

Table 3.1-A, Land Use Plan Statistical Summary

Land Use	Acreage	Percent of Total	Percent of Developable Area ¹	Target Dwelling Units
RESIDENTIAL				
Medium Density Residential (MDR)	501.33	23.2	32.8	1,652
Medium High Density Residential (MHDR)	467.76	21.6	30.6	3,245
High Density Residential (HDR)	140.35	6.5	9.2	1,629
Very High Density Residential (VHDR)	30.85	1.4	2.0	477
Total Residential	1,140.29	52.7	74.7	7,003
INDUSTRIAL				
Heavy Industrial (HI)	81.17	3.8	5.3	
Total Industrial	81.17	3.8	5.3	
BUSINESS				
Mixed Use/Air Park (MU/AP)	24.45	1.1	1.6	
Mixed Use	252.73	11.7	16.6	159 ³
Total Business	277.18	12.8	18.2	159³
COMMERCIAL				
Commercial Retail (CR)	28.27	1.3	1.9	
Total Commercial	28.27	1.3	1.9	
OTHER				
Open Space (OS)	375.94	17.4		
Public Facilities/Schools (PF)	84.30	3.9		
Right-of-Way (ROW)	175.5	8.1		
Total Other	641.44	29.4		
TOTAL	2,162.65	100%	100%	7,162

1. Assumes total of 1,526.91 acres of developable land. This does not include land uses in "other" category.
2. The ROW acreage includes local streets, which are not depicted on the Land Use Plan. See **Figure 3.1-2, Circulation Plan**.
3. This number represents a maximum, rather than a target.

Table 3.1-B, Land Use Summary By Planning Area

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Mixed Use (MU)		46.18	
A-7	Open Space (OS)		9.97	
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0-8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0
C-1	Medium Density Residential (MDR)	122	40.77	2.0-5.0
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0
C-5	Open Space (OS)		9.97	
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0
C-8	Medium High Density Residential (MHDR)	105	14.96	5.0-8.0
D-1	Open Space (OS)		110.66	
E-1	Open Space (OS)		5.61	

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
E-2	Mixed Use (MU)		148.74	
E-3	Open Space (OS)		69.33	
E-4	Mixed Use (MU)		12.25	
E-5	Mixed Use (MU)	19 ¹	5.31	
E-6	Mixed Use (MU)	120 ¹	33.99	
E-7	Mixed Use (MU)	5 ¹	1.32	
E-8	Mixed Use (MU)	15 ¹	4.94	
F-2	Medium Density Residential (MDR)	158	71.64	2.0-5.0
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0
F-4	Commercial Retail (CR)		6.23	
G-1	Commercial Retail (CR)		4.43	
G-2	Open Space (OS)		1.44	
G-3	Open Space (OS)		1.19	
G-4	Open Space (OS)		5.6	
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0
G-6	Open Space (OS)		13.65	
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0
G-9	Open Space (OS)		6.17	
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0

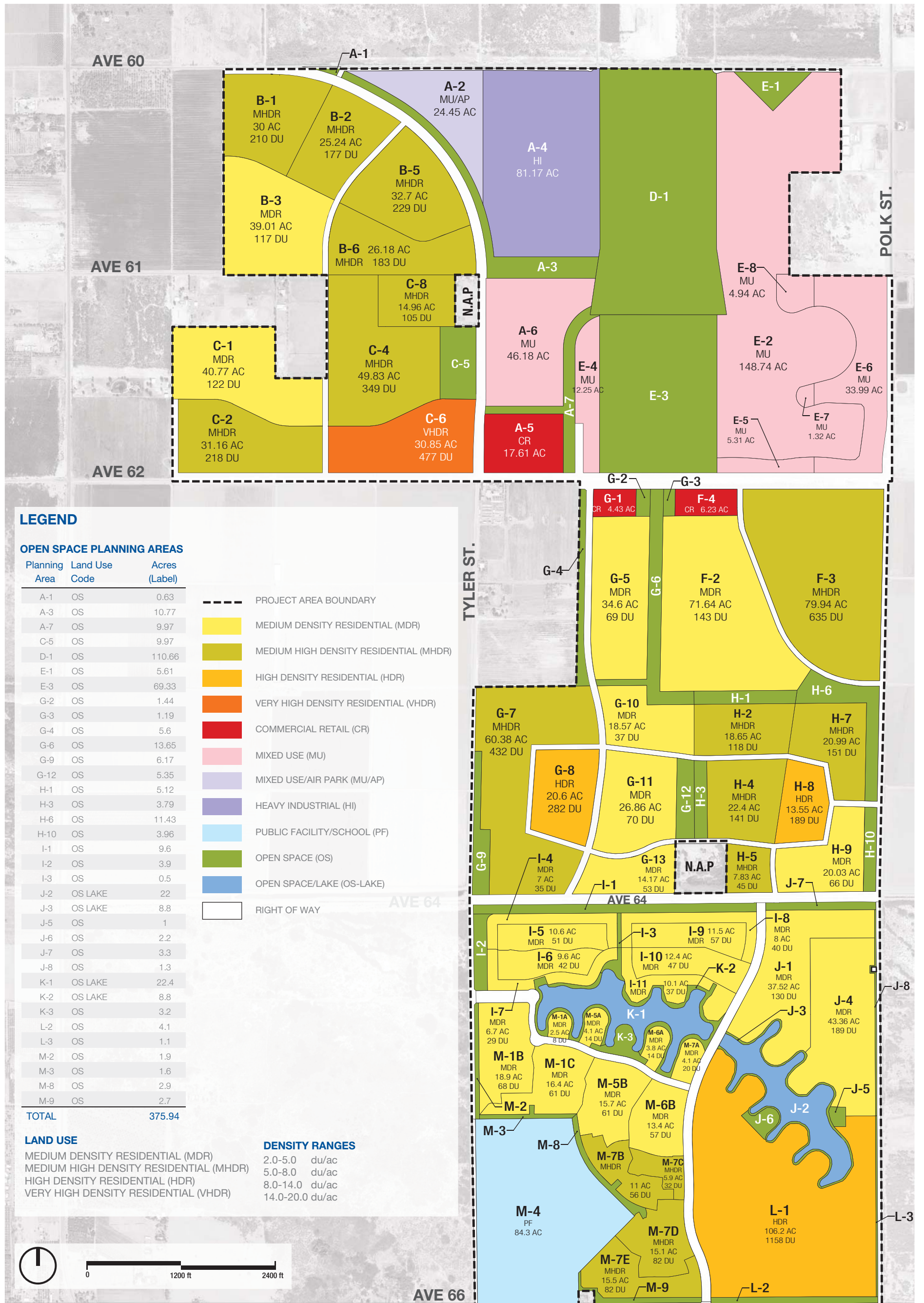
Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
G-12	Open Space (OS)		5.35	
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0
H-1	Open Space (OS)		5.12	
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0
H-3	Open Space (OS)		3.79	
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0
H-6	Open Space (OS)		11.43	
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0-8.0
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0
H-10	Open Space (OS)		3.96	
I-1	Open Space (OS)		9.6	
I-2	Open Space (OS)		3.9	
I-3	Open Space (OS)		0.5	
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0
I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0
J-1	Medium Density Residential (MDR)	130	37.52	2.0-5.0
J-2	Open Space/Lake (OS-LAKE)		22	
J-3	Open Space/Lake (OS)		8.8	
J-4	Medium Density Residential (MDR)	189	43.36	2.0-5.0
J-5	Open Space (OS)		1	
J-6	Open Space (OS)		2.2	
J-7	Open Space (OS)		3.3	
J-8	Open Space (OS)		1.3	
K-1	Open Space/Lake (OS-LAKE)		22.4	
K-2	Open Space/Lake (OS)		8.8	
K-3	Open Space (OS)		3.2	
L-1	High Density Residential (HDR)	1,158	106.2	8.0-14.0
L-2	Open Space (OS)		4.1	
L-3	Open Space (OS)		1.1	
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0
M-2	Open Space (OS)		1.9	
M-3	Open Space (OS)		1.6	

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
M-4	Public Facility (PF)		84.3	
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0-8.0
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0-8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
Right-of-Way			175.5	
TOTALS		<u>7.162</u>	<u>2.162.65</u>	

Note: Planning Area's B-4, B-7, C-3 and C-7 have been intentionally left out.

1. This number represents a maximum rather than a target.



Residential

Residential areas within the project will provide a target of 6,988 dwelling units but with a maximum not to exceed 7,171 dwelling units, with an overall gross density of 4.03 dwelling units per gross residential acre¹. The residential element of the plan comprises 1,140.29 acres or 52.7 percent of the total project site. Four residential land uses are proposed, Medium Density Residential, Medium High Density Residential, High Density Residential and Very High Density Residential, establishing appropriate transitions to adjacent non-residential land uses, both on and off site. It should be noted that residential planning areas can be developed to a lower density than that specified by the planning area land use designation, without requiring an amendment to the Specific Plan. Additionally, the maximum density and the total number of dwelling units within the high and very density residential designations can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Residential uses north of Avenue 62 and west of Tyler Street are intended to house employees of the nearby commercial, business and industrial uses.

Each of the residential land use designations (i.e., Medium Density Residential, Medium High Density Residential, High Density Residential and Very High Density Residential) can be developed with several product types, including those which permit clustering of residential density within planning areas. Residential product types include: Detached Dwelling Units, Cluster Detached Dwelling Units, and Cluster Attached Dwelling Units.

The residential designations each have a unique set of development standards designed to accommodate a variety of housing product types. These different residential designations are intended to provide flexibility to develop in areas that are affected by the open space requirements and noise impacts of the Jacqueline Cochran Regional Airport safety zones and the limitations imposed by the utility easement beltway in the central portion of the site. By allowing for clustering of development within planning areas, it is possible to retain residential densities while providing adequate open space for drainage ways, recreational and other lakes and golf courses, the power line easement, and the airport zones.

Under the Specific Plan, it is possible to develop one or more of the product types within an individual planning area or neighborhood, provided that the development is consistent with the project's Design Guidelines.

¹ The overall gross density for residential areas was determined by dividing the total number of dwelling units by the acreage devoted to residential land uses, open space, public facilities, and local residential streets. The gross density—if determined by dividing the total dwelling units by the residential acreage—is 6.28. This density is reduced to 3.31 if the entire site acreage is used.

Medium Density Residential (2.0–5.0 du/acre)

Medium Density Residential units are proposed throughout the project site. A target of 1,652 dwelling units on 501.33 acres is planned. This is based on a density range of 2.0–5.0 du/ac. The purpose of this designation is to provide a medium density residential alternative in the plan. The intent is to: 1) locate lower density land uses at the project periphery adjacent to off-site lands currently in low intensity uses such as agriculture; 2) provide a transition between open space/recreational areas and medium density land uses; 3) provide for limited residential uses where permitted within the Extended Runway Centerline (ERC) airport safety zone; and 4) create a housing opportunity offering greater private open space amenity than is available in the higher density residential zones.

In addition to One-Family Dwellings development, additional uses within the Medium Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; water wells and appurtenant facilities; day care centers; libraries; religious institutions; community centers; and schools. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Medium High Density Residential (5.0–8.0 du/acre)

Medium High Density Residential units are proposed throughout the project site. A target of 3,245 dwelling units on 467.76 acres is planned. This is based on a density of range of 5.0–8.0 du/acre. The purpose of this designation is to provide a lower density creative residential alternative in the plan. The intent is to: 1) create the potential for single-family, first home buyers; 2) offer a desirable housing product for retirees and second home buyers; and 3) accommodate residents seeking more private open space amenities than are available in the High and Very High Density Residential areas.

In addition to Multiple Family Dwellings development, additional uses within the Medium High Density Residential land use designation may include such uses as dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; water wells and appurtenant facilities; day care centers; libraries; religious institutions; community centers; and schools. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

High Density Residential (8.0–14.0 du/acre)

High Density Residential land uses are located in close proximity to community amenities and trails systems for the project. A target of approximately 1,629 dwelling units on 140.35 acres is planned, assuming a density range of 8.0–14.0 du/acre. The purpose of this designation is to enhance the range of housing types provided within the project area. The intent is to: 1) achieve an affordable living environment for those working in the project vicinity; 2) provide a more compact housing product within close proximity to commercial services and employment opportunities; 3) create pedestrian-oriented communities with common open space and recreation facilities; and 4) orient residential areas around community focal points such as churches and community centers. The maximum density and the target number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

In addition to Multiple Family Dwellings development, additional uses within the High Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; and water wells and appurtenant facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Very High Density Residential (14.0–20.0 du/acre)

Very High Density Residential land uses are located in close proximity to community commercial cores established for the project. A target of approximately 477 dwelling units on 30.85 acres is planned, assuming a density range of 14.0–20.0 du/acre. The purpose of this designation is to enhance the range of housing types provided within the project area. The intent is to: 1) achieve an affordable living environment for those working in the project vicinity; 2) provide a more compact housing product within close proximity to commercial services and employment opportunities; 3) create pedestrian-oriented communities with common open space and recreation facilities; and 4) orient residential areas around community focal points such as churches and community centers. The maximum density and the target number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

In addition to Multiple Family Dwellings development, additional uses within the Very High Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; and water wells and appurtenant facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Industrial

One industrial land use category is proposed offering a range of employment opportunities that respond to market conditions and the skills base of the local labor force. A total of approximately 81.17 acres of heavy industrial uses are concentrated in the northern portion of the site, and in close proximity to the Jacqueline Cochran Regional Airport and Polk Street, a designated arterial highway providing access to the airport and points east.

The purpose of this designation is to allow for more intense, industrial/manufacturing uses or other uses compatible with the regional airport. The intent is to: 1) encourage land uses which provide relatively greater levels of employment than is typically associated with industrial uses such as warehousing and distribution; 2) allow for uses that require outdoor storage; and 3) maximize use of the regional transportation system, including air, rail and the road network.

In addition to Manufacturing-Heavy development, additional uses within the Heavy Industrial land use designation may include such uses as water wells and appurtenant facilities; aerial services including advertising, photography and tours; aerospace/aeronautical or automotive museums; aircraft taxiways; catering services/flight kitchens; conference facilities; golf courses and appurtenant facilities; convenience stores; dry cleaners; flight schools; hospitals; hotels and motels; intermodal cargo transfer terminals; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; paper recycling facilities; parcel delivery services; and rental car agencies including the storage of rental cars; motor sports race track and facilities necessary and incidental thereto including, a race track, private garages, fuel storage, buildings for vehicle display, tuning shop, and observation tower; cogeneration plants; structures and facilities necessary and incidental to the development, generation and transmission of electric power and gas such as power plants, booster or conversion plants, transmission lines, pipelines and the like; and incarceration and detention facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Business

Mixed Use/Air Park

The Mixed Use/Air Park designation is located in the northernmost portion of the project site, just south of the Jacqueline Cochran Regional Airport. Approximately 24.45 acres are devoted to this land use. The purpose of this designation is to accommodate a wide range of land uses including office, service, commercial, airport-related, incubator business, and very light industrial. The intent is to: 1) complement expansion plans for the Jacqueline Cochran Regional Airport by providing needed services and commercial uses located near the Tyler Street airport entrance; 2) create a supportive environment for start-up businesses; 3) devise standards that encourage combinations of uses such as office/sales/storage/assembly; and 4) provide support services for business and industrial uses in the project area.

In addition to General Commercial development, additional uses within the Mixed Use/Air Park land use designation may include such uses as aviation equipment assembly; communication equipment and microwave sales and installation; computer and office equipment sales, service, repair and assembly; conference facilities; country clubs, manufacture of dairy products, not including dairies; emergency and urgent care medical facilities; libraries; manufacture of grain and bakery products; health and exercise centers; hospitals; ice houses; jewelry manufacture and repair; manufacture of wearing apparel and accessories; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture of handbags, luggage, footwear, and other personal leather goods; manufacture of cutlery, tableware, hand tools and hardware; manufacture of plumbing and heating items; vehicle storage and impoundment; manufacture of office and computing machines; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; manufacture of non-alcoholic beverages; manufacture of confectionery products; manufacture and repair of refrigeration and heating equipment; printing of periodicals, books, forms, cards, and similar items; public parks and public playgrounds; golf courses; religious institutions; facilities for research and development of precision components and products; facilities related to alternative energy development; large-scale recreational uses such as a motor sports race track and facilities related thereto, including but not limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle display areas, and ancillary uses in support thereof; water wells and appurtenant facilities; aerial service businesses including advertising, photography and tours; aerospace/aeronautical museums; aircraft equipment sales, service and repair; contractor storage yards; flight schools; intermodal cargo transfer facilities; manufacture of furniture and fixtures, including cabinets, partitions, and similar small items; manufacture of bicycles; parcel delivery services; warehousing and distribution; community centers; schools; meat and poultry processing not including slaughtering or rendering of animals; paper shredding facilities; research and manufacture of drugs and pharmaceuticals; manufacture of soaps, cleaners and toiletries; wrought iron fabrication; machine, welding and blacksmith shops; breweries, distilleries and wineries; paper storage and recycling within a building; recycling processing facilities; paper and paperboard mills; manufacture of containers and boxes; and above ground natural gas storage. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance an approved list of allowable uses.

Mixed Use

The Mixed Use designation is located in the northernmost portion of the project site, just south of the Jacqueline Cochran Regional Airport. Approximately 252.73 acres are devoted to this land use. The purpose of this designation is to accommodate a wide range of land uses including office, service, commercial, airport-related, incubator business, mixed-use residential units, very light industrial, and large scale recreational development. The intent is to: 1) complement expansion plans for the Jacqueline Cochran Regional Airport by providing needed services and commercial uses located near the Tyler Street airport entrance; 2) devise standards that encourage combinations of uses such as office/sales/storage/assembly; 3) provide support services for business and industrial uses in the project area; and 4) provide standards unique to development of large scale recreational use.

In addition to General Commercial and Manufacturing-Heavy development, additional uses within the Mixed Use/Air Park land use designation may include such uses as above ground natural gas storage less than 6,000 gallons; aerial service businesses including advertising, photography and tours; aircraft equipment sales, service and repair; contractor storage yards; aircraft taxiways; aviation equipment assembly; breweries, distilleries and wineries; catering services/flight kitchens; community centers; computer and office equipment sales, service, repair and assembly; conference facilities; convenience stores; country clubs; dry cleaners; emergency and urgent care medical facilities; facilities for research and development of precision components and products; flight schools; hospitals; hotels and motels; ice houses; intermodal cargo transfer facilities; jewelry manufacture and repair; facilities related to large scale recreational uses such as a motor sports race track and facilities related thereto, including but not limited to race track, private garages, single-family residential including duplex units defined as a structure with two dwelling units placed beside one another sharing a common wall, clubhouse, tuning shop, observation tower, museum, vehicle display areas, underground fuel storage and ancillary uses in support thereof; libraries; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture and repair of refrigeration and heating equipment; manufacture of bicycles; manufacture of confectionery products; manufacture of dairy products, not including dairies; manufacture of furniture and fixtures, including cabinets, partitions, and similar small items; manufacture of grain and bakery products; manufacture of cutlery, tableware, hand tools and hardware; manufacture of non-alcoholic beverages; manufacture of office and computing machines; manufacture of plumbing and heating items; manufacture of wearing apparel and accessories; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; outdoor film studio; paper storage and recycling within a building; parcel delivery services; public parks and public playgrounds; religious institutions; rental car agencies including the storage of rental cars; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; research and manufacture of drugs and pharmaceuticals; schools; warehousing and distribution; water wells and appurtenant facilities. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Due to the unique nature of large scale recreational development north of Avenue 62, the Mixed Use designation allows for two or more different types of uses contiguous to one another, planned as a unit. For instance, with racetrack development north of Avenue 62, certain planning areas could develop as residential product types adjacent to racetrack facilities.

These planning areas will also allow for duplex units, or structures with two dwelling units placed beside one another sharing a common wall.

A maximum of 159 dwelling units on 46 acres is planned as part of these mixed-use development areas.

Commercial-Retail

A Commercial Retail land use designation, comprising a total of 28.27 acres, is proposed for the project. The purpose of this designation is to provide a range of commercial uses in support of broader employment and residential uses within the Plan. The intent is to: 1) provide limited neighborhood commercial uses to serve residential communities; 2) incorporate commercial uses within walking distance of residential neighborhoods and employment uses; 3) locate highway-oriented commercial uses at the project periphery or along major through roadways, with easy access to and visibility from the arterial highway system; 4) accommodate "big box" commercial uses on larger sites, to serve regional markets; and 5) provide appropriate circulation, parking and loading areas to handle traffic generated by commercial land uses.

In addition to general commercial development, additional uses within the Commercial Retail land use designation may include such uses as public parks and public playgrounds; golf courses; country clubs; animal hospitals with all kennels entirely indoors; health clubs; computer sales and repair stores; parcel delivery services; libraries; religious institutions; community centers; schools; and water wells and appurtenant facilities; and electric vehicle charging stations. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Public Facilities/Schools

This land use designation totals 84.30 acres and consists of property owned by the Coachella Valley Unified School District as a site for multiple schools. SPA3 recognizes that these schools have been constructed and are operational.

In addition to One-Family Dwelling development, additional uses within the Public Facilities/Schools land use designation may include such uses as government offices, courthouses, police stations, fire stations, libraries, museums, public schools, water wells and appurtenant facilities; day care centers; religious institutions; community centers; private schools and communication facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Open Space

The purpose of this designation is to provide open space and recreational amenities to serve populations in the project neighborhoods. Approximately 375.94 acres of open space is provided. The intent is to: 1) allow for a variety of passive and active recreational experiences for neighborhood residents and visitors, ranging in scale from community parks to more intimate neighborhood parks; 2) incorporate walking and bicycle trails and linkages between residential neighborhoods and commercial/employment centers, where appropriate; 3) comply with land use restrictions for Jacqueline Cochran Regional Airport safety zones which require the maintenance of open space clear of obstructions; 4) accommodate stormwater runoff

originating both on and off site which must be controlled and conveyed through the property; and 5) provide a variety of landscape settings ranging from highly landscaped to native communities for visual interest as well as buffering between adjoining land uses.

The Specific Plan allows for large-scale recreational uses such as a motor sports race track in the northern portion of the site as a permitted land use. Any such large-scale recreation use, if developed, would be sited around the drainage corridors. The Specific Plan also allows clubhouses and their associated shopping and dining facilities. Additionally, uses such as lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water will be a permitted use in the southern portion of the site. The intent is to: 1) diversify the recreational opportunities available to area residents and visitors; 2) create an attractive amenity around the clubhouses that is enhanced through landscaping and design treatments; 3) develop a central meeting place (clubhouse) that offers uses that are incidental to the primary recreation use; and 4) stimulate job growth and creation of new employment opportunities.

Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Roads

Major road rights-of-way totaling approximately 176 acres within the project site will be implemented in conjunction with the project.

3.1.2 Project-Wide Development Standards

To ensure a logical, orderly, and sensitive development of land uses proposed for the Kohl Ranch, special development criteria, standards, and mitigation measures have been created for each Planning Area. These area-specific standards, discussed in detail in Section 3.2 – Land Use, Planning, and Development Standards by Neighborhood, provide for appropriate transitions to neighboring land uses.

In addition to these specific techniques, project-wide development standards also have been developed as part of the Comprehensive Land Use Plan and are designed to complement the individual conditions within each Planning Area. These general standards are:

- 1) The total Specific Plan shall be developed with a target of 7,162 dwelling units but with a maximum not to exceed 7,171 dwelling units on 1,140.29 acres of residential uses and approximately 48 acres of mixed use, as illustrated on the Specific Land Use Plan. However, the maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. General uses permitted will include residential, industrial, commercial, business, open space/golf course, recreational and other lakes, country club, and public facilities.
- 2) Uses and development standards shall be in accordance with the County of Riverside Zoning Ordinance No. 348 for this Specific Plan in Section 2 – Specific Plan Zoning

- Ordinance, and shall be defined by Specific Plan objectives, future detailed plot plans and potential conditional use permits as appropriate.
- 3) Standards relating to signage, landscaping, parking and other related design elements shall conform to the County of Riverside Zoning Ordinance No. 348 for this Specific Plan in Section 2, and Coachella Valley Water District Ordinance 1302.1. When appropriate and necessary to meet the goals of this Specific Plan, the standards will exceed the zoning ordinance requirements (see individual Planning Areas, Section 3.2 – Land Use, Planning, and Development Standards by Neighborhood).
 - 4) All project lighting shall be in accordance with County of Riverside standards.
 - 5) The development of property shall be in accordance with the mandatory requirements of all Riverside County ordinances including Ordinance No. 348 for this Specific Plan in Section 2, Ordinance No. 460, and state laws; and shall conform substantially with the adopted Specific Plan as filed in the office of the Riverside County Planning Department, unless otherwise amended.
 - 6) All development on the site will be consistent with this Specific Plan No. 303 and subsequent amendments, as on file with the Riverside County Planning Department, and with all applicable laws of the State of California.
 - 7) Prior to issuance of a building permit for construction of any use contemplated by this approval, the applicant shall first obtain clearance from the County of Riverside Planning Department verifying that all pertinent conditions of the Specific Plan approval have been satisfied for the phase of development in question.
 - 8) Lots created pursuant to this Specific Plan and any subsequent tentative maps, shall be in conformance with the development standards of the Specific Plan herein applied to the property.
 - 9) Agriculture shall continue to be an allowable use in in accordance with Section 2.0 – Specific Plan Zoning Ordinance.
 - 10) Utility service to this Specific Plan shall be provided by individual authorized service providers or through the creation of a Public Utilities District (PUD).
 - 11) A target maximum number of dwelling units is specified for each residential planning area. The target is based on the applicable density range and may or may not represent the maximum number of units allowed within the density range for any single planning area. Therefore, any given implementing subdivision may increase the number of units beyond the target density up to the maximum allowed for its planning area without a specific plan amendment, provided that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171 dwelling units. Further the number of dwelling units contained in an implementing subdivision application may exceed the maximum number of allowable units specified in the Specific Plan for any single planning area by not more than ten percent (10%) without an amendment to this plan, provided that the maximum number of dwelling units in other planning areas are reduced by an equivalent number of units such that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171

dwelling units. See Section 3.1.11, Specific Plan Administration, for the approach to tracking units during the build out of the project. Density transfers will be required to receive administrative approval.

- 12) A total of 5 acres of parkland shall be provided for each one-thousand (1,000) in population within the Kohl Ranch Specific Plan boundary or payment of a fee in-lieu thereof, or a combination of both for neighborhood or community park and recreational facilities. Fees shall be paid in accordance with Riverside County Ordinance 460.

3.1.3 Circulation Plan

The Kohl Ranch Specific Plan circulation system is designed to provide direct and convenient access to all portions of the project site, and to provide efficient connections to major transportation corridors in the project vicinity such as the new SR-86S Freeway (east of the Whitewater River)². In addition, the proposed circulation system has been coordinated with other planning efforts in the area, such as the planned development at Jacqueline Cochran Regional Airport and the South Valley Implementation Plan. Based on a thorough traffic impact analysis (Appendix J of EIR No. 396), with recommended improvements, the study area intersections at buildout will operate at acceptable levels of service consistent with Riverside County policies. Precise access locations and the phasing of roadway improvements shall be determined at the plot plan or tentative tract map level, subject to approval by the Riverside County Transportation Department.

The project traffic analysis estimates that 88,464 trip-ends per day will be generated by the project at buildout (Appendix J of EIR No. 396). With an internal trip discount of ten percent (the percentage of trips assumed not to leave the Kohl Ranch), it is reduced to 79,618 external trip-ends.

The roadway designations are depicted in **Figure 3.1-2, Circulation Plan**. Roadway cross-sections are depicted in **Figure 3.1-3, Roadway Cross Sections**.

On-site, Avenue 62 is currently identified in the Coachella Valley Association of Governments (CVAG) Transportation Project Prioritization Study (TPPS), between Harrison Street and Polk Street. This is a priority list for transportation projects in the Coachella Valley. Transportation Uniform Mitigation Fees (TUMF), Measure A and other funding sources are administered through CVAG to assist local jurisdictions with roadway improvements. Prioritization on the list is based on the following factors: roadway surface condition, system continuity, level of service, and accident rate. The TPPS is typically updated every five (5) years. Avenue 62 is the only roadway on site that appears on the TPPS list.

Public Transit

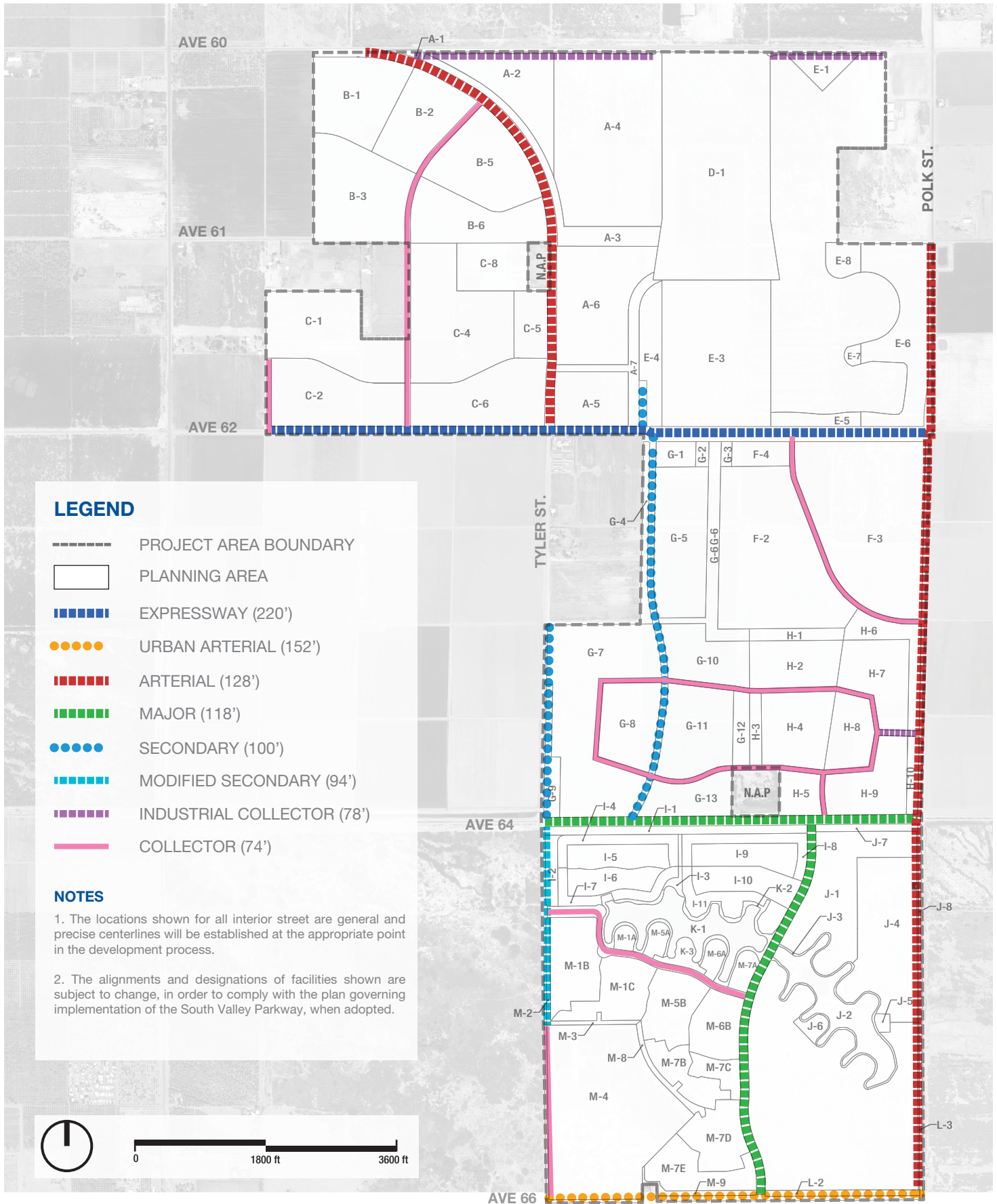
The SunLine Transit Agency (STA) is the regional public transportation operator in the Coachella Valley and will service the project area. According to the SunLine Transit Facilities Design Manual dated December 2006 and the Bus Route Map for Line 91, effective January 2, 2011, one bus route (Line 91) currently operates along Avenue 66 providing both eastbound and westbound service seven days a week. The nearest bus stop is located at Avenue 66 and Middleton Street located at the southwest corner of the project boundary. EIR 396 includes mitigation measures that require coordination with SunLine Transit, bus stops and bus turn-outs.

² For clarification, please note that the designation SR-86S refers to the new freeway constructed east of the Whitewater River. The "old" Highway 86 is located west of the project site and is referred to in this Specific Plan as Harrison Street, its local street name.

Circulation Development Standards

- 1) Any application for any subdivision within the specific plan boundary shall comply with the standards of Ordinance 460..
- 2) Each subdivision shall comply with the on-site and off-site street improvement recommendations and mitigation measures outlined in the subsequent traffic studies for each individual project.
- 3) All roadways intersecting four lane facilities or greater shall be a minimum of 74 feet of right-of-way and constructed in accordance with Standard 103, Ordinance 461 from the four-lane facility to the nearest intersection.
- 4) All typical sections shall be per Ordinance 461, or as approved by the Transportation Department.
- 5) All intersection spacing and/or access openings shall be per Standard 114, Ordinance 461, or as approved by the Transportation Department.
- 6) No textured pavement accents will be allowed within County rights-of-way.
- 7) Mid-block cross-walks are not allowed.
- 8) Driveways and access points. No driveways or access points as shown in this specific plan are approved. All access points shall conform to Transportation Department standard access spacing, depending upon the street's classification.
- 9) Drainage. The drainage facilities necessary for this project will generally be outside of the road rights-of-way and maintained by an owners association, county service area, or by the Coachella Valley Water District. Drainage facilities required for road purposes will be maintained by the transportation department or CVWD.
- 10) Commercial. Per the General Plan, "Neighborhood Commercial Uses must be located along Secondary or greater highways, at or near intersections with Secondary Highways."
- 11) Schools/Parks. The Transportation Department's policy regarding streets adjacent to school sites and park sites requires a minimum of 66' right-of-way (Standard 104).
- 12) Any landscaping within public road rights-of-way will require approval by the Transportation Department and assurance of continuing maintenance through the establishment of a landscape maintenance district or similar mechanism as approved by the Transportation Department.
- 13) All trails developed as part of this specific plan shall be designated as shown on **Figure 3.1-10, Public Facility Sites** or as agreed to by Riverside County Regional Parks and Recreation District; and shall be located within or adjacent to the right-of-way pursuant to **Figure 3.4-18, Tyler Street Trail** and **Figure 3.4-19 Avenue 62 Trail**. Class I bikeways shall be developed in accordance with the standards contained within Chapter 1000 of the California Department of Transportation – Highway Design Manual (Fourth Edition).
- 14) All roadway widths will be at the discretion of the Transportation Department, once the ultimate land uses for the individual planning areas are determined.

- 15) All projects, including subdivisions within the specific plan boundary, shall be subject to a Mitigation Monitoring Program, included as part of EIR No. 396 and any updated environmental assessments associated therewith.
- 16) Prior to the issuance of building permits, the landowner shall convey an avigation easement to Jacqueline Cochran Regional Airport, which shall be recorded, or shall provide evidence that such an easement covering the property has already been recorded..



LEGEND

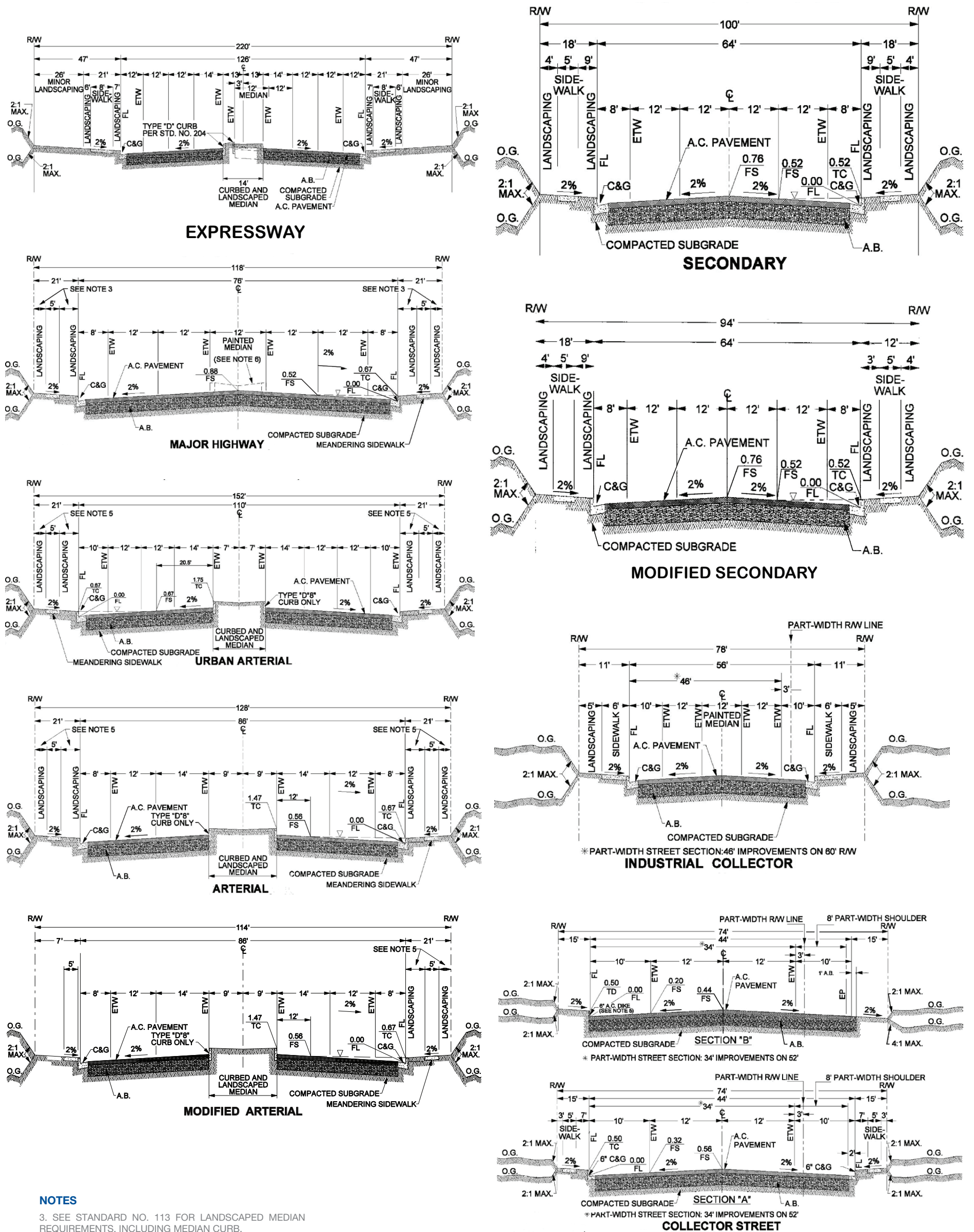
- PROJECT AREA BOUNDARY
- PLANNING AREA
- EXPRESSWAY (220')
- URBAN ARTERIAL (152')
- ARTERIAL (128')
- MAJOR (118')
- SECONDARY (100')
- MODIFIED SECONDARY (94')
- INDUSTRIAL COLLECTOR (78')
- COLLECTOR (74')

NOTES

1. The locations shown for all interior street are general and precise centerlines will be established at the appropriate point in the development process.
2. The alignments and designations of facilities shown are subject to change, in order to comply with the plan governing implementation of the South Valley Parkway, when adopted.



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NOTES

- 3. SEE STANDARD NO. 113 FOR LANDSCAPED MEDIAN REQUIREMENTS, INCLUDING MEDIAN CURB.
- 5. DISTANCE FROM SIDEWALK TO R/W, AND TO FLOW LINE, VARIES. SEE STANDARD NO. 404 FOR DETAILS OF MEANDERING SIDEWALK.

3.1.4 Drainage Plan

Drainage flows enter the property from the northwest corner of the site and along the western and northern boundary, drain through the property and outlet along the southeast boundary between Avenues 66 and "D" Street as depicted in **Figure 3.1-4, Conceptual Drainage Plan**. Development of the project will require the collection and conveyance of flood flows through the project in a manner which will ensure the protection of the developed properties from a 100-year flood. In addition, storm flows will be redispersed along the eastern boundary to approximate existing flow conditions, to avoid adversely impacting downstream properties. The proposed flood control system is designed to collect the storm flows as they enter the site in collection basins, transport the flows through the site in graded swales and drainage pipes and discharge the flows over weirs, on the east side of the project.

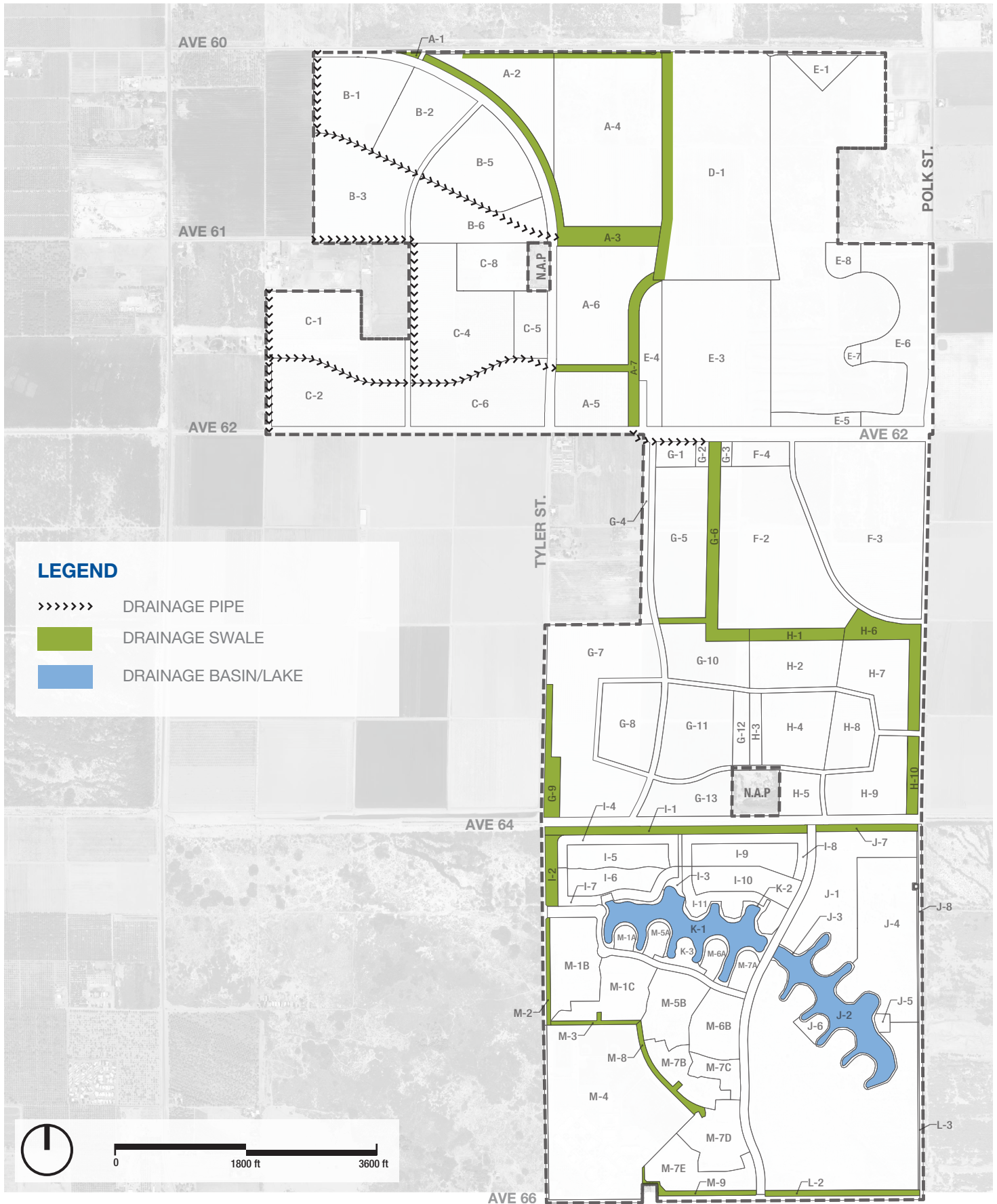
In order to provide the required level of on-site flood protection, the storm flows will be intercepted on the east side of Tyler Street and on the south side of Avenue 60. The storm flows will then be conveyed through the project area through utilization of drainage pipe. In order to approximate existing historical runoff conditions, the difference between the on-site developed and undeveloped runoff flows will be controlled by use of on-site retention basins. The size and depth of these basins will be determined when a final development plan is prepared. Since each collection basin will be designed to handle predicted flood flows the width of the collection basins will vary. Generally, these collection basins will be trapezoidal in shape with 3:1 side slopes and a top width between 100 and 200 feet. Once the storm flows have passed through the graded swales and drainage pipes they will enter the dispersal basins along the eastern boundary of the project. These basins will vary in top width from 200 to 300 feet and will have a mild gradient toward the south. As peak flows progress in a southerly direction, they will spill over a side weir designed to outlet storm flows toward the east in a manner consistent with existing conditions. Retained water will be pumped in a sheet flow dispersal at rates less than presently occur.

On-site runoff will be intercepted and conveyed through the development by means of a conventional catch basin, swales and storm drain system, in accordance with Coachella Valley Water District (CVWD) standards, so that the increase in on-site runoff resulting from the development will be detained on-site and allowed to percolate into the ground or be captured and reused. The collector storm drain system will be designed to utilize street flow carrying capacity and flows into catch basins and inlets when the quantity exceeds the top of curb.

A high degree of protection from the 100-year flood will be provided to all building pads on the project site as the recommended Flood Control Plan is implemented. Moreover, downstream properties will no longer have to contend with the uncertainty of the existing uncontrolled storm flows, and will have the benefit of controlled flows from the project area.

Drainage Development Standards

- 1) All drainage facilities will be designed and constructed in accordance with the Riverside County Flood Control and Conservation District (RCFCD) standards and specifications, and the Standard Specifications for Public Works Construction, (GREEN BOOK).
- 2) Drainage facilities will be subject to the review and approval of the Riverside County Transportation Department.
- 3) Design of drainage facilities will be reviewed by CVWD in conjunction with their review of the sewer and water facilities.
- 4) Drainage plans shall be submitted to CVWD for review and approval. This is to ensure that all proposed facilities are compatible with existing CVWD and U.S. Bureau of Reclamation (USBR) facilities.
- 5) The capital cost of all on-site facilities will be the responsibility of the applicant. Such facilities will be dedicated to Riverside County and a homeowners or County Service Area for maintenance and operations.
- 6) All areas within the Specific Plan area will be required to prepare a Storm Water Pollutant Prevention Plan (SWPPP) in accordance with the requirements of the National Pollutant Discharge Elimination System (NPDES) standards. Industrial developments will require an additional SWPPP to operate.
- 7) All projects proposing construction activities including: cleaning, grading, or excavation that results in the disturbance of at least five acres total land area, or activity which is part of a larger common plan of development of five acres or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the specific plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program. Mitigation measures may include, but not be limited to: on-site retention; covered storage of all outside storage facilities; vegetated swales; monitoring programs; etc.
- 8) The drainage plan for Kohl Ranch shall take into account the existing agricultural drainage facilities in this area. Possible conflicts with these facilities shall be evaluated by the developer's engineer and CVWD.



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3.1.5 Conceptual Community Structure Plan

The Community Structure Plan identifies the major project open space and recreation areas which define the project neighborhoods, and the landscape treatments for project roadways and entries.

Open Space and Recreation

The open space areas are an integral part of the Kohl Ranch Specific Plan. They provide recreational amenities to residents and workers, and are used to screen development edges from potential noise and visual impacts associated with surrounding arterials. The open space system also provides aesthetically pleasing views both from within the project and from surrounding roadways and adjacent properties.

The open space and recreation system for the Kohl Ranch Specific Plan includes neighborhood and community parks, large-scale recreational uses, passive open space and a project-wide trail system. The open space system is organized around, and if necessary, will be adjusted to accommodate any changes in the project drainage network and the Jacqueline Cochran Regional Airport safety zones which limit the land uses and densities which can be located on certain portions of the site. Approximately 376 acres are devoted to the Open Space land use category, including Open Space-Lake. Under a golf course scenario, this would increase by approximately 150 acres.

Large-Scale Recreational Uses

Allowable land uses could accommodate a golf course that would parallel the drainage system, and serve as a central view focus and buffer between land uses for the majority of residential unit in the southern portion of the project site. The major recreational use in this area could be a golf course, which could alternatively be developed for recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water). The golf course would be equipped with a driving range and clubhouse facility. The golf course is a permitted use allowable under the provisions of the Section 2 - Specific Plan Zoning. If developed, it would be sited to maximize the golf course frontage and views for residential units. While the golf course would be privately owned, it is envisioned that the golf course (or in the alternative, recreational lakes) would serve neighborhood residents.

In the northern portion of the project site, a private motor sports race track, and uses incidental thereto, would be allowed uses within the open space designation in Neighborhood E. The facilities allowed within the open space planning areas in Neighborhood E would include a race track, private garages and uses incidental thereto such as a tuning shop and observation tower.

Local Parks

Seven local parks are planned to serve residents and/or employees of the project as depicted in **Figure 3.1-5, Community Structure**. If large-scale recreational uses such as a golf course or race track are built, Parks 4, 5, and 6 may be incorporated into the adjacent large-scale recreational facility. The parks are located throughout the site, and are connected by the project-wide trail system (**Figure 3.1-10, Public Facilities Sites**). Under the golf course alternative, an enhanced amenity package is planned which would supplement the recreation opportunities provided by the local parks. **Table 3.1-C, Potential Park Uses** on the following page describes potential park uses for the project, with and without the golf course scenario, **Table 3.1-D, Potential Park Uses (Golf Course Scenario)**, and with or without the racing facility **Table 3.1-E, Potential Park Uses (Racetrack Scenario)**.

- Park 1 – The local park in the northwestern portion of the site located adjacent to Tyler Street, serves the residential uses to the north of Avenue 62. The size of the park would total approximately 10 acres. This park is located to provide easy access to the large area of contiguous natural open space just south of the airport via a project trail along the north side of Tyler Street.
- Park 2 – The community park located to the south of Avenue 62 is approximately 4.0 acres. This park is linked to the other local parks by trails, some of which follow the drainage channels/paseos.
- Park 3 – A 9.1-acre park is located in the center of the site between two residential neighborhoods. This park can be accessed from the north, south, east, and west by the trail system, which also connects the park with public facility sites to the south.
- Parks 4, 5, and 6 – The local parks in the southern portion of the site are intended to be private to serve residents and are connected to each other and the residential neighborhoods they serve via a trail system planned around the lakes. Under the golf course scenario, these parks would be constructed as parts of the golf course.

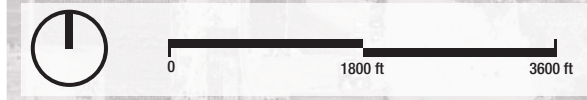
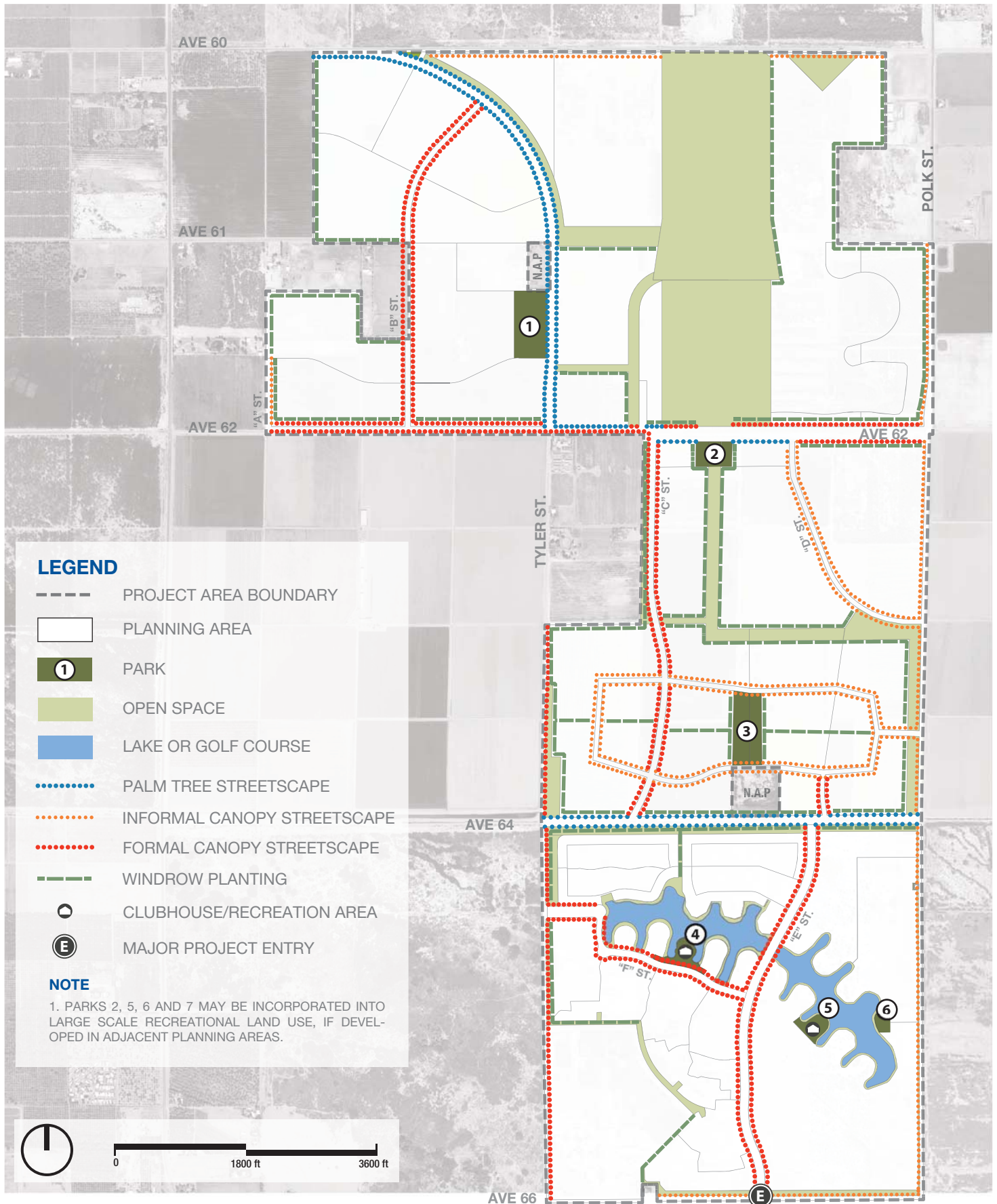


Table 3.1-C, Potential Park Uses¹

Park	Description	Size	Potential Park Uses
1	<ul style="list-style-type: none"> ▪ Neighborhood park in northwest portion of site in Neighborhood C ▪ Serves residential uses to the north of Avenue 62 ▪ Connected to large area of natural open space via Trail along Tyler Street 	9.97 acres	Ball fields Picnic area Tot lot Play equipment
2	<ul style="list-style-type: none"> ▪ Community park south of Avenue 62 in Neighborhood G ▪ Within airport safety zone ▪ Portion of park within drainage area ▪ Linked to other local parks by trails 	4.0 acres	Ball field Picnic area Tot lot Play equipment Tennis courts Recreation center
3	<ul style="list-style-type: none"> ▪ Neighborhood park in the center of the site in Neighborhoods G and H ▪ Trail system connects park with public facility sites to the north and south 	9.1 acres	Ball fields Picnic area Shuffle board Horse shoes
4 – 6	<ul style="list-style-type: none"> ▪ Small Neighborhood parks serving multiple neighborhoods in southern portion of the site (Neighborhoods J and K) ▪ Within drainage area ▪ Connected to public facilities and other parks through trail system 	6.4 acres	Picnic area Lawn bowling Shuffle board Horse shoes Senior center

1. Potential park uses identified in the table are provided as examples of the types of uses that would be appropriate in each park under the three scenarios. Actual park uses and facilities may differ from those listed.

Table 3.1-D, Potential Park Uses (Golf Course Scenario)

Park	Description	Size	Potential Park Uses
1	Same as Table 3.1-C	9.97 acres	Same as Table 3.1-C
2	Same as Table 3.1-C	4.0 acres	Same as Table 3.1-C
3	Same as Table 3.1-C	9.1 acres	Same as Table 3.1-C
4 – 6	Parks could be converted to golf course use	0.0 acres	Not applicable

Table 3.1-E, Potential Park Uses (Race Track Scenario)

Park	Description	Size	Potential Park Uses
1	Same as Table 3.1-C	9.97 acres	Same as Table 3.1-C
2	Same as Table 3.1-C	4.0 acres	Same as Table 3.1-C
3	Same as Table 3.1-C	9.1 acres	Same as Table 3.1-C
4 – 6	Same as Table 3.1-C	6.4 acres	Same as Table 3.1-C

Trail System

The proposed development is planned with an off-street (pavement) trail system that parallels streets and drainage ways throughout the site. **Figure 3.1-10, Public Facilities Sites** shows proposed trail locations, however, the final location of the off-street trail system may be adjusted to accommodate final engineering of the drainage system and development plans. A conceptual design for drainage swales and pipeline is shown in **Figure 3.1-4, Conceptual Drainage Plan**. Street-parallel trails will be constructed per the designations identified on **Figure 3.1-10, Public Facilities Sites** and illustrated in the cross sections found in **Figure 3.4-18, Tyler Street Trail** and **Figure 3.4-19, Avenue 62 Trail** found in Section 3.4 – Landscape Design Guidelines. The off-street system is supplemented by sidewalks and bike lanes within the paved roadway. In addition to providing access to the local parks, the trail system also provides a pedestrian linkage to the schools and to key commercial sites. The proposed trail system provides connections through the site to implement its portion of the County-planned trail system for the area.

Naturalized Open Space

The system of drainage channels throughout the project site will offer a scenic amenity to residents of the Kohl Ranch Specific Plan. The channels will be graded and will contain native desert vegetation that will also provide a buffer between land uses. The acreage of this natural open space will vary depending upon whether the golf courses or other recreational uses are built. Planning Area D-1 (110.66 acres) will remain in open space due to airport-related restrictions.

Landscape Concept

The landscape concept for the Kohl Ranch Specific Plan provides criteria for the treatment of areas within roads and easements, medians, land use transitions, development edges and project entries. The landscape concept has been conceived to organize the development and to establish a unified landscape framework that provides continuity throughout the project area and supports the community themes. Proposed landscape materials are intended to direct and guide the resident or visitor to the development, screen sensitive views, and frame or create focal points and views as the motorist and pedestrian moves through and around the project.

The landscape concept consists of a hierarchy of recommended plantings that correspond to the roadway classifications and project entry statements. More detailed descriptions of these landscape features are provided in Section 3.4 – Landscape Design Guidelines. The landscape concept is based upon the use of natural desert vegetation both for its visual effect and drought tolerant characteristics. The use of water efficient irrigation practices is encouraged, with canal water used for golf courses and public open space areas.

Five basic streetscape planting schemes are proposed:

- Palm Tree Streetscape;
- Formal Canopy Streetscape;
- Informal Canopy Streetscape;
- Windrow Planting; and
- Expressway Planting.

Project Entries

Major project entries generally are identified by groves of palm trees, which create visual interest through the introduction of a vertical element and a beacon that is visible at a distance across the flat landscape. The palm tree plantings are carried into the site along major project roadways, using a less dense spacing between plantings than the concentrated plantings at project entries. Major project entries are identified at the following locations:

- Avenue 60 at northern project boundary;
- Avenue 62 and Polk Street; and
- “E” Street and Avenue 66.

Minor project entries will be treated with a scaled-down version of the palm tree concept. Entries and intersections will be precisely located as internal roadways are designed.

Streetscapes

In order to achieve a cohesive overall circulation system for the Kohl Ranch Specific Plan and provide a strong community structure, a consistent streetscape treatment is recommended for public and private rights-of-way. The general landscape concept combines the use of palms trees with citrus understory, massed in selected locations, interchanged with formal and informal canopy plantings and use of other desert plant material. The streetscapes are broken down into three major categories with special features and plant forms as indicated.

The proposed plantings along major project roadways are based upon the roadway significance and strategic location within the project site. Because of its importance as a connection and northern project entry, Tyler Street, north of Avenue 62, is proposed to employ the palm tree planting concept, with breaks in the plantings where tall trees are prohibited through the airport safety zones. If the race track scenario is developed, Windrow Plantings will be used along its frontage to screen from residential uses to the west.

Formal canopy streetscape plantings are planned along Avenue 62, "B" Street, "C" Street, "E" Street, "F" Street, Tyler Street (south of Avenue 62) and several local roadways that provide internal circulation between neighborhoods. These streets provide definition to the project neighborhoods. Informal canopy streetscape plantings are recommended for local roads that are more interior to the site.

Buffers and Land Use Transitions

Windrow plantings are recommended to serve as buffers between land use types, along drainage channels and project edges, and within residential neighborhoods to mitigate against high desert winds. Windrows also can be used to provide structure to project neighborhoods and can be integrated with the project trail system, but must be sited to take into consideration airport safety zones and screening of the race track, if developed.

Community Structure Development Standards

- 1) All detailed landscaping programs for planning areas and roadways will be prepared by a qualified landscape architect for review by the County.
- 2) Common open space areas within each planning area may be devoted to passive or active uses, and will, to the extent feasible, be coordinated with any open space in adjoining planning areas to create a continuous network. The exact design and layout of facilities will be accomplished in conjunction with detailed future tract layouts.
- 3) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped common areas shall be submitted to the Riverside County Planning Department for approval. The improvement plans shall include, but not be limited to, the following:
 - Final grading plan;
 - Irrigation plans prepared by a landscape architect;
 - A landscaping plan with specifications for mulching and staking methods; locations, type, size and quantity of plantings;
 - Fence treatment plans; and
 - Special treatment/buffer area treatment plans.
- 4) The applicant developer and/or builder shall be responsible for maintenance and upkeep of all common landscaped areas and irrigation systems within its ownership parcels until such time as these operations are the responsibility of other parties.
- 5) At the time of recordation of any final subdivision map which contains a common open space area, the subdivision shall have those common areas transferred to the master property owners association or an appropriate public maintenance agency who will take responsibility for maintenance.
- 6) Irrigation of common open space areas and golf courses will be accomplished with canal water and/or reclaimed water to the extent possible.
- 7) For further landscape development standards, please refer to Section 3.4 – Landscape Design Guidelines.

3.1.6 Water and Sewer Plan

The project study area is completely within the service boundary of the Coachella Valley Water District (CVWD) which provides water and sewer service to the project area. The water and sewer plan is based on an analysis of water and sewer infrastructure which evaluates availability of services, calculates water demands and sewer generation quantities, and proposes infrastructure facilities which allow for the advancement of development. The purpose of the water and sewer plan is to provide for the backbone improvements which will be capable of serving the ultimate project, in addition to the individual portions independent of their progression.

Existing Water and Sewer Conditions

There are some domestic water distribution facilities within the project boundary. Domestic water provided within the CVWD service boundary comes from ground water which is chlorinated, the Colorado River and the State Water Project. Previous studies conducted in the area indicate that some ground water must be treated for arsenic to meet the State Drinking Water Standards. A Domestic Water, Non-potable Water, Canal Water and Sanitation System Installation and Service Agreement (Document No. 2002-538101) is currently on file at CVWD which provides for mitigation of arsenic when required, through payment of fees for arsenic treatment facilities. CVWD is located at 85-995 Avenue 52 in Coachella, California.

There is an existing tank site southwest of the intersection of Avenue 68 and Harrison Street with a 24-inch waterline that brings water to the project. An 18-inch water line is located in Tyler Street at the intersection of Tyler and Avenue 66 which extends north along the project boundary to Neighborhood I. A 24-inch water line is also located at the intersection of Tyler Street and Avenue 66. This line runs east along the existing school site boundary (Planning Area M-4). An existing arsenic treatment plant is located on the northeast corner of Tyler Street and Avenue 66.

There are some sewer facilities located within the project boundary. An existing sewer line currently services the existing school site (Planning Area M-4) and runs through Neighborhoods J, L, and M to an existing sewage pump station located in Planning Area J-4. An existing 12-inch sewer force main runs north to Avenue 62 and intersects with an existing CVWD force main. It is then pumped to Wastewater Reclamation Plant No. 4.

CVWD operates and maintains its Wastewater Reclamation Plant No. 4 located between Avenue 62 on the north, Avenue 64 on the south, adjacent to the Whitewater River on the east, and approximately 600 feet east of Fillmore Street on the west. This plant has a design capacity of approximately 5.0 MGD, and is estimated to be currently operating at half capacity. The plant currently treats to a secondary treatment level using stabilization ponds for finishing. The ponds are used for flow stabilization by allowing the depth to increase during peak wet water events.

Proposed Water System and Water Conservation Measures

This project will utilize a “dual water system” consisting of potable and non-potable water systems. The potable system will provide water for domestic purposes and provide water for fire protection. The non-potable system will provide water for irrigation purposes.

The proposed potable water system improvements as depicted in **Figure 3.1-6, Conceptual Potable Water Plan** will operate within a single pressure zone serving ground elevations from 88 to 167 feet below sea level. Hydraulic control for the project should be set using a reservoir high water level of approximately 83 feet above sea level. An existing tank site has been constructed at CVWD’s recommended pad elevation of approximately 60 feet above sea level.

The potable water system is proposed to be fed by wells to be located within or near the project boundary. Four wells are anticipated to pump peak daily demands through an on-site system from 12-inch to 30-inch diameter transmission lines looped for maximum efficiency and fire flow delivery. Internal water lines feeding individual houses will range from 6-inch to 8-inch. Excess water during low and average flows will be conveyed to reservoirs to be located southwesterly as reflected in **Figure 3.1-7, Off-Site Water Improvements**, of the project. An existing 24-inch diameter transmission main will convey flows to the project when operating from gravity flow. Per CVWD’s Master Water Plan, transmission mains are proposed adjacent to, and through the Specific Plan. The Specific Plan water system shall connect to any existing adjacent water facilities that have available capacity.

The potable water system will be designed to deliver gravity flow during high power cost windows. Well pumps will be controlled by level sensing devices and transmitters on the reservoirs. The water system will utilize a certain volume from the tanks, prior to the well pumps cycling, to ensure that adequate circulation of water in the tanks is maintained.

In accordance with the Water Conservation Plan, water conservation measures will be incorporated into the project plan to include water saving devices and systems. Further, the use of surface water will be used for irrigation purposes. Water conservation for the Kohl Ranch project will be achieved through:

- Dual-source water system to reduce groundwater consumption;
- Recycled and/or canal water for irrigation;
- Drip and other efficient irrigation;
- Intelligent irrigation controllers;
- Native and non-native drought-tolerant planting materials;
- Tiered water rates; and
- Use of proposed lake to double as a forebay for the non-potable water treatment.

Groundwater use will be reduced through the initial use of Colorado River water for non-potable water uses as part of a dual-source water system. The design will include a potable water system designed for indoor use only, as well as a separate non-potable water system for outdoor landscaping. The potable water system will be connected to the CVWD domestic water system, which relies upon groundwater. The non-potable system will use Colorado River water delivered by the existing agricultural canal system for landscaping and outdoor use. The majority of the landscaping throughout the project site is designated (in general terms) as “desert landscaping.” And, the project’s proposed high density residential uses reduce the amount of landscaped open space per unit, limits the number of individual pools, and encourages the use

of public facilities.

This non-potable system will be designed to meet recycled water standards in anticipation of the possible future availability of tertiary water when it becomes available from the nearby Water Reclamation Plant 4 (WRP-4). The project will be sewerred and the majority of the demand on the Aquifer from the project will be from indoor use. In the event recycled wastewater from WRP-4 becomes available, project demand on the Aquifer will be further reduced; recycled water can be used to replace Colorado River water or groundwater for agriculture use in the area or on project landscaping. Additional Colorado River water could be freed up for agricultural use or to recharge the Aquifer.

CVWD has three WRP's that produced recycled water for a combined total of 8,289 acre-feet in 2008, which was used to reduce the demand on the groundwater basin. In addition, Desert Water Agency, a partnership with CVWD in the management of the Aquifer, operates a reclamation plant in Palm Springs and produced 3,000 acre-feet of recycled water per year, which also reduces the demand on the groundwater basin. This dual water system is being required of new, larger developments in the Lower Valley, including Kohl Ranch, as a result of SB1557, which finds that the use of potable domestic water for nonpotable uses is wasteful if suitable nonpotable water sources are available.

Additionally, CVWD is working with the cities in its service area to limit the amount of water that can be used for outdoor landscaping. As the result of the adoption of state-wide indoor water conservation measures requiring low flush toilets, shower and faucet flow restrictors, and other devices, the amount of water used inside homes has been significantly reduced. With the large number of new homes constructed, these conservation programs have reduced impacts of new development on the Aquifer.

The non-potable water system as reflected in **Figure 3.1-8, Conceptual Non-Potable Water System**, water source will be diverted canal water that would be delivered into an on-site system of lakes and pond(s). Hydropneumatic booster station(s) will pump water from the lakes and pond(s) throughout the development for irrigation purposes. The on-site water system consists of 8-inch to 42-inch diameter transmission lines looped for maximum efficiency and delivery at appropriate service pressures.

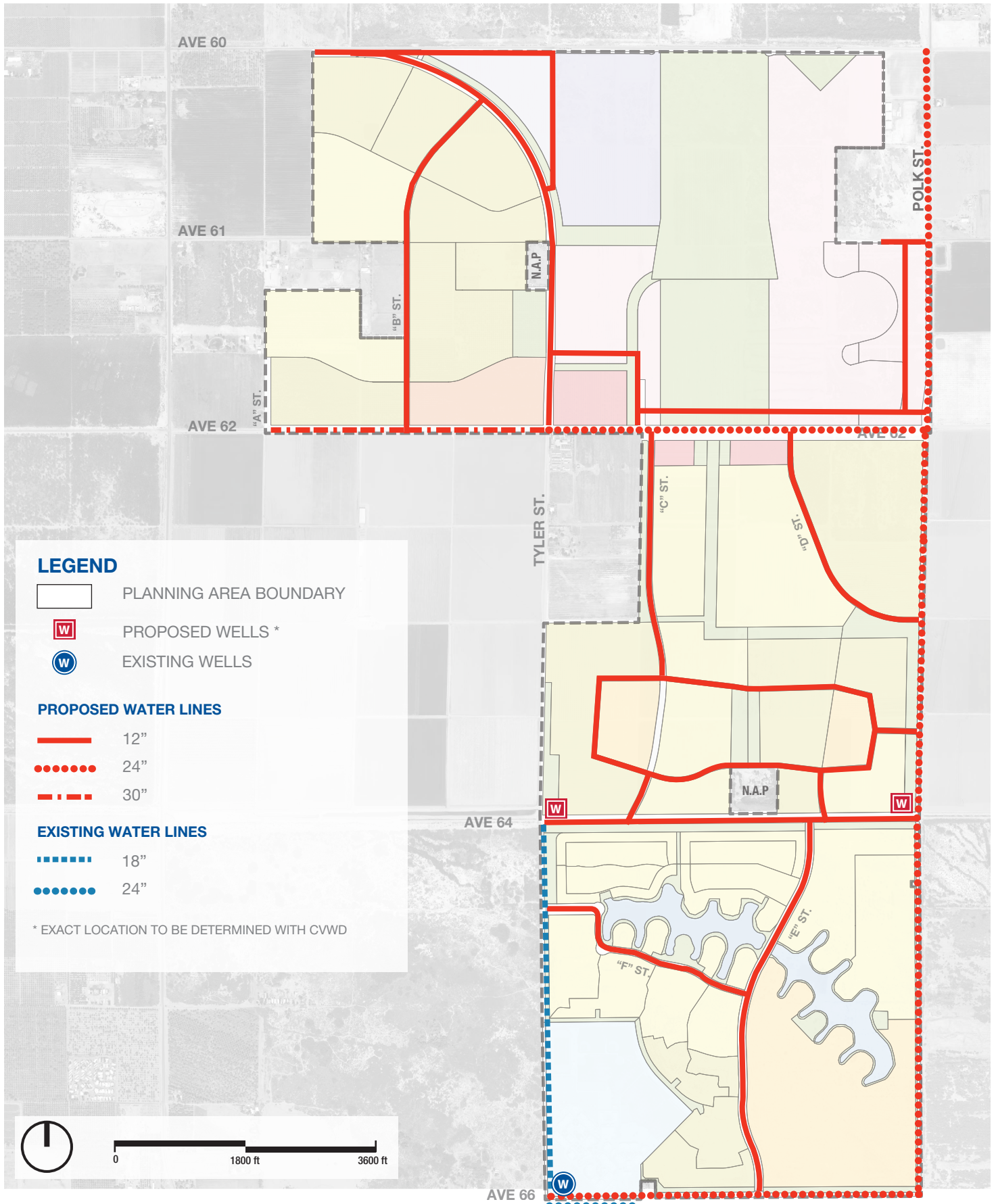
Proposed Sewer

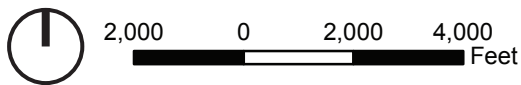
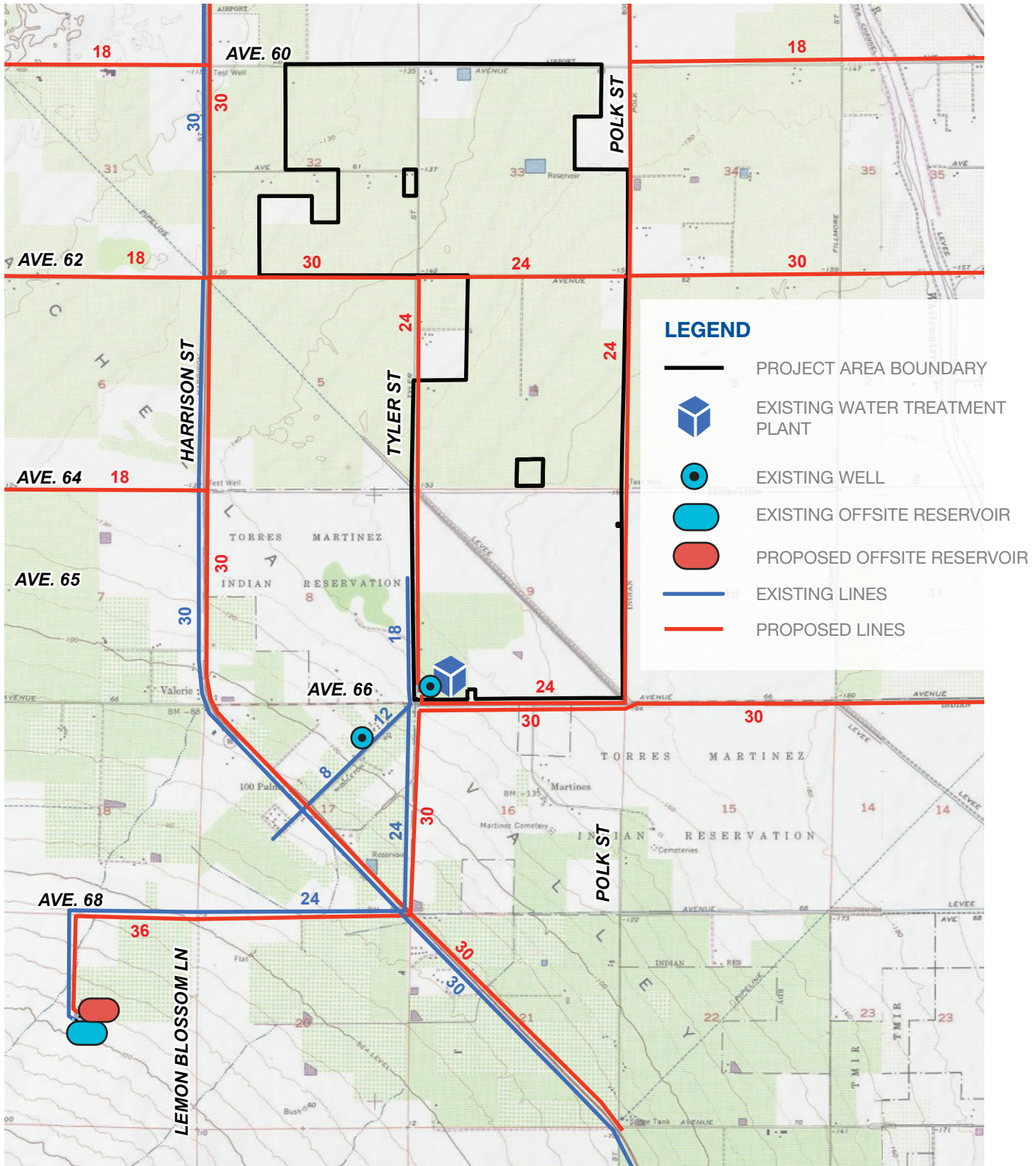
The Specific Plan's sewer improvements as reflected in **Figure 3.1-9, Sewer Plan**, are based on an analysis of sewer infrastructure that elevates availability of services, calculates sewage generation, and proposes infrastructure facilities which allow for the advancement of the development. The purpose of the sewer plan is to provide for the backbone improvements that will be capable of serving the ultimate project, in addition to the individual portions independent of their progression.

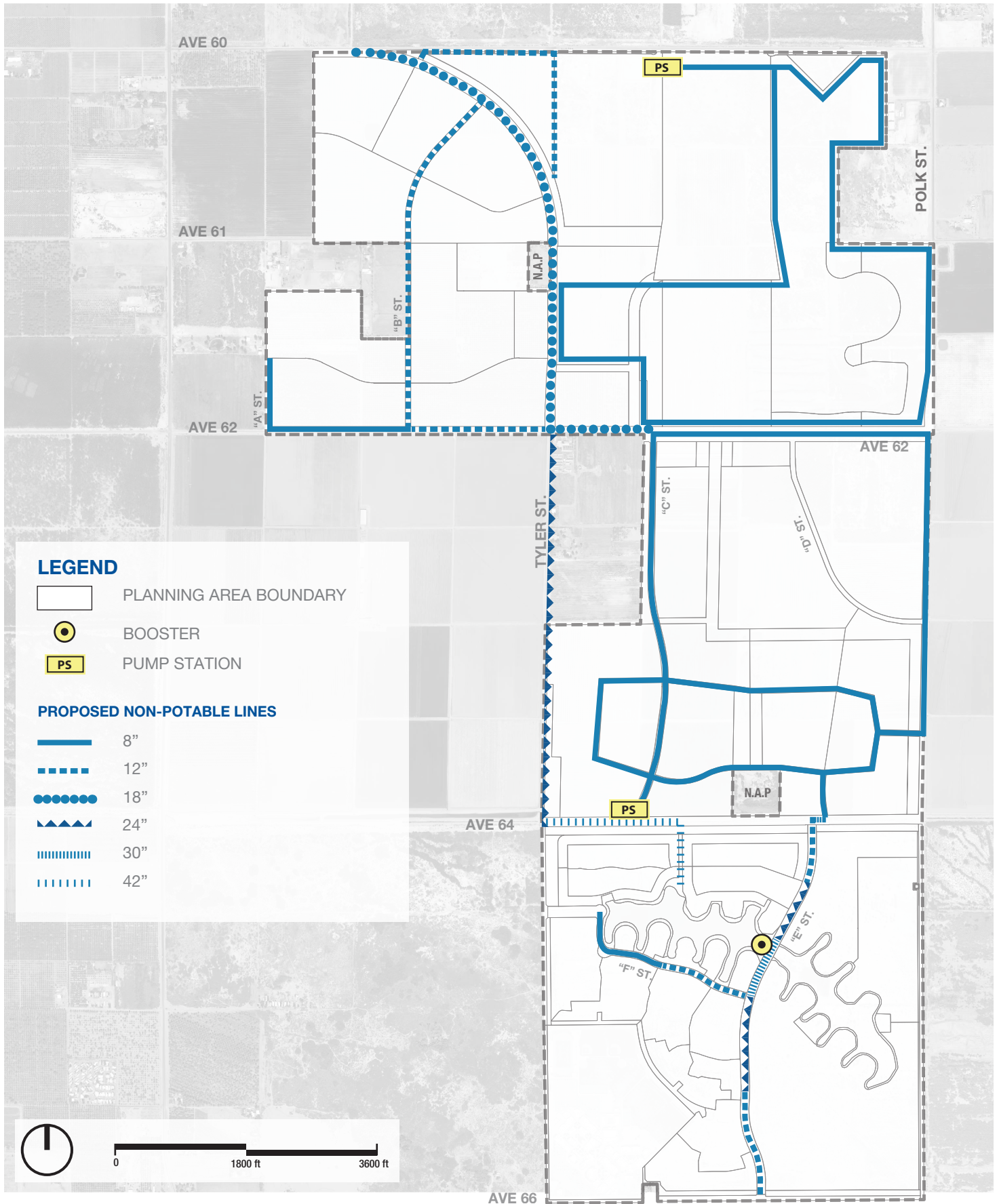
The Specific Plan proposed sewer system conveys flows to the existing sewage pump station and ultimately to CVWD's treatment plant. The Specific Plan sewer system shall connect to any adjacent existing sewer facility that has capacity.

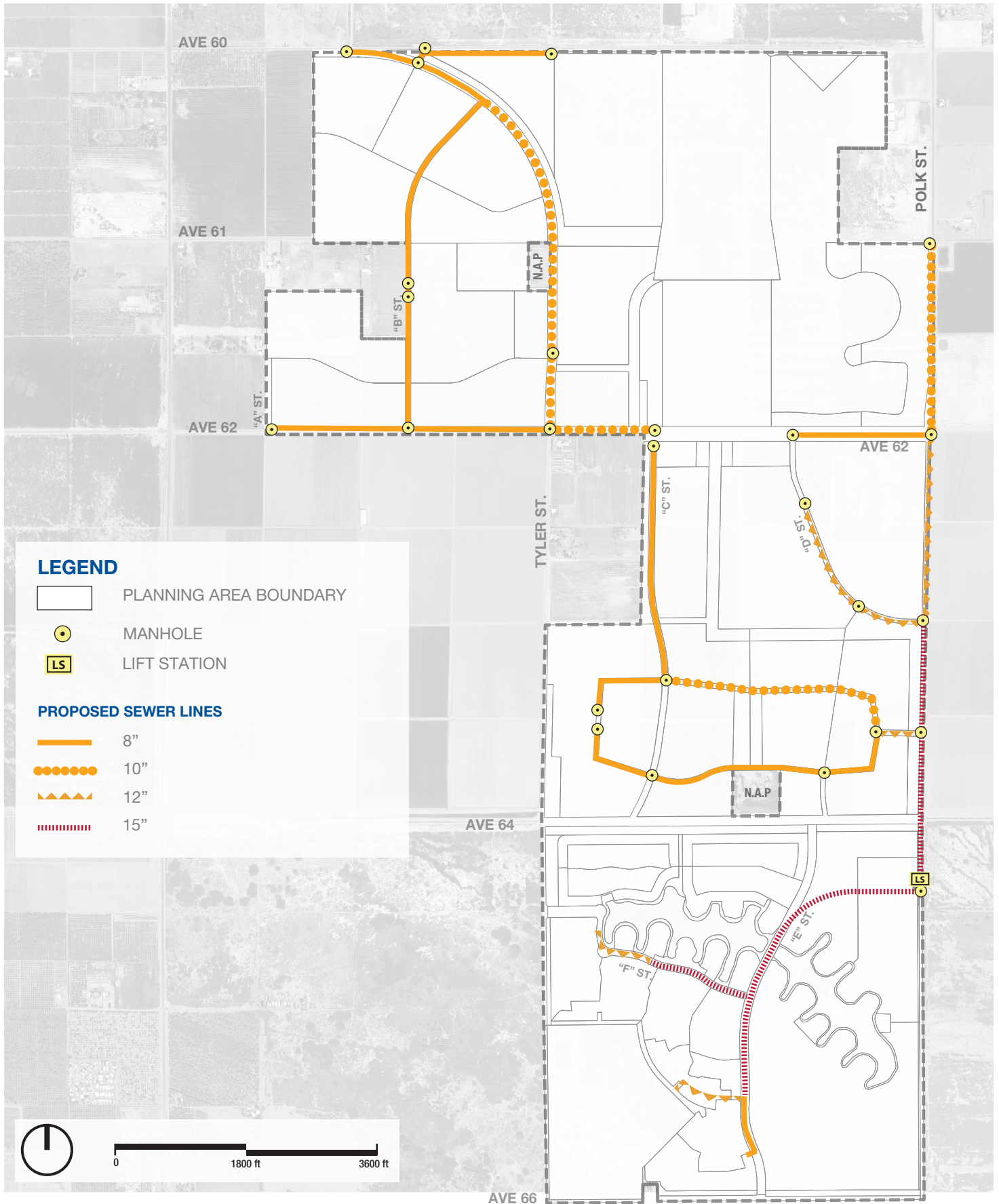
Water and Sewer Development Standards

- 1) All water and sewer facilities will be designed and constructed in accordance with the CVWD standards and specifications, American Water Works Association (AWWA), American National Standards Institute (ANSI) and the Standard Specifications for Public Works Construction, (GREEN BOOK).
- 2) The capital cost of on-site and off-site facilities necessary to serve the project site will be the responsibility of the applicant. Such facilities will be dedicated to CVWD, after construction, for the District to maintain and operate. Fee credits may be available for transmission, storage, and wells. As discussed above, a Domestic Water, Non-potable Water, Canal Water and Sanitation System Installation, and Service Agreement (Document No. 2002-538101) is currently on file at CVWD which provides for mitigation of arsenic through payment of fees for arsenic treatment facilities.
- 3) In accordance with the Water Conservation Plan (see Appendix F), water conservation measures will be incorporated into the project plan. The following measures will be utilized to reduce the impact on the underground basin and increase efficiency in water usage:
 - (a) A dual water system (potable/non-potable) will be installed to service landscaped areas.
 - (b) Irrigation lines will utilize canal water and/or treated effluent to irrigate landscape areas.
 - (c) Water conservation plumbing fixtures or water saving devices and systems shall be used in all construction.
 - (d) Where possible, the existing tile drains will be maintained to prevent high salt water from migrating to the groundwater basin.
 - (e) The project development shall conform with state, county, and CVWD regulations regarding water conservation and reclamation.
- 4) All development is required to landscape in accordance with **Table 3.4-A, Kohl Ranch Plant Palette** found in Section 3.4, and CVAG Ordinance 1302.1, Landscape and Irrigation System Design Criteria. This will ensure additional water conservation through the use of desert-appropriate landscape materials.









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3.1.7 Public Facilities Sites and Project Phasing

In order to insure timely development of public facilities, a phasing plan has been prepared for parks, schools and other public facilities as depicted in **Figure 3.1-10, Public Facilities Sites**.

Project Phasing Plan

Four phasing scenarios have been established for the proposed project as depicted in **Figure 3.1-11, Phasing Plan**. Each scenario represents an area of the site that is likely to develop as a unit. The purpose of a scenario is to establish the infrastructure required to initiate development, assuming that the scenario site is the portion of the total project site to be developed first. However, the scenario numbers do not necessarily indicate the sequence in which the site will be built out. For example, Phasing Scenario 2 and Phasing Scenario 4 could be constructed first. It is anticipated that the project will be developed over a twenty-five year period, in response to market demands, and according to a logical extension of roadways, public utilities and infrastructure. Efforts by local government to stimulate economic development in the project area, through the planned development at Jacqueline Cochran Regional Airport, can be expected to serve as an impetus to development of revenue-producing land uses in the northern portion of the project site. The project phasing scenarios are further described below.

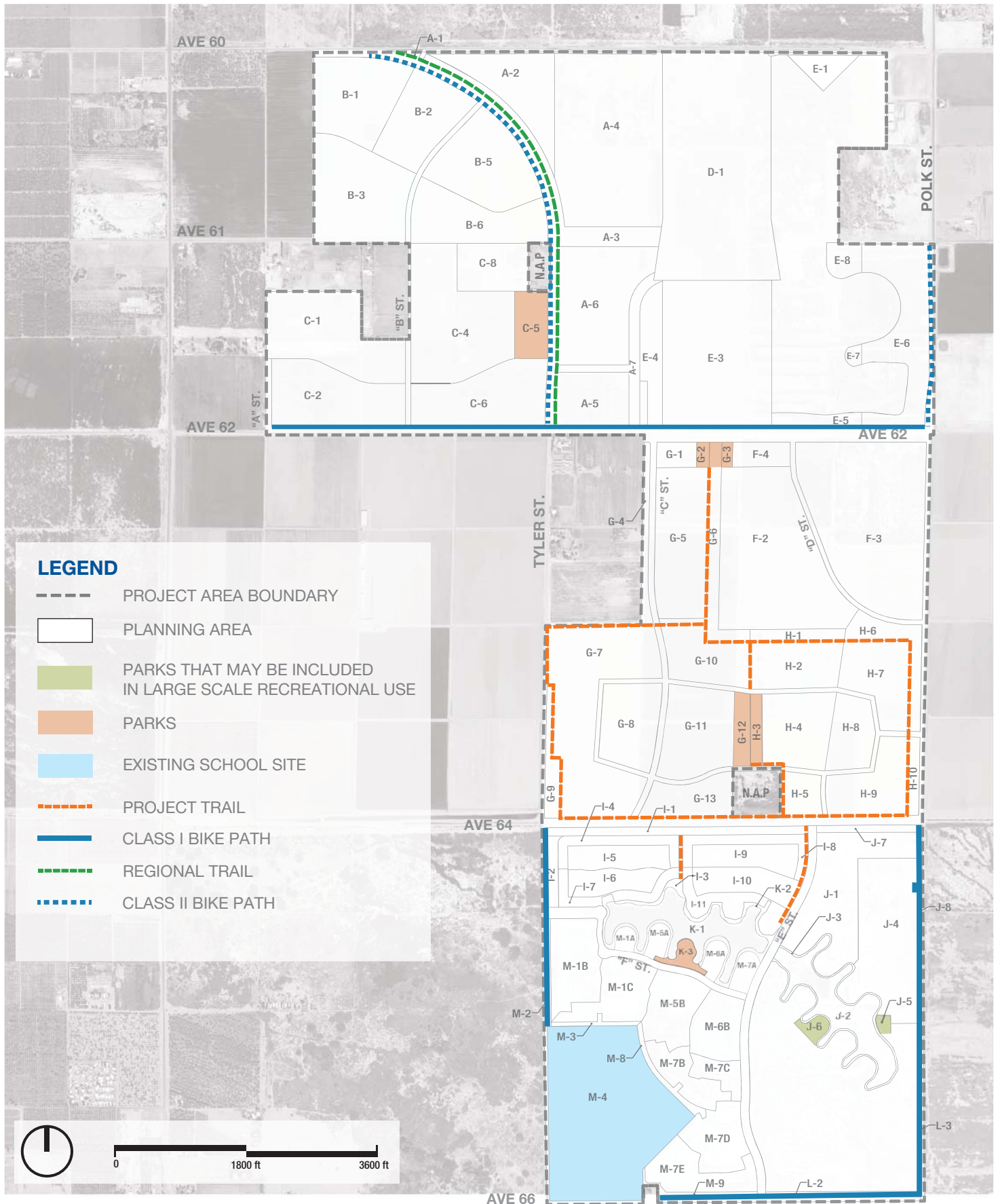
Public Facilities Phasing Schedule

Public facilities shall be phased as indicated in **Table 3.1-F, Public Facilities Phasing Schedule**, below.

Table 3.1-F, Public Facilities Phasing Schedule

Planning Area	Public Facility	Size of Site (acres)	Milestones and Requirements (to be determined)
PHASING SCENARIO 1			
K-1/J-2	Lake	44.4	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
K-3	Park	3.2	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
J-6	Park	2.2	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
J-5	Park	1.0	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
PHASING SCENARIO 2			
-	-	-	Payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or industrial subdivision map is filed for Neighborhood E.
PHASING SCENARIO 3			
C-5	Park	9.97	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood C.
PHASING SCENARIO 4			
G-3	Park	1.19	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G.
G-2/G-6	Park	15.09	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G.
G-12/H-3	Park	9.14	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G or H.

Note: The park acreages assume that the golf course or race track would not be built. Planning Area M-4 is an existing school.

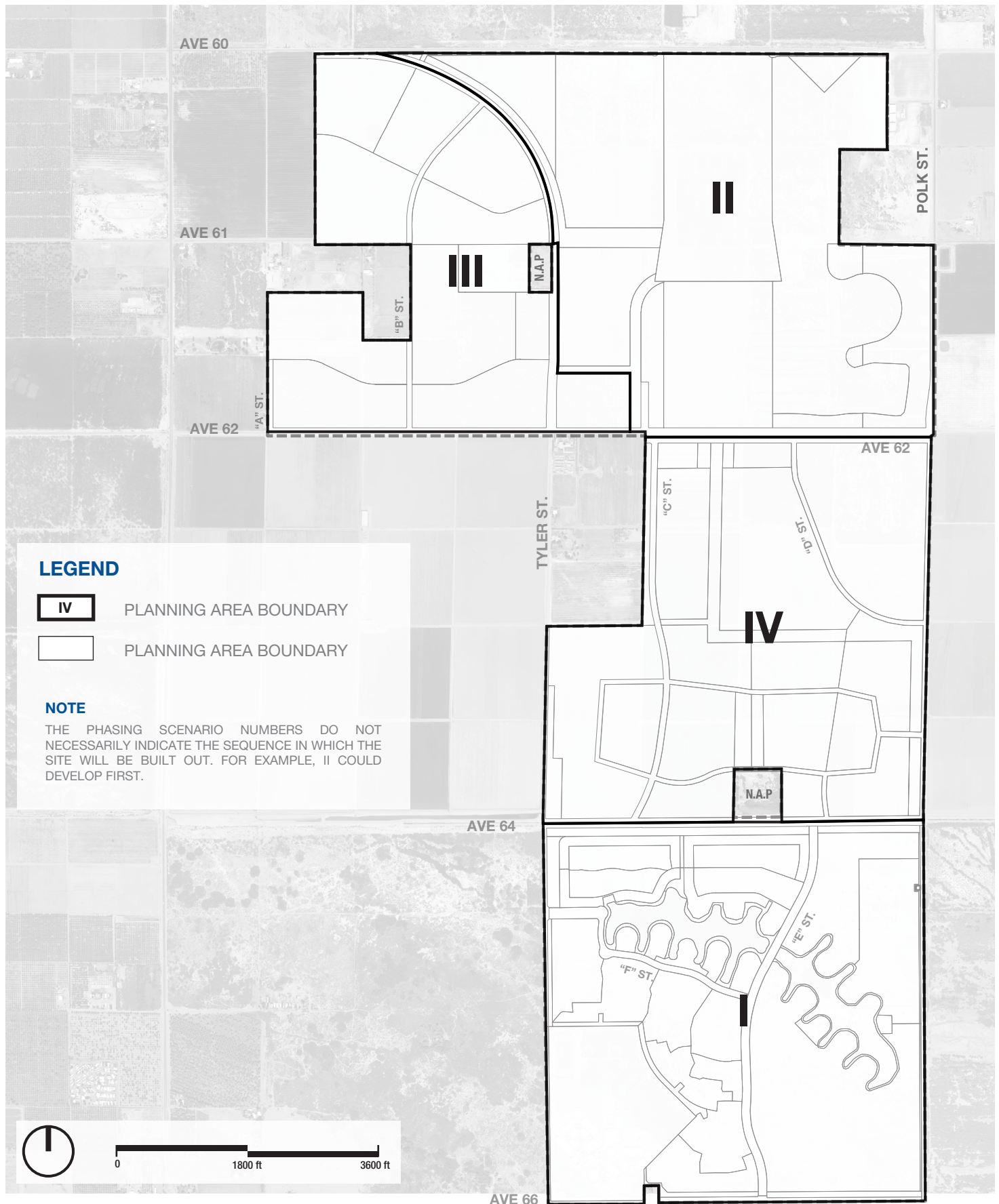


LEGEND

- PROJECT AREA BOUNDARY
- PLANNING AREA
- PARKS THAT MAY BE INCLUDED IN LARGE SCALE RECREATIONAL USE
- PARKS
- EXISTING SCHOOL SITE
- - - PROJECT TRAIL
- CLASS I BIKE PATH
- - - REGIONAL TRAIL
- · · CLASS II BIKE PATH



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Phasing Scenario 1

Phasing Scenario 1 encompasses the southern segment of the project area. The infrastructure improvements described in this section assume that Scenario 1 would be the first area of the site to develop. Transportation improvements would involve both major and secondary roadways, including partial width of Polk Street from Avenue 66 to Avenue 64; Avenue 64 from Polk Street to Tyler Street; "F" Street between "E" Street and Tyler Street; and "E" Street between Avenue 64 and Avenue 66. Grading activities for this Scenario would be accomplished by stockpiling export on the Scenario 4 site to the north. Drainage for this scenario would require that off-site flows be picked up along the westerly boundary in master planned facilities, conveyed through the site and then released at a controlled rate along the eastern boundary of Polk. Sewer facilities would gravity flow to the easterly portion of the scenario site to the existing sewage pump station. From that location, an existing sewer force main will convey the flows to the existing waste water lines along Avenue 62. Water facilities to support the development of this scenario begin with the extension by others, of the existing 24-inch diameter waterline in Avenue 66 to the east at the intersection of Avenue 66 and Polk Street. A 24-inch diameter waterline will be constructed by others, in Polk Street between Avenue 66 and Avenue 64. A 12-inch diameter waterline will be constructed in Avenue 64 between Tyler Street and Polk Street in "E" Street and "F" Street. Two wells exist to provide adequate water supply and backup (one on the school site and one on Middleton Street just to the southwest of the project area). Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-G, Phasing Scenario 1 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	I-4	7.0	35
	I-5	10.6	51
	I-6	9.6	42
	I-7	6.7	29
	I-8	8.0	40
	I-9	11.5	57
	I-10	12.4	47
	I-11	10.1	37
	J-1	37.52	130
	J-4	43.36	189
	M-1A	2.5	8
	M-1B	18.9	68
	M-1C	16.4	61
	M-5A	4.1	14
	M-5B	15.7	61
	M-6A	3.8	14
M-6B	13.4	57	
M-7A	4.1	20	

Land Use	Planning Areas	Acreage	Units
Medium High Density Residential	M-7B	11	56
	M-7C	5.9	32
	M-7D	15.1	82
	M-7E	15.5	82
High Density Residential	L-1	106.2	1,158
Open Space	I-1	9.6	
	I-2	3.9	
	I-3	0.5	
	J-2	22.0	
	J-3	8.8	
	J-5	1.0	
	J-6	2.2	
	J-7	3.3	
	J-8	1.31	
	K-1	22.4	
	K-2	8.8	
	K-3	3.2	
	L-2	4.1	
	L-3	1.1	
	M-2	1.9	
	M-3	1.6	
M-8	2.9		
M-9	2.7		
Subtotal Scenario 1		490.69	2,370

Note: Planning Area M-4 (84.3 acres) is an existing school site.

Phasing Scenario 2

Phasing Scenario 2 is situated in the northeast corner of the project area. The infrastructure improvements described in this section assume that Scenario 2 would be the first area of the site to develop. Transportation improvements would involve expressway, arterial and secondary roadways, including partial width of Polk Street north of Avenue 62 to project boundary; and Avenue 62 between "C" Street and Polk Street. Grading activities for this scenario would be limited to the Scenario 2 site. There would be limited (if any) import/export required off-site. Drainage for Scenario 2 would require that off-site flows be picked up along the western and northern Scenario 2 boundary and in an interim swale along the westerly boundary of Tyler Street and conveyed to the proposed on-site open space channel areas (Planning Areas A-3 and A-7). Off-site flows from these open space areas would be conveyed along a drainage channel running along the west edge of Scenario 2 daylighting south of site through Scenario 4 that returns to a sheet flow condition, with no additional drainage facilities required. Sewer facilities would gravity flow to the southeast corner of the drainage areas to a gravity sewer presently being constructed by CVWD. Water facilities to support the development of this scenario begin with the extension of the existing 24-inch diameter waterline in Avenue 66 east to Polk Street and north in Polk Street to Avenue 60. A proposed 24-inch waterline in Avenue 62 between Tyler Street and Polk Street is also needed. Connecting to these 24-inch lines will be 12-inch lines in Tyler Street, Avenue 60 and internal to the Scenario 2 area to serve the area. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-H, Phasing Scenario 2 Details

Land Use	Planning Areas	Acreage	Units
Mixed Use/Air Park	A-2	24.45	
Mixed Use	A-6	46.18	
	E-2	148.74	
	E-4	12.25	19
	E-5	5.31	120
	E-6	33.99	5
	E-7	1.32	15
Heavy Industrial	E-8	4.94	
	A-4	81.17	
Open Space	A-1	0.63	
	A-3	10.77	
	A-7	9.97	
	D-1	110.66	
	E-1	5.61	
	E-3	69.33	
Subtotal Scenario 2		565.32	159

Phasing Scenario 3

Phasing Scenario 3 is located at the northwestern corner of the project area. The infrastructure improvements described in this section assume that Scenario 3 would be the first area of the site to develop. Transportation improvements would involve expressway, arterial and secondary roadways, including "A" Street north of Avenue 62; "B" Street between Avenue 62 and Tyler Street; Tyler Street north of Avenue 62; and partial width of Avenue 62. Grading activities would be limited to the Scenario 3 site. Drainage for Scenario 3 would require that off-site flows be picked up along the western project boundary, conveyed through the site and then released to the adjacent Scenario 2 area. Two small off-site retention facilities may be required east of Scenario 3. The retention facilities would have overflow weirs on the downstream side to return the storm flow to its original sheet flow condition. Sewer facilities would gravity flow to the southeast corner of the site to a gravity sewer in Avenue 62 presently being constructed by CVWD. Water facilities to support the development of this scenario begin with either the extension of the existing 18-inch diameter waterline at Tyler Street and "F" Street to the intersection of Avenue 62 and Tyler Street, or a 24-inch line will be extended westerly from Polk Street in Avenue 62 to its intersection with Tyler Street. From the intersection of Avenue 62 and Tyler Street, a 12-inch diameter waterline will be constructed north on Tyler Street to Avenue 60. From the intersection of Avenue 62 and Tyler Street, a 30-inch waterline will be constructed west to the project boundary. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-I, Phasing Scenario 3 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	B-3	39.01	117
	C-1	40.77	122
Medium High Density Residential	B-1	30	210
	B-2	25.24	177
	B-5	32.70	229
	B-6	26.18	183
	C-2	31.16	218
	C-4	49.83	349
Very High Density Residential	C-8	14.96	105
	C-6	30.85	477
Commercial Retail	A-5	17.61	
Open Space	C-5	9.97	
Subtotal Scenario 3		348.28	2,187

Phasing Scenario 4

Phasing Scenario 4 site is located in the central portion of the project area and abuts the southern boundary of the Phasing Scenario 2 and the northern boundary of Phasing Scenario 2. The infrastructure improvements described in this section assume that Scenario 4 would be the first area of the site to develop. Transportation improvements would involve expressway, major and secondary roadways, including partial width of Polk Street between Avenue 62 and Avenue 64; Avenue 64 between Tyler Street and Polk Street; "C" Street from Avenue 64 to Avenue 62; "D" Street from Polk to Avenue 62; Avenue 62 between "C" Street; and Tyler Street between Avenue 64 and the northwest corner of Phase 4. Grading activities for this scenario would be limited to the Scenario 4 site, with the exception of a portion of open space area D-1 in the Scenario 2 site and a portion of open space areas K-1 and J-2 in the Scenario 1 site, which would be used for a borrow/stockpile area or detention/retention areas. Drainage for this scenario would require that off-site flows be picked up in interim swales along the northerly and westerly boundaries, conveyed through the site and then released to the Scenario 1 site immediately to the south. In addition, master planned off-site retention facilities would be required immediately to the south. The retention facilities would have overflow weirs on the downstream side to return the storm flow to its original sheet flow condition. Sewer facilities would either gravity flow to the north to a CVWD gravity line in Avenue 62 or flow through the site to the southeast corner of the scenario to an existing sewer pump station at Polk and Avenue 64. From there, an existing force main will convey the flows to the existing wastewater treatment facility. Water facilities to support the development of this scenario begin with the extension of an existing 24-inch diameter waterline at the intersection of Avenue 66 and Tyler Street to Polk Street and north in Polk Street to Avenue 62. A 24-inch diameter waterline will be constructed in Avenue 62 between Polk Street and "C" Street. A 12-inch diameter waterline will be constructed in Avenue 64 between Tyler Street and Polk Street and within the phase. Two wells will be constructed along the waterline alignment to provide adequate water supply and backup. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-J, Phasing Scenario 4 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	F-2	71.64	158
	G-5	34.6	69
	G-10	18.57	37
	G-11	26.86	70
	G-13	14.17	53
	H-9	20.03	66
Medium High Density Residential	F-3	79.94	635
	G-7	60.38	432
	H-2	18.65	118
	H-4	22.4	141
	H-5	7.83	45
	H-7	20.99	151
High Density Residential	G-8	20.6	282
	H-8	13.55	189
Commercial Retail	F-4	6.23	
	G-1	4.43	
Open Space	G-2	1.44	
	G-3	1.19	
	G-4	5.6	
	G-6	13.65	
	G-9	6.17	
	G-12	5.35	
	H-1	5.12	
	H-3	3.79	
	H-6	11.43	
	H-10	3.96	
Subtotal Scenario 4		498.57	2,446

Project Phasing Standards

- 1) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped areas for that stage of development, shall be submitted to the County Planning Department for approval. The improvement plans shall include, but not be limited to the following:
 - Final grading plan.
 - Irrigation plans certified by a landscape architect.
 - A hardscaping plan with location, type and quantity of potential recreational amenities/facilities prepared by a licensed landscape architect.
 - Wall or fencing treatment details consistent with the Specific Plan.

Plans for landscaping, grading and irrigation systems shall be submitted to the Coachella Valley Water District for review. This is to ensure efficient water management.
- 2) Construction of the development permitted hereby, including recordation of final subdivision maps, may be done progressively in stages, provided adequate vehicular access is constructed for all dwelling units in each stage of development and further provided that such phase of development conforms substantially with the Specific Plan Phasing Program.

3.1.10 Illustrative Grading Plan

The study area is relatively flat with approximately 45 feet of topographical relief. The existing topography tends to slope from the northwest to the southeast at a nominal rate of between 0.30 percent and 0.40 percent. Earthwork characteristics and grading recommendations as presented in the Geotechnical Feasibility Report were utilized in this study (EIR396 – Appendix B).

The grading concept illustrates site development feasibility and provides a “balanced” earthwork scenario not dependent upon import or export of material. Grading is designed to conform to the drainage conveyance requirements while following existing topographical patterns as reflected in **Figure 3.1-12, Grading Plan**. All development areas are designed with positive drainage towards acceptable drainage conveyances. Prior to the issuance of a grading permit from Riverside County, the developer's engineer shall submit the grading plans to the Coachella Valley Water District for review and approval. This is to ensure that all existing facilities and easements are taken into account prior to construction.

Grading Plan Development Standards

- 1) All grading activities shall be in substantial conformance with the overall Conceptual Grading Plan, the California Building Code and Riverside County Ordinance No. 457.
- 2) Prior to any development within any area of the Specific Plan, an overall Conceptual Grading Plan for the portion in process shall be submitted for Planning Department approval. The Grading Plan for each such area shall be used as a guideline for subsequent detailed grading plans for individual stages of development within that area, and shall include preliminary pad and roadway elevations.
- 3) Unless otherwise approved by the County of Riverside, Building and Safety Department, all cut and fill slopes shall be constructed at inclinations of no steeper than two (2) horizontal feet to one (1) vertical foot.
- 4) A grading permit shall be obtained from the County of Riverside, as required by the County Grading Ordinance, prior to grading.
- 5) Soil stabilizers should be used to control dust as required by SCAQMD Rule 403.
- 6) Erosion control practices shall be implemented during grading activities.
- 7) The grading contractor shall be required to obtain an encroachment permit from CVWD prior to entering any right-of-ways which belong to CVWD or USBR.
- 8) All projects proposing construction activities including: clearing, grading, or excavation that results in the disturbance of at least five acres total land area, or activity which is part of a larger common plan of development of five acres or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the specific plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program.



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3.1.11 Comprehensive Maintenance Plan

Maintenance of open space, recreational facilities and roadways is important in establishing the appearance and quality of the Kohl Ranch Specific Plan area. It is anticipated that maintenance responsibilities for the project will be shared by several entities, as outlined below.

Parks and Recreation

Local parks will be dedicated to the Coachella Valley Recreation and Parks District (CVRPD), the new special district created to maintain Open Space as described in 8.b, below, or will remain private. The CVRPD, the new district, or Home Owners Association (HOA) will be responsible for park maintenance and recreation programs associated with these facilities.

Open Space

To maintain project open space (arterial and local streetscapes, trails, drainage courses, and natural open space), a new special district is required, such as a landscaping and lighting district, community services district, or county service area. This decision will be made at a future stage in project development, in concert with County agencies or HOA. The special district would contract with an existing local agency or a private firm for actual services.

Street Lighting

To operate and maintain local street lighting, a new special district is required, such as a landscaping and lighting district, community services district, or county service area. This decision will be made at a future stage in project development, in concert with County agencies. The special district would contract with an existing local agency or a private firm for actual services.

Drainage Facilities

CVWD shall not be responsible for the construction, operation or maintenance of stormwater/drainage facilities within the project area. CVWD shall review the hydrology design of the project and evaluate how drainage from the site could affect regional stormwater facilities. The drainage plan shall be subject to mitigation based upon the findings of the CVWD.

Riverside County and a property owners association or County Service Area are assumed to be responsible for maintaining any storm drains, canals or basins within the project area. Riverside County will not be responsible for drainage maintenance unless specifically indicated by the County.

Project Roadways

All public project roadways and private streets will be designed and constructed to standards acceptable to the County. All public roadways will be entered into the Riverside County System of roads for operation and maintenance as approved by the Board of Supervisors.

3.1.12 Airport Zones and Utility Easement Beltway

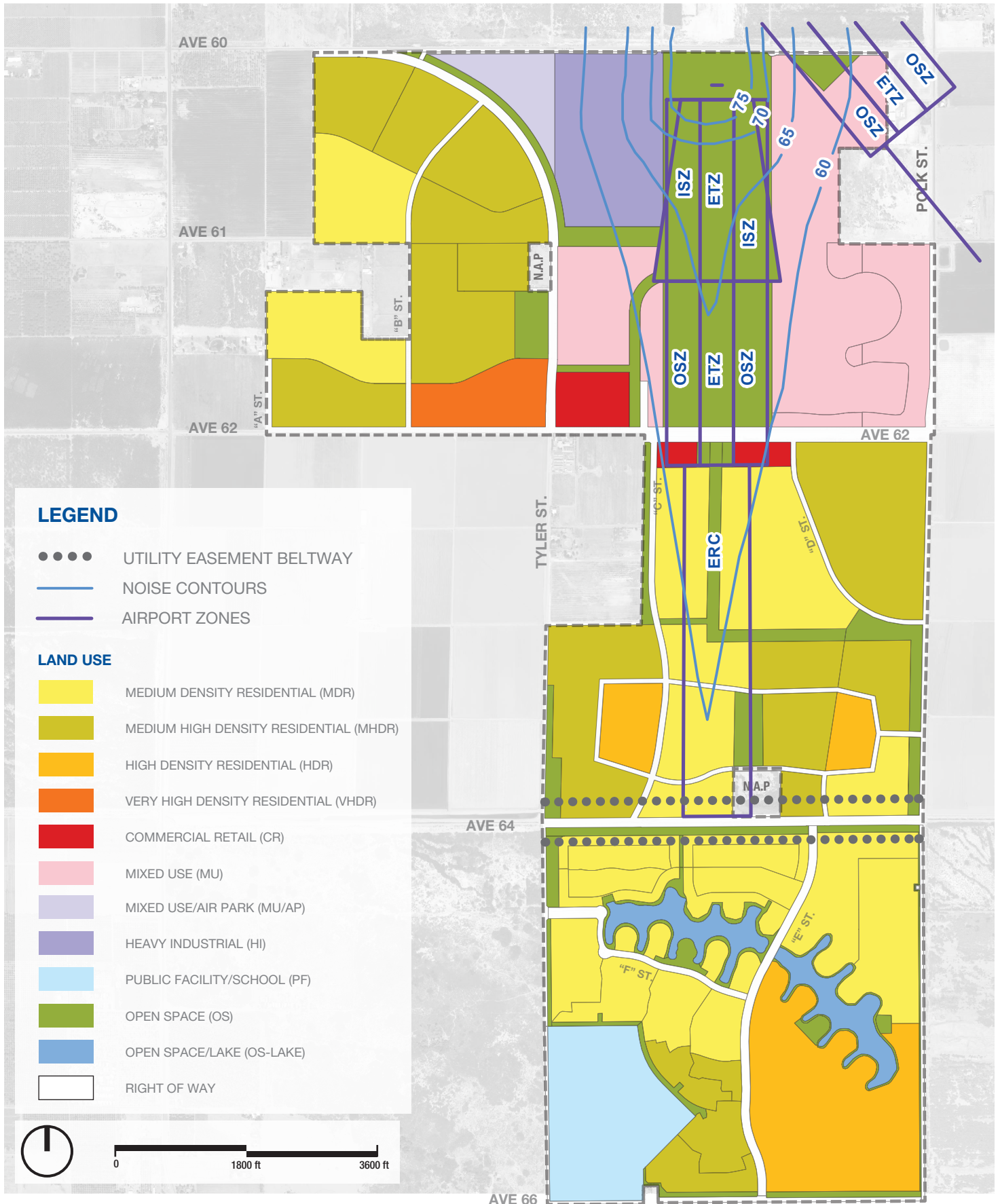
Jacqueline Cochran Regional Airport Land Use Compatibility

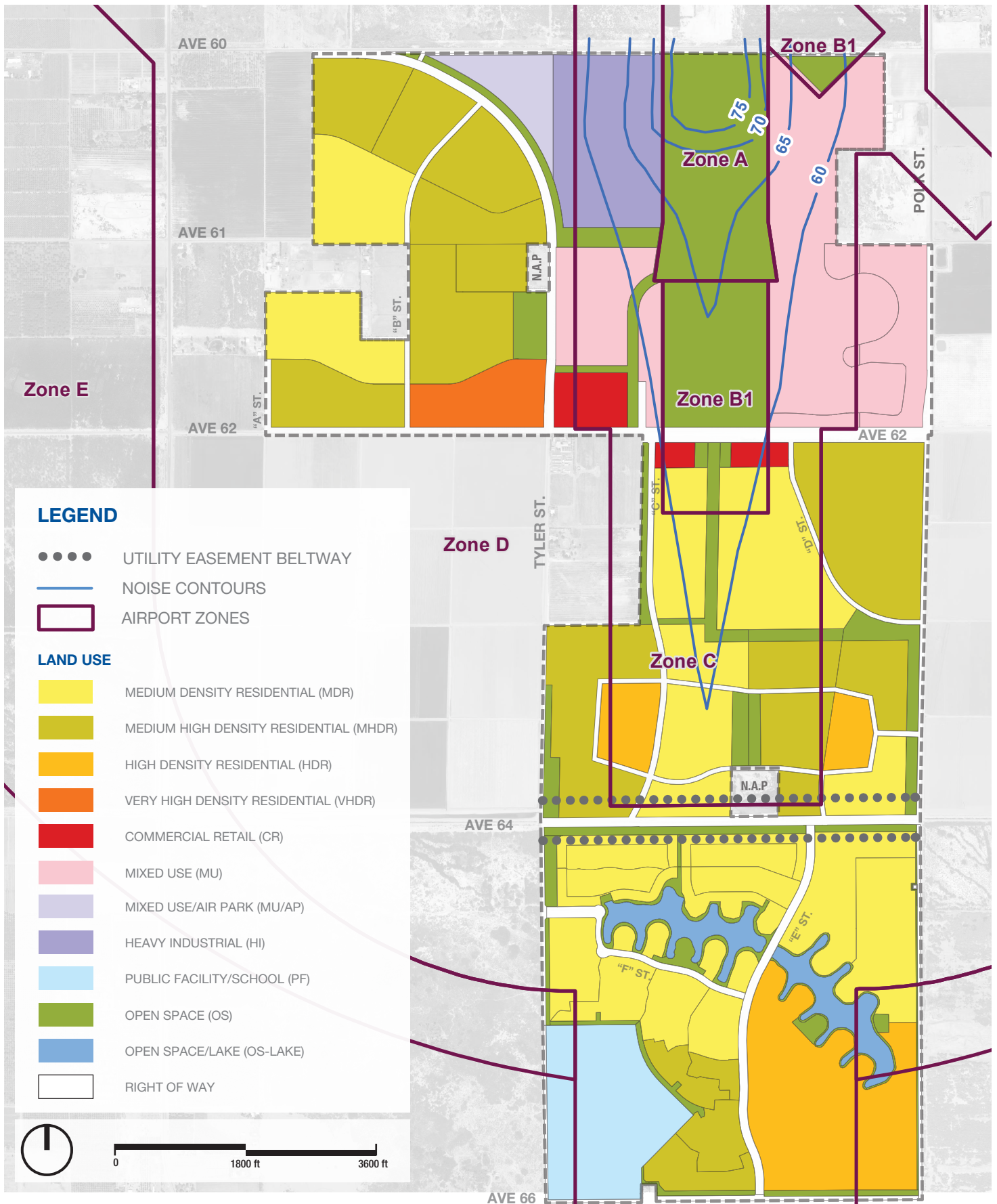
The Kohl Ranch site is located immediately south of Jacqueline Cochran Regional Airport, a general aviation transport airport. Based on proximity of the site to the airport, the Specific Plan has been designed to meet the following objectives:

- reflect current use of and future plans for the airport;
- respond to constraints posed by the airport influence area, and ensure land use compatibility with respect to noise, safety and height; and
- maximize opportunities presented by proximity to this resource through careful design of the project circulation system and strategic location of employment-based uses.

In October 1990, the Riverside County Department of Aviation prepared the Thermal Airport Master Plan. In December 2004, the Riverside County Economic Development Agency prepared a new Airport Master Plan for the renamed Jacqueline Cochran Regional Airport. The Master Plan identifies the potential for limited commercial air carrier and air cargo service, and evaluates several alternatives for expansion of the airport to meet current and future demands through the year 2013. Plans for both airside and landside improvements are identified, including: extension of Runway 17-35 across Avenue 60 onto the Kohl Ranch property; retention of Runway 12-30 as the crosswind runway; general aviation terminal through expansion/redevelopment of existing facilities; separate air carrier and air cargo area; additional T-hangars; and heliport. The Master Plan calls out property acquisition of approximately 128 acres south of Avenue 60 for expansion of runway 17-35. The Master Plan also delineates Airport Safety Zones and noise contours related to planned airport operations as reflected in **Table 3.1-K, Airport Safety Zones** and depicted on **Figure 3.1-13, Airport Safety Zones and Utility Easement Beltway**.

In accordance with State requirements, the County Airport Land Use Commission (ALUC) prepared the Comprehensive Land Use Plan (CLUP) for Thermal Airport in August 1992 and updated it in 2005. The CLUP designates an airport influence area and includes land use compatibility guidelines that address airport noise, safety, height restrictions, and general concerns related to aircraft overflight. The "airport influence area" around Jacqueline Cochran Regional Airport is divided into six compatibility zones. Five zones affect the Kohl Ranch Specific Plan, as shown on **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The areas of the Kohl Ranch project amended/modified by SPA3 are consistent with the land use compatibility guidelines for noise, safety and height contained in the CLUP. The Kohl Ranch project is consistent with the proposed airport expansion and improvement plans described in the Master Plan for the Jacqueline Cochran Regional Airport.





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Table 3.1-K, Airport Safety Zones

Zone		Maximum Density	Maximum Lot Coverage by Structures	Land Use
Symbol	Name			
ISZ	Inner Safety Zone	0 (No structures permitted)	0 (No structures permitted)	No petroleum or explosives storage. No petroleum or natural gas pipelines. No above-ground power lines.
OSZ	Outer Safety Zone	Uses in structures ² : 25 persons/acre Uses not in structures: 50 persons/acre	25% of net area	No residential, hotels, places of public assembly, public utility stations/plants, and facilities that process flammable materials, or that could be damaged in an aircraft accident, among others.
ETZ	Emergency Touchdown Zone	0 (No structures permitted)	0 (No structures permitted)	No structures or other objects that would represent obstructions are allowed, such as but not limited to: large trees, heavy fences and walls, tall and steep berms and retaining walls, non-frangible street light and sign standards, billboards.
ERC	Extended Runway Centerline Zone	3 du/net acre. Uses in structures ² : 100 persons/acre.	50% of gross area or 65% of net area, whichever is greater.	No uses involving as the primary activity, manufacture, storage or distribution of explosives or flammable materials.

Source: Jacqueline Cochran Regional Airport Master Plan, December 14, 2004.

The project supports the development goals for the Jacqueline Cochran Regional Airport by improving circulation in the project vicinity and through the appropriate location of land uses throughout the Kohl Ranch site. Avenue 60 is planned to be closed due to the extension of runway 17-35 across this roadway onto the Kohl Ranch property. Consequently, the Specific Plan proposes the elimination of Avenue 60 where it would intersect with runway 17-35, and proposes Avenue 62 as the primary east-west access through the site.

In addition, land uses planned for the areas closest to the airport property, such as the Mixed Use/AirPark, Mixed Use, Open Space, and Heavy Industrial designations, respond to and support the master planned development intended for the airport. The more intense uses are clustered around the western segment of Avenue 60, as well as along Polk Street, to take advantage of direct access to the airport. Open Space is concentrated in the airport safety zones.

On January 8, 2015, the Riverside County Airport Land Use Commission reviewed Kohl Ranch Specific Plan Amendment No. 3 and found the document to be conditionally consistent with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (JCRAUCP). These recommended ALUC conditions will be considered for adoption as conditions of approval by the County.

Airport Noise Compatibility Guidelines

The CLUP presents noise compatibility guidelines for the Jacqueline Cochran Regional Airport vicinity, as follow:

- Discourage new single-family dwellings and prohibit mobile homes within the 60 CNEL contour. Where homes are permitted within the 60 CNEL contour, the need for sound insulation should be studied and noise easements should be acquired.
- Within the 65 CNEL, new residential construction should not be undertaken. New hotels or motels are permitted if the need for sound insulation is evaluated.
- Institutional uses should be discouraged within the 65-70 CNEL range. If no alternative location is available, the need for sound insulation should be studied.
- Commercial, industrial and recreational uses are considered compatible with noise levels between the 65 and 70 CNEL.

The Kohl Ranch property is impacted by noise from Jacqueline Cochran Regional Airport. To address airport noise in the Specific Plan, primarily open space uses are located within the 65, 70, and 75 CNEL noise contours. A limited amount of airpark/mixed use and industrial uses are within the 65 CNEL contour, consistent with the guidelines. Land uses within the 60 CNEL contour include open space, industrial and commercial retail uses. Some medium density residential uses also are within the 60 CNEL contour. Cluster provisions have been incorporated into Section 2.0 – Specific Plan Zoning so that residential uses can be located outside areas impacted by airport noise. In addition, common walls within medium high, high and very high density units are anticipated to attenuate airport noise within the 60 CNEL contour.

Airport Vicinity Height Guidelines

The Federal government has developed standards for determining obstructions in navigable airspace. Federal Aviation Regulations Part 77 defines a variety of imaginary surfaces at certain altitudes around airports. The Part 77 surfaces include the primary surface, approach surface, transitional surface, horizontal surface, and conical surface. Collectively, the Part 77 surfaces around an airport define a bowl-shaped area with ramps sloping up from each runway end. The Part 77 standards are not absolute height restrictions, but instead identify elevations at which structures may present a potential safety problem. Penetrations of the Part 77 surface generally are reviewed on a case by case basis. The CLUP uses the Part 77 criteria as the basis for height limitations in the vicinity of Jacqueline Cochran Regional Airport.

Height limitations are not anticipated to pose a development constraint for the Kohl Ranch site. Based on the Part 77 surfaces identified in the CLUP, structures above approximately 85 feet in height in the vicinity of Avenue 62, 150 feet in height in the vicinity of Avenue 64, and 315 feet in height in the vicinity of Avenue 66, could potentially create a problem. Development in these areas is not expected to exceed the relevant height limitations.

Airport Safety Compatibility Guidelines

The CLUP for Jacqueline Cochran Regional Airport establishes land use compatibility guidelines for land use compatibility zones that affect the project site. These guidelines are summarized below in **Table 3.1-L, Basic Compatibility Criteria**.

In addition to the guidelines for the five zones, the CLUP prohibits particularly hazardous land uses in all designated safety zones. These land uses include uses which would impede the ability of the pilot to see the airfield as identified in Footnote 1 to **Table 3.1-L**.

Open space is concentrated within the most restrictive safety zones, the Emergency Touchdown Zone (ETZ) and the Inner Safety Zone (ISZ), with the exception of a small mixed use land designation in the northeastern most corner of The Kohl Ranch Specific Plan. Land uses proposed in the Specific Plan within the Outer Safety Zone (OSZ) are limited to open space, commercial, and mixed use land uses. The Extended Runway Centerline (ERC) zone, while less restrictive, limits the density of uses within the zone. Consequently, open space and medium density residential uses have been sited within this area. Residential cluster provisions incorporated into Section 2.0 – Specific Plan Zoning, encourage common open space areas to be located within the ERC, with transfer of residential density to areas outside the safety zone.

Airport Compatibility Design Standards for Mixed Use Residential

Mixed Use structures in Planning Areas A-6, and E-2 through E-8 of The Kohl Ranch Specific Plan, allowing for overnight occupancy shall incorporate the following features:

- Signage depicting areas allowing overnight occupancy from areas of non-overnight occupancy.

Airport Compatibility Design Guidelines for Lakes

Recreational lakes within The Kohl Ranch Specific Plan shall be designed to incorporate the following features in order to reduce or eliminate the habitat and foraging opportunities for birds in the water areas:

- Use of linear shaped water areas.
- Formed, vertical banks that make access to land at the water's edge difficult.
- Restrict any vegetation that offers shelter and nesting opportunities.
- Do not include islands in water areas.
- Eliminate shallows and muddy areas.
- Control availability of forage – fish, frogs, algae and insects.
- Provide for human activity in or near the water.

Through the incorporation of these features into the design of the recreational lakes, the lakes will not attract the type or concentration of bird activity that would create a hazard to aircraft in flight.

Utility Easement Beltway

A utility easement beltway traverses the site in an east-west direction, roughly along the Avenue 64 right-of-way. This beltway contains easements for CVWD irrigation pipes, the Avenue 64 Evacuation Channel, and a power line easement for an existing 161 kV power line. This area is proposed to remain in street right of way and open space, and to incorporate an element of the project-wide trail system, connecting with parks and public facilities throughout the site. Residential development in the planning areas immediately to the north of the easement beltway would be clustered outside of the beltway, to maintain the residential densities in these areas and preserve open space areas.

Table 3.1-L, Basic Compatibility Criteria

Zone	Locations	Maximum Densities / Intensities				Req'd Open Land ²	Additional Criteria	
		Residential (d.u./ac) ¹	Other Uses (people/ac) ²				Prohibited Uses ⁴	Other Development Conditions ⁵
		Average ⁶	Single Acre ⁷	with Bonus ⁸				
A	Runway Protection Zone and within Building Restriction Line	0	0	0	0	All Remaining	<ul style="list-style-type: none"> All structures except ones with location set by aeronautical function Assemblages of people Objects exceeding FAR Part 77 height limits Storage of hazardous materials Hazards to flight⁹ 	<ul style="list-style-type: none"> Avigation easement dedication
B1	Inner Approach/Departure Zone	0.05 (average parcel size ≥20.0 ac.)	25	50	65	30%	<ul style="list-style-type: none"> Children's schools, day care centers, libraries Hospitals, nursing homes Places of worship Blgds with >2 aboveground habitable floors Highly noise-sensitive outdoor nonresidential uses¹⁰ Aboveground bulk storage of hazardous materials¹¹ Critical community infrastructure facilities¹² Hazards to flight⁹ 	<ul style="list-style-type: none"> Locate structures maximum distance from extended runway centerline Minimum NLR of 25 dB in residences (including mobile homes) and office buildings¹³ Airspace review required for objects >35 feet tall¹⁴ Avigation easement dedication
B2	Adjacent to Runway	0.1 (average parcel size ≥10.0 ac.)	100	200	260	No Req't	Same as Zone B1	<ul style="list-style-type: none"> Locate structures maximum distance from runway Minimum NLR of 25 dB in residences (including mobile homes) and office buildings¹³ Airspace review required for objects >35 feet tall¹⁴ Avigation easement dedication
C	Extended Approach/Departure Zone	0.2 (average parcel size ≥5.0 ac.)	75	150	195	20%	<ul style="list-style-type: none"> Children's schools, day care centers, libraries Hospitals, nursing homes Blgds with >3 aboveground habitable floors Highly noise-sensitive outdoor nonresidential uses¹⁰ Hazards to flight⁹ 	<ul style="list-style-type: none"> Minimum NLR of 20 dB in residences (including mobile homes) and office buildings¹³ Airspace review required for objects >70 feet tall¹⁵ Deed notice required
D	Primary Traffic Patterns and Runway Buffer Area	(1) ≤0.2 (average parcel size ≥5.0 ac.) or ¹⁶ (2) ≥5.0 (average parcel size ≤0.2 ac.)	100	300	390	10%	<ul style="list-style-type: none"> Highly noise-sensitive outdoor nonresidential uses¹⁰ Hazards to flight⁹ 	<ul style="list-style-type: none"> Airspace review required for objects >70 feet tall¹⁵ Children's schools, hospitals, nursing homes discouraged¹⁷ Deed notice required
E	Other Airport Environs	No Limit	No Limit ¹⁸		No Req't		<ul style="list-style-type: none"> Hazards to flight⁹ 	<ul style="list-style-type: none"> Airspace review required for objects >100 feet tall¹⁵ Major spectator-oriented sports stadiums, amphitheaters, concert halls discouraged beneath principal flight tracks¹⁸
*	Height Review Overlay	Same as Underlying Compatibility Zone		Not Applicable		Same as Underlying Compatibility Zone		<ul style="list-style-type: none"> Airspace review required for objects >35 feet tall¹⁴ Avigation easement dedication

See Chapter 3 for airport-specific additions or exceptions to these policies

NOTES:

- ¹ Residential development must not contain more than the indicated number of dwelling units (excluding secondary units) per gross acre. Clustering of units is encouraged. See Policy 4.2.5 for limitations. Gross acreage includes the property at issue plus a share of adjacent roads and any adjacent, permanently dedicated, open lands. Mixed-use development in which residential uses are proposed to be located in conjunction with nonresidential uses in the same or adjoining buildings on the same site shall be treated as nonresidential development. See Policy 3.1.3(d).
- ² Usage intensity calculations shall include all people (e.g., employees, customers/visitors, etc.) who may be on the property at a single point in time, whether indoors or outside.
- ³ Open land requirements are intended to be applied with respect to an entire zone. This is typically accomplished as part of a community general plan or a specific plan, but may also apply to large (10 acres or more) development projects. See Policy 4.2.4 for definition of open land.
- ⁴ The uses listed here are ones that are explicitly prohibited regardless of whether they meet the intensity criteria. In addition to these explicitly prohibited uses, other uses will normally not be permitted in the respective compatibility zones because they do not meet the usage intensity criteria.
- ⁵ As part of certain real estate transactions involving residential property within any compatibility zone (that is, anywhere within an airport influence area), information regarding airport proximity and the existence of aircraft overflights must be disclosed. This requirement is set by state law. See Policy 4.4.2 for details. Easement dedication and deed notice requirements indicated for specific compatibility zones apply only to new development and to reuse if discretionary approval is required.
- ⁶ The total number of people permitted on a project site at any time, except rare special events, must not exceed the indicated usage intensity times the gross acreage of the site. Rare special events are ones (such as an air show at the airport) for which a facility is not designed and normally not used and for which extra safety precautions can be taken as appropriate.
- ⁷ Clustering of nonresidential development is permitted. However, no single acre of a project site shall exceed the indicated number of people per acre. See Policy 4.2.5 for details.
- ⁸ An intensity bonus may be allowed if the building design includes features intended to reduce risks to occupants in the event of an aircraft collision with the building. See Policy 4.2.6 for details.
- ⁹ Hazards to flight include physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations. Land use development that may cause the attraction of birds to increase is also prohibited. See Policy 4.3.7.
- ¹⁰ Examples of highly noise-sensitive outdoor nonresidential uses that should be prohibited include amphitheatres and drive-in theaters. Caution should be exercised with respect to uses such as poultry farms and nature preserves.
- ¹¹ Storage of aviation fuel and other aviation-related flammable materials on the airport is exempted from this criterion. Storage of up to 6,000 gallons of nonaviation flammable materials is also exempted. See Policy 4.2.3(c) for details.
- ¹² Critical community facilities include power plants, electrical substations, and public communications facilities. See Policy 4.2.3(d) for details.
- ¹³ NLR = Noise Level Reduction, the outside-to-inside sound level attenuation that the structure provides. See Policy 4.1.6.
- ¹⁴ Objects up to 35 feet in height are permitted. However, the Federal Aviation Administration may require marking and lighting of certain objects. See Policy 4.3.6 for details.
- ¹⁵ This height criterion is for general guidance. Shorter objects normally will not be airspace obstructions unless situated at a ground elevation well above that of the airport. Taller objects may be acceptable if determined not to be obstructions. See Policies 4.3.3 and 4.3.4.
- ¹⁶ Two options are provided for residential densities in *Compatibility Zone D*. Option (1) has a density limit of 0.2 dwelling units per acre (i.e., an average parcel size of at least 5.0 gross acres). Option (2) requires that the density be *greater than* 5.0 dwelling units per acre (i.e., an average parcel size *less than* 0.2 gross acres). The choice between these two options is at the discretion of the local land use jurisdiction. See Table 2B for explanation of rationale. All other criteria for *Zone D* apply to both options.
- ¹⁷ Discouraged uses should generally not be permitted unless no feasible alternative is available.
- ¹⁸ Although no explicit upper limit on usage intensity is defined for *Zone E*, land uses of the types listed—uses that attract very high concentrations of people in confined areas—are discouraged in locations below or near the principal arrival and departure flight tracks. This limitation notwithstanding, no use shall be prohibited in *Zone E* if its usage intensity is such that it would be permitted in *Zone D*.

Source: Riverside County Airport Land Use Compatibility Plan Policy Document (adopted October 2004)

3.1.11 Specific Plan Implementation and Administrative Process

Pursuant to Government Code Section 65451 all specific plans must contain a "program of implementation measures including regulations, programs, public works projects, and financing measures" necessary to implement the specific plan. This section outlines the methods by which development in the specific plan will be processed, including infrastructure financing mechanisms and other necessary programs.

As it relates to the implementation and administrative process, Riverside County Zoning Ordinance 348 shall be adhered to at all times.

Specific Plan Time Frames

Riverside County requires the adoption of a phasing plan for each Specific Plan (SP). Each plan must be monitored for reasonable progress toward implementation. The phasing program for the Kohl Ranch Specific Plan is described above in Section 3.1.7 – Public Facility Sites and Project Phasing. The project developer will work with the County to adhere to the phasing plan and to ensure the logical and timely completion of the project.

Financing Plan

The developer shall be responsible for financing construction of the infrastructure improvements required to support the Project, such as perimeter and internal streets, water lines, sewers, and storm drains. All necessary infrastructure improvements shall be developed in conjunction with the roadway improvements. The financing of construction, operation, and maintenance of public improvement and facilities will include funding through a combination of financing mechanisms. However, the developer shall be ultimately responsible for all fair share costs associated with implementing the Project, including but not limited to the costs of providing infrastructure and complying with mitigation measures, conditions of approval, and other requirements of the Project.

Financing may involve a combination of impact fees and exacting, special assessment districts, landscaping and lighting districts, and other mechanisms agreed to by the developer and the County of Riverside as noted below. Developer funded improvements may be subject to a reimbursement agreement or credits against fees pursuant to provisions of a development agreement or conditions of approval. The County and developer will cooperate to ensure that the public facilities are built in accordance with all requirements of the Specific Plan and EIR. A development agreement and conditions of approval may be used to facilitate this process.

Developer Funding

In many cases, certain onsite facilities are tied directly to individual projects. In these cases, it is reasonable to expect the developer (or property owner) to pay the entire cost of the facility in order to secure development rights. Onsite local streets, utility connections from main trunk lines, and drainage facilities are good examples of facilities that are normally required concurrent with development of an individual parcel funded by the developer.

Special Assessment Districts

A special assessment district is a type of benefit district that requires a vote by the property owners to encompass a defined and limited geographic area. The County or other agencies may form a special assessment district under one of several different statutory acts to construct

public improvements such as streets, storm drains, sidewalks, streetlights, sewers, parks landscape, and other similar capital facilities. The special assessment district can issue bonds to finance those improvements and levy a special assessment to pay debt service on those bonds.

A special assessment district may fund improvements within the entire Specific Plan area or smaller areas in the Specific Plan where special improvements are constructed that directly benefit only certain property owners. Special assessments districts may only be used to pay for projects that are of specific and direct benefit to the property owner being assessed. The amount of the assessment must directly relate to the amount of benefit received by the property owner.

Landscaping and Lighting Districts

Landscaping and lighting districts (LLD) may be used for installation, maintenance, and servicing of landscaping and lighting through annual assessments on benefiting properties. LLDs may also provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities.

Other Funding Sources

Other sources may be available to finance improvement projects, such as government grants, private developer coalitions, or various types of bonds not listed above.

Dwelling Unit Tracking System

Each Residential Planning Area within the Kohl Ranch Specific Plan has an identified density range consistent with the County General Plan, Flexibility Range, and a Target number of dwelling units as identified in **Table 3.1-N, Residential Planning Area Flexibility Range**. The Flexibility Range is consistent with a variation of no more than 10% from the allowable density range. The Flexibility Range was determined based on several factors: (1) ultimately the Planning Area acreage may change as a result of final engineering, (2) the flexibility may be necessary to accommodate airport restrictions which continue to change over time, and (3) based on the fact that there is an overall unit cap within the SP. Flexibility can be given to the density range and target number of dwelling units within each Residential Planning Area, under certain circumstances, as described below. Flexibility can be given to the density range and target number of dwelling units within each residential Planning Area, under certain circumstances, as described below.

Additionally, all Planning Area acreages may be revised when more accurate information is available. Up to a 15 percent variation in Planning Area acreage may be allowed without a Specific Plan Amendment or determination of substantial project conformance with adopted specific plan, but will be required to receive administrative approval. A 16 to 30 percent variation in Planning Area acreage may be allowed with a 'determination of substantial project conformance with adopted specific plan,' and any acreage change in Planning Area acreage of over 30 percent will require a Specific Plan Amendment. A Planning Area acreage change would not result in a Flexibility Range change, without requiring a Specific Plan Amendment. This accommodation should cover reasonable changes in planning area size caused by roadway realignments such as the one that precipitated changes reflected in Specific Plan No. 303 Amendment 2 (SP00303A2).

Any developer proposing residential development within the Kohl Ranch Specific Plan, either through individual or multiple Planning Areas, will be required to report how many dwelling units will be developed within each Planning Area, as shown on **Table 3.1-O, Reporting Mechanism**, and if any modifications to the Land Use Table are necessary. This report will need to be submitted to the County of Riverside Planning Department to ensure the total number of dwelling units will not exceed 7,171. **Table 3.1-P, Master Log**, will be the running tabular record of all proposed and developed dwelling units, for use by the Planning Department.

Table 3.1-N, Residential Planning Area Flexibility Range

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range DU/AC	Flexibility Range DU/AC
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0	4.5-8.8
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0	4.5-8.8
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0	1.8-5.5
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0-8.0	4.5-8.8
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0	4.5-8.8
C-1	Medium Density Residential (MDR)	122	40.77	2.0-5.0	1.8-5.5
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0	4.5-8.8
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0	4.5-8.8
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0	12.6-22.0
C-8	Medium High Density Residential (MHDR)	105	14.96	5.0-8.0	4.5-8.8
F-2	Medium Density Residential (MDR)	158	71.64	2.0-5.0	1.8-5.5
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0	4.5-8.8
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0	1.8-5.5
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0	4.5-8.8
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0	7.2-15.4
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0	1.8-5.5
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0	1.8-5.5
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0	1.8-5.5
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0	4.5-8.8
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0	4.5-8.8
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0	4.5-8.8
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0-8.0	4.5-8.8
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0	7.2-15.4
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0	1.8-5.5
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0	1.8-5.5
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0	1.8-5.5
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0	1.8-5.5
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0	1.8-5.5
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0	1.8-5.5

I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0	1.8-5.5
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0	1.8-5.5
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0	1.8-5.5
J-1	Medium Density Residential (MDR)	130	37.52	2.0-5.0	1.8-5.5
J-4	Medium Density Residential (MDR)	189	43.36	2.0-5.0	1.8-5.5
L-1	High Density Residential (HDR)	1,158	106.2	8.0-14.0	7.2-15.4
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0	1.8-5.5
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0	1.8-5.5
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0	1.8-5.5
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0	1.8-5.5
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0	1.8-5.5
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0	1.8-5.5
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0	1.8-5.5
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0	1.8-5.5
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0	4.5-8.8
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0-8.0	4.5-8.8
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0-8.0	4.5-8.8
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0	4.5-8.8
TOTAL		<u>7,003</u>	<u>1,140.29</u>		

Development within the Dwelling Unit Range

The Density Range is established at the time of the adoption of the Specific Plan. Any proposed development within the Density Range is in conformance with the Specific Plan. When completing the Reporting Mechanism, and if the proposed development is within the Density Range, a justification statement is not required, and a Specific Plan Amendment or Specific Plan Substantial Conformance Report is not required, as well.

Development Below or Above Density Range but Within the Flexibility Range

The Flexibility Range is defined as the range within 10 percent of the allowable Density Range for each Planning Area. The Flexibility Range gives more flexibility to the Density Range for each Planning Area to account for minor modifications, and is permitted under certain circumstances, without requiring a Specific Plan Amendment, but a Specific Plan Substantial Conformance Report is required.

The Developer, in requesting use of the Flexibility Range, shall state those special circumstances in **Table 3.1-O**, which would justify the use of the Flexibility Range. Examples that would justify use of the Flexibility Range include, but are not limited to: a decrease or increase in Planning Area size at the final engineering level; or the decrease of residential development acreage as a result of the development of parks, trails, or public facilities. The Planning staff shall determine if the request for the Flexibility Range is justified, and in granting approval of the Flexibility Range, staff shall find as follows:

- 1) That the Planning Area for the desired Flexibility Range is adequate in size and shape to accommodate proposed development; and
- 2) That the site for the proposed development contains streets and public facilities properly designed to carry the type and quantity of traffic generated by the subject development; and
- 3) That the proposed development will not impair the integrity and character of the Planning Area in which it is located or otherwise have an adverse effect on adjacent property or the permitted use thereof; and
- 4) That the proposed use is not contrary to this Specific Plan.

Development Outside of Flexibility Range

If the proposed development is below or above the existing allowable Density Range and the Flexibility Range, a Specific Plan Amendment will be required.

The Difference of Dwelling Units Developed Compared to the Target

Each residential Planning Area has a designated Dwelling Unit Target, which in total comprises 7,161 dwelling units. When a Planning Area is developed, a Residential Development Report, **Table 3.1-O, Reporting Mechanism**, is submitted to the Planning Department to record the number of dwelling units. Any units over or under the Dwelling Unit Target number of dwelling units will be added or taken away from the remaining balance of units to ensure the 7,171 maximum dwelling unit total for the Specific Plan is not exceeded and added or subtracted from the Unit Pool. When the Unit Pool reaches 50 ± units, a redistribution to/from other planning areas shall be completed by the Master Developer and submitted to the County Planning Department.

3.2 LAND USE, PLANNING & DEVELOPMENT STANDARDS

Development standards for the Kohl Ranch have been established at four levels: Section 3.1 – Project-Wide Planning Standards, Section 3.3 – Design Guidelines, Section 3.4 – Landscape Design Guidelines, and Land Use, Planning & Development Standards, which are provided in this section.

Neighborhood Concept

The development proposal for the Kohl Ranch Specific Plan is founded upon the concept of self-sufficient neighborhoods, each defined by strong edges and an internal circulation system, and identified by a similar orientation, intent, and theme. Where appropriate, design guidelines are tailored to reinforce the unique character and theme of the project neighborhoods. Linking all of the neighborhoods is an extensive open space and trail system, relating to the airport safety zones and drainage ways, which provide residents and workers with active and passive recreational opportunities and scenic amenity. This open space network also serves to buffer incompatible uses and neighborhoods from one another as reflected in **Figure 3.2-1, Neighborhoods**. As described earlier in this chapter, neighborhoods in the northern portion of the site generally have a high concentration of employment-based uses, while neighborhoods in the southern portion are predominantly residential.

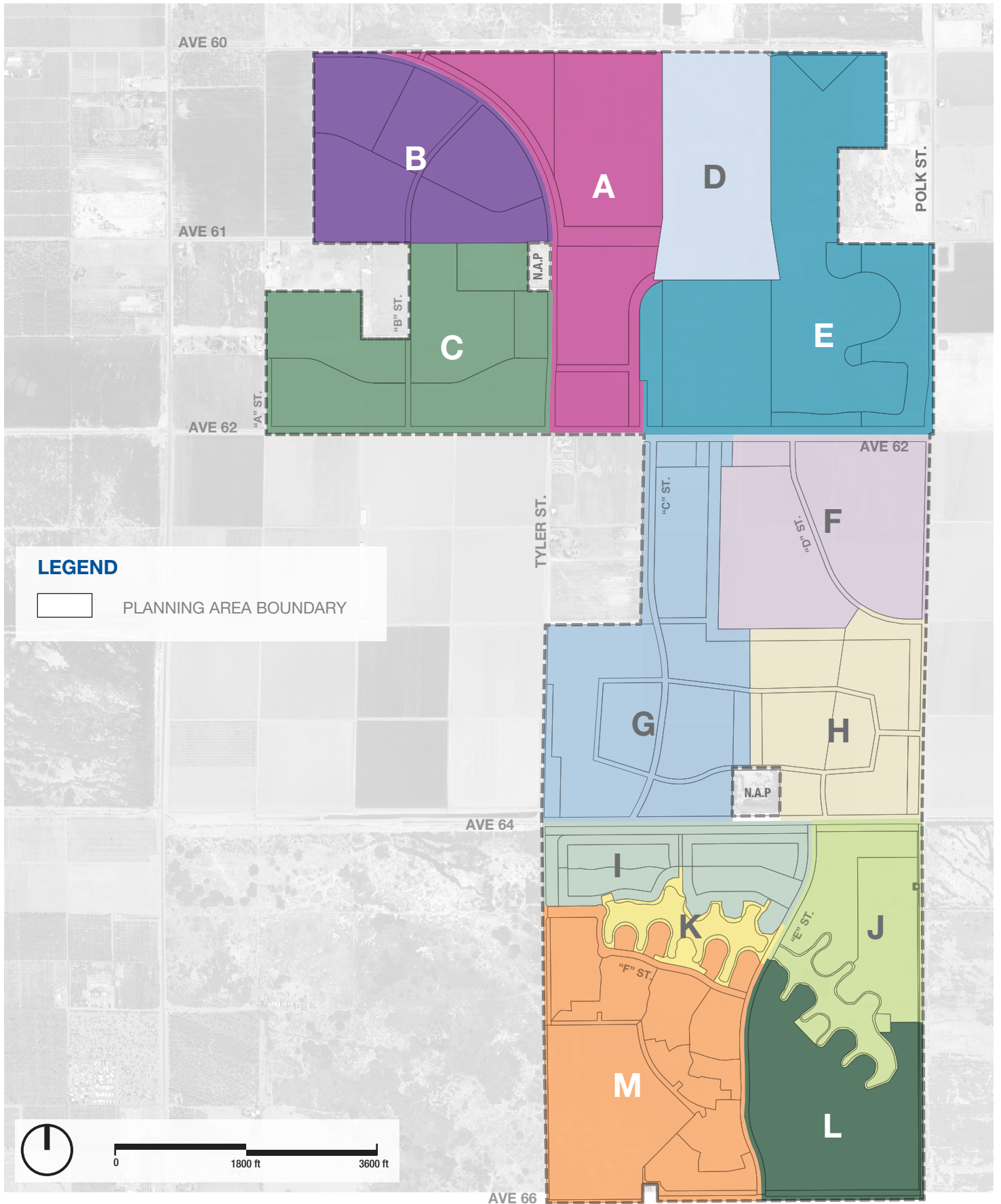
The Kohl Ranch Specific Plan identifies 95 planning areas on the site grouped within 13 larger areas referred to as neighborhoods. Neighborhoods define logical development areas based on land use and the relationship to planned roadways and infrastructure. The thirteen project neighborhoods were conceived to create cohesive, well-balanced communities, upon completion of the project. The neighborhoods relate generally to the phasing plan for the project, so that infrastructure and amenities can be readily provided as each neighborhood is developed. However, the neighborhood boundaries are slightly different than the boundaries of the project phasing scenarios which are based on the logical provision of infrastructure as well as overall market forces.

Definitions of the project planning areas are based upon logical, separate units of development, adjoining land uses, available infrastructure and physical landscape features.

Flexibility of the Standards

The Land Use, Planning & Development Standards described here and in Section 2 – Specific Plan Zoning Ordinance, are designed to provide a high degree of flexibility for future development of the Kohl Ranch site. This flexibility is critical, in order to respond to changing market forces during the life of the project, and to accommodate future conditions related to development of Jacqueline Cochran Regional Airport

This flexibility is reflected in provisions for alternate allowable land uses that are specified in the descriptions of neighborhoods and planning areas below. Such alternate land uses include golf courses and related facilities, other large recreational uses such as race track facilities and lakes. The conditions under which alternative land uses are allowed, along with the planned use of buffers and organization of land uses throughout the project will ensure that development of the project can be managed over time and that incompatibilities between adjacent land uses can be avoided.



THE KOHL RANCH

COACHELLA VALLEY, CALIFORNIA

Flexibility is also evident in the plan through the incorporation of provisions which allow the clustering of residential uses, through the transfer of private open space to common open space. As a result, overall residential densities remain the same, and a shared amenity is created. The cluster option is particularly well-suited to land within airport-restricted zones, and to planning areas that are adjacent to drainage facilities which may be expanded under a golf course scenario.

Organization of this Section

Detailed descriptions of the neighborhoods are provided below, followed by neighborhood-wide planning standards which describe airport-related constraints, allowable land uses, neighborhood access, and streetscape, entry and intersection treatments. Following the neighborhood descriptions and planning standards are descriptions and standards for each individual planning area. A brief description, summary of land use and development standards specified in Section 2 – Specific Plan Zoning Ordinance, and applicable planning standards are provided for each planning area. For each planning area, these standards describe such features as alternate allowable land uses and conditions, access, and edge and buffer treatments.

A foldout, plan view graphic is provided for each of the thirteen neighborhoods. Each of these graphics is located at the beginning of the corresponding neighborhood section, and should be kept open during review of the planning area descriptions and standards which precede it. These graphics indicate the location of all planning areas, access points, and all landscape features including streetscapes, intersection details, project entries, edges, and buffers.

Appropriate references in the text are made to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines. The graphics for the Design Guidelines are derived from the Community Structure Plan and are intended to convey the project intent with respect to open space, landscaping, site planning and architecture. These elements will create the framework for development of the site. Although development may conform closely to elements of the illustrative plans provided in this section, it is anticipated that actual development will respond to market conditions.

A Specific Plan Zoning Ordinance was prepared in conjunction with this Specific Plan document. The zoning provisions within that ordinance establish allowable uses and development standards for each planning area. The zoning provisions should be used in conjunction with the planning standards for each planning area.

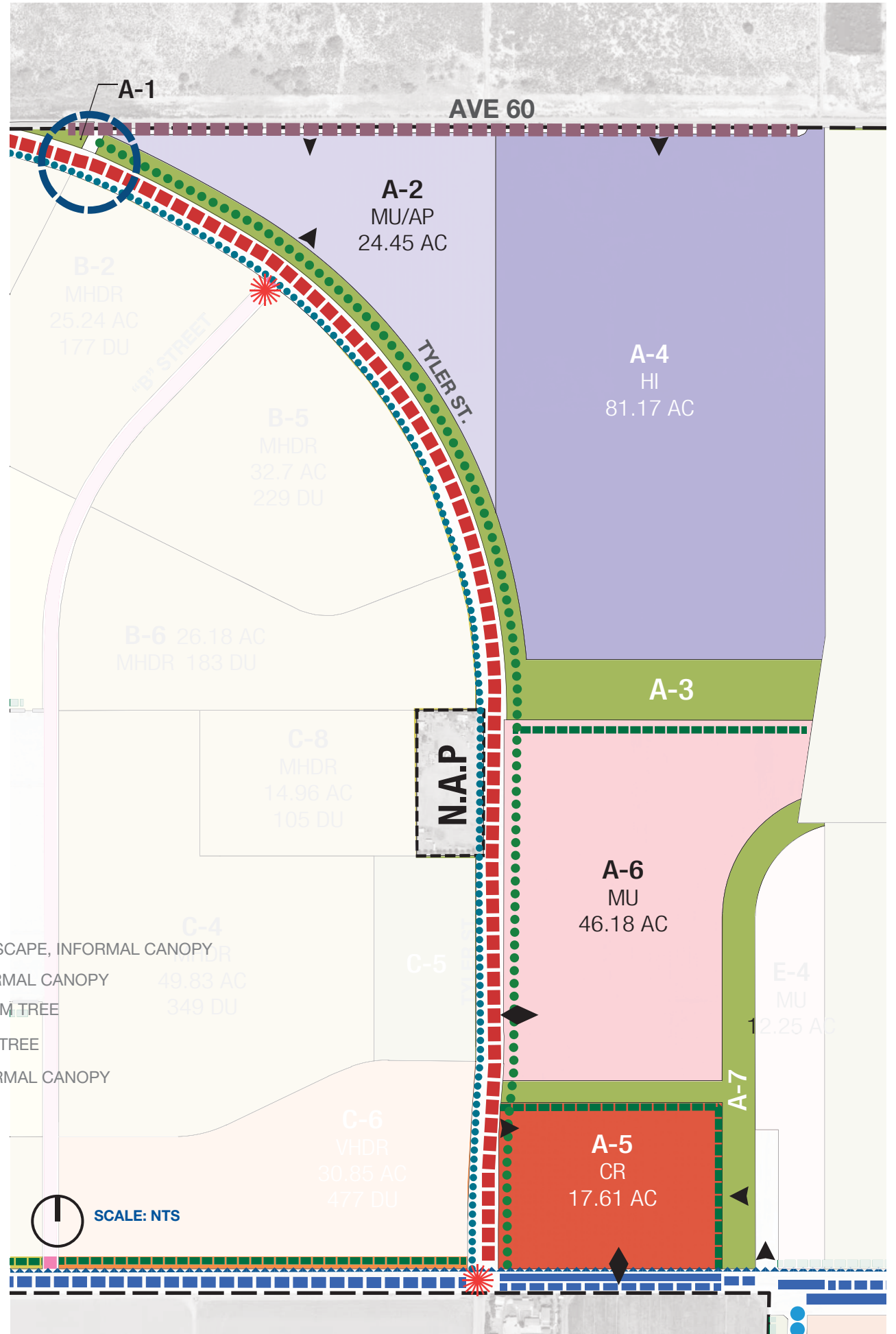
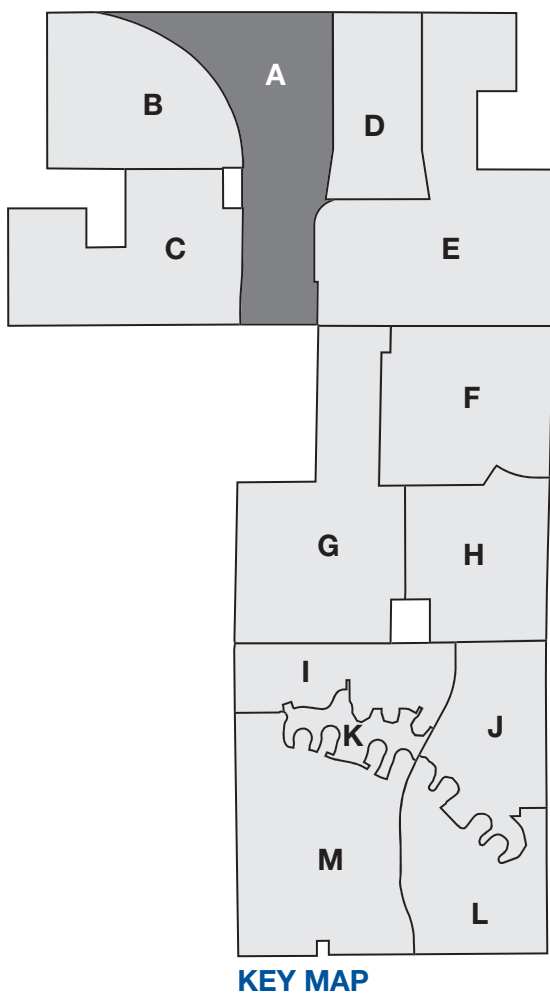
3.2.1 Neighborhood A

Neighborhood A, which consists of seven planning areas, is located in the north-central portion of the project site, just south of Avenue 60 and east of Tyler Street as depicted in **Figure 3.2-2, Neighborhood A**. The neighborhood totals approximately 190 acres including 21.37 acres of open space, 24.45 acres of mixed use/air park, 17.61 acres of commercial retail, 81.17 acres of heavy industrial, and 46.18 acres of mixed use. The neighborhood has a natural outward orientation and response to the regional context. Land uses targeted for Neighborhood A are intended to serve an area larger than the neighborhood itself, providing jobs and services to areas north and west of the site.

The uses provided for within Neighborhood A relate to planned uses at the Jacqueline Cochran Regional Airport located immediately to the north. A mix of commercial, industrial, mixed use, and service uses are intended to support airport development. The open space areas in the neighborhood include elements of the project-wide trail system that are generally coincident with drainage ways and major streets. Primary access to the neighborhood is provided via Avenue 60 and Tyler Street. A major project entry is provided at the intersection of Tyler Street and Avenue 60.

NEIGHBORHOOD A PLANNING STANDARDS

- 1) Portions of the commercial retail, mixed use, and industrial land uses within this neighborhood are located within Zone C of the Airport Compatibility Land Use Plan (CLUP). The mixed use/air park area and portions of the commercial-retail, mixed use and industrial uses are located within Zone D as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with these zones are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- 2) A large-scale recreational use such as a motor sports race track is a permitted use in two of the seven planning areas.
- 3) Tyler Street provides access to Planning Areas A-1 through A-7.
- 4) The northwest corner of Planning Area A-2 at Avenue 60/Tyler Street will be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- 5) The intersections of Tyler Street and Avenue 62 and Tyler Street and “B” Street will be developed in accordance with **Figure 3.4-5, Special Intersection Detail**.
- 6) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**.
- 7) Tyler Street, north of Avenue 62 will be developed in accordance with **Figure 3.4-8, Arterial Streetscape 1**.
- 8) Avenue 60 (along north edge of Planning Areas A-2 and A-4) will be developed in accordance with **Figure 3.4-16, Industrial Collector Streetscape**.
- 9) The east side of Tyler Street will be developed in accordance **Figure 3.4-19, Tyler Street Trail**.
- 10) The north side of Avenue 62 will be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.



LEGEND

- PROJECT AREA BOUNDARY
- ▬ INDUSTRIAL COLLECTOR STREETSCAPE, INFORMAL CANOPY
- ▬ EXPRESSWAY STREETSCAPE, FORMAL CANOPY
- ▬ EXPRESSWAY STREETSCAPE, PALM TREE
- ▬ ARTERIAL STREETSCAPE 1, PALM TREE
- ▬ COLLECTOR STREETSCAPE 1, FORMAL CANOPY
- ▬ WINDROW PLANTING
- ▬ CLASS I BIKE PATH
- ▬ REGIONAL TRAIL
- ▬ CLASS II BIKE PATH
- ▲ FULL ACCESS
- ◆ RIGHT IN/RIGHT OUT ACCESS
- ☀ INTERSECTION DETAIL



Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Heavy Industrial (HI)		46.17	
A-7	Open Space (OS)		9.97	
TOTAL			190.77	

NEIGHBORHOOD A PLANNING AREAS**A-1: OPEN SPACE**

Planning Area A-1 provides for development of 0.63 acres of open space. This area is to be used as a collection point for storm flows entering the project site.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-1 is located in Zone D of the CLUP. Development in this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area A-1 is accessed from Tyler Street.
- (3) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (4) Large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (5) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-19, Tyler Street Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

A-2: MIXED USE/AIR PARK

Planning Area A-2 provides for 24.45 acres of mixed use/air park development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-2 is located in Zone D of the CLUP. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-2 is accessed from Tyler Street.
- (4) The edge treatment for the interface between Planning Area A-2's mixed use/air park and the drainage uses in the adjacent planning area, is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-3: OPEN SPACE

Planning Area A-3 provides for the development of 10.77 acres of open space which will be part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-3 is located in Zones C and D of the CLUP. Development in these zones will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-3 is accessed from Tyler Street.
- (4) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The edge treatment for the interface between the drainage uses in this planning area and the heavy industrial and mixed use/air park land uses in the adjacent planning areas are described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (6) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-18, Tyler Street Trail**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

A-4: HEAVY INDUSTRIAL

Planning Area A-4 provides for 81.17 acres of heavy industrial development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-4 is located in Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-4 is accessed from Tyler Street.
- (4) The edge treatment for the interface between Planning Area A-4's heavy industrial use and the drainage uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-5: COMMERCIAL RETAIL

Planning Area A-5 provides for 17.61 acres of commercial development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-5 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area A-5 is accessed from Tyler Street and Avenue 62.
- (3) The edge treatment for the interface between the commercial use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-19, Tyler Street Trail**. A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-6: MIXED USE

Planning Area A-6 provides for 46.18 acres of mixed-use development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-6 is located within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development in these zones will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-6 is accessed from Tyler Street.
- (4) The edge treatment for the interface between the drainage uses and the heavy industrial land uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-18, Tyler Street Trail**.
- (6) An observation tower built within Planning Area A-6 and built as part of a large scale recreational use shall not exceed 70 feet.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 3.3 – Design Guidelines for Mixed Use Development and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-7: OPEN SPACE

Planning Area A-7 provides for 9.97 acres of open space which will be part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-7 is located within Zone C of the CLUP for the airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-7 is accessed from Tyler Street and Avenue 62.
- (4) The edge treatment for the interface between the drainage in this planning area and the heavy industrial and commercial land uses in the adjacent planning areas is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

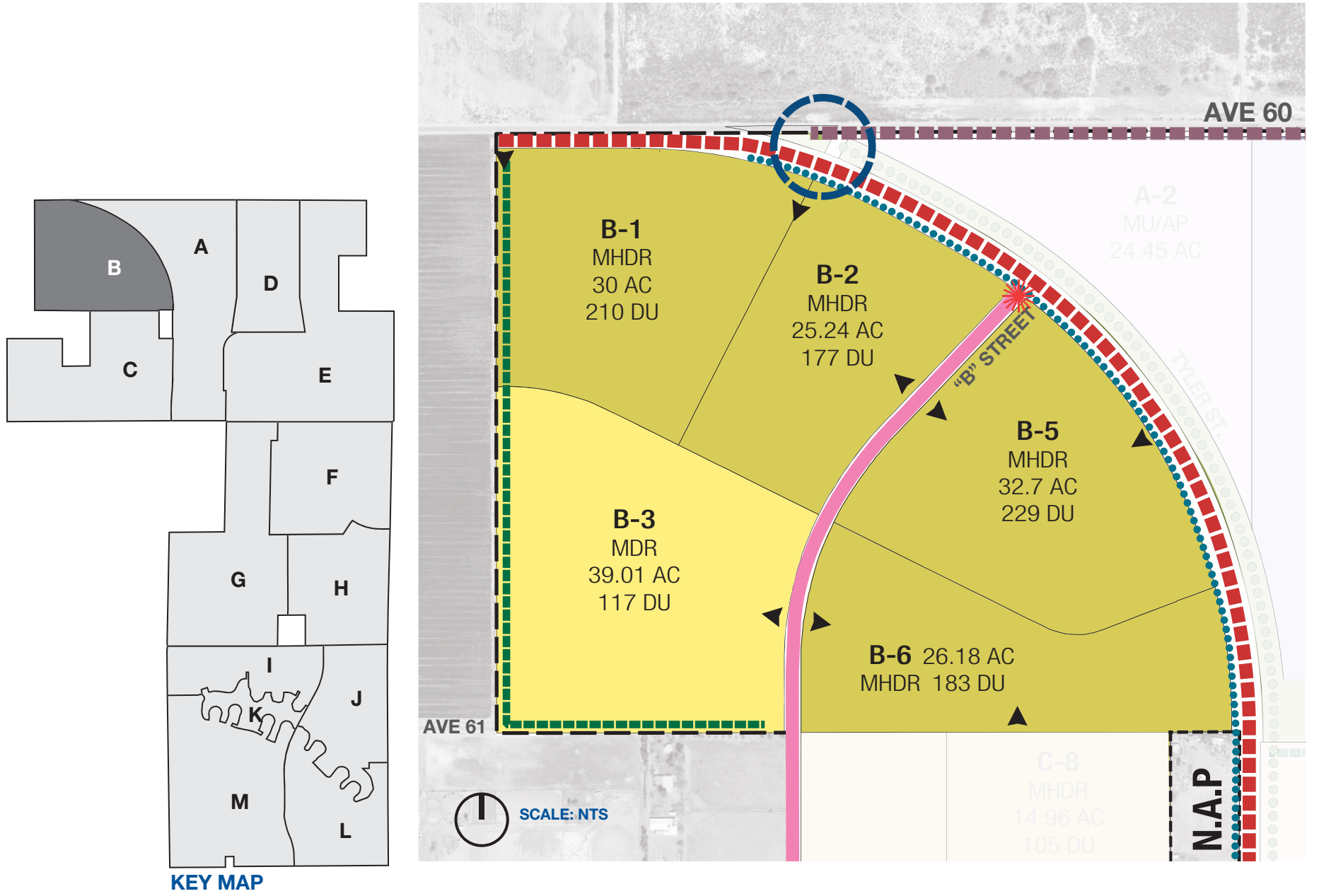
A-8: *Planning Area A-8 has been combined with adjacent planning area E-4 to provide a new Planning Area E-4 described below in Section 3.2.8.*

3.2.2 Neighborhood B

Neighborhood B contains five planning areas and is comprised of 153.13 acres as reflected in **Figure 3.2-3, Neighborhood B**. It is located in the northwestern portion of the project site, just south of "B" Street and Tyler Street. Proposed land uses are focused along Tyler Street, and include Medium Density Residential (39.01 acres) and Medium High Density Residential (114.12 acres). A target of 916 residential units is planned for Neighborhood B. These units are intended to provide housing for employees of the business, commercial and industrial areas in the northern portion of the project site. Primary access to the neighborhood is provided via Tyler Street and "B" Street. A major project entry is provided at the Avenue 60/Tyler Street intersection.

NEIGHBORHOOD B PLANNING STANDARDS

- (1) The entire neighborhood is located within Zone D of the CLUP for the airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1 – Project Wide Planning Standards of this Specific Plan as depicted in **Table 3.1-G, Basic Compatibility Criteria**.
- (2) Avenue 60 provides access to Planning Areas B-1.
- (3) Tyler Street provides access to Planning Areas B-2, B-5 and B-6.
- (4) "B" Street provides access to Planning Areas B-2, B-3, B-5 and B-6.
- (5) A Major Project Entry is located in the northwest corner of the neighborhood at Avenue 60/Tyler Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) A Special Intersection Detail is provided for the intersection of "B" Street and Tyler Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (7) Tyler Street will be developed in accordance with **Figure 3.4-8, Arterial Streetscape 1**.
- (8) "B" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.



Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
B-1	Medium High Density Residential (MHDR)	210	30	5.0–8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0–8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0–5.0
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0–8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0–8.0
TOTAL		916	153.13	

NEIGHBORHOOD B PLANNING AREAS**B-1: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area B-1 provides for the development of 30.00 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 210 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-1 is accessed from Tyler Street.
- (2) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (3) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-2: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area B-2 provides for the development of 25.24 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning). A target of 177 dwelling units is planned within a density range 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-2 is accessed from Tyler Street and “B” Street.
- (2) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (3) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-3: MEDIUM DENSITY RESIDENTIAL

Planning Area B-3 provides for the development of 39.01 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 117 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-3 is accessed from “B” Street.
- (2) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer.**
- (3) Please refer to Section 33.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-4: *Planning Area number has intentionally been skipped in the numbering sequence.*

B-5: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area B-5 provides for the development of 32.7 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development. A target of 229 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-5 is accessed from “B” Street and Tyler Street.
- (2) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (3) Please refer to Section 3.1 – Project-Wide Planning Standards.