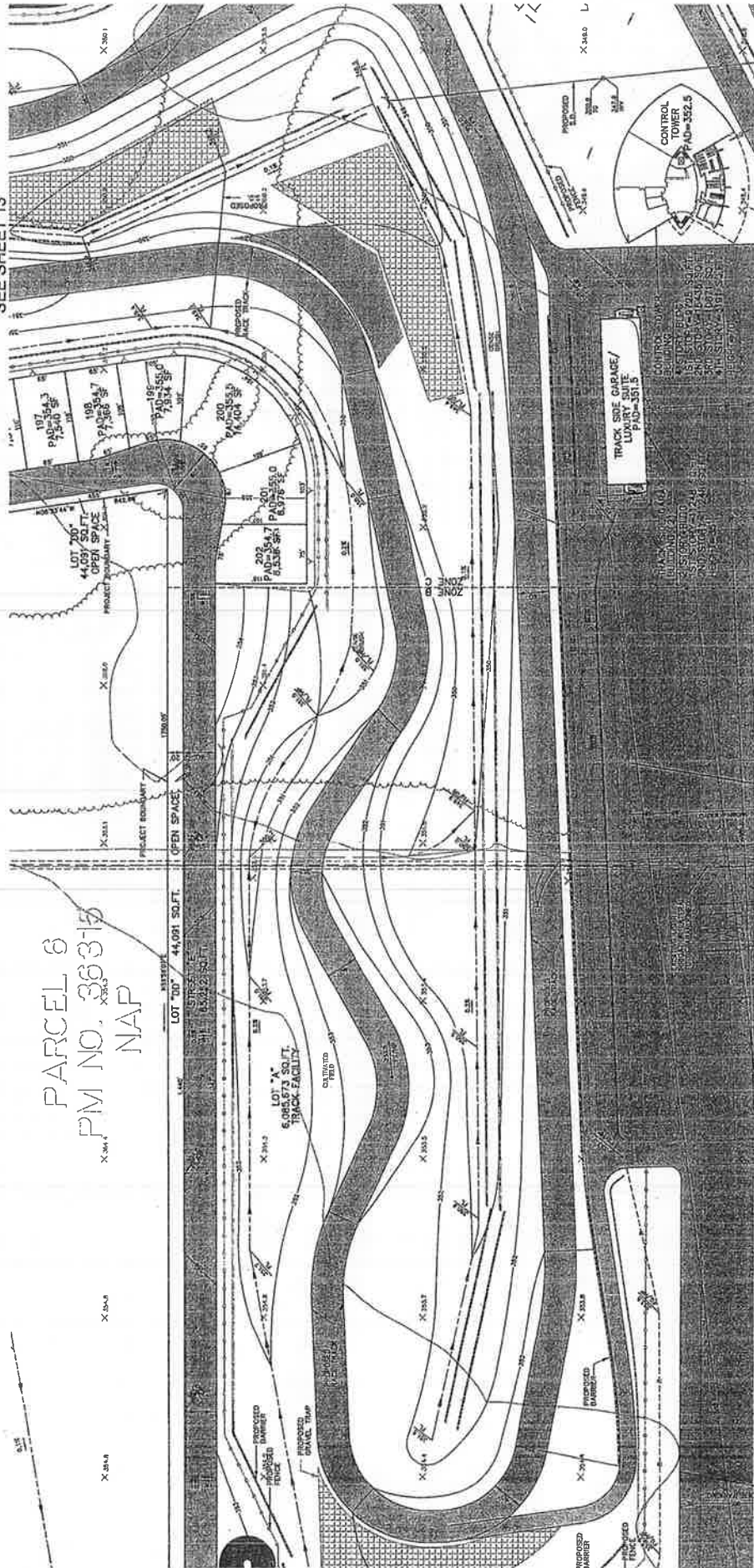
**BENCH MARK:** CALTRANS BM NO. 814-148 L  
 1/4" = 1" SCALE  
 ELEVATION: 376.53  
 USCGES DATUM: 500' 1972 ADJUSTMENT.

PARCEL 6  
PM NO. 36315  
NAP

SEE SHEET 13

SEE SHEET 12

SEE SHEET 5



COUNTY OF RIVERSIDE  
PLOT PLAN NO.  
THERMAL MOTORSPORTS  
TRACK & CLUB

DATE: 8/17/10  
DRAWN BY: [Name]  
CHECKED BY: [Name]  
DATE: 8/17/10  
SCALE: AS SHOWN

LAND DEVELOPMENT CONSULTANTS  
CIVIL ENGINEERS  
1250 BROOKHOLLOW DRIVE, SUITE 303  
CANAAN, CT 06026  
TEL: 860-277-7700 FAX: 860-277-7701

LAND PLANNERS SURVEYORS  
CIVIL ENGINEERS  
1250 BROOKHOLLOW DRIVE, SUITE 303  
CANAAN, CT 06026  
TEL: 860-277-7700 FAX: 860-277-7701

PREPARED FOR:  
MR. PHIL CLAYTON  
TRM 122, LLC  
50-855 WASHINGTON STREET, #234  
LA QUINTA, CA 92553  
(915) 466-4774

72" HIGH BY 15" HIGH BURN  
(2) SEC SUPPLY, TO FLAT TOP  
PROPOSED AC PAVEMENT / TRACK  
PROPOSED GRAVEL TRAP

EXISTING AVE 51/AVE 62 DRAIN LINE  
EXISTING SANITARY SEWER  
EXISTING EASEMENT  
PROPOSED SAFETY BARRIERS  
PROPOSED PEDESTRIAN FENCING

LEGEND:

SYMBOL	DESCRIPTION
[Symbol]	PROJECT BOUNDARY
[Symbol]	EXISTING RIGHT-OF-WAY
[Symbol]	ULTIMATE RIGHT-OF-WAY
[Symbol]	EXISTING CENTERLINE
[Symbol]	PROPOSED LOT LINES

DATE	AMENDMENTS	DESCRIPTION

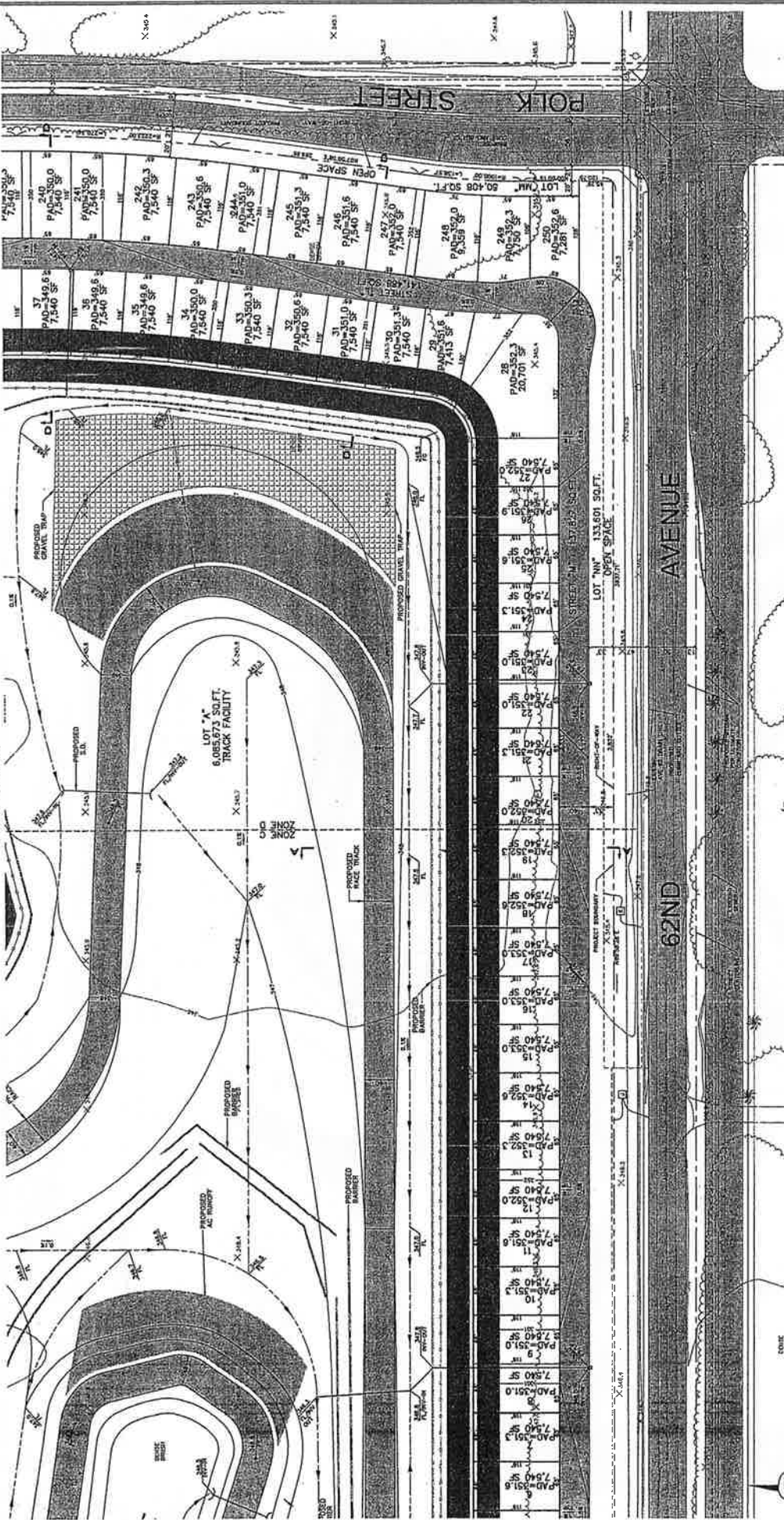
GRAPHIC SCALE  
1 inch = 80' PL

BENCH MARK: CALTRANS BK NO. 89V-142 L  
3 1/2" PAVED BOX IN CORNER AT THE S.E. CORNER  
OF TRANSPORTATION 1869-142 L D'S BLOW GROUP  
AND 3' E.L. OF 89V02780  
ELEVATION = 375.3  
USCGA DATUM + 500' 1973 ADJUSTMENT.

PREPARED BY: [Name]  
DATE: 8/17/10

SEE SHEET 11

SEE SHEET 7



**COUNTY OF RIVERSIDE**  
**PLOT PLAN NO.**  
**THERMAL MOTORSPORTS**  
**TRACK & CLUB**

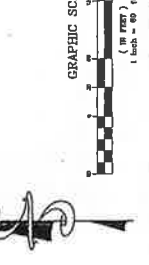
ISSUED BY: RL  
 DATE: 8/17/10  
 SCALE: 1" = 40' (PLAN)  
 SHEET: 10 OF 15  
 JOB NO. 446

70' WIDE BY 15' HIGH BERM  
 (2:1 SIDE SLOPES TO FLAT TOP)  
 PROPOSED AC PAVEMENT / TRACK  
 PROPOSED GRAVEL TRAP

EXISTING AC 61/AS 62 DRAIN LINE  
 EXISTING SAFETY BARRIER  
 EXISTING CASSETTE  
 PROPOSED SAFETY BARRIERS  
 PROPOSED PEDESTRIAN FENCING

PROJECT BOUNDARY  
 EXISTING RIGHT-OF-WAY  
 ULTIMATE RIGHT-OF-WAY  
 EXISTING CONTIGUOUS  
 PROPOSED LOT LINES

**LEGEND:**



**LAND DEVELOPMENT CONSULTANTS**  
 LAND PLANNERS  
 SURVEYORS  
 CIVIL ENGINEERS  
 1850 BROOKHOLLOW DRIVE, SUITE 200  
 ANAHEIM, CA 92806  
 TEL: 714.944.1100  
 FAX: 714.944.1101  
 WWW: WWW.LDCCONSULTANTS.COM

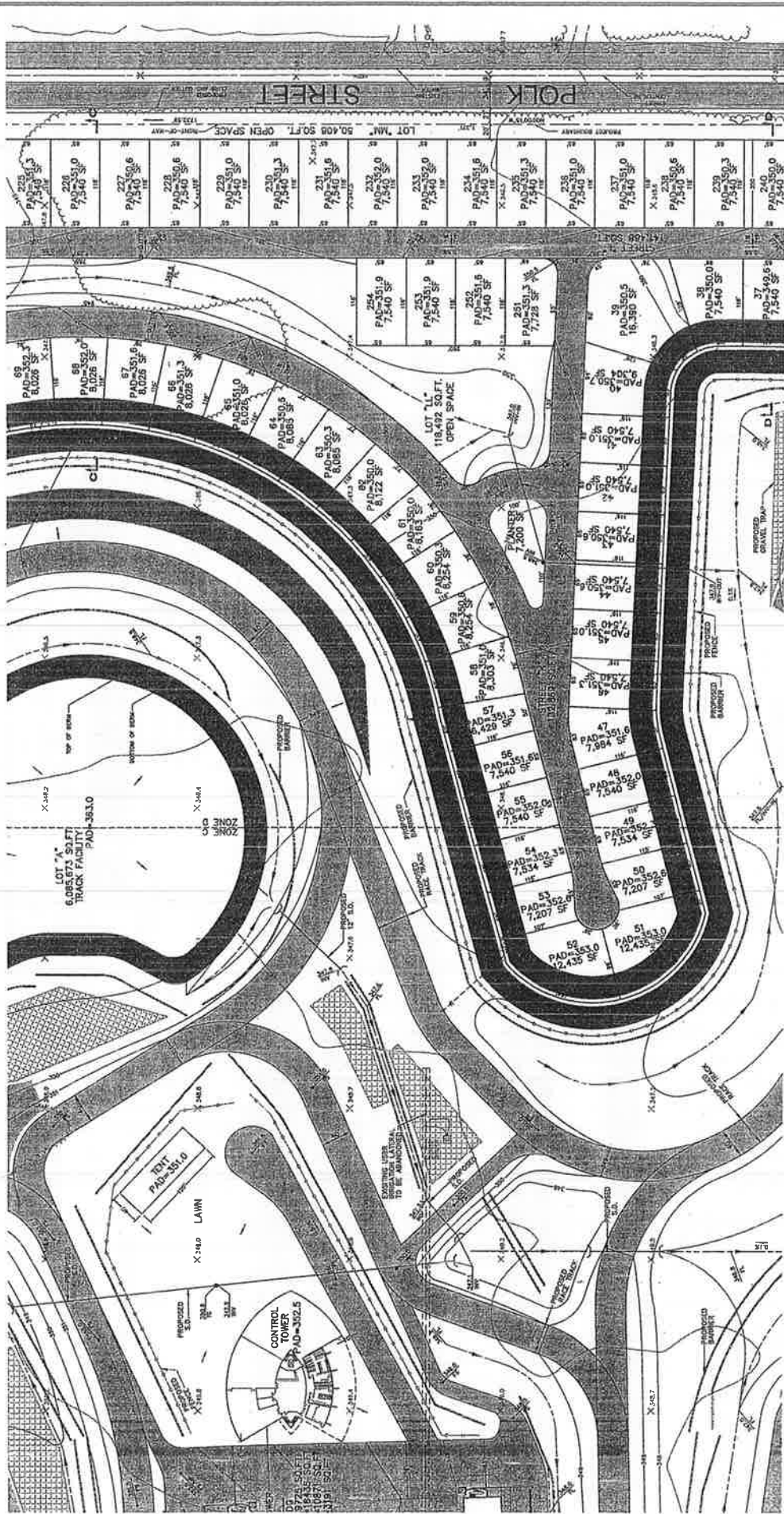
**THERMAL MOTORSPORTS TRACK & CLUB**  
 PREPARED FOR:  
**MR. PHIL CLAYTON**  
**TRM 122, LLC**  
 50-855 WASHINGTON STREET, #2334  
 LA QUINTA, CA 92253  
 (310) 481-4774

AMOUNTS  
 \$2,000,000

**AERIAL INFO:**  
 COMPILED BY AERIAL MAPPING CORPORATION  
 10-22-08  
 FROM AERIAL PHOTOGRAPHY DATED 10-22-08  
 AND 5' S.E. OF P.M. 8007580  
 SAN JUAN CAUTIN, CA 92508  
 ELEVATION = 375.53  
 CHECKS DRAWN # 500 1972 ADJUSTMENT

DATE: 8/17/10  
 SCALE: 1" = 40' (PLAN)  
 SHEET: 10 OF 15  
 JOB NO. 446

SEE SHEET 12



SEE SHEET 8

**COUNTY OF RIVERSIDE**  
**PLOT PLAN NO.**  
**THERMAL MOTORSPORTS TRACK & CLUB**

ISSUED BY: [Signature]  
 DATE: 8/17/10  
 DRAWN BY: RN  
 DATE: 8/17/10  
 CHECKED BY: [Signature]  
 DATE: 8/17/10

LAND PLANNERS SURVEYORS  
**LAND DEVELOPMENT CONSULTANTS**  
 CIVIL ENGINEERS  
 1850 IMPERIAL VALLEY DRIVE, SUITE 303  
 VISTA, CA 92081-3700  
 TEL: 619-592-7700 FAX: 619-592-7700

**THERMAL MOTORSPORTS TRACK & CLUB**  
 PREPARED FOR:  
**MR. PHIL CLAYTON**  
**TRM 122, LLC**  
 50-855 WASHINGTON STREET, #2234  
 LA QUINTA, CA 92553  
 (310) 486-8774

**LEGEND:**  
 PROJECT BOUNDARY  
 EXISTING RIGHT-OF-WAY  
 ULTIMATE RIGHT-OF-WAY  
 EXISTING CENTERLINE  
 PROPOSED LOT LINES

**GRAPHIC SCALE**  
 1 inch = 40 ft.

**BENCH MARK:** CALTRANS BM NO. RV-1143 L  
 2.77' BRASS DISK IN CONCRETE AT THE S.W. CORNER  
 OF THE INTERSECTION OF WASHINGTON STREET AND  
 TRANSPORTATION 1986 R-148 L 0.3' BELOW GROUND  
 AND 3' SE. OF PFP 5007580  
 ELEVATION = 374.53  
 USGCS DATUM = 500' 1972 ADJUSTMENT.

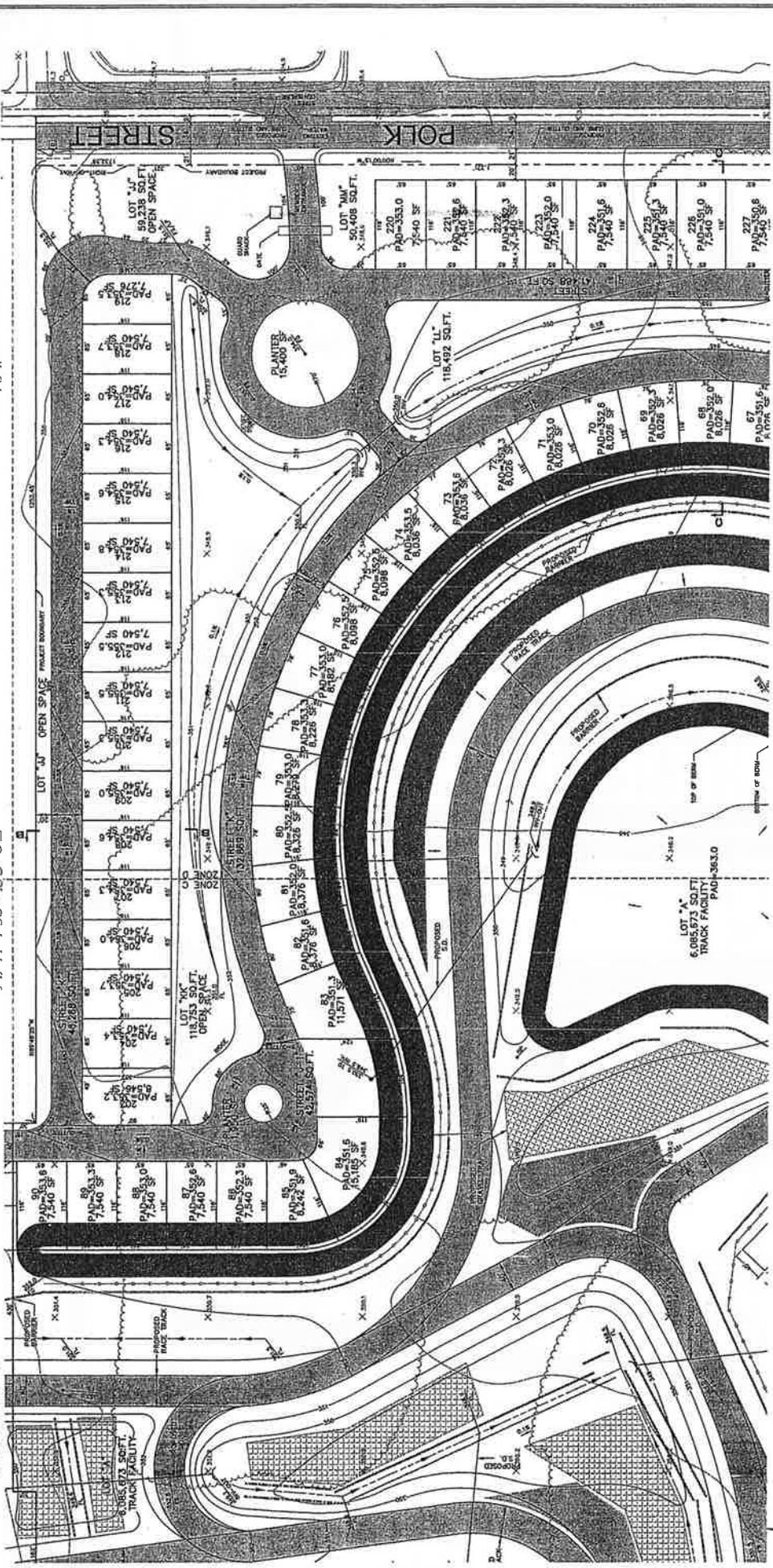
DATE: [ ]  
 DESCRIPTION: [ ]  
 AERIAL INFO:  
 OBTAINED BY: [ ]  
 FROM AERIAL PHOTOGRAPHY DATED: 10-12-09  
 FOR A FILE OF: [ ]  
 SAN BERNARDINO COUNTY  
 (909) 386-2626

SHEET  
**11 OF 15**  
 JOB NO. 484

SEE SHEET 13

APN 750-100-02

NAP



SEE SHEET 9

**COUNTY OF RIVERSIDE**

**PLOT PLAN NO. THERMAL MOTORSPORTS TRACK & CLUB**

DATE: 8/17/10  
 DRAWN BY: RN  
 CHECKED BY: RN  
 DATE: 8/17/10  
 SHEET: 12 OF 15  
 JOB NO. 484

**LAND DEVELOPMENT CONSULTANTS**

LAND PLANNERS SURVEYORS  
 CIVIL ENGINEERS

MR. PHIL GLAYTON  
 TRM-122, LLC  
 50-855 WASHINGTON STREET, #C234  
 LA QUINA, CA 92553  
 (910) 486-4774

DATE: 8/17/10

**THERMAL MOTORSPORTS TRACK & CLUB**

PREPARED FOR: MR. PHIL GLAYTON, TRM-122, LLC

50-855 WASHINGTON STREET, #C234  
 LA QUINA, CA 92553  
 (910) 486-4774

**LEGEND:**

- PROJECT BOUNDARY
- EXISTING RIGHT-OF-WAY
- ULTIMATE RIGHT-OF-WAY
- EXISTING CENTERLINE
- PROPOSED LOT LINES
- EXISTING AVENUE 91/AVE 62 GRADE LINE
- EXISTING SANITARY SEWER
- EXISTING EASEMENT
- PROPOSED SAFETY BARRIERS
- PROPOSED PEDESTRIAN FENCING
- EXISTING AC PAVEMENT / TRACK
- PROPOSED DRIVE, TRAP

**GRAPHIC SCALE**

1 inch = 80 ft.

**AERIAL INFO:**

CALTRANS BM NO. RIV-148 L  
 2 1/2" BRASS DISK IN CONCRETE AT THE S.W. CORNER  
 FROM CORNER PHOTOGRAPHY DATED 10-22-09  
 AND 3' E. OF P.P. 8907980  
 SAN RAFAEL, CA 94908  
 (415) 888-4768

**AMENDMENTS**

DATE	DESCRIPTION

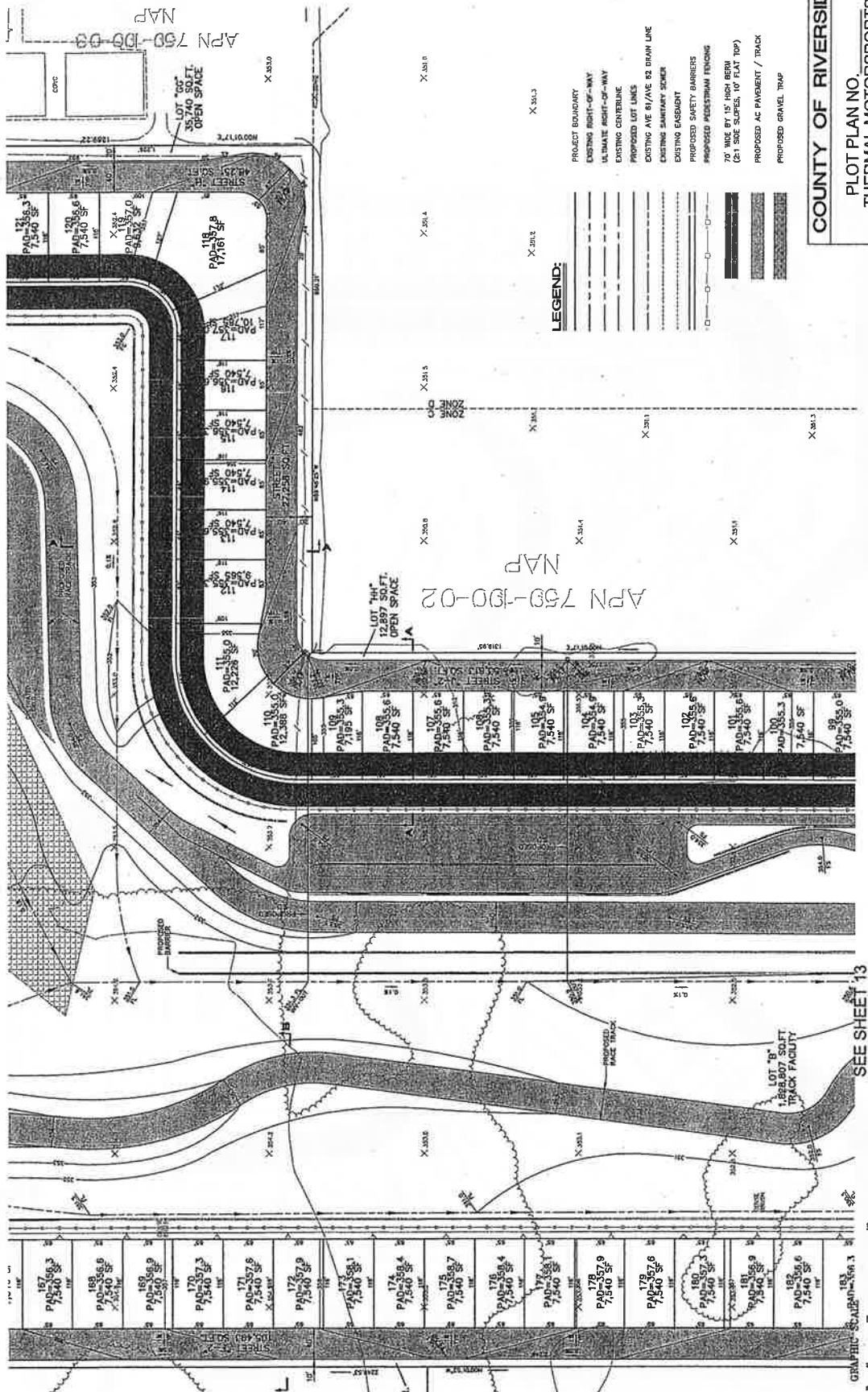
**BERCH MARK:** CALTRANS BM NO. RIV-148 L  
 2 1/2" BRASS DISK IN CONCRETE AT THE S.W. CORNER  
 FROM CORNER PHOTOGRAPHY DATED 10-22-09  
 AND 3' E. OF P.P. 8907980  
 SAN RAFAEL, CA 94908  
 (415) 888-4768

ELEVATION = 375.53  
 USGCS DATUM + 500' 1972 ADJUSTMENT.





SEE SHEET 15



APN 750-100-03  
NAP  
35,740 SQ FT  
OPEN SPACE

APN 750-100-02  
NAP  
LOT 114  
12,197 SQ FT.  
OPEN SPACE

PARCEL 5  
PM NO. 36634  
NAP

PARCEL 6  
PM NO. 36715  
NAP



**COUNTY OF RIVERSIDE**  
PLOT PLAN NO. THERMAL MOTORSPORTS TRACK & CLUB

ISSUED BY	DATE
ISSUED BY	8/17/10
ISSUED BY	8/17/10
ISSUED BY	8/17/10
ISSUED BY	8/17/10

PROJECT NO. 14 OF 15 SHEET

**LAND DEVELOPMENT CONSULTANTS**  
LAND PLANNERS SURVEYORS CIVIL ENGINEERS

THE ENGINEERING CENTER  
2885 SOUTH CALIFORNIA AVENUE, SUITE 305  
CITY OF RIVERSIDE, CALIFORNIA 92507  
TEL: (951) 510-1770 FAX: (951) 510-1771

PREPARED UNDER SUPERVISION OF:  
MRS. M. H. CLAYTON  
50-855 W. TRM RD., #234  
LA GUINIA, CA 92553  
(910) 466-4774

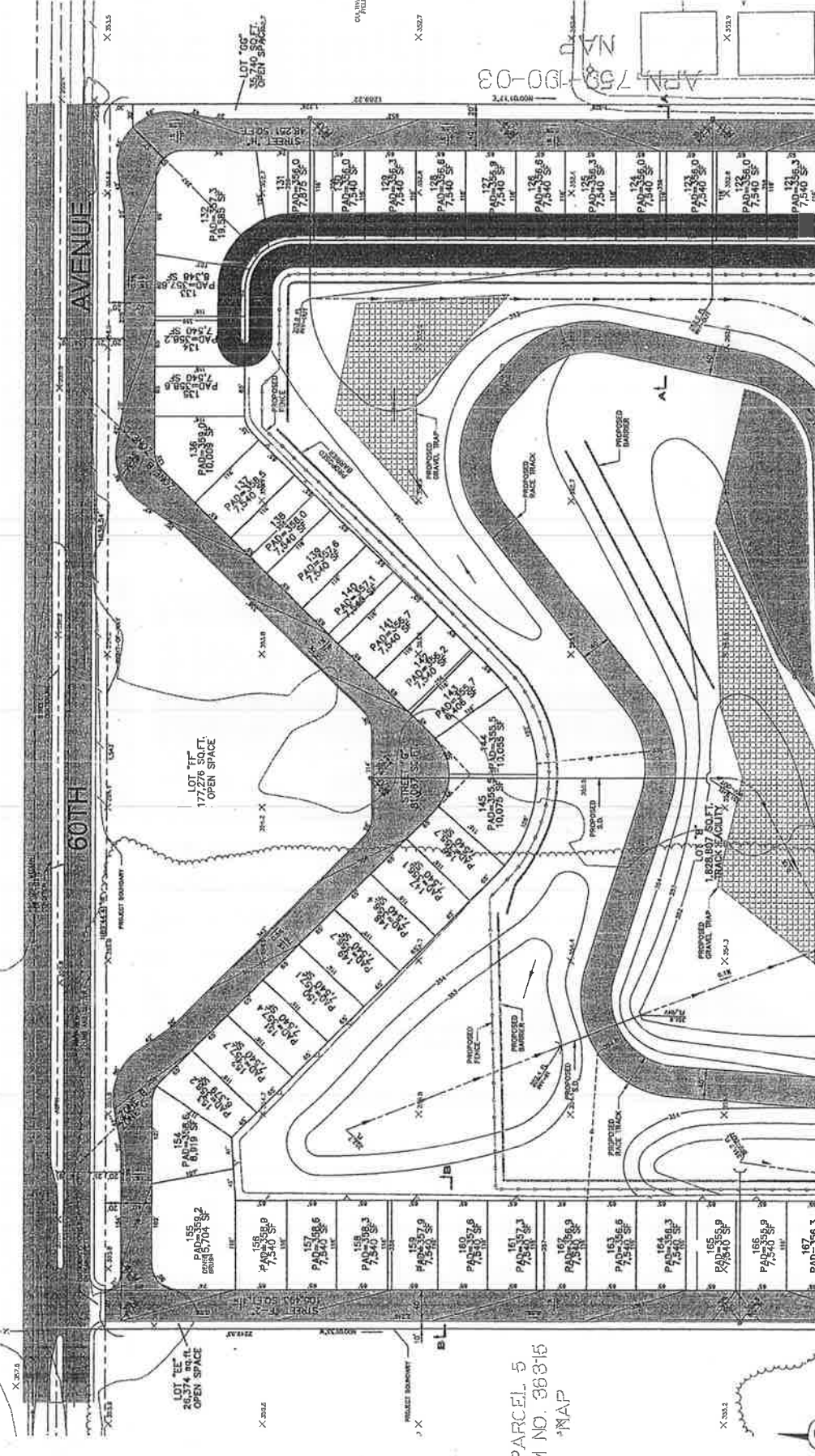
DATE	DESCRIPTION

**BENCH MARK:** CALTRANS BN. NO. RIV-145 L  
7 1/2" W. BY 2" DIA. GALV. STAINLESS CALIFORNIA BENT  
TRANSPARENCY 1985 4-114 L 0.3' BELOW GROUND  
AND 3' E.L. OF PM# 36750  
ELEVATION = 375.33  
USCGS DATUM + 500' 1972 ADJUSTMENT.

**AERIAL INFO:**  
COURTESY OF AERIALIMAGING CORPORATION  
FROM AERIAL PHOTOGRAPHY DATED 10-22-09  
30" MAG. 34" COV. 2408  
SAN 500' 1972 ADJUST.  
(909) 288-7200

- LEGEND:**
- PROJECT BOUNDARY
  - EXISTING RIGHT-OF-WAY
  - ULTIMATE RIGHT-OF-WAY
  - EXISTING CENTERLINE
  - PROPOSED LOT LINES
  - EXISTING AVE 81/AVE 82 DRAIN LINE
  - EXISTING SANITARY SOMER
  - EXISTING EASEMENT
  - PROPOSED SAFETY BARRIERS
  - PROPOSED PEDESTRIAN FENCING
  - 70" WIDE BY 15" HIGH BERM (2:1 SIDE SLOPES, 10' FLAT TOP)
  - PROPOSED AC PAVEMENT / TRACK
  - PROPOSED GRAVEL TRAP

60TH AVENUE



PARCEL 5  
PM NO. 86316  
MAP

**COUNTY OF RIVERSIDE**  
PLOT PLAN NO.  
THERMAL MOTORSPORTS  
TRACK & CLUB

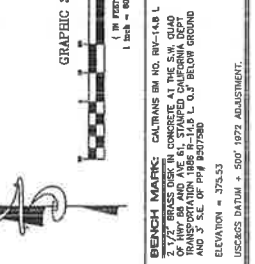
LAND PLANNERS  
SURVEYORS  
CIVIL ENGINEERS  
**LAND DEVELOPMENT CONSULTANTS**  
1550 BEECHHOLLOW DRIVE, SUITE 303  
CANA, CA 92523  
TEL: 951-887-7700 FAX: 951-887-7707

PREPARED FOR:  
**THERMAL MOTORSPORTS TRACK & CLUB**  
MR. PHIL CLAYTON  
TRM 122, LLC  
50-355 WASHINGTON STREET, #234  
LA QUINTA, CA 92553  
(310) 486-4774

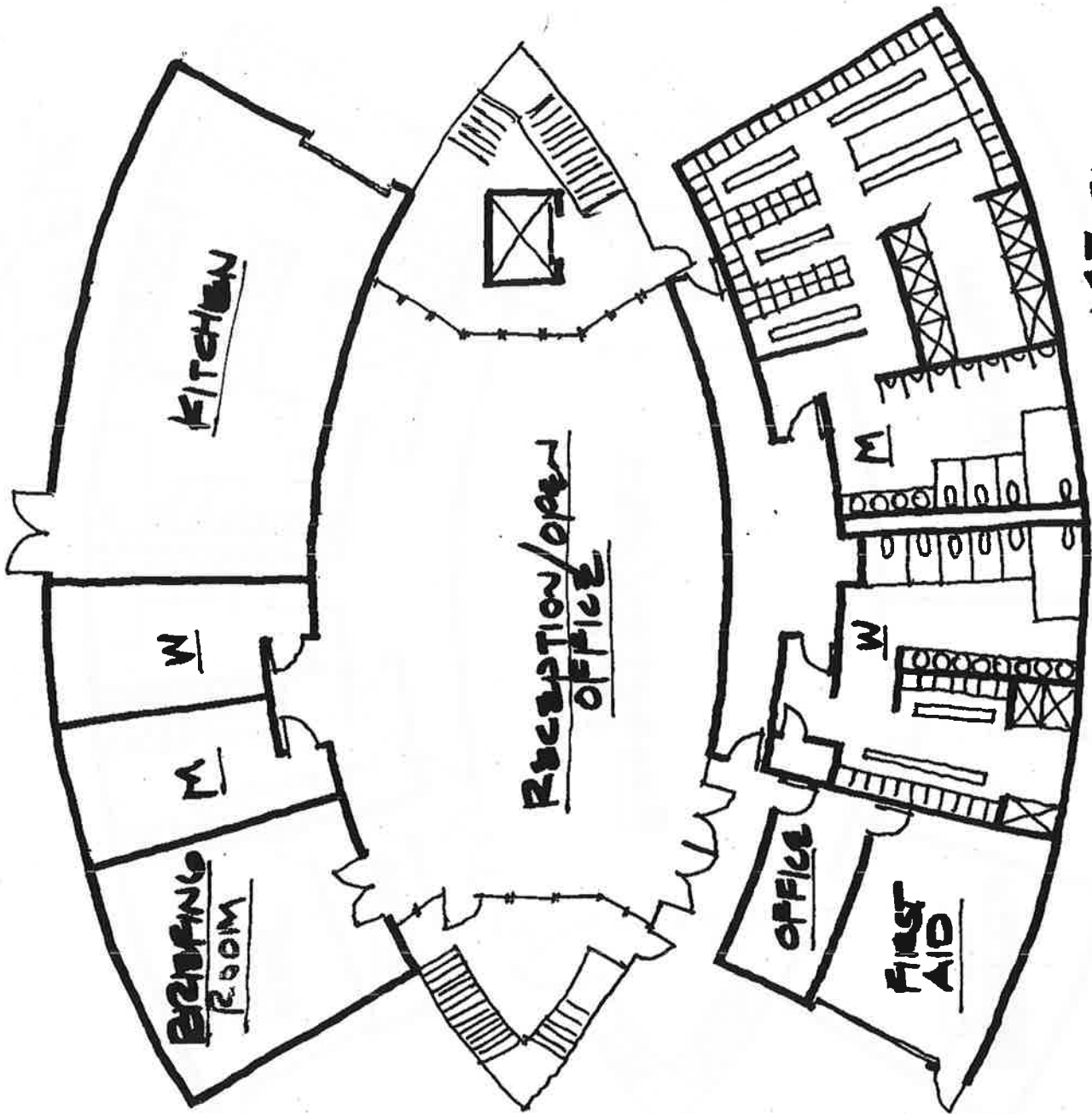
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DESCRIPTION: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DESCRIPTION: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DESCRIPTION: \_\_\_\_\_

**BENCH MARK:** CALTRANS BM NO. RV-148 L  
2 1/2" BRASS DISK IN CONCRETE AT THE S.W. CORNER  
OF THE INTERSECTION OF 60TH AVENUE AND 3RD STREET  
ELEVATION = 375.53  
USGCS DATUM + 500' 1972 ADJUSTMENT.

**LEGEND:**  
PROJECT BOUNDARY  
EXISTING RIGHT-OF-WAY  
ULTIMATE RIGHT-OF-WAY  
EXISTING CENTERLINE  
PROPOSED LOT LINES  
EXISTING 60' AVE 62' DRAIN LINE  
EXISTING SANITARY SEWER  
EXISTING EASEMENT  
PROPOSED SAFETY BARRIERS  
PROPOSED RECREATION FENCING  
7' WIDE BY 15" HIGH BERM  
(2:1) SLOPE, TO FLAT TOP  
PROPOSED AC PAVEMENT / TRACK  
PROPOSED GRAVEL TRAP



**GRAPHIC SCALE**  
1" = 40' FT.  
1" = 40' FT.



1ST FLOOR  
NTS

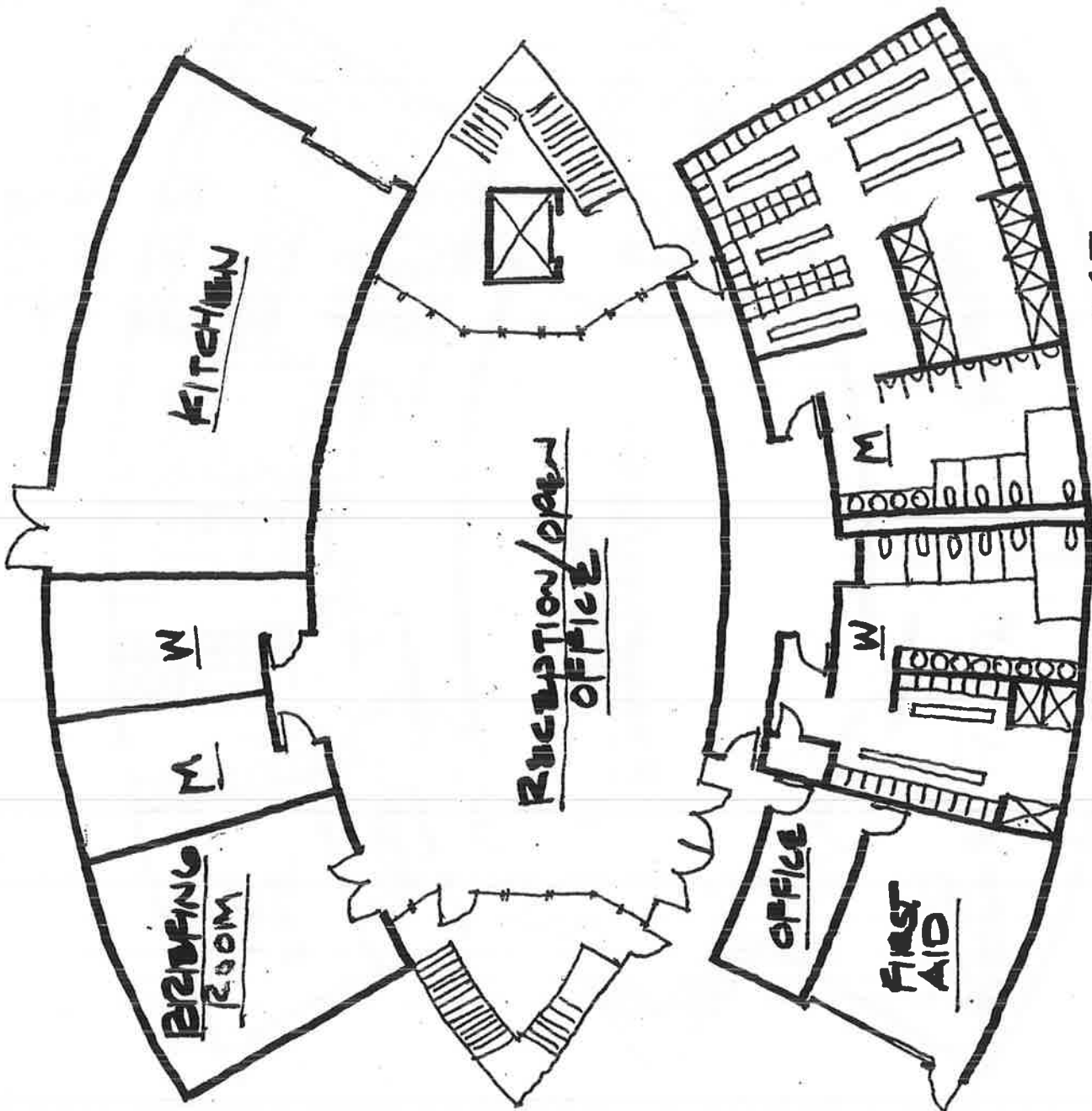
THERMAL MOTOR SPORTS  
10006.00

R<sup>2</sup>A Architecture  
2900 Bristol Street, Suite E205  
Costa Mesa, CA 92626-7909  
ph: 714.435.0380  
fx: 714.435.0383

DATE: 09/23/10



SHEET: 2 of 5



1ST FLOOR  
NTS

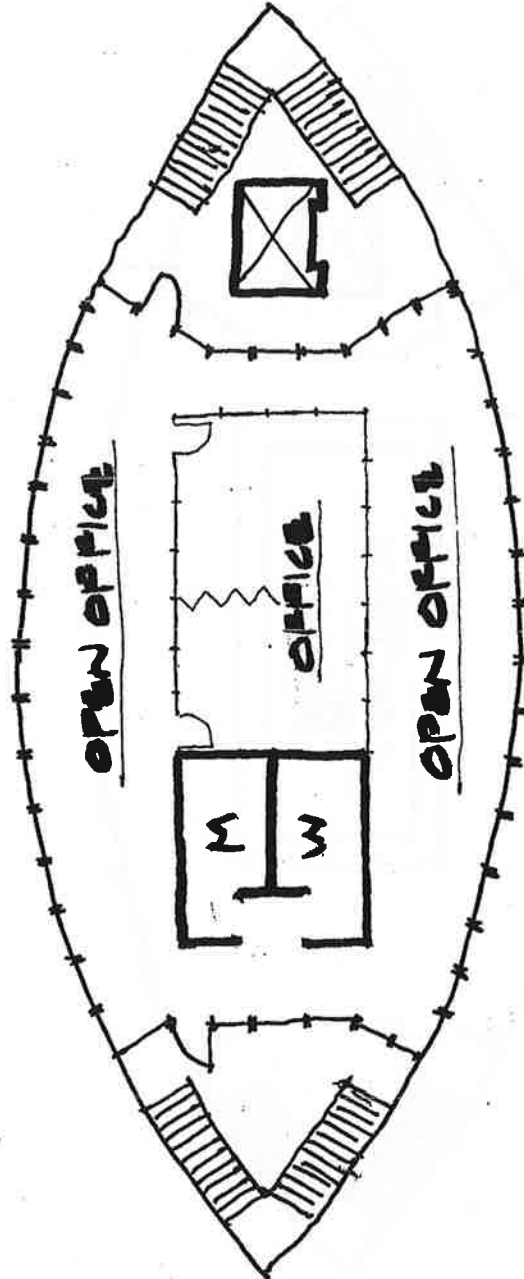
THERMAL MOTOR SPORTS  
10006.00

R<sup>2</sup>A Architecture  
2900 Bristol Street, Suite E205  
Costa Mesa, CA 92626-7909  
ph: 714.435.0380  
fx: 714.435.0383

DATE: 10.13.10



SHEET: 1 of 4



2<sup>ND</sup> FLR  
 N.T.S.

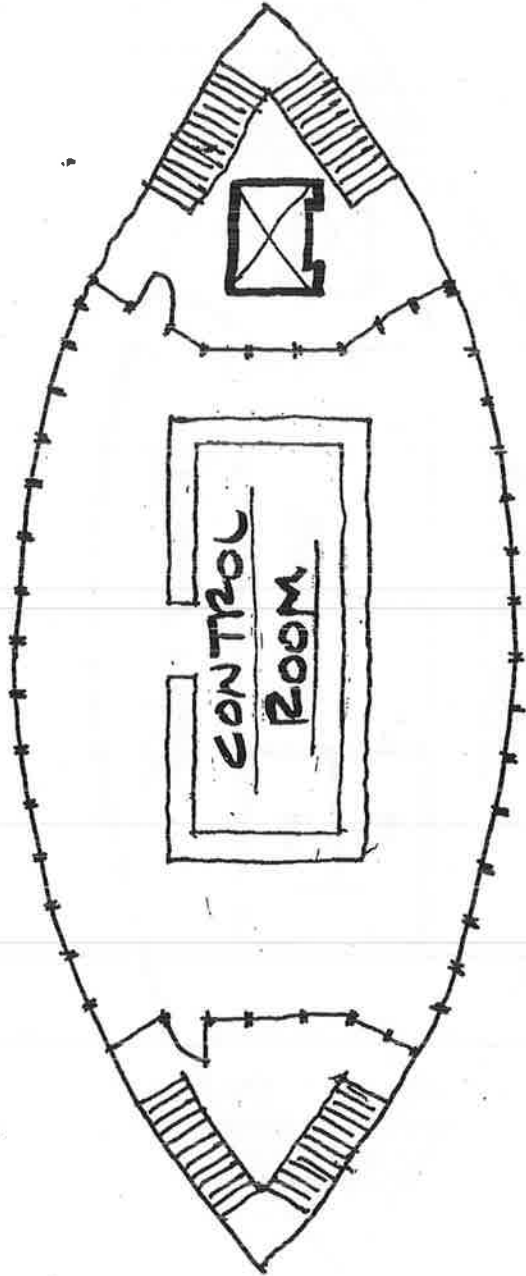
THERMAL MOTOR SPORTS  
 10006.00

R<sup>2</sup>A Architecture  
 2900 Bristol Street, Suite E205  
 Costa Mesa, CA 92626-7909  
 ph: 714.435.0380  
 fx: 714.435.0383

DATE: 10.13.10



SHEET: 2 of 4



3<sup>RD</sup> FLR  
N.T.S.

THERMAL MOTOR SPORTS  
10006.00

R<sup>2</sup>A Architecture  
2900 Bristol Street, Suite E205  
Costa Mesa, CA 92626-7909  
ph: 714.435.0380  
fx: 714.435.0383

DATE: 10.13.10

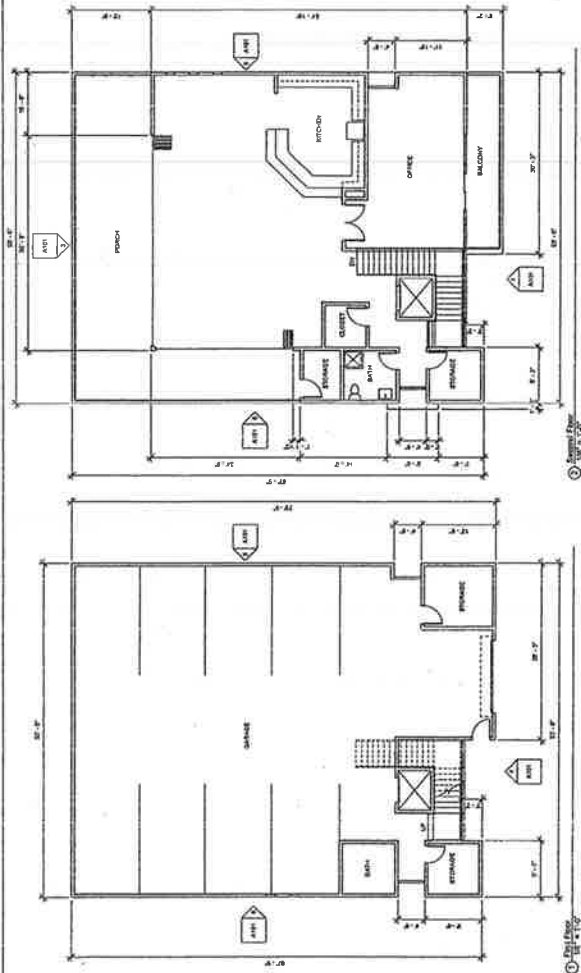


SHEET: 3 of 4

Revised Control Tower Occupancy Calculations  
13-Oct-10

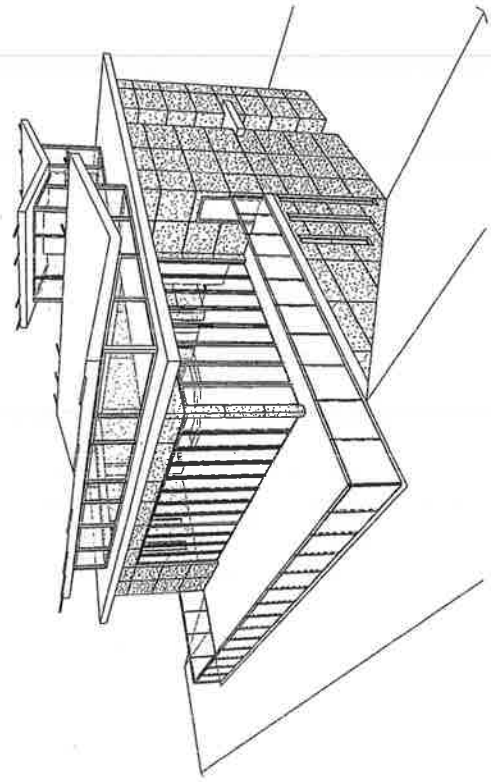
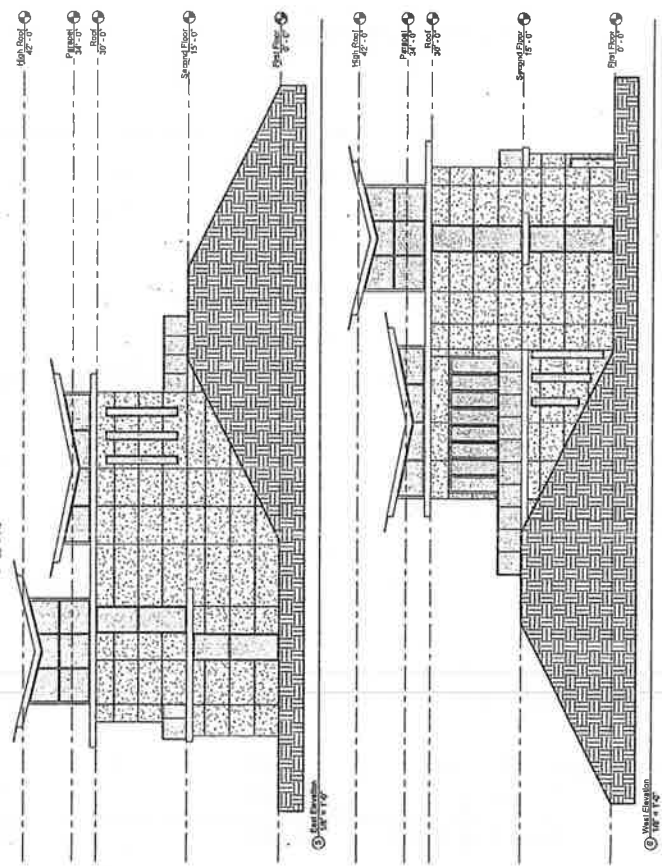
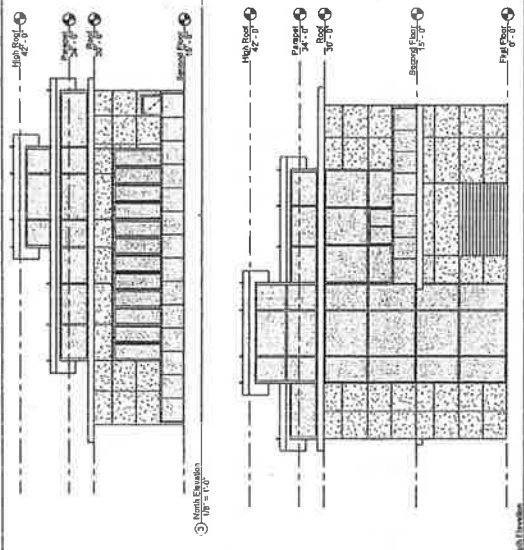
	Area (sf)	sf/Occ	Max Occ	Adjust Factor*	Net Occ
<b>1st Floor</b>					
Reception/Office	2,147	100	21	50%	11
Briefing Room	1,150	20	58	0%	58
Kitchen	1,248	200	6	0%	6
Mens Locker Room	1,227	50	25	0%	25
Women's Locker Room	689	50	14	0%	14
First Aid	432	240	2	0%	2
Office	200	100	2	50%	1
<b>Subtotal</b>	<b>7,093</b>		<b>127</b>		<b>116</b>
<b>2nd Floor</b>					
Reception/Office	3,000	100	30	50%	15
Office	300	100	3	50%	2
Office	275	100	3	50%	1
Office	240	100	2	50%	1
<b>Subtotal</b>	<b>3,815</b>		<b>38</b>		<b>19</b>
<b>3rd Floor (Removed)</b>					
Control Room - Lower	-	100	-	50%	-
<b>Subtotal</b>	<b>-</b>		<b>-</b>		<b>-</b>
<b>New 3rd Floor</b>					
Control Room - Upper	3,000	100	30	50%	15
<b>Subtotal</b>	<b>3,000</b>		<b>30</b>		<b>15</b>
<b>Building Total</b>	<b>13,908</b>		<b>195</b>		<b>150</b>

\*Adjustment Factor is applied by ALUC to Office and Retail uses. Assumes that these uses are normally occupied at 50% of maximum occupancy.



**BUILDING DATA**

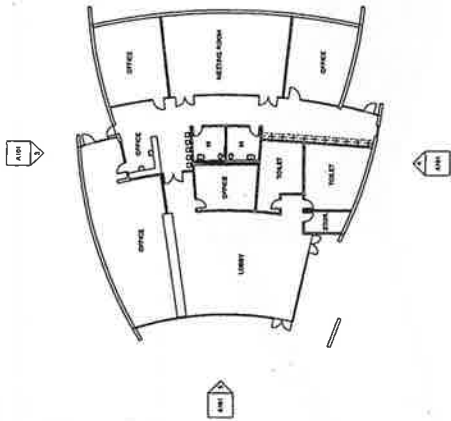
NAME: [REDACTED]  
 ADDRESS: [REDACTED]  
 CITY: [REDACTED]  
 COUNTY: [REDACTED]  
 PROJECT NO.: [REDACTED]  
 DATE: [REDACTED]



**MEMBER'S ENTERTAINMENT GARAGE**

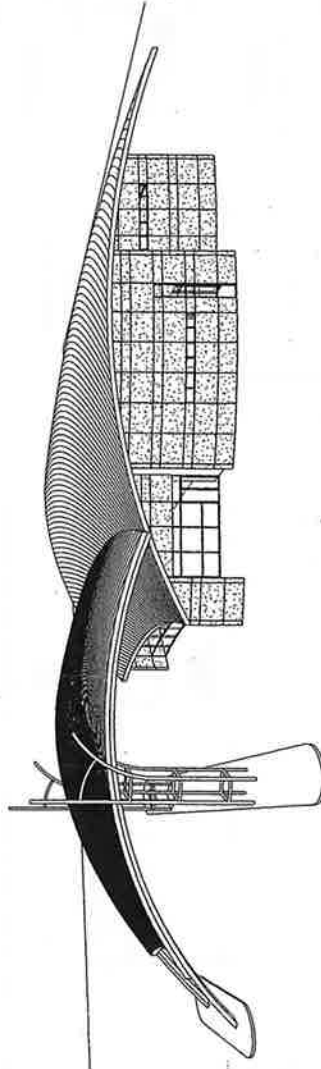
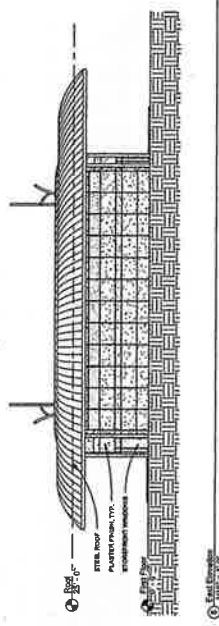
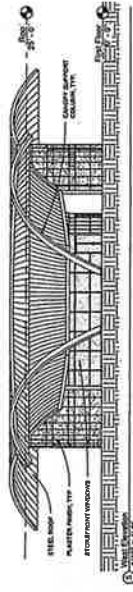
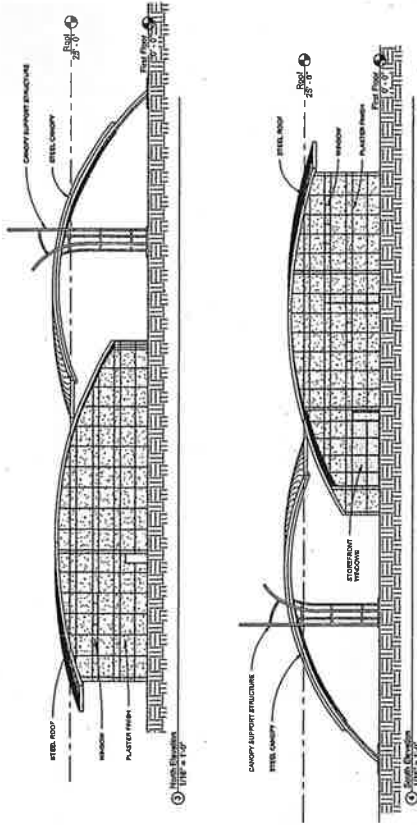
Thermal Motorsports Park  
Thermal, California





**BUILDING DATA**

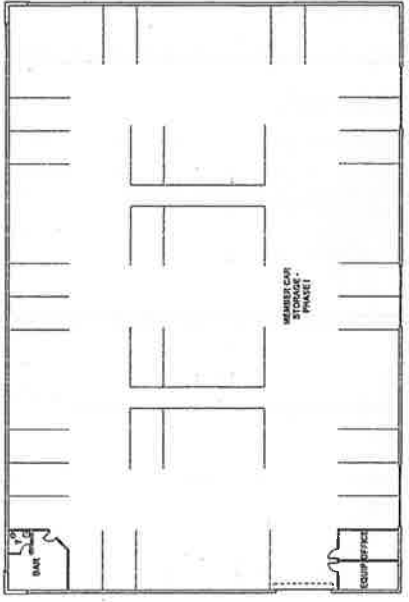
AREA: 1,800 S.F.  
 OCCUPANCY CLASSIFICATION: B  
 CONSTRUCTION TYPE: IV  
 FULLY SPRINKLERED



**REGISTRATION BUILDING**

Thermal Motorsports Park  
 Thermal, California

A101

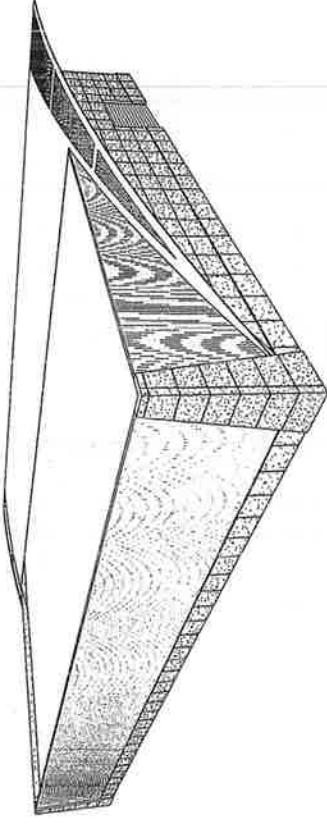


A102

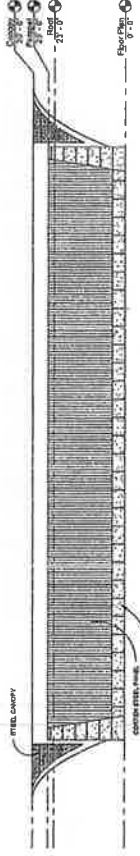
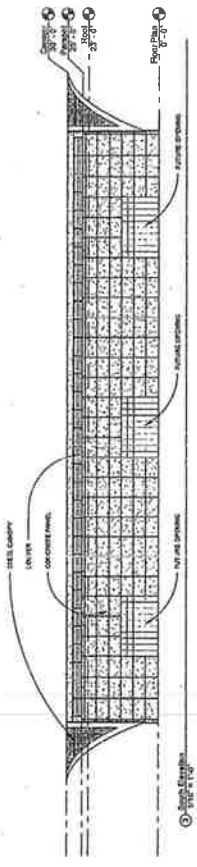
A103

**BUILDING DATA**  
 PROJECT NO. 11-0111 - 11/11/11  
 DRAWING NO. 11-0111-11-11  
 CONSTRUCTION TYPE III  
 FULLY SPANLED  
 ALL DIMENSIONS IN FEET AND INCHES  
 UNLESS OTHERWISE NOTED

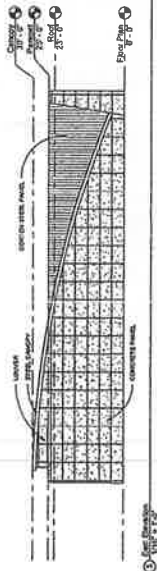
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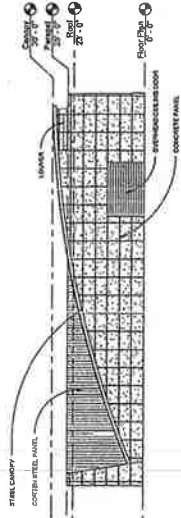
A105



A103



A105

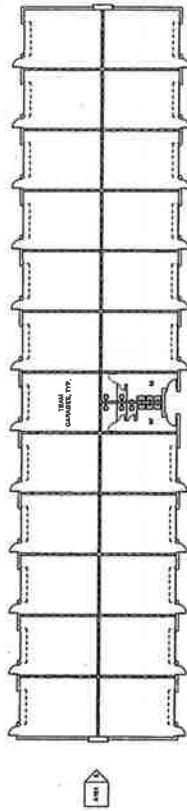


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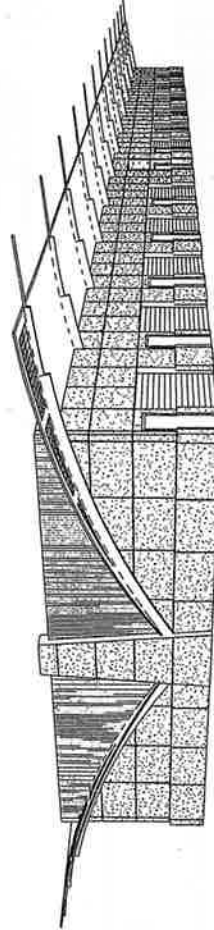
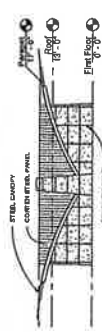
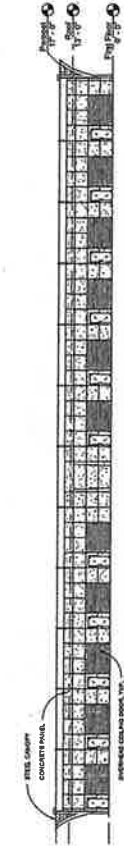
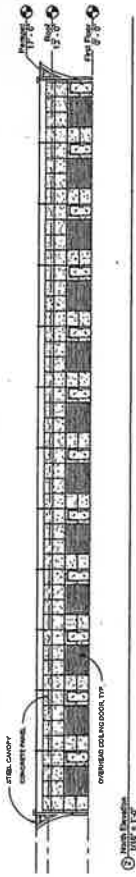
# MEMBER CAR STORAGE

## Thermal Motorsports Park Thermal, California





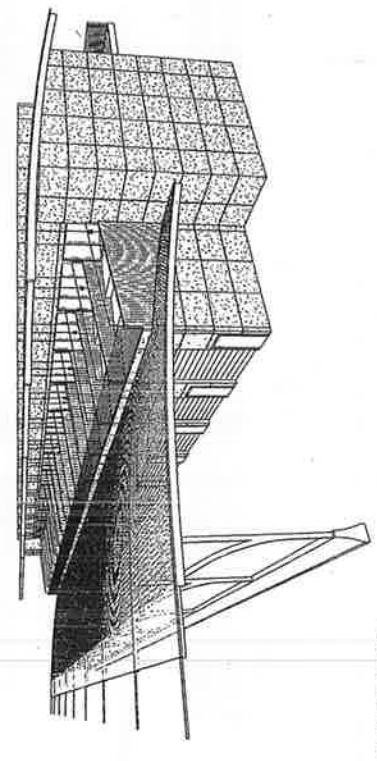
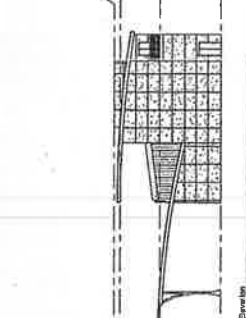
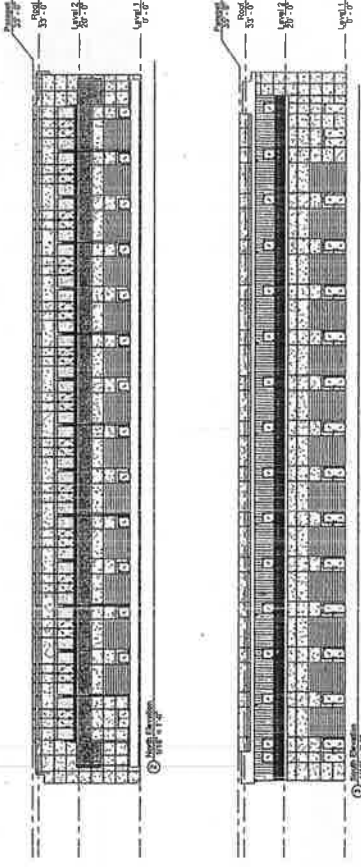
**BUILDING DATA**  
 100' x 100'  
 100' x 100'  
 100' x 100'  
 100' x 100'  
 100' x 100'  
 100' x 100'



**TEAM GARAGES**

Thermal Motorsports Park  
 Thermal, California

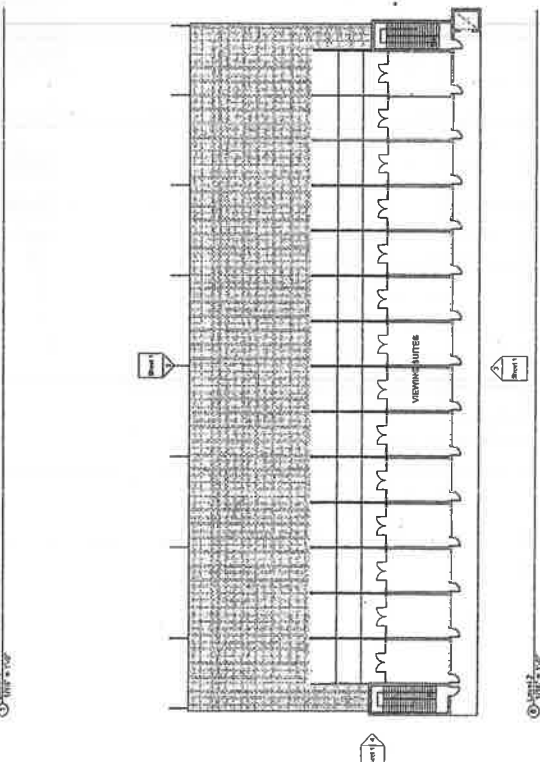
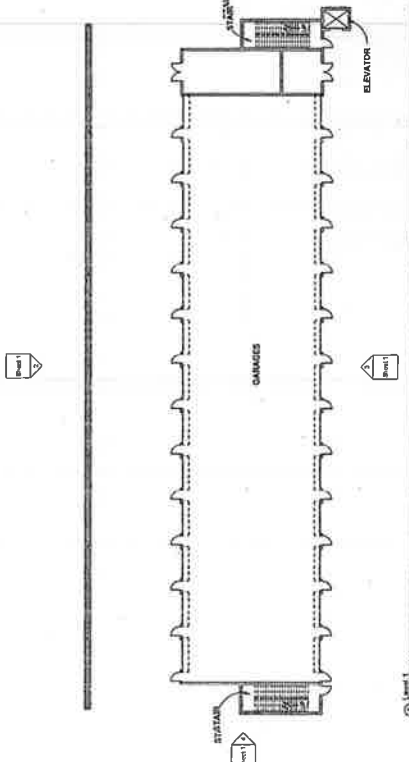




**BUILDING DATA**

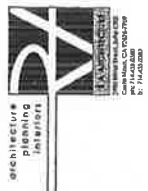
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 CODE: ANS 2000-15  
 CODE: ANS 2000-15  
 CODE: ANS 2000-15

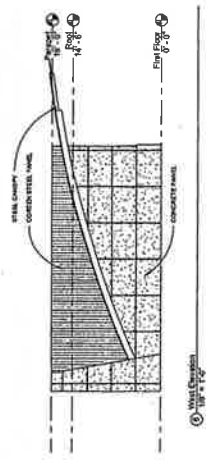
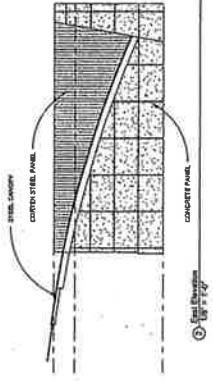
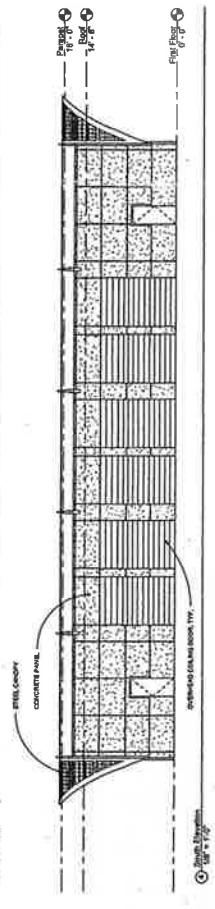
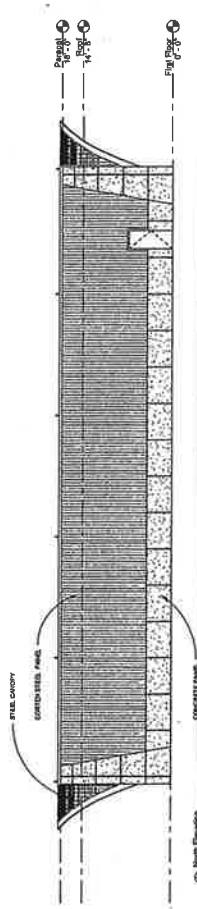
CONSTRUCTION TYPE: IIR  
 ADDED 11/20/00 - 1-20M BUREAU - 3 F.A. IIR  
 FULL SPRINKLERED  
 15' CLEARANCE BETWEEN R.A.  
 15' CLEARANCE  
 ALL STORIES 8' STAIR ENCLAVEMENT AREA  
 INTERCOMPARTMENTED



**TRACK SIDE GARAGES**

Thermal Motorsports Park  
 Thermal, California





**BUILDING DATA**  
 AREA: 4,021 S.F.  
 OCCUPANCY CLASSIFICATION: H  
 CONSTRUCTION TYPE: III  
 FULLY FINISHED  
 WITH CEILING & FLOOR

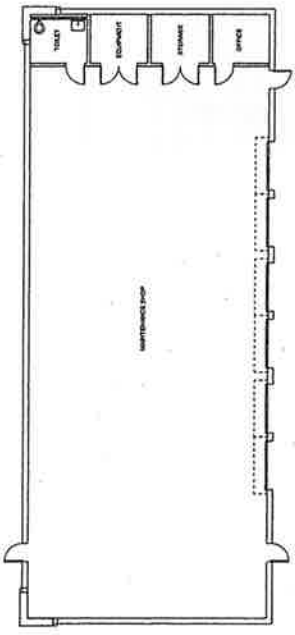


Fig. 102

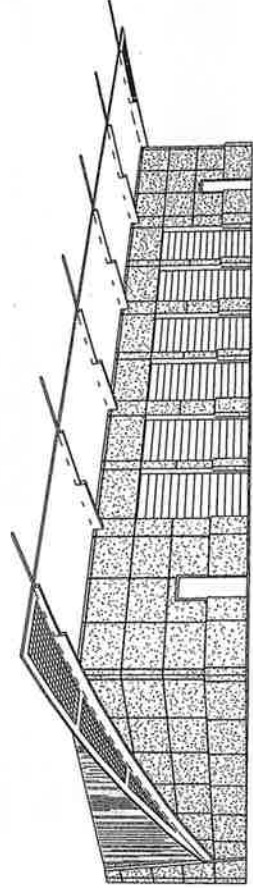
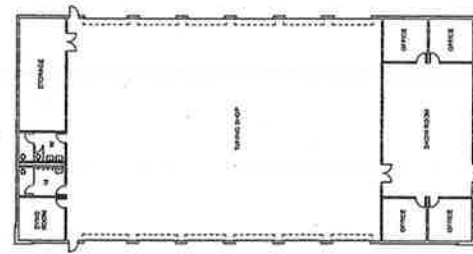


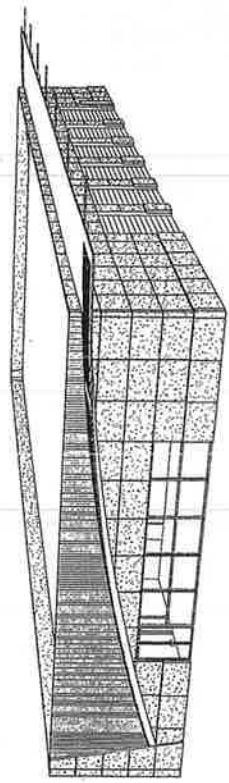
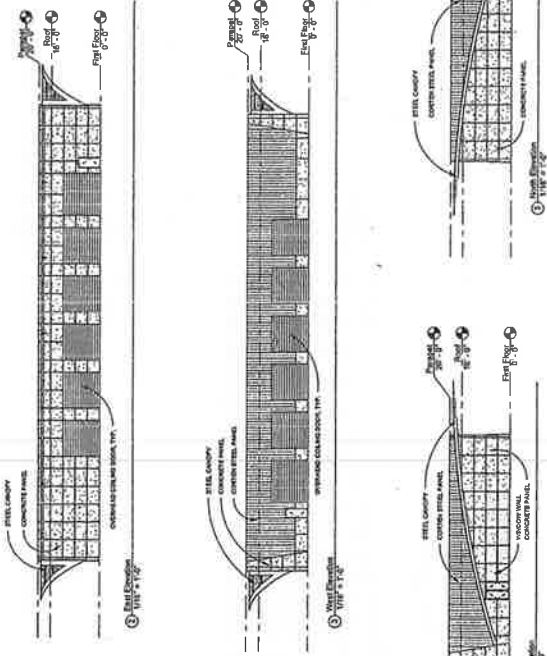
Fig. 103

**MAINTENANCE BUILDING**  
 Thermal Motorsports Park  
 Thermal, California





**BUILDING DATA**  
 AREA: 10,000 SQ FT  
 FOUNDATION: CONCRETE  
 WALLS: CONCRETE  
 ROOF: STEEL DECK WITH INSULATION  
 FLOORING: POLYURETHANE  
 MECH. SYSTEMS: HANGERS, DUCTS, PIPES  
 ALL DIMENSIONS IN FEET AND INCHES



1 - South Perspective

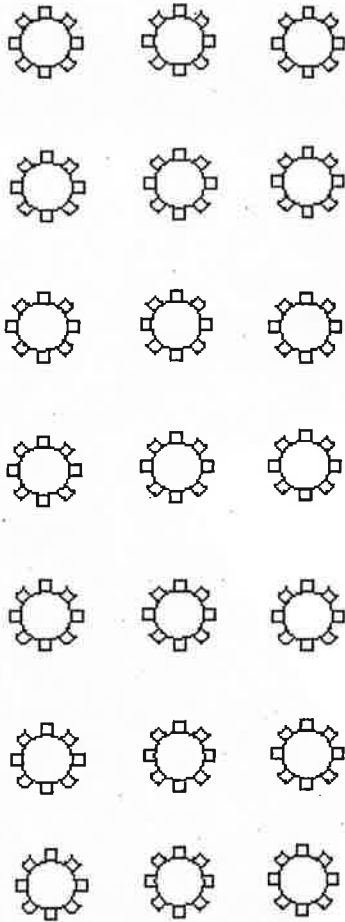
**TUNING SHOP**

Thermal Motorsports Park  
 Thermal, California

120' - 0"

40' - 0"

8' x 12' Stage



PLEASE REFER TO FOLLOWING DRAWINGS FOR EXACT BUILDING DIMENSIONS

REV.	DESCRIPTION	BY	DATE
1	NOT FOR CONSTRUCTION		

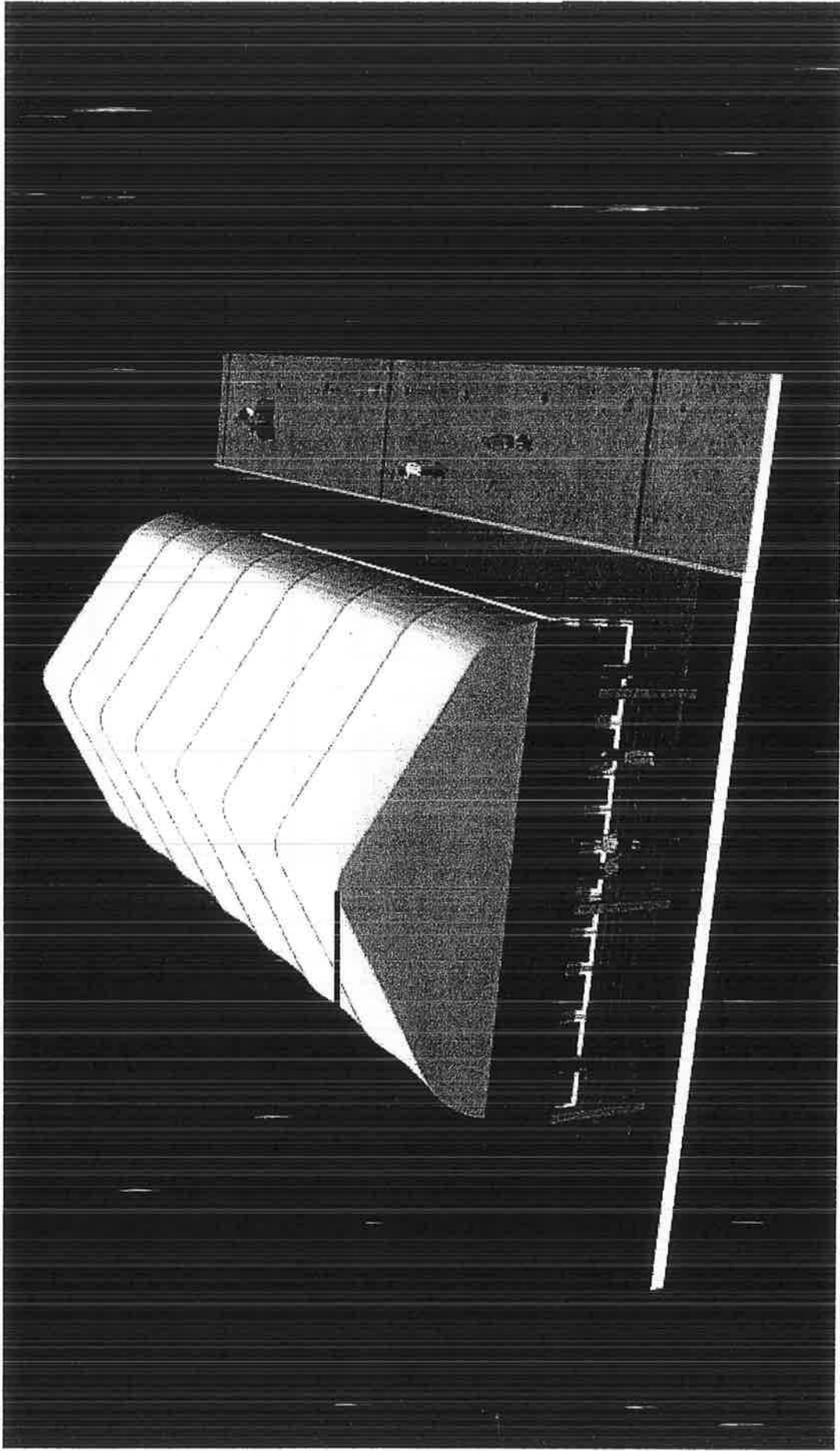
  

DRAWING NOT DESIGNED TO SCALE	
DEALER: Vision Building Systems	
PROJECT ID:	ORDER ID:
SCALE: N.T.S.	
DATE: 10/20/2010	TIME: 10:00 AM
THIS DRAWING IS PROPERTY OF VISION BUILDING SYSTEMS. ANY REPRODUCTION OR USE OF THIS DRAWING WITHOUT THE EXPRESS WRITTEN CONSENT OF VISION BUILDING SYSTEMS, INC. IS PROHIBITED.	

# VISION

## VISION SYSTEMS

PROJECT LAYOUT	
REV.	DATE
1	1
1	1



**Thermal Motorsports Park Track  
Event Tent  
Elevation 1**





Thermal Motorsports Track & Club  
 Plot Plan Submittal to Riverside County ALUC  
 Submitted: September 2010

Buildings Summary Table

Building Site ID	Building Category	Building Type	Wall Materials	Roof Materials	Total Lot Area (s.f.)	ALUC Zone Location	Pad Elevation from Plan (+500 feet over actual elevation)	Planned Pad Ground Elevation (MSL)	Planned Building Height (feet)	Planned Top of Structure (MSL)	-137.5		
											Structure to Planned Runway 35 Elevation	Distance from RWY 35 Centerline Extended	Distance from Future RWY 35 End on Centerline
FL-73	Founders Lot		CC Block	Str Steel	8,036	D	353.6	-146.4	42.0	-104.4	33.1	<750	3671
FL-74	Founders Lot		CC Block	Str Steel	8,036	D	353.5	-146.5	42.0	-104.5	33.0	<750	3609
FL-75	Founders Lot		CC Block	Str Steel	8,098	D	352.5	-147.5	42.0	-105.5	32.0	<750	3518
FL-76	Founders Lot		CC Block	Str Steel	8,098	D	352.5	-147.5	42.0	-105.5	32.0	<750	3442
FL-77	Founders Lot		CC Block	Str Steel	8,182	D	353.0	-147.0	42.0	-105.0	32.5	<750	3371
FL-78	Founders Lot		CC Block	Str Steel	8,226	D	353.3	-146.7	42.0	-104.7	32.8	<750	3305
FL-79	Founders Lot		CC Block	Str Steel	8,279	D	353.0	-147.0	42.0	-105.0	32.5	<750	3246
FL-80	Founders Lot		CC Block	Str Steel	8,326	D	352.5	-147.5	42.0	-105.5	32.0	<750	3196
FL-81	Founders Lot		CC Block	Str Steel	8,376	C	352.0	-148.0	42.0	-106.0	31.5	<750	3155
FL-82	Founders Lot		CC Block	Str Steel	8,376	C	351.6	-148.4	42.0	-106.4	31.1	<750	3125
FL-83	Founders Lot		CC Block	Str Steel	11,571	C	351.3	-148.7	42.0	-106.7	30.8	<750	3107
FL-84	Founders Lot		CC Block	Str Steel	15,185	C	351.6	-148.4	42.0	-106.4	31.1	1,515	3007
FL-85	Founders Lot		CC Block	Str Steel	8,242	C	351.9	-148.1	42.0	-106.1	31.4	1,515	2920
FL-86	Founders Lot		CC Block	Str Steel	7,540	C	352.3	-147.7	42.0	-105.7	31.8	1,515	2865
FL-87	Founders Lot		CC Block	Str Steel	7,540	C	352.6	-147.4	42.0	-105.4	32.1	1,515	2810
FL-88	Founders Lot		CC Block	Str Steel	7,540	C	353.0	-147.0	42.0	-105.0	32.5	1,515	2756
FL-89	Founders Lot		CC Block	Str Steel	7,540	C	353.3	-146.7	42.0	-104.7	32.8	1,515	2702
FL-90	Founders Lot		CC Block	Str Steel	7,540	C	353.6	-146.4	42.0	-104.4	33.1	1,515	2648
FL-91	Founders Lot		CC Block	Str Steel	7,540	C	354.3	-145.7	42.0	-103.7	33.8	1,515	2547
FL-92	Founders Lot		CC Block	Str Steel	7,540	C	354.6	-145.4	42.0	-103.4	34.1	1,515	2495
FL-93	Founders Lot		CC Block	Str Steel	7,540	C	354.9	-145.1	42.0	-103.1	34.4	1,515	2443
FL-94	Founders Lot		CC Block	Str Steel	7,540	C	355.3	-144.7	42.0	-102.7	34.8	1,515	2393
FL-95	Founders Lot		CC Block	Str Steel	7,540	C	355.6	-144.4	42.0	-102.4	35.1	1,515	2343
FL-96	Founders Lot		CC Block	Str Steel	7,540	C	355.6	-144.4	42.0	-102.4	35.1	1,515	2294
FL-97	Founders Lot		CC Block	Str Steel	7,540	C	355.3	-144.7	42.0	-102.7	34.8	1,515	2245
FL-98	Founders Lot		CC Block	Str Steel	7,540	C	355.0	-145.0	42.0	-103.0	34.5	1,515	2198
FL-99	Founders Lot		CC Block	Str Steel	7,540	C	355.0	-145.0	42.0	-103.0	34.5	1,515	2151
FL-100	Founders Lot		CC Block	Str Steel	7,540	C	355.3	-144.7	42.0	-102.7	34.8	1,515	2106
FL-101	Founders Lot		CC Block	Str Steel	7,540	C	355.6	-144.4	42.0	-102.4	35.1	1,515	2061
FL-102	Founders Lot		CC Block	Str Steel	7,540	C	355.6	-144.4	42.0	-102.4	35.1	1,515	2018
FL-103	Founders Lot		CC Block	Str Steel	7,540	C	355.3	-144.7	42.0	-102.7	34.8	1,515	1975
FL-104	Founders Lot		CC Block	Str Steel	7,540	C	354.9	-145.1	42.0	-103.1	34.4	1,515	1934
FL-105	Founders Lot		CC Block	Str Steel	7,540	C	354.9	-145.1	42.0	-103.1	34.4	1,515	1895
FL-106	Founders Lot		CC Block	Str Steel	7,540	C	355.3	-144.7	42.0	-102.7	34.8	1,515	1856
FL-107	Founders Lot		CC Block	Str Steel	7,540	C	355.6	-144.4	42.0	-102.4	35.1	1,515	1820
FL-108	Founders Lot		CC Block	Str Steel	7,540	C	355.6	-144.4	42.0	-102.4	35.1	1,515	1785
FL-109	Founders Lot		CC Block	Str Steel	7,195	C	355.3	-144.7	42.0	-102.7	34.8	1,515	1751
FL-110	Founders Lot		CC Block	Str Steel	12,388	C	355.0	-145.0	42.0	-103.0	34.5	1,515	1724
FL-111	Founders Lot		CC Block	Str Steel	12,226	C	355.0	-145.0	42.0	-103.0	34.5	<750	1739
FL-112	Founders Lot		CC Block	Str Steel	9,665	C	355.3	-144.7	42.0	-102.7	34.8	<750	1858
FL-113	Founders Lot		CC Block	Str Steel	7,540	C	355.6	-144.4	42.0	-102.4	35.1	<750	1936
FL-114	Founders Lot		CC Block	Str Steel	7,540	C	355.6	-144.4	42.0	-102.4	35.1	<750	1996
FL-115	Founders Lot		CC Block	Str Steel	7,540	C	356.3	-143.7	42.0	-101.7	35.8	<750	2058
FL-116	Founders Lot		CC Block	Str Steel	7,540	C	356.6	-143.4	42.0	-101.4	36.1	<750	2119
FL-117	Founders Lot		CC Block	Str Steel	10,785	C	357.0	-143.0	42.0	-101.0	36.5	<750	2180
FL-118	Founders Lot		CC Block	Str Steel	17,161	C	357.8	-142.2	42.0	-100.2	37.3	<750	2240
FL-119	Founders Lot		CC Block	Str Steel	9,632	C	357.0	-143.0	42.0	-101.0	36.5	<750	2261
FL-120	Founders Lot		CC Block	Str Steel	7,540	C	356.6	-143.4	42.0	-101.4	36.1	<750	2249
FL-121	Founders Lot		CC Block	Str Steel	7,540	C	356.3	-143.7	42.0	-101.7	35.8	2167	2232
FL-122	Founders Lot		CC Block	Str Steel	7,540	C	356.0	-144.0	42.0	-102.0	35.5	2167	2217
FL-123	Founders Lot		CC Block	Str Steel	7,540	C	356.0	-144.0	42.0	-102.0	35.5	2167	2205
FL-124	Founders Lot		CC Block	Str Steel	7,540	C	356.0	-144.0	42.0	-102.0	35.5	2167	2193
FL-125	Founders Lot		CC Block	Str Steel	7,540	C	356.3	-143.7	42.0	-101.7	35.8	2167	2184
FL-126	Founders Lot		CC Block	Str Steel	7,540	C	356.6	-143.4	42.0	-101.4	36.1	2167	2177
FL-127	Founders Lot		CC Block	Str Steel	7,540	C	356.9	-143.1	42.0	-101.1	36.4	2167	2172
FL-128	Founders Lot		CC Block	Str Steel	7,540	C	356.6	-143.4	42.0	-101.4	36.1	2167	2168
FL-129	Founders Lot		CC Block	Str Steel	7,540	C	356.3	-143.7	42.0	-101.7	35.8	2167	2167
FL-130	Founders Lot		CC Block	Str Steel	7,540	C	356.0	-144.0	42.0	-102.0	35.5	2167	2170
FL-131	Founders Lot		CC Block	Str Steel	7,875	C	356.0	-144.0	42.0	-102.0	35.5	2167	2164
FL-132	Founders Lot		CC Block	Str Steel	19,585	C	357.3	-142.7	42.0	-100.7	36.8	<750	2138
FL-133	Founders Lot		CC Block	Str Steel	8,384	C	357.6	-142.4	42.0	-100.4	37.1	<750	2077
FL-134	Founders Lot		CC Block	Str Steel	7,540	C	358.2	-141.8	42.0	-99.8	37.7	<750	2013
FL-135	Founders Lot		CC Block	Str Steel	7,540	C	358.6	-141.4	42.0	-99.4	38.1	<750	1951
FL-136	Founders Lot		CC Block	Str Steel	10,009	C	359.0	-141.0	42.0	-99.0	38.5	<750	1844
FL-137	Founders Lot		CC Block	Str Steel	7,540	C	358.5	-141.5	42.0	-99.5	38.0	<750	1793
FL-138	Founders Lot		CC Block	Str Steel	7,540	C	358.0	-142.0	42.0	-100.0	37.5	<750	1742
FL-139	Founders Lot		CC Block	Str Steel	7,540	C	357.6	-142.4	42.0	-100.4	37.1	<750	1692
FL-140	Founders Lot		CC Block	Str Steel	7,540	C	357.1	-142.9	42.0	-100.9	36.6	<750	1643
FL-141	Founders Lot		CC Block	Str Steel	7,540	C	356.7	-143.3	42.0	-101.3	36.2	<750	1596
FL-142	Founders Lot		CC Block	Str Steel	7,540	C	356.2	-143.8	42.0	-101.8	35.7	<750	1550
FL-143	Founders Lot		CC Block	Str Steel	6,406	C	355.7	-144.3	42.0	-102.3	35.2	<750	1521
FL-144	Founders Lot		CC Block	Str Steel	10,055	C	355.5	-144.5	42.0	-102.5	35.0	1472	1472

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											Structure to Planned Runway 35 Elevation	Distance from RWY 35 Centerline Extended	Distance from Future RWY 35 End on Centerline
FL-145	Founders Lot		CC Block	Str Steel	10,075	C	355.5	-144.5	42.0	-102.5	35.0	<750	1359
FL-146	Founders Lot		CC Block	Str Steel	7,540	C	355.7	-144.3	42.0	-102.3	35.2	<750	1309
FL-147	Founders Lot		CC Block	Str Steel	7,540	C	356.1	-143.9	42.0	-101.9	35.6	<750	1260
FL-148	Founders Lot		CC Block	Str Steel	7,540	C	356.4	-143.6	42.0	-101.6	35.9	1214	1214
FL-149	Founders Lot		CC Block	Str Steel	7,540	C	356.7	-143.3	42.0	-101.3	36.2	<750	1168
FL-150	Founders Lot		CC Block	Str Steel	7,540	C	357.1	-142.9	42.0	-100.9	36.6	<750	1125
FL-151	Founders Lot		CC Block	Str Steel	7,540	C	357.4	-142.6	42.0	-100.6	36.9	<750	1085
FL-152	Founders Lot		CC Block	Str Steel	7,540	C	357.7	-142.3	42.0	-100.3	37.2	<750	1047
FL-153	Founders Lot		CC Block	Str Steel	6,379	C	358.2	-141.8	42.0	-99.8	37.7	<750	1022
FL-154	Founders Lot		CC Block	Str Steel	8,919	C	358.6	-141.4	42.0	-99.4	38.1	<750	981
FL-155	Founders Lot		CC Block	Str Steel	15,704	C	359.2	-140.8	42.0	-98.8	38.7	800	828
FL-156	Founders Lot		CC Block	Str Steel	7,540	C	358.9	-141.1	42.0	-99.1	38.4	800	813
FL-157	Founders Lot		CC Block	Str Steel	7,540	C	358.6	-141.4	42.0	-99.4	38.1	800	814
FL-158	Founders Lot		CC Block	Str Steel	7,540	C	358.3	-141.7	42.0	-99.7	37.8	800	804
FL-159	Founders Lot		CC Block	Str Steel	7,540	C	357.9	-142.1	42.0	-100.1	37.4	800	800
FL-160	Founders Lot		CC Block	Str Steel	7,540	C	357.6	-142.4	42.0	-100.4	37.1	800	801
FL-161	Founders Lot		CC Block	Str Steel	7,540	C	357.3	-142.7	42.0	-100.7	36.8	800	808
FL-162	Founders Lot		CC Block	Str Steel	7,540	C	356.9	-143.1	42.0	-101.1	36.4	800	819
FL-163	Founders Lot		CC Block	Str Steel	7,540	C	356.6	-143.4	42.0	-101.4	36.1	800	836
FL-164	Founders Lot		CC Block	Str Steel	7,540	C	356.3	-143.7	42.0	-101.7	35.8	800	856
FL-165	Founders Lot		CC Block	Str Steel	7,540	C	355.9	-144.1	42.0	-102.1	35.4	800	882
FL-166	Founders Lot		CC Block	Str Steel	7,540	C	355.9	-144.1	42.0	-102.1	35.4	800	911
FL-167	Founders Lot		CC Block	Str Steel	7,540	C	356.3	-143.7	42.0	-101.7	35.8	800	944
FL-168	Founders Lot		CC Block	Str Steel	7,540	C	356.6	-143.4	42.0	-101.4	36.1	800	980
FL-169	Founders Lot		CC Block	Str Steel	7,540	C	356.9	-143.1	42.0	-101.1	36.4	800	1019
FL-170	Founders Lot		CC Block	Str Steel	7,540	C	357.3	-142.7	42.0	-100.7	36.8	800	1061
FL-171	Founders Lot		CC Block	Str Steel	7,540	C	357.6	-142.4	42.0	-100.4	37.1	800	1104
FL-172	Founders Lot		CC Block	Str Steel	7,540	C	357.9	-142.1	42.0	-100.1	37.4	800	1150
FL-173	Founders Lot		CC Block	Str Steel	7,540	C	358.1	-141.9	42.0	-99.9	37.6	800	1198
FL-174	Founders Lot		CC Block	Str Steel	7,540	C	358.4	-141.6	42.0	-99.6	37.9	800	1247
FL-175	Founders Lot		CC Block	Str Steel	7,540	C	358.7	-141.3	42.0	-99.3	38.2	800	1297
FL-176	Founders Lot		CC Block	Str Steel	7,540	C	358.4	-141.6	42.0	-99.6	37.9	800	1349
FL-177	Founders Lot		CC Block	Str Steel	7,540	C	358.1	-141.9	42.0	-99.9	37.6	800	1402
FL-178	Founders Lot		CC Block	Str Steel	7,540	C	357.9	-142.1	42.0	-100.1	37.4	800	1456
FL-179	Founders Lot		CC Block	Str Steel	7,540	C	357.6	-142.4	42.0	-100.4	37.1	800	1510
FL-180	Founders Lot		CC Block	Str Steel	7,540	C	357.3	-142.7	42.0	-100.7	36.8	800	1566
FL-181	Founders Lot		CC Block	Str Steel	7,540	C	356.9	-143.1	42.0	-101.1	36.4	800	1622
FL-182	Founders Lot		CC Block	Str Steel	7,540	C	356.6	-143.4	42.0	-101.4	36.1	800	1679
FL-183	Founders Lot		CC Block	Str Steel	7,540	C	356.3	-143.7	42.0	-101.7	35.8	800	1736
FL-184	Founders Lot		CC Block	Str Steel	7,540	C	356.0	-144.0	42.0	-102.0	35.5	800	1794
FL-185	Founders Lot		CC Block	Str Steel	7,540	C	355.7	-144.3	42.0	-102.3	35.2	800	1853
FL-186	Founders Lot		CC Block	Str Steel	7,540	C	355.3	-144.7	42.0	-102.7	34.8	800	1911
FL-187	Founders Lot		CC Block	Str Steel	6,837	C	355.0	-145.0	42.0	-103.0	34.5	800	1971
FL-188	Founders Lot		CC Block	Str Steel	7,540	C	354.7	-145.3	42.0	-103.3	34.2	<750	2027
FL-189	Founders Lot		CC Block	Str Steel	7,540	C	354.3	-145.7	42.0	-103.7	33.8	<750	2096
FL-190	Founders Lot		CC Block	Str Steel	7,540	C	354.0	-146.0	42.0	-104.0	33.5	<750	2159
FL-191	Founders Lot		CC Block	Str Steel	7,540	C	353.7	-146.3	42.0	-104.3	33.2	<750	2223
FL-192	Founders Lot		CC Block	Str Steel	8,565	C	353.3	-146.7	42.0	-104.7	32.8	<750	2286
FL-193	Founders Lot		CC Block	Str Steel	8,070	C	353.0	-147.0	42.0	-105.0	32.5	<750	2413
FL-194	Founders Lot		CC Block	Str Steel	7,540	C	353.3	-146.7	42.0	-104.7	32.8	<750	2490
FL-195	Founders Lot		CC Block	Str Steel	7,540	C	353.7	-146.3	42.0	-104.3	33.2	<750	2554
FL-196	Founders Lot		CC Block	Str Steel	7,540	C	354.0	-146.0	42.0	-104.0	33.5	<750	2617
FL-197	Founders Lot		CC Block	Str Steel	7,540	C	354.3	-145.7	42.0	-103.7	33.8	<750	2681
FL-198	Founders Lot		CC Block	Str Steel	7,366	C	354.7	-145.3	42.0	-103.3	34.2	<750	2745
FL-199	Founders Lot		CC Block	Str Steel	7,934	C	355.0	-145.0	42.0	-103.0	34.5	<750	2810
FL-200	Founders Lot		CC Block	Str Steel	14,404	C	355.5	-144.5	42.0	-102.5	35.0	935	2873
FL-201	Founders Lot		CC Block	Str Steel	8,976	C	355.0	-145.0	42.0	-103.0	34.5	<750	2890
FL-202	Founders Lot		CC Block	Str Steel	8,536	C	354.7	-145.3	42.0	-103.3	34.2	755	2861
FL-203	Founders Lot		CC Block	Str Steel	8,546	C	353.2	-146.8	42.0	-104.8	32.7	<750	2823
FL-204	Founders Lot		CC Block	Str Steel	7,540	C	353.4	-146.6	42.0	-104.6	32.9	<750	2858
FL-205	Founders Lot		CC Block	Str Steel	7,540	C	353.7	-146.3	42.0	-104.3	33.2	<750	2898
FL-206	Founders Lot		CC Block	Str Steel	7,540	C	354.0	-146.0	42.0	-104.0	33.5	<750	2940
FL-207	Founders Lot		CC Block	Str Steel	7,540	C	354.3	-145.7	42.0	-103.7	33.8	<750	2982
FL-208	Founders Lot		CC Block	Str Steel	7,540	D	354.6	-145.4	42.0	-103.4	34.1	<750	3025
FL-209	Founders Lot		CC Block	Str Steel	7,540	D	355.0	-145.0	42.0	-103.0	34.5	<750	3069
FL-210	Founders Lot		CC Block	Str Steel	7,540	D	355.3	-144.7	42.0	-102.7	34.8	<750	3113
FL-211	Founders Lot		CC Block	Str Steel	7,540	D	355.5	-144.5	42.0	-102.5	35.0	<750	3158
FL-212	Founders Lot		CC Block	Str Steel	7,540	D	355.5	-144.5	42.0	-102.5	35.0	<750	3204
FL-213	Founders Lot		CC Block	Str Steel	7,540	D	355.3	-144.7	42.0	-102.7	34.8	<750	3251
FL-214	Founders Lot		CC Block	Str Steel	7,540	D	354.8	-145.2	42.0	-103.2	34.3	<750	3298
FL-215	Founders Lot		CC Block	Str Steel	7,540	D	354.6	-145.4	42.0	-103.4	34.1	<750	3346
FL-216	Founders Lot		CC Block	Str Steel	7,540	D	354.3	-145.7	42.0	-103.7	33.8	<750	3394

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FL-217	Founders Lot		CC Block	Str Steel	7,540	D	354.0	-146.0	42.0	-104.0	33.5	<750	3443
FL-218	Founders Lot		CC Block	Str Steel	7,540	D	353.7	-146.3	42.0	-104.3	33.2	<750	3492
FL-219	Founders Lot		CC Block	Str Steel	7,276	D	353.5	-146.5	42.0	-104.5	33.0	<750	3542
FL-220	Founders Lot		CC Block	Str Steel	7,540	D	353.0	-147.0	42.0	-105.0	32.5	2797	3846
FL-221	Founders Lot		CC Block	Str Steel	7,540	D	352.6	-147.4	42.0	-105.4	32.1	2797	3891
FL-222	Founders Lot		CC Block	Str Steel	7,540	D	352.3	-147.7	42.0	-105.7	31.8	2797	3937
FL-223	Founders Lot		CC Block	Str Steel	7,540	D	352.0	-148.0	42.0	-106.0	31.5	2797	3983
FL-224	Founders Lot		CC Block	Str Steel	7,540	D	351.6	-148.4	42.0	-106.4	31.1	2797	4029
FL-225	Founders Lot		CC Block	Str Steel	7,540	D	351.3	-148.7	42.0	-106.7	30.8	2797	4076
FL-226	Founders Lot		CC Block	Str Steel	7,540	D	351.0	-149.0	42.0	-107.0	30.5	2797	4124
FL-227	Founders Lot		CC Block	Str Steel	7,540	D	350.6	-149.4	42.0	-107.4	30.1	2797	4172
FL-228	Founders Lot		CC Block	Str Steel	7,540	D	350.6	-149.4	42.0	-107.4	30.1	2797	4220
FL-229	Founders Lot		CC Block	Str Steel	7,540	D	351.0	-149.0	42.0	-107.0	30.5	2797	4269
FL-230	Founders Lot		CC Block	Str Steel	7,540	D	351.3	-148.7	42.0	-106.7	30.8	2797	4318
FL-231	Founders Lot		CC Block	Str Steel	7,540	D	351.6	-148.4	42.0	-106.4	31.1	2797	4368
FL-232	Founders Lot		CC Block	Str Steel	7,540	D	352.0	-148.0	42.0	-106.0	31.5	2797	4418
FL-233	Founders Lot		CC Block	Str Steel	7,540	D	352.0	-148.0	42.0	-106.0	31.5	2797	4469
FL-234	Founders Lot		CC Block	Str Steel	7,540	D	351.6	-148.4	42.0	-106.4	31.1	<750	4520
FL-235	Founders Lot		CC Block	Str Steel	7,540	D	351.3	-148.7	42.0	-106.7	30.8	<750	4571
FL-236	Founders Lot		CC Block	Str Steel	7,540	D	351.0	-149.0	42.0	-107.0	30.5	<750	4622
FL-237	Founders Lot		CC Block	Str Steel	7,540	D	351.0	-149.0	42.0	-107.0	30.5	<750	4674
FL-238	Founders Lot		CC Block	Str Steel	7,540	D	350.6	-149.4	42.0	-107.4	30.1	<750	4726
FL-239	Founders Lot		CC Block	Str Steel	7,540	D	350.3	-149.7	42.0	-107.7	29.8	<750	4779
FL-240	Founders Lot		CC Block	Str Steel	7,540	D	350.0	-150.0	42.0	-108.0	29.5	<750	4832
FL-241	Founders Lot		CC Block	Str Steel	7,540	D	350.0	-150.0	42.0	-108.0	29.5	<750	4885
FL-242	Founders Lot		CC Block	Str Steel	7,540	D	350.3	-149.7	42.0	-107.7	29.8	<750	4936
FL-243	Founders Lot		CC Block	Str Steel	7,540	D	350.6	-149.4	42.0	-107.4	30.1	<750	4987
FL-244	Founders Lot		CC Block	Str Steel	7,540	D	351.0	-149.0	42.0	-107.0	30.5	<750	5037
FL-245	Founders Lot		CC Block	Str Steel	7,540	D	351.3	-148.7	42.0	-106.7	30.8	<750	5086
FL-246	Founders Lot		CC Block	Str Steel	7,540	D	351.6	-148.4	42.0	-106.4	31.1	<750	5136
FL-247	Founders Lot		CC Block	Str Steel	7,540	D	352.0	-148.0	42.0	-106.0	31.5	<750	5186
FL-248	Founders Lot		CC Block	Str Steel	9,359	D	352.0	-148.0	42.0	-106.0	31.5	<750	5236
FL-249	Founders Lot		CC Block	Str Steel	7,750	D	352.3	-147.7	42.0	-105.7	31.8	<750	5304
FL-250	Founders Lot		CC Block	Str Steel	7,281	D	352.6	-147.4	42.0	-105.4	32.1	<750	5368
FL-251	Founders Lot		CC Block	Str Steel	7,728	D	351.3	-148.7	42.0	-106.7	30.8	2641	4486
FL-252	Founders Lot		CC Block	Str Steel	7,540	D	351.6	-148.4	42.0	-106.4	31.1	2641	4434
FL-253	Founders Lot		CC Block	Str Steel	7,540	D	351.9	-148.1	42.0	-106.1	31.4	2641	4382
FL-254	Founders Lot		CC Block	Str Steel	7,540	D	351.9	-148.1	42.0	-106.1	31.4	2641	4330
<b>Founders Lot Subtotal</b>					<b>2,041,869</b>								
TS-1	Tuning Shop		CC Block	Str Steel	53,497	C	355.5	-144.5	20.0	-124.5	13.0	777	3973
MS-1	Member Storage		CC Block	Str Steel	48,506	C	354.5	-145.5	30.0	-115.5	22.0	751	3434
MS-2	Member Storage		CC Block	Str Steel	48,506	C	354.0	-146.0	30.0	-116.0	21.5	751	3308
MS-3	Member Storage		CC Block	Str Steel	48,506	C	353.5	-146.5	30.0	-116.5	21.0	751	3182
CT-1	Control Tower (part of Track Lot)		Str Steel	Str Steel	6,085,673	C	352.5	-147.5	70.0	-77.5	60.0	1186	3486
MB-1	Maintenance Building		CC Block	Str Steel	146,136	C	358.5	-141.5	18.0	-123.5	14.0	844	2403
RA-1	Registration & Admin		CC Block	Str Steel	53,496	C	356.0	-144.0	25.0	-119.0	18.5	834	3737
ET-1	Event Tent (part of Track Lot)		Tent	Tent		C	351.0	-149.0	26.0	-123.0	14.5	1486	3468
TG-1	Team Garage		CC Block	Str Steel	41,980	D	355.5	-144.5	17.0	-127.5	10.0	2080	4130
TG-2	Team Garage		CC Block	Str Steel	41,980	D	356.5	-143.5	17.0	-126.5	11.0	2080	3878
TG-3	Team Garage		CC Block	Str Steel	41,980	D	358.5	-141.5	17.0	-124.5	13.0	2080	3628
TG-4	Team Garage		CC Block	Str Steel	41,980	C	355.5	-144.5	17.0	-127.5	10.0	1958	3943
TG-5	Team Garage		CC Block	Str Steel	41,980	C	356.5	-143.5	17.0	-126.5	11.0	1958	3686
TG-6	Team Garage		CC Block	Str Steel	41,980	C	358.5	-141.5	17.0	-124.5	13.0	1958	3435
TG-7	Team Garage		CC Block	Str Steel	41,980	C	354.5	-145.5	17.0	-128.5	9.0	1837	4013
TG-8	Team Garage		CC Block	Str Steel	41,980	C	356.5	-143.5	17.0	-126.5	11.0	1837	3753
TG-9	Team Garage		CC Block	Str Steel	41,980	C	358.0	-142.0	17.0	-125.0	12.5	1837	3495
TS-1	Track Side Garage (Part of Track)		CC Block	Str Steel		C	351.5	-148.5	35.0	-113.5	24.0	880	3369
TS-2	Track Side Garage (Part of Track)		CC Block	Str Steel		C	348.5	-151.5	35.0	-116.5	21.0	902	3693
KT-1	Kart Track		N/A	N/A	510,697	C	354.0	-146.0	0.0	-146.0	-8.5	1211	3142
<b>Subtotal Other Buildings</b>					<b>7,372,837</b>								
<b>TOTAL FOR SITE</b>													

## COUNTY OF RIVERSIDE

### Airport Land Use Commission – Application for Major Land Use Action Review

**ALUC Case Number:**

**County of Riverside Environmental Assessment (E.A.) Number:** EA40361 ~~PM36013~~ **PM36013**

**Project Case Type (s) and Number(s):** SP00303A2, GPA01104, CZ007742, ~~TR55487~~, PM36315, PP24690

**Lead Agency Name:** County of Riverside Planning Department

**Address:** 4080 Lemon Street, 9<sup>th</sup> Floor, Riverside, CA 92501

**County Planning Contact Person:** Jay Olivas

**Telephone Number:** (951) 955-1195

**Applicant's Name:** Regent Properties, Inc.

**Applicant's Address:** 11990 San Vicente Blvd, Suite 200  
Los Angeles, CA 90049

## PROJECT INFORMATION

### A. Project Description

#### 1. Background

The Kohl Ranch Specific Plan ("Project") is located in the Coachella Valley portion of Riverside County (**Figure 1-Vicinity Map**). The site is just south of Jacqueline Cochran Regional Airport, and is roughly east of Harrison Street/Highway 86, west of Highway 111 and north of Highway 195<sup>1</sup>. The project is bounded by Avenue 60 on the north, Polk Street on the east, Avenue 64 on the South and Tyler Street on the west.

The Project which is the subject of this application ("Project") consists of a Specific Plan amendment to the approved Kohl Ranch Specific Plan, a General Plan Amendment, a change of zone to correct zoning text, a parcel map, a commercial parcel map, and a plot plan for a motor sports park.

The original Kohl Ranch Specific Plan ("SP") was previously analyzed in the Kohl Ranch Specific Plan EIR No. 396 (adopted November 16, 1999). Prior to the approval of the SP, it was submitted to ALUC for review, and ALUC at that time found it consistent with the then applicable land use compatibility plan ("ALUCP") for the Jacqueline Cochrane Airport. Amendment No. 1 to the SP was also submitted to ALUC for review in 2002, and was found at that time to be consistent with the applicable ALUCP. Since that time, the ALUCP was amended, and as a result, portions of the SP may be in conflict with the current ALUCP.

In April, 2010, ALUC found that the applicant's request for a determination of Substantial Conformance that the development of a motorsports park within the Open Space and Heavy Industrial areas of the SP was consistent with the ALUCP. This proposed use was determined through the Substantial Conformance process to not inherently present substantial changes from the approved uses and would not likely cause new significant environmental impacts or a substantial increase in the severity of previously identified significant effects.

The proposed Project is intended to bring a major portion of the SP into compliance with the ALUCP, including the area of Kohl Ranch north of Avenue 62 and to modify circulation within the SP in accordance with Riverside County Transportation plans for the area. No significant modifications of the land uses south of Avenue 62 are being proposed as part of this Project. Currently proposed land uses south of Avenue 64

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<sup>1</sup> Please note that for clarification purposes, this Specific Plan refers to the "old" Highway 86 as Harrison Street, its local street name. The designation SR-86S refers to the new freeway constructed east of the Whitewater River.

were previously approved by ALUC as part of TM36293. A finding of consistency with the ALUCP from ALUC for the current Project, therefore, applies only to the land uses north of Avenue 62. Potential adjustments to the area between Avenue 62 and Avenue 64 to assure compliance with the ALUCP will be the subject of a later application.

The SP, as modified by the Project, consists of a balanced array of land uses including residential, business, commercial, industrial, open space/recreation and public facilities. Both living and working opportunities will be available within the SP. The residential portion includes 7,161 residential units distributed among three different density classifications on 1,140.29 acres, with a gross residential density of 4.03 units per acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Approximately 376.73 acres of open space provide for passive and active recreation, including trails and parks. The SP also allows for the development of large scale recreational uses such as golf courses and a motorsports park, which are identified as allowable uses in the existing SP.

The business, commercial and industrial land use categories will comprise 24.45, 31.06 and 331.02 acres respectively. Commercial areas will serve the project as well as neighboring communities. Business and industrial uses will be oriented toward the Jacqueline Cochrane Airport as well as larger regional markets, and are intended to provide employment opportunities to project area residents.

## 2. Detailed Description

The applications described above constitute the Project for purposes of this Application for Major Land Use Action Review document. Two major project areas are affected by these applications: between Avenue 62 and Avenue 64, and north of Avenue 62. **Figure 2, Current Land Use Plan** and **Figure 3, Proposed Land Use Plan** are provided for comparison and for ease of understanding the following descriptions of changes proposed since Amendment 1 was approved.

### South of Avenue 64

SPA No. 303, Amendment 2 proposes to: relocate Avenue 64 to its original straight alignment through the Project area between Polk Street and Tyler Street; reconfigure the Open Space areas and identify the currently proposed lake; eliminate "C" Street south of Avenue 64; and revise densities, lot sizes and planning area boundaries as reflected in **Figure 2, Current Land Use Plan**. The commercial uses have also been eliminated from this portion of the project. In part, the roadway realignment resulted from the desire to place the flood evacuation channel, Avenue 64 and the utility easement beltway parallel with and adjacent to each other. Subsequent to the approval of SPA303A1, three schools were constructed and now operate within Planning Area M-4 at the corner of Tyler Street and Avenue 66. Tentative Tract Map TR36293, which ALUC found consistent with the ALUCP in March 2010, supports these changes.

### Between Avenue 62 and Avenue 64

The realignment and redesignation of Avenue 62 necessitated a slight change in the alignment of "D" Street. A minor reduction in acreages within Planning Areas F-3, F-4, G-1, G-2, G-3 and G-6 was caused by the wider cross section and straightening of Avenue 62. No other changes are proposed for this area and no decision is requested at this time for land uses in this area.

### North of Avenue 62

The realignment and redesignation of Avenue 62 necessitated changes in the alignments of other streets (e.g., Tyler Street, Avenue 60, "B" Street). Street realignments necessitated changes in planning area boundaries and drainage facilities. The roadway changes include: the elimination of "C" Street north of Avenue 62; the elimination of "A" Street where it previously connected Avenue 60 to Avenue 62 diagonally through the site; and the extension of Tyler Street northerly to connect with Avenue 60. Other changes include: a relocation

of the commercial uses from “A” Street which was the main thoroughfare through the Project in the prior amendments to SP No. 303, to Avenue 62 which now serves this purpose; a consolidation of parks to one centrally-located facility fronting on Tyler Street; the elimination of office uses as a separate land use designation; revised drainage facilities which were realigned to accommodate all roadway and land use changes; and adjustments needed to accommodate the proposed motor sports race track.

The proposed motorsports race track would be constructed on the northeasterly 332 acres of Kohl Ranch at the southeast corner of Polk and Avenue 60 with SP303. This private facility would be the first world class road course built in Southern California. Features would include a membership program similar to a golf course country club allowing only members, their invited guests, staff and credentialed 3rd parties within the gate. Exceptions would be for groups such as car clubs, corporations and manufacturers utilizing the track for testing. Unlike other tracks, individuals would not be able to purchase a pass for a few laps around the track. (**Figure 4-Race Track Site Plan**)

The Gran Prix track would be 4.5 miles in length and contain multiple configurations so that club members could have a track available while separate events could be taking place simultaneously. The maximum capacity for the track would be 40 cars racing at any one time for safety purposes. The track would be 40-foot wide with an asphaltic surface. A 30-foot wide area would be located on either side of the track called the ‘verge.’ The verge is a planted area designed to slow down a car that leaves the track. Beyond the verge are various types of safety areas, including ‘runoff’ areas. The ‘runoff’ areas vary in width, but would have a maximum slope of 1%. A runoff area could be dirt, asphalt or a gravel pit, depending on the location of the runoff, the anticipated speed of a car in distress, or the proximity to buildings or other features outside the track. Some of the safety features included in the track area would be tire walls, barricades and railing. Security fencing would run the entire length of the track with only designated safety, emergency and trackside personnel allowed inside the fence at any time.

The majority of the track would be surrounded by a 15-foot earthen berm with a 10-foot sound wall located on top of that berm. These features would serve to keep track noise within the property. In addition to the berm and wall, the racetrack itself would be strategically placed within the site in a manner to block noise to adjacent uses. At the toe of these berms, a walking and bicycle trail would encircle the track area, but remain separated from the track by a security fence.

Private garages referred to as Member Garages would be constructed around the track. These for-sale trackside lots would vary in size; typically 65-foot wide by 115-foot deep and would be located adjacent to the 15-foot berm that has a 10-foot sound wall at the top designed for noise attenuation. The garages constructed on these lots would range in size from 2,000 square feet to 6,000+ square feet; approximately 51-foot wide by 70-foot deep. Garages would be constructed into the berm and excess dirt would be stockpiled in the Phase 2 area of the track. This would be kept for future use in berm and track construction in that phase. The bottom floor of the Member Garage would allow for storage of 4 to 8 cars with room for tools and an elevator shaft. The upper floor would allow for an office, kitchen and entertaining room. As Member Garages are constructed, a portion of the sound wall on top of the berm may be removed from each garage in order to construct an observation deck. Structures would not exceed 42-feet in height. Overnight occupancy would be prohibited.

Pit Areas would be the only points of entry for the track and would be located within a clear view of the Observation Tower. The Observation Tower<sup>2</sup> would house track control personnel. The Observation Tower would be a four story structure of approximately 70-feet in height containing a banquet area, kitchen,

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<sup>2</sup> The Track Control Tower is the tallest building proposed for the property and it fully complies with all ALUC land use criteria. This building height is well below the FAA’s Federal Aviation Regulations (FAR) Part 77 air navigation protection imaginary surfaces. To provide site lines to the entire Track area, the Tower is fixed by its function in Zone C. The TMP Track planners have studied various locations for the Tower and have proposed it in a location that ultimately sited it more than 300 feet east of its original planned location. This site places the planned Tower location approximately 1,200 feet east of the Runway 35 centerline extended and approximately 5,000 feet from the existing end of Runway 35.

lockers and restrooms on the 1st floor; a briefing room, hospitality area and viewing deck on the 2nd floor; a member area with lounge and viewing deck on the 3rd floor; and a control tower with restricted access on the 4th floor.

The pit area would feature closely guarded and gated pit entry and exit points. There would be two types of areas within the pits; a “hot pit” and a “cold pit.” In the “hot” pit, cars would drive under an enforced speed limit. Inside the hot pit area would be an area for a car to pull off and stop or for service. There would be two cold pit areas. One area would provide a lane behind the hot pit allowing for a smooth transition out of the pits and into the paddock. The other would be a short area off the hot pit, providing access to the pit-side garages. The paddock is an approximately ten acre paved and striped asphalt surface designated for vehicular travel. The paddock would be located in the central area of the track. Its primary purpose would be for parking and staging. Sample activities that could occur in the paddock on a day-to-day basis are as follows:

- Members could visit the track in their own car driven from their Member Garage. This vehicle would remain in the paddock while member is waiting for access to the track or visiting the Tower.
- A member of the Porsche Owner’s Club for instance, could rent the track for the weekend and bring in his own car via a transport vehicle. That transport vehicle could stay in the paddock for the duration of the weekend. However, no overnight occupancy would be allowed in the transport vehicle.
- A racing school could contract with the track for usage. A student of that racing school could park in the paddock and partake in classroom instruction held in the briefing room of the Observation Tower. The racing school would likely have its own fleet of vehicles which would be kept on-site in a secured area such as the pit-side garages or in the Member’s Garage. During class training, the fleet of instructional vehicles would be brought into the paddock and staged until used for driver training.

Because cars would be stored on-site, there would be a perimeter wall and infrared sensors for security. Hours of operation would vary depending on the time of year, day of the week or weekend. It is anticipated the track would open between 7:00 a.m. and 9:00 a.m. and close between 5:00 p.m. and 7:00 p.m. Logistically, deliveries could occur at any time, but are anticipated to take place between the hours of 6:00 a.m. and 8:00 a.m. Security personnel would be on-site 24 hours a day. The track could be rented any day of the week; however, Monday is anticipated to be closed in order for track maintenance to occur.

Uses “outside the gate” would include a Kart Track, Tuning Shop and Registration / Administration building. The Kart Facility would be approximately 14 acres. “KARTING” is a microcosm of racing, and is a variant of open-wheel motor sport with small, open, four-wheeled vehicles called karts, go-karts, or gearbox/shifter karts depending on the design. The facility would allow for member use, corporate programs, public kart rentals and arrive and drive. Member usage would entail race events for members and their families using either their own race karts or track-owned rental karts. Corporate programs would involve companies renting the facility for their own marketing, promotional, team building and other events. The karts would be track-owned rental units. Public Kart rentals would allow anyone within the appropriate age limit to rent track owned karts. Age limits would restrict those 10 years and under while ages 10-12 years would be based upon size of the child. Arrive and drive would allow the public to bring their own race type karts to use in non-racing mode. The Kart track itself would be approximately 25-foot wide and one mile long. Approximately 15 karts would be on the track at one time for about 5 hours per day.

Hours of operation are anticipated to be from 7:00 a.m. to dusk. The flow of customers would vary day to day but increase on the weekends and during holiday periods. The facility anticipates constant rental use of approximately 30 karts. It is anticipated that 10-15 karts would be on the track at a time, for a period of 5 hours per day with fewer karts for the remainder of the day. Spectators would also be allowed

The Tuning Shop would house an office for accounting, operations and management; a showroom featuring parts and components for modifying and improving the performance of high-end sports cars; and the rear of the facility would contain service bays with lifts and a chassis dynamometer. These items would be located below grade. Activities in the Tuning Shop would include race car tuning and related services. This building



would be open to the public although it is anticipated that the primary clientele would be the motorsports track members. The Registration / Administration Building would be for registration of track guests and track administrative personnel. Directly behind the Administration / Registration Building would be a fuel island. This island would not have an attendant and fuel would be paid for via a card reader. This fuel island is anticipated to dispense premium and high-octane grades of fuel. All tanks would be located below grade.

Additional facilities are anticipated during Phase 2 of the race track including a Clubhouse, Vintage Car Showroom located next to the Tuning Shop, a Racing Museum located south of the Vintage Car Showroom, a Maintenance Building used for storage of track and safety equipment and a 1-1.5 acre Alternative Energy Park housing structures with industrial uses related to alternative energy.

### 3. ALUC Review

The Project is consistent with the adopted Airport Land Use Compatibility Plan (ALUCP) for Jacqueline Cochran Regional Airport (JCRA) and this proposed use would provide significant safety, noise, overflight and land use protections. The proposed layout of the track and the clustering of its amenities would maintain a significant open space area in the approach area to Runway 35 and Runway 30 that supports the goals of ALUCP. This concept is also consistent with the FAA airport planning criteria for the area beyond the ends of runways by maintaining the existing and planned Runway Protection Zones (RPZs) completely clear of non-compatible land uses. As such, this use would also be consistent with the intent of the JCRA long-term Airport Layout Plan. There are several added benefits of the TMP Plan beyond meeting the minimum requirements that make it an ideal use to consider for this portion of the Kohl Ranch property:

- Creates a buffer zone of non-noise-sensitive land use in the approach area to JCRA;
- Produces revenue and economic benefit for the community and the County;
- Preserves and protects land for the ultimate extension of Runway 35; and
- Preserves and protects land for the planned Runway 35 approach RPZ.

The following is a summary of key issues that have guided the track design team as they developed their concept for the track and all of its associated facilities. Following these land use constraints, the designers have a track plan that is a compatible use with the JCRA and that allows substantial flexibility and opportunity for a first class racing facility.

#### Key Issues:

1. **Preserve Open Land for Aircraft Safety** – The TMP track would provide a large open paved track area with a series of turns upon which drivers can practice and race. This large-area layout also provides the opportunity for a substantial open land component, particularly in the western portion of the property along the Runway 35 centerline extended south across the property and east of the runway centerline extended. The TMP track concept clusters buildings and amenities in a way that allows for these new uses while also meeting the open land requirements of the ALUCP. The purpose of the open land area is to provide substantial safety benefits for pilots and aircraft passengers in case of an emergency landing off of the JCRA.
2. **Limit Intensity of Use** – The TMP track would limit the number of people in the track area to less than the number of people on a public road, a public parking lot or a golf course at any one point in time. FAA criteria for a RPZ have been completely met and protected through the development of this plan. As required by the FAA, no fuel storage, public assembly or tall structures would be allowed within the RPZ area. The ALUCP also provides criteria for limiting the number of people in certain areas around the airport. Item 5 of this summary provides additional detail on each of these areas.

3. **Reduce Airport Noise Concerns** – People participating in auto racing would not be impacted by aircraft noise given their acceptance of noise associated with auto racing. The track design also includes significant noise buffer features that have been designed in to reduce any associated racing noise impacts.
4. **Reduce Aircraft Overflight Concerns** – The approach/departure area to the JCRA would be over open land and portions of the proposed track. The majority of aircraft overflights in this vicinity will be south to north over the western portion of the property by aircraft arriving to JCRA Runway 17/35. There will also be some small aircraft operations over the property associated with Runway 12/30. As stated previously, open land areas and clustering of track amenities provide large open land areas in the case of an emergency landing off of JCRA.
5. **Comply with ALUC Zones A, B1, C and D** – The proposed track area and associated facilities would meet all of the land use restrictions in the JCRA ALUCP. The following are the key planning criteria associated with ALUC Zones A, B1, C and D. **Table 1** provides a comparison between the criteria and the proposed TMP intensity. **Table 2** provides the Building Occupancy Calculation methodology.
  - a. **Zone A** would be maintained as 100 percent “open land” under the ALUC designation. ALUC Zone A encompasses the FAA’s existing and proposed Runway Protection Zone and the area south of Avenue 60 planned for the future extension of Runway 35. This area is closest to the existing runway and would be kept clear of structures, people and hazards to air navigation. All buildings adjacent to Zone A are set back further than the JCRA Master Plan, Building Restriction Line for buildings up to 50 feet tall. The TMP Track Plot Plan setbacks preserve and protect Zone A but it is not part of the Track Plot Plan property.
  - b. **Zone B1** would be maintained as 100 percent “open land” under the ALUC designation. The only track uses proposed for Zone B1 are portions of the track pavement, pit stop areas and the paved “paddock” area for loading, unloading and staging vehicles for racing. No buildings are proposed for Zone B1.
  - c. **Zone C** would provide approximately 90 percent “open land” under the ALUC designation. Track uses in Zone C would include portions of the track area, founders lots, parking and garage areas, the Kart Track, Administration/Registration Building, Tuning Shop, Members’ Common Garages, Maintenance Building, Fuel Island (with below-ground fuel tanks), Track Control Tower, Trackside Garages and Team Garages while remaining within the intensity of 75 to 150 people per acre. The intensity of use can be increased to 195 people per acre in this zone if special building precautions are taken to protect occupants from aircraft accidents. ALUC policy allows up to 80 percent of the land in this zone to be developed. Similar to Zones A and B1, the TMP plan clusters buildings and facilities in order to keep the open land area in larger tracts that are aligned with the approach/departure area of Runway 17/35.

The Track Control Tower is the tallest building proposed for the property and it fully complies with all ALUC land use criteria. This building is well below the FAA’s Federal Aviation Regulations (FAR) Part 77 air navigation protection imaginary surfaces. To provide site lines to the entire Track area, the Tower is fixed by its function in Zone C. The TMP Track planners have studied various locations for the Tower and have proposed it in a location that ultimately sited it more than 300 feet east of its original planned location. This site places the planned Tower location approximately 1,200 feet east of the Runway 35 centerline extended and approximately 5,000 feet from the existing end of Runway 35.

- d. **Zone D** would provide approximately 83 percent “open land” under the ALUC designation. Approximately 62 acres of the TMP Track are located in Zone D. This zone allows 100 to 300 people per acre intensity with a bonus of up to 390 people per acre if special building

precautions are taken to protect occupants from aircraft accidents. Planned Track uses for Zone D include portions of the track area, Founders Lots on the eastern portion of the property and three of the nine Team Garages on the far western portion of the property. Similar to Zones A, B1 and C, the TMP plan clusters buildings and facilities in order to keep the open land area in larger tracts that are aligned with the approach/departure area of Runway 17/35.

**Table 1**  
**Thermal Motorsports Park**  
**Comparison to ALUC Compatibility Criteria**

ALUC Zone	ALUC Maximum Intensity (people/acre)			TMP Proposed Intensity (people/acre)			TMP Area (acres)	ALUC Required Open Land	TMP Proposed Open Land
	Avg.	Single	Bonus	Avg.	Single	Bonus			
A*	0	0	0	0	0	n/a	110*	All remaining	100%
B1	25	50	65	<25	<50	65	75	30%	100%
C	75	150	195	<75	<150	<195	195	20%	90%
D	100	300	390	<100	<300	<390	62	10%	83%
Total							332*		91%

\*Note: Zone A is included here for information purposes only. The TMP Track area preserves and protects ALUC Zone A by is not part of the proposed Plot Plan for the Track.

Source: Table 2A – Basic Compatibility Criteria, Riverside County Airport Land Use Compatibility Plan Policy Document (Adopted October 2004)  
 Prepared by: Johnson Aviation, September 2010

**Table 2**  
**Thermal Motorsports Park Track**  
**Building Occupancy Calculations**

Building Type (# Lots on Plan)	Total Floor Area per Building	Occupancy Rate (s.f./occupant)	Maximum Occupancy Estimate*	Planned Building Occupancy Estimate*	People per Acre Average*
Founders Lots – 254	7,150	500	14.3	7.2	39.0
Tuning Shop – 1	11,310	200	56.6	28.3	23.0
Member Storage – 3	25,220	500	50.4	25.2	22.6
Control Tower – 1	40,222	200	201.1	100.6	1.9
Maintenance Bldg – 1	4,027	500	8.1	4.1	4.1
Registration/Admin – 1	7,850	200	39.3	19.7	19.7
Event Tent – 1	4,800	50	96.0	48.0	1.9
Team Garages – 9	12,515	500	25.0	12.5	13.0
Trackside Garages – 2	22,500	200	112.5	56.3	1.9

\*Note: Maximum Occupancy Estimate is based on a calculation of total floor area divided by the minimum square feet per occupant as outlined in Appendix C of the California Airport Land Use Planning Handbook. Planned Building Occupancy uses a 50% adjustment factor as outlined in Appendix C to account for normal use of facilities. Control Tower, Maintenance Building, Registration/Admin Building, Event Tent and Trackside Garages are located within a very large open land parcel of the Track and as a result the averages for these facilities would be less than two people per acre.

Source: Appendix C, California Airport Land Use Planning Handbook (January 2002)  
 Prepared by: Johnson Aviation, September 2010



Established in 1918 as a public agency  
**Coachella Valley Water District**

**Directors:**

Patricia A. Larson, President  
Peter Nelson, Vice President  
Tellis Codekas  
Franz W. De Klotz  
Russell Kitahara

**Officers:**

Steven B. Robbins, General Manager-Chief Engineer  
Julia Fernandez, Board Secretary

September 16, 2010

Redwine and Sherrill, Attorneys

File: 0163.1  
0421.1  
0721.1  
070809-1  
070809-2  
070809-3  
070809-4

Jay Olivas  
Riverside County Planning Department  
38-686 El Cerrito Road  
Palm Desert, CA 92211

**RECEIVED**

**SEP 30 2010**

Riverside County  
Planning Department  
Desert Office

Dear Mr. Olivas:

Subject: Specific Plan No. 303, Amendment No. 2, Screen Check No. 1

This letter supersedes District's letter dated March 16, 2010.

This project lies within the area of the Eastern Coachella Valley Master Stormwater Planning Project, which will provide flood protection to the communities of Thermal, Vista Santa Rosa, Oasis, Mecca and North Shore. The District, in cooperation with Riverside County and the Torres Martinez Desert Cahuilla Indian Tribe, are in the early stages of this planning effort. Upon completion of the design phase, developers and property owners within the area may be required to dedicate right-of-way for flood control facilities and/or participate in the financing of a portion of these facilities.

The report that established the entire Kohl Ranch Specific Plan existing conditions runoff from regional flooding from Martinez Canyon and Valley Floor Drainage areas has been approved by the District by a letter dated June 16, 2010 (copy of letter enclosed). The established runoff from both sources shall be received onto and discharged from the Specific Plan in a manner that is reasonably compatible with predevelopment conditions.

Prior to District approval of future development plans within the Kohl Ranch Specific Plan subject to regional flooding, the developer will be required to pay fees for a flood management review by our consultant. Flood protection measures shall include detailed hydraulic analysis and plans for flood control that comply with Riverside County Ordinance No. 458, District, FEMA and California Drainage Law regulations and standards.

This area is designated Zone D on Federal Flood Insurance rate maps, which are in effect at this time by the Federal Emergency Management Agency (FEMA).

The developer shall provide written notice to all downstream property owners located within 600 feet of this area of the proposed construction of flood control facilities before commencing construction of any District approved flood control facilities. Said notice shall include wording that indicates that the project includes construction of flood control facilities, which may affect downstream properties.

Prior to issuance of grading permits for development plans subject to regional flooding, the District will require the developer to:

- Execute an agreement with the District which shall include provisions outlined in the District Ordinance No. 1234. A copy of the Ordinance No. 1234 is enclosed for your convenience.
- Submit to the District a Flood Control Facility Operations and Maintenance Manual for review and approval.
- Grant flooding easements over the flood control facilities in a form and content reasonably acceptable to the District.
- Submit final construction plans for the proposed flood control facilities and a detailed hydrological and hydraulic design report for review and approval.

Prior to occupancy, the District requires the developer to:

- Complete the construction of the flood control facilities and submit “as-built” topography, construction drawings and engineering analysis for District review to verify that the design capacity is adequate.

The county shall require mitigation measures to be incorporated into the development to prevent flooding of the site or downstream properties. These measures shall require on-site retention of the incremental increase of runoff from the 100-year storm.

Design for retention basins for this area must consider high groundwater levels and clay soils.

This project lies within the Study Area Boundary of the Coachella Valley Water Management Plan (September 2002).

A District approved Water Supply Assessment (WSA) and/or Water Supply Verification (WSV) is required for this project. The developer is in contact with the District discussing the preparation of the WSA/WSV. The District requests the County to withhold approvals for this project until the WSA/WSV has been approved.

In compliance with a joint agreement with the Coachella Valley Unified School District and the Coachella Valley Water District, this project has contributed to the funding and construction of offsite domestic water and sanitary sewer system facilities. These facilities include a domestic water well, water treatment facility, domestic water transmission pipeline, domestic water reservoir, sanitary sewer pipeline, and sewer lift station. The original project was entitled to satisfy potable water requirements for 300 equivalent dwelling units (EDU) and sewer requirements for 30 EDU's for its contributions to the facilities.

The District will provide sanitation service to this area and such service will be subject to the satisfaction of terms and conditions established by the District and exercised from time to time, including but not limited to fees and charges, water conservation measures, etc.

The District may need additional facilities to provide for the orderly expansion of its sanitation systems. These facilities may include pipelines, booster pumping stations, lift stations and other facilities. The developer may be required to install these facilities and provide land and/or easements on which some of these facilities will be located. These sites shall be shown on the tract map as lots and/or easements to be deeded to the District for such purpose.

This notice of sanitation service availability can only be used and relied upon for the specific property for which it was issued and shall expire three (3) years from date of issuance

Sanitation service remains at all times subject to changes in regulations adopted by the District's Board of Directors including reductions in or suspensions of service.

The District requires restaurants to install a grease interceptor, including a sample box, sanitary tee and running trap with cleanout, prior to any discharge to its sanitation facilities. The size of the grease interceptor will be determined and approved by the District. Installation of the interceptor will be inspected by the District.

The District requires detail, repair and lube auto shops and car washes to install an oil and sand separator, including a sample box, sanitary tee and running trap with cleanout, prior to any discharge to its sanitation facilities. The size of the oil and sand separator will be determined and approved by the District. Installation of the oil and sand separator will be inspected by the District.

There are existing U.S. Bureau of Reclamation (USBR) facilities not shown on the Specific Plan. There may be conflicts with these facilities. We request the appropriate public agency to withhold the approval of the development plans until utility clearances have been made with the District regarding these facilities. The USBR conflicts include but are not limited to Avenue 64 Evacuation Channel and Irrigation Laterals 123.45-1.3-6.0, 123.45-1.3-3.9LT, 2.2, 2.5/123.45-1.3-4.6.0LT, 1.5, 1.7 and 2.0.

This area is underlain with agricultural drainage lines. There are District and Private facilities not shown on the Specific Plan. There may be conflicts with these facilities. We request the appropriate public agency to withhold the approval of development plans until utility clearance have been made with the District regarding these facilities. The District and Private conflicts include but are not limited to District Agricultural Drain lines Avenue 61, 62 and 65 and Private subsurface tile lines No. 85 and No. 85A.

Surface and subsurface drainage facilities in the vicinity of this project were designed and constructed for agricultural drainage. The District will consider use of these drainage facilities for urban drainage if (1) the surface and subsurface drainage facilities can physically handle the new urban drainage, (2) the area is incorporated into the National Pollutant Discharge Elimination System permit and Waste Discharge Requirements for the discharge of stormwater in the Whitewater River Watershed, which is known as the MS4 Permit and (3) the project is annexed into a future district(s) for recovery of capital and operation/maintenance costs associated with the new urban drainage system.

September 16, 2010

Nonpotable water or Colorado River water is available for use for cemeteries, parks, highway landscape areas, new industrial facilities and golf courses. The project may be required to use nonpotable water for such uses. The District may need additional facilities for the orderly expansion of its nonpotable water distribution system in order to serve the subject land. These facilities may include additional piping, reservoirs, booster pumping stations, etc. The developer may be required to install these facilities and provide land and/or easements to be deeded to the District for such purpose.

If you have any questions, please call Tesfaye Demissie, Stormwater Engineer, extension 2605.

Yours very truly,



Mark L. Johnson  
Director of Engineering

Enclosure/1/as

cc: Kohl Ranch II, LLC (with enclosures)  
Kohl Ranch Company, LLC  
11990 San Vicente Boulevard, Suite 200  
Los Angeles, CA 90049

Alan French  
Riverside County Transportation Department  
4080 Lemon Street, 8<sup>th</sup> Floor  
Riverside, CA 92501

Majeed Farsha (with enclosures)  
Riverside County Department of Transportation  
38-686 El Cerrito Road  
Palm Desert, CA 92211

Michael Mistica  
Department of Environmental Health  
Post Office Box 1280  
Riverside, CA 92502

TD:ch/eng/sw/10/SEPT/Specific Plan 303-Amendment 2

0126.22  
Sun City

0141.

0106.51

ORDINANCE NO. 1234

File: 0106.51  
0141.

AN ORDINANCE OF THE  
COACHELLA VALLEY WATER DISTRICT  
ESTABLISHING REQUIREMENTS RELATING TO  
STORMWATER POLICIES ON PUBLIC NOTIFICATION  
AS CONDITIONS TO APPROVAL OF DEVELOPMENTS  
IN AREAS SUBJECT TO SPECIAL FLOOD HAZARDS  
AND REPEALING ALL ORDINANCES IN CONFLICT THEREWITH

The Board of Directors of the COACHELLA VALLEY WATER DISTRICT  
(hereafter "District") finds and determines as follows:

1. There are areas within the Coachella Valley which are subject to flash flooding, primarily from the hills and mountains surrounding the valley; flood control facilities in those areas should be designed using the "standard project storm" and "standard project flood" as design criteria rather than the "100-year storm" and "100-year flood" in order to minimize flood damage, the "standard project storm" and "standard project flood" require a greater level of protection.

2. There are areas within the jurisdiction of the District which have been identified as flood hazard areas by the Federal Insurance Administration on maps prepared by the Federal Insurance Administration entitled "The Flood Insurance Study for the County of Riverside" dated January 18, 1979, as amended, including the Federal Insurance Rate Map Panel 1625, revised September 30, 1988. Such areas are subject to Riverside County Ordinance No. 458 (Ordinance No. 458) regulating development in flood hazard areas and implementing the National Flood Insurance Program. The Ordinance provides that no land shall be developed within a flood hazard area until the applicable requirements of the Ordinance have been met.

3. Said Ordinance No. 458 is based on the use of the 100-year storm and 100-year flood as the design criteria for the level of flood protection required as a condition to approval by the County of the proposed development. In areas within its jurisdiction, the District serves as the County's agent in determining whether the requirements of Ordinance No. 458 have been met.

4. The flood protection facilities constructed by a developer in compliance with the requirements of Ordinance No. 458 will normally not be



owned, operated or maintained by the District. The District, by approving a plan for such facilities, does not accept ownership of said facilities, responsibility for construction of same in conformance with the plan, or responsibility for the operation and/or maintenance of the facilities once completed. Nevertheless, by reason of its flood control functions in other areas, it is possible that the District will be misperceived as responsible for any damage that may result from improper construction, operation or maintenance of such private flood control facilities.

5. In order to reduce the risk of unfounded lawsuits against the District by reason of the foregoing and in order to reduce the cost of defending any such suits as may be filed, the District should:

A. Require notification in writing to property owners downstream a distance of six hundred (600) feet from the development's lower boundary (subject to the General Manager's discretion to extend said distance as reasonably required by geographic circumstances) that flood protection work is being done as part of the upstream development in accordance with Ordinance No. 458 and may affect downstream properties; and

B. Require the developer to agree that upon transfer of the ownership of the flood control facilities to a homeowners association or other entity (which transfer shall not require the District's approval or consent), the obligation of the developer shall be assumed by the transferee insofar as it relates to operation and/or maintenance of said facilities; and

C. Require the developer to seek neither damages nor indemnity from the District based on the District's approval of the developer's design for flood protection facilities.

6. In the event that an area within the County of Riverside which is presently subject to Ordinance No. 458 becomes part of a city by annexation or otherwise, if that city requests the District to approve proposed plans for flood control facilities related to new development within that area, which plans are based upon a design criteria using a design storm and design flood of lower magnitude than a standard project

storm and standard project flood, the policy set forth herein should continue to apply.

NOW, THEREFORE, BE IT ORDAINED by the Board of Directors of the COACHELLA VALLEY WATER DISTRICT that conditions to approval by the District of a plan for flood control facilities for proposed development as meeting the requirements of Ordinance No. 458 shall include the following:

1. The developer shall give written notice of the proposed flood control plan proposed in connection with the development to property owners downstream a distance of six hundred (600) feet from the development's lower boundary (subject to the General Manager's discretion to extend the distance as reasonably required by geographic circumstances); said notice shall include a statement that said flood protection work is being proposed in accordance with Ordinance No. 458 and may affect downstream properties; and

2. The developer shall execute an agreement with the District which shall include the following provisions:

A. A provision that upon the transfer of the ownership of the flood control facilities to a homeowners association or other entity (which transfer shall not require the approval or consent of District), the obligation of the developer shall be assumed by the transferee in ownership of said facilities insofar as said obligation relates to the operation and/or maintenance of said facilities.

B. A provision that the developer agrees that it will seek neither damages nor indemnity from District based on or related to the design of the flood control facilities within the project.

C. A provision that the developer will cause to be recorded in the chain of title to the flood control facilities within the project a document which will cause these obligations regarding the operation and maintenance of the flood control facilities to run with the land and to be binding upon successors and assigns of the developer in connection with the ownership of said facilities and shall further cause these obligations to be included in any contract between the developer and a homeowners association for the conveyance of the flood control facilities.

3. The foregoing conditions to approval by the District shall also apply in cases where a flood hazard area within the County which is presently subject to Ordinance No. 458 becomes part of a city by annexation or otherwise and the city requests the District to approve proposed plans for flood control facilities related to new development in that area based upon design criteria using a design storm and design flood having a magnitude which is less than a standard project storm and standard project flood.

**REPEALS:**

All other ordinances or parts of ordinances in conflict with the provisions of this Ordinance are hereby expressly repealed.

BE IT FURTHER ORDAINED that this Ordinance shall become effective upon adoption.

ADOPTED this 8th day of December, 1992.

  
\_\_\_\_\_  
President

I, the undersigned Acting Assistant Secretary of the Coachella Valley Water District, do hereby certify that the foregoing is a true and correct copy of Ordinance No. 1234 of said District introduced and passed at meeting of said Board held December 8, 1992, and that said Ordinance was passed by the following vote:

Ayes:	Five
Directors:	McFadden, Rummonds, DeLay, Fish, Codekas
Noes:	None
Absent:	None

I further certify that said Ordinance was thereupon signed by the President of the Board of Directors of said District.

Julia Hernandez  
Acting Assistant Secretary

(SEAL)



Established in 1918 as a public agency

## Coachella Valley Water District

**Directors:**

Patricia A. Larson, President  
Peter Nelson, Vice President  
Tellis Codekas  
Franz W. De Klotz  
Russell Kitahara

**Officers:**

Steven B. Robbins, General Manager-Chief Engineer  
Julia Fernandez, Board Secretary

June 16, 2010

Redwine and Sherrill, Attorneys

File: 0126.1

Jay Olivas  
Riverside County Planning Department  
4080 Lemon Street, 8<sup>th</sup> Floor  
Riverside, CA 92501

Dear Mr. Olivas:

Subject: Kohl Ranch Specific Plan Existing Conditions Offsite Runoff Analysis Approval

The Coachella Valley Water District (CVWD) and our consultants Northwest Hydraulics (NHC) have reviewed and approved *Existing Conditions Offsite Runoff Analysis of Martinez Canyon and The Valley Floor for Kohl Ranch Specific Plan*, dated June, 2010, prepared by Albert A. Webb Associates (enclosed).

The Kohl Ranch Specific Plan No. 303 boundary comprises approximately 2,100 –acres of land located north of Avenue 66, east of Polk Street and south of Avenue 60. In general, the area south of Avenue 64 which includes the Tentative Tract Map No 33487 is subject to flooding from Martinez Canyon while area to the north is subject to Valley Floor drainage flows.

This report establishes the entire Specific Plan existing or pre-project conditions from offsite regional flooding from Martinez Canyon and from Valley Floor Drainage areas which will be used in the design of flood control and drainage facilities.

Subsequent report submittals will address the “proposed” or project conditions for TTM 33487 and future development plans in which the flood control & drainage facilities will be reviewed to ensure compliance with FEMA, CVWD, County and California Drainage Law regulations and standards.

Jay Olivas  
Riverside County  
Planning Department

2

June 16, 2010

If you have any questions, please contact Georgia Celehar-Bauer, Principal Stormwater Engineer, at extension 2288.

Yours very truly,

Mark L. Johnson  
Director of Engineering

cc: Majeed Farshad (with enclosure)  
Riverside County Transportation Department  
38686 El Cerrito Road  
Palm Desert, CA 92211

David Mares  
Riverside County Planning Department  
38686 El Cerrito Road  
Palm Desert, CA 92211

Bob Lyman  
Riverside County Building and Safety  
38686 El Cerrito Road  
Palm Desert, CA 92211

Scott Hildenbrand  
Joseph Caldwell  
Albert A. Webb Associates  
3788 McCray Street  
Riverside, CA 92506

Tom Levy (copied via e-mail)  
Tom Levy Consulting

Jim Price (copied via e-mail)

GCB:ch\eng\109\june\Kohl ranch runoff analysis

FILE  
Jal GCB/10/16/10  
P. Price  
6/15/10

# AGUA CALIENTE BAND OF CAHUILLA INDIANS

TRIBAL HISTORIC PRESERVATION



November 3, 2010

Jay Olivas, Project Planner  
Transportation and Land Management Agency  
Riverside County Planning Department  
38-686 El Cerrito Road  
Palm Desert, CA 92211

**RECEIVED**

DEC 07 2010

Riverside County  
Planning Department  
Desert Office

**RE: Request for Native American Consultation for Specific Plan Amendment 00303A2 (Kohl Ranch II), Riverside County, CA**

Dear Mr. Olivas:

The Agua Caliente Band of Cahuilla Indians appreciates your efforts to include the Tribal Historic Preservation Office (THPO) in your project. The proposed project location is not within the Reservation boundaries; however, it is within the Tribe's Traditional Use Area. Although the project area is located outside of the Agua Caliente Cultural Register area we are concerned about impacts to cultural resources because the high sensitivity for resources in the vicinity. We suggest you contact the Eastern Information Center at the University of California for specific information. Because of this, the Agua Caliente THPO requests the following for the project:

1. A 100% cultural resources inventory of the project area by a qualified archaeologist prior to any development activities in this area.
2. Approved Cultural Resource Monitor(s) be present during all ground disturbing activities. Experience has shown that there is always a possibility of encountering buried cultural resources during construction related excavations, or archaeological testing/data recovery. Should buried cultural deposits be encountered, the Monitor may request that destructive construction halt and the Monitor shall notify a Qualified (Secretary of the Interior's Standards and Guidelines) Archaeologist to investigate and, if necessary, prepare a mitigation plan for submission to the City and the Agua Caliente THPO.
3. For parcels containing cultural resources an Archaeological Monitor(s) be present during any ground disturbing activities related to the project, including construction. Should buried cultural deposits (including human remains), be encountered, the Archaeological Monitor shall notify a Qualified (Secretary of the Interior's Standards and Guidelines) Archaeologist to investigate and, if necessary, prepare a mitigation plan for implementation.
4. Additionally, in accordance with State law, the County Coroner should be contacted if any human remains are found during earthmoving activities. If the remains are determined to be of Native American origin, the Native American Heritage Commission (NAHC) shall be contacted. The NAHC will make a determination of the Most Likely Descendent (MLD). The City will work with the designated MLD to determine the final disposition of the remains.
5. Copies of any cultural resource documentation (reports and site records) that might be generated in connection with these efforts for permanent inclusion in the Agua Caliente Cultural Register.



The Tribe requests consultation pursuant to SB 18 (Government Code §65352.3) on the Kohl Ranch II Specific Plan Amendment SP003003A2. Please contact our offices for further consultation and information about Approved Cultural Resource Monitors. Again, the Agua Caliente Tribe appreciates your interest in our cultural heritage. If you have questions or require additional information, please call me at (760) 699-6907. You may also email me at [ptuck@aguacaliente-nsn.gov](mailto:ptuck@aguacaliente-nsn.gov).

Cordially,

Patricia Garcia-Tuck, Director  
Tribal Historic Preservation Office  
**AGUA CALIENTE BAND  
OF CAHUILLA INDIANS**

c: Agua Caliente Cultural Register

X:\CONSULTATIONS Letters\2010\External\Traditional Use Area \RC\_SB18\_SP003003A2\_11\_03\_10.doc



737

**COUNTY OF RIVERSIDE**  
**TRANSPORTATION AND LAND MANAGEMENT AGENCY**

George A. Johnson · Agency Director  
**Planning Department**  
Carolyn Syms Luna · Planning Director

*[Handwritten signature]*

September 27, 2010

Agua Caliente Band of Cahuilla Indians  
Richard Milanovich, Chairperson  
5401 Dinah Shore Drive  
Palm Springs, CA 92262

10-01-10A11:39 RCVD

*C- Milanovich please*

**RE: Native American Consultation request for Specific Plan No. 303, Amendment No. 2 (SP00303A2)**

Dear Chairperson Milanovich,

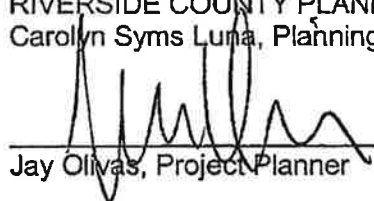
The County of Riverside requests your participation in the review of SP00303A2 located at Section 9, Township 7 South, Range 8 East, also referred to as Assessor's Parcel Number 759-190-004, etc. This project is subject to the California Environmental Quality Act (CEQA) and Section 65352.3 of the Government Code (Senate Bill 18 [2004]).

Staff contacted the Native American Heritage Commission (NAHC) which gave the County your tribe's name. The County is seeking a consultation with your tribe concerning the potential of Native American cultural resources that may be impacted by the above project. The project is currently being reviewed under CEQA and an archaeological and cultural resource survey may be requested to determine the presence or absence of cultural resources. Any information you have regarding cultural places will be kept strictly confidential and will not be divulged to the public.

Riverside County feels that your assistance is vital to the preservation and conservation of ancestral tribal sites during the current growth that the County is experiencing. It is the desire of Riverside County to protect the past and our collective heritage as we proceed towards the future. Please forward any comments or requests for meetings concerning this project to Leslie Mouriquand, County Archaeologist by December 29, 2010.

Sincerely,

RIVERSIDE COUNTY PLANNING DEPARTMENT  
Carolyn Syms Luna, Planning Director

  
Jay Olivas, Project Planner

Attachments  
USGS  
Project site plan and description (Exhibits A-1, A-2)

cc: Leslie Mouriquand, County Archaeologist  
Kohl Ranch II, LLC

Y:\Planning Master Forms\Misc Staff Forms and Documents\Cultural Resources\Native American Consultation Request Letter.doc

Riverside Office · 4080 Lemon Street, 9th Floor  
P.O. Box 1409, Riverside, California 92502-1409  
(951) 955-3200 · Fax (951) 955-3157

Desert Office · 38686 El Cerrito Road  
Palm Desert, California 92211  
(760) 863-8277 · Fax (760) 863-7555

September 7, 2010

Jay Olivas, Project Planner  
Riverside County Planning Department  
4080 Lemon Street, 9<sup>th</sup> Floor  
Riverside, CA 92501

RE: Specific Plan No. 303, Amendment No. 2 – Kohl Ranch

Dear Mr. Olivas:

The SunLine Transit Agency (SunLine) would like to thank you for the opportunity to review and comment on the second amendment to adopted Specific Plan No. 303 (Kohl Ranch), which will encompass approximately 535 acres and be located within the Thermal Community of unincorporated Riverside County.

SunLine staff reviewed the second amendment to the adopted Kohl Ranch Specific Plan No. 303 and due to the size of the development; there might be a potential demand in the future to provide transit service once the project is completed. As the regional transit provider, SunLine's goal is to partner with the jurisdictions to create sustainable transit service to emphasize smart growth principles. SunLine recommends that the County Planning Department and developer work with SunLine to incorporate transit friendly designs into the project to allow SunLine to meet the potential future transit needs. The current design guidelines outlined within the proposed second amendment of the Kohl Ranch Specific Plan are not transit friendly.

Staff encourages the County Planning Department and developer to review SunLine's Transit Facility Design Manual to incorporate some of the concepts discussed in the manual throughout the Specific Plan. Furthermore, we continue to monitor on-going developments so that we can address mobility concerns to meet the transportation needs of Coachella Valley residents. SunLine is willing to meet with you to discuss our suggestions and assist with any questions regarding the Transit Facility Design Manual. Should you have any concerns or questions, please contact me at 760-343-3456, ext. 162.

Sincerely,



Brenda Ramirez  
Assistant Transit Planner

cc: C. Mikel Oglesby, General Manager  
Eunice Lovi, Director of Planning and Development