

**Objective 3.** Provide access points with wayfinding at intersections and within communities wherever feasible.

**Policy 4. Facilitate a Supportive Trail System:** A map of the identified regional and local trails facilities (parking, water, restrooms, etc.) shall be produced at a minimum of every three years that identifies regional trails, local trails, and other existing facilities relevant to the trail system.

**Objective 1.** Generate an Existing and Desired Support Facilities Map that supports local and regional trails in coordination with municipalities, private developers, significant right-of-way owners, and others to identify gaps in service, access and other areas.

Updates to The Existing and Desired Support Facilities Map Trails may either coincide with General Plan updates or at the discretion of and necessity as indicated by County Staff or the Board of Supervisors.

**Objective 2.** The County may form a partnership to maintain a widely distributed map of existing trail amenities, updating the map on an as needed basis depending on the conditions and construction of new facilities.

**Policy 5. Require Trail Planning:** Regional Trails may be identified in the County's Comprehensive Plan as an asset to the community that improves circulation, health, safety, and recreation. Planning Areas and Municipalities are strongly encouraged to include regional trails as a component of their planning efforts, with the intent of tying development projects to the Regional Trail System. Areas are also encouraged to consider the long-term viability of connecting routes using sidewalks, bikeways or transit in limited instances.

**Objective i.** During each General Plan Update, Area Plan Update, and Municipality General Plan may reflect a strong, interconnected system of Regional Trails as a component of the circulation, health, safety, and recreation elements of the plan.

#### GOAL

### PROVIDE ACCESSIBLE TRAILS

The County shall create a trail system that is accessible to all users, whenever feasible, is enhanced for users with different abilities, and promotes a safe system.

#### Access and Accessibility Policies

**Policy 1. Access to Trails:** The County may consider constructing and promoting the use of additional trail access points near neighborhoods, downtowns, commercial districts, and other areas.

**Policy 2. Accessibility:** Riverside County could, where feasible, trails improvements or construction will consider all federal, state and other laws and guidance for the development of accessible trails and trail support facilities.

**Objective 1.** All new County maintained trails or reconstruction of trails shall consider the construction that provides access to users that utilize an electronic or other non-electric mobility assistance device, so long as the construction does not negatively impact the natural environment.

**Objective 2.** The County shall maintain all Class 1 Bikeways and Regional Trails in a manner that is accessible to all users, regardless of ability, to the extent feasible, based upon site conditions and budget. This includes adequate crossings, delineation of space, and other assistance that can enhance the experience for different users. The County shall also provide improvements to other trail classifications whenever possible to ensure continual improvements in the system for all users.

**Policy 3. Allowable Mobility Assistance Devices:** Electric personal assistive mobility device as defined by the California Statutes. 2007, Ch. 106, Sec. 1 shall be allowed to operate on trails so long as they are operated in a safe and courteous manner.

**Policy 4. County Liability:** Users should exercise reasonable caution and care while



operating such devices while on trails and facilities. The adoption of this policy does not represent an endorsement that the County's facilities and properties are safe for the use of mobility assistance devices.

**Policy 5. Informed and Empowered Trail**

**Users:** Riverside County may post information on the expected condition, difficulty, slope, length, and other relevant seasonal conditions of the trail at all county maintained trailheads and online to empower trail users with the information needed to understand if they will be able to safely use the trail.

**Objective 1.** The County, to the extent feasible, should resolve to maintain information on trail condition, difficulty, slope, length, and other relevant seasonal conditions at trailheads and online through maintenance cycles, or as funding becomes available.

**Policy 6. Prohibited Mobility Devices:** Internal combustion engines are not considered a mobility assistance device. Nor are devices where engine power (wattage) output contributes to unsafe speeds for a given trail's conditions, and will impact others reasonable enjoyment of the trail.

**Policy 7. Sensory Assistance Devices:**

Wherever possible and feasible, the County shall provide for a multitude of trails, access points, signs, and other assistance devices oriented towards providing people with auditory, visual, cognitive, physical, and other disabilities with the use of the trail.

**Objective 1.** The County will identify opportunities to provide outdoor experiences to areas where population may not have access to trails, such as those located near assisted living facilities or other areas.

**GOAL**

**STRATEGICALLY CLOSE TRAIL SYSTEM GAPS**

The County will be an advocate, leading coordinator and supporter for closing gaps

in the regional trail system. The County will facilitate public private partnerships to co-locate facilities and services in public and non-public right-of-way. It will also acquire and manage lands, and when appropriate, transfer lands to other entities for management purposes.

**Trail System Policies**

**Policy 1. Acquisition Policy:** The County will pursue the acquisition of lands for development of the countywide trail system and its support facilities. The County shall prioritize acquisition projects, identify appropriate strategies and approaches for acquisition, and utilize partnerships as necessary to acquire lands for development of the regional and local trail system.

**Objective 1.** Prioritize trail acquisition projects: The County Trails Technical Advisory Committee will develop acquisition criteria, ranking procedures and preferred acquisition characteristics for projects every fiscal year where funds are readily available to acquire land.

**Objective 2.** Acquisition needs: The County shall utilize various acquisition strategies that combine a long-term strategy of fee simple purchases, purchase of acquisition rights, with that of short-term acquisition strategies such as easements to ensure trail connectivity. This policy shall be used to:

- Create Regional Trail Corridors managed by the County;
- Reduce In-holdings and potentially hazardous adjacent properties;
- Increase access to natural areas access or create new trails;
- Trails under the management, leadership, guidance and promotion by external stakeholders; and
- Maintenance and administrative facilities.

**Objective 3.** Acquisition Strategies: The County may utilize a diverse set of strategies to acquire rights to develop lands or acquire development rights including, but not limited to:

- Fee Simple Purchases and Donations;
- Easements which allow for the construction of Trail and associated facilities through developer contributions or zoning and development regulations;
- Dual easements in collaboration with other agencies, such as the School Board, Public Utilities, and others;
- Right of first refusal purchases where lands are not currently for sale; and
- Transportation, railroads, utility, or other public and private linear rights-of-ways.

**Policy 2. County Liability:** Consideration shall be given to public safety and to protecting the County of Riverside, County Board of Supervisors, Regional Park and Open-Space District and its Board of Directors, agents and employees against claims for injury and/or property damage arising from/out of the use of trails.

**Policy 3. Co-location of Trails and Other Services:** Where desirable and practical, trails shall use public owned right-of-way such as flood control channels, levees, roadway corridors, and public utility corridors if these facilities provide for a high-quality user experience. Consideration should be given to areas with high vehicular traffic, landscaping and shade, ensuring scenic view sheds, and ecosystem protection shall be considered in utilizing public right-of-way.

**Policy 4. Landowner Liability Program:** The County may develop an adjacent Landowner Trail Liability Program that will approach landowners who may provide easements or other securities for the development of the trail system. This program will expand upon indemnity as provided in CA Gov't Code § 831.4. Other considerations may be considered as part of this program.

**Policy 5. Non-County Trail Management:** The County shall make resources available for the purchase of lands that will develop municipal trails that clearly contribute to a regional trail corridor. Where purchases

or funds are made, management of the constructed facility will be turned over to the municipal area receiving the trail. Coordination and additional resources for these land purchases may include that of the County's Councils of Governments, special districts and other prospective public agencies.

**Policy 6. Rail-to-Trail and Rail-with-Trail Considerations:** Special emphasis is placed on the preservation of abandoned railroad right-of-way for trail use and investigate the feasibility of collocating bicycle paths on unused portions of existing rights-of-way. Additional consideration should be given to the development of facilities along railroad right-of-way.

**Objective 1.** The County will maintain an inventory of potential Rail-to-Trail and Rail-with-Trail Projects that can be used for potential trail connectivity. This inventory should also include a list of relevant resources, both financial and technical that can be used to implement projects.

**Policy 7. Trail Acceptance:** Agencies with the capacity to manage trails will review proposed development projects for impacts to the Regional Trail System. If a project crosses or is next to a planned regional trail alignment, the County shall request that the project be conditioned to dedication of a regional trail easement as defined in the adopted County Trail Guidelines or Master Plan. Dedication of such easement may be accepted and conveyed to the County. When the easement is accepted, it will then be conveyed to the County and the County will be lead agency for development and maintenance of the recreation trail. Trails that are developed or funded as part of a Specific Plan may be accepted for maintenance into the County-maintained trail system, if they provide for linkage into the regional system, possess special conditions, available funding, and warrant early acceptance as determined by the General Manager. The County shall work with the Planning Department to ensure that the conditions are met and shall notify Risk Management when trails are accepted.

**GOAL****ADMINISTER THE TRAIL SYSTEM IN AN ECONOMICALLY SUSTAINABLE MANNER**

The management of the system shall be supported through the development of Riverside County, users in the system, and other means.

**Trail Administration Policies**

**Policy 1. Trail Assessment Fee:** It is recommended that the County implement an annual assessment in the support of trail maintenance and development. This fee is to be included as an assessment on annual property taxes. The amount of this assessment is subject to Board of Supervisors approval, and is recommended to be a minimum of \$25.00. The assessment may require a vote as a measure to be scheduled during the County's election cycle.

**Policy 2. Development to Support**

**Trails:** Fees collected under the authority of Ordinance 659, Development Mitigation, shall be distributed at the discretion of the Board of Supervisors as part of the annual County budget or through direct allocation authorized by the Board. No more than 10% of each trail project should be used for administrative expenses and/or project management, excepting projects of high complexity and those requiring the use of consultant services. Commercial and Industrial properties have previously been excluded from these fees. Based upon findings on trail use within the County, as well as best practices exercised by neighboring jurisdictions (see Appendices) It is recommended that these development types are subject to developer impact fees for trails.

**Policy 3. Trail Reinvestment:** Fees collected at County trail facilities shall be re-invested by the Board of Supervisors into the maintenance and management of Regional Trail facilities managed by the County.

**Policy 4. Use Fees:** The County shall consider usage fees for trails, trailheads, and associated amenities annually by the Riverside County Regional Park and Open-Space District Advisory Commission (DAC) in accordance with Policy J-5 and in conformance with Government Code 54001.

**Objective i.** The County shall investigate the use of development support for trails, fee waivers, trail reinvestment and use fees as an overall measure of developing the system. The County will create an annual report showing how these fees and programs support the overall County Trails Plan.

**Objective ii.** The County may review, on an annual basis, the need to develop and maintain an annual user pass for County maintained facilities. This will also include the provisions for rate increases to maintain pace with a developing trails system, changes in the local economy, and other measures. An assessment fee should be considered in these analyses.

**GOAL****ENFORCE TRAIL SAFETY, USE, AND JURISDICTION**

The County shall identify the appropriate area under which it is authorized to enforce uses, work with external agencies to ensure collaborative enforcement, and work within its jurisdiction to ensure a safe, high-quality user experience.

**Trail Safety and Enforcement Policies**

**Policy 1. Authorization:** Riverside County shall be responsible for the enforcement of recreation areas and trails, and other areas deemed necessary through agreements with partner agencies. The County may enforce speed, user type, openings and closures, and other restrictions as deemed necessary to provide for a sustainable, safe and comfortable trail system.

**Objective 1.** Riverside County shall produce an Enforcement Jurisdiction Map to illustrate the County's enforcement authority. This enforcement area will be the governing document related to the regulation, monitoring and use of trails and recreational areas under the jurisdiction of the County.

**Objective 2.** The County shall make a concerted effort to identify appropriate management of the areas outside of the County's enforcement area. The enforcement authorities within Riverside County shall meet on a twice-per-year basis to ensure the safety of Riverside's residents and visitors.

**Policy 2. Collaborative Enforcement:** The County shall forward concerns related to hazardous trail conditions and unsafe trail users to the appropriate managing entity where a substantial amount of complaints received are outside of the established enforcement. The County shall also take measures to provide information on the risks of users to the managing authority and take other measures, as deemed appropriate by county administration. The County may also provide information on how to mitigate enforcement need to managing entities, such as best management practices in reducing access to restricted areas, signage, and enforcement of trespassing provisions. These efforts can be accomplished through trainings, meetings with appropriate agencies, and other measures.

#### GOAL

### **SIMULTANEOUSLY DEVELOP LAND, TRANSPORTATION AND TRAIL IMPROVEMENTS**

Development in Riverside County will blend development and transportation impacts into the considerations and needs of the Countywide Trail System. The County will identify trail alignments to be incorporated into plans and to market potential return on investment to developers and commercial areas.

## **Development Policies**

**Policy 1. Blend Developers Plans with Trail Plans:** The identification of alignments and trail concepts shall be illustrated and promoted as an amenity and considered a requirement to connect Regional Trails as a component of new developments in the County.

**Objective 1.** The County may consider maintaining a map, updated in perpetuity of development proposals, and planned regional trails and municipal trails connecting with county facilities and approved through a formal planning process. The map will better guide improvements in the County and focus on where new trail development is imminent, with an emphasis on managing entities.

**Policy 2. Regional Trail Connectivity:** Development located on an identified Regional Trail on The Regional Trails Map shall be required to provide a trail, open to the public that provides seamless connectivity between areas adjacent to the development.

**Policy 3. Development Bonus Program:** The County shall consider implementing a program that provides development with density bonuses when trail facilities beyond those identified in County adopted Regional Trail Facilities maps are constructed as a component of new development or re-development. Facilities must be regional in nature, or connect with local trails identified in the County Trails Master Plan.

**Policy 4. Development Impact Fee Bank:** A program may be developed within the County that allows developers to place funds for mitigation of impacts specific to Regional Trails identified by the County into a fund for the development of trails outside of the planned project. The developer must have the option to enter into this program and not provide funding for a local trail system over a planned regional trail project.

**Policy 5. Easements and Mitigation:**

The County may consider receiving easements or funds in-lieu of easements from development projects, within the plan area, to acquire, plan, study, design, construct, or manage the trail system.

**Policy 6. Incorporation into Comprehensive Plan and Area Plans:**

Riverside County shall identify and ensure that trail connectivity is encouraged in the appropriate elements of the Comprehensive Plan. This includes references to regional trail plans, and alignments where feasible in the following elements, not excluding others:

- **Circulation Element** to consider co-location of trails with transportation facilities;
- **Multipurpose Open Space Element** to consider the internal and intra-connectivity potential of parks and natural areas using trails and greenways;
- **Noise Element** to evaluate the relationship of trails and reduction of noise pollution from reduced automotive traffic, addition of vegetative buffers through the use of trails, etc.;
- **Air Quality Element** to consider the improvements in air quality through the development of facilities that will remove vehicles from the transportation system; and
- **Healthy Communities Element;** The use of trails in communities as a tool to improve community health.

**Policy 7. Trails Master Plan:** The County shall work with Federal, State, adjacent County, and Riverside's Municipal governments, and with special districts to identify regional trail alignments throughout a Master Plan at the County Level. The County may provide technical assistance to external entities to when regional trails would be connected. The adopted plan may be incorporated into the Comprehensive General Plan of Riverside County and distributed through the County's Geographic Information System for planning purposes. The County may require an amendment process once a plan is adopted if

an area is inconsistent with the Comprehensive General Plan of Riverside County. The Plan shall emphasize Regional Planning at its core, with other smaller trails identified in municipalities, specific parks, and natural areas.

**Objective 1.** As a component of the Trails Master Plan, the County shall maintain a prioritized list of trail projects to be published and advertised for departments, external agencies and other groups.

**GOAL****DEVELOP HISTORICAL TRAIL ROUTES, THEMES AND RESOURCES**

Historical and cultural routes located within Riverside County which establish regional connectivity shall be identified and include trail facilities to improve the trail experience along the routes. The Identification and listing of these routes will provide for additional funding opportunities by external stakeholders, and opportunities for the County to increase tourism.

**History Policies**

**Policy 1. Identify Alignments:** The County shall identify relevant historical alignments of trails and cultural routes which promote regional connectivity, and highlight places of historical significance along the routes.

**Objective 1.** The County shall map relevant historical routes, existing trails in the nearby vicinity, and other relevant information every three years.

**Policy 2. Route Co-location:** Where historical trails are located and offer regional connectivity, the County, municipalities and other managing and implementing agencies shall work towards the development of the system along the corridor, and highlight historical sites as necessary.

**Policy 3. Pursue Unique Funding Opportunities:** Wherever feasible, designation of historical routes providing regional connectivity shall be promoted to encourage the application of funding sources to develop routes, interpretation opportunities and other trail enhancements.

#### GOAL

### ACTIVELY FUND TRAIL PROJECTS

The County shall identify a funding approach and strategies for the long term and short term investment in the trail system. This includes the evaluation of past policy changes and the potential creation of new funding sources towards the development of the trail system.

#### Funding Policies

**Policy 1. Funding Approach:** The County shall aggressively pursue and encourage partners to utilize diverse funding sources to develop County's component of the regional trail system. This includes utilizing public-private partnerships for the overall development of the system in a long and short term framework for funding projects.

**Policy 2. Funding Strategies:** With funding budgets inaccessible for trail projects, the County may utilize funds from various sources, including but not limited to:

- **Fees:** User fees, congestion fees, or other fees programs provide an opportunity to generate revenue to fund infrastructure projects, such as construction, programs, and other needs.
- **Grants:** Competitive grants through public agencies or through private or non-profit foundations can generate additional resources for projects and programs.
- **Fundraising campaigns:** Fundraising through neighborhood groups, advocacy groups, or even crowd-funding can help generate additional resources for specific trail rehabilitation, other projects, and trail-related programs.

#### Policy 3. Evaluate Past Policy Decisions:

The County shall evaluate the findings of the trail user and agency surveys conducted as part of this planning effort (see Appendices for full reports), and make recommendations on reinstating trail-related development fees on commercial and industrial land uses. The County will additionally study the impact of a twelve-dollar County Assessment Parcel Tax and its potential funding impacts on the trail system.

#### Policy 4. Highway System investment

**Requirement:** The County will advocate for a percentage of all new highway expansion projects to include development of trail systems. The County shall work with other partners to pursue a 1% to 5% funding allocation toward regional trails with all highway construction projects.

#### Policy 5. Roadway System Facility

**Connectivity:** All new roadway projects in Riverside County shall consider the potential to connect on-street bikeways and pedestrian facilities with trails, or construct an appropriate mix of facilities to accommodate expected trail user types as a component of the roadway.

#### Policy 6. Create Public-Private

**Partnerships:** The County shall consider the use of Public-Private Partnerships to entice various agencies, organizations, and enterprises to managed, develop and maintain the trail system. Partnerships should especially be considered between partners aligned with the mission of the County, such as health providers, schools, businesses, and other private ventures.

#### Policy 7. Endowments and Other Funds:

The County may consider the implementation of a program that receives endowments, private donations, and other private funding sources geared towards the management and maintenance of County trails and trail amenities.

**GOAL****COORDINATE THE MAINTENANCE AND MANAGEMENT OF THE COUNTYWIDE SYSTEM IN A COLLABORATIVE AND CONSISTENT MANNER**

The County shall coordinate management strategies between agencies and other potential partners to ensure an efficiently managed, countywide trail system. The County shall encourage structures of management that enable the County to more efficiently manage the workload in the trail system.

**Trail Coordination Policies****Policy 1. Diversified Management:**

The Riverside County Parks and Open Space District will continue to develop and manage regional trails which fall within District parks. Where regional trails are implemented outside of parks but within County jurisdiction, the District shall defer management of the trail to the following agencies, based upon trail conditions:

- Along utility and/or drainage rights-of-way: Riverside County Flood Control District;
- Adjacent to or following roadway; corridors: Riverside County Department of Transportation;
- In either of the above scenarios or in other situations: Riverside County Transportation Commission.

The District shall coordinate with and communicate the planned intent of regional trails to the above entities, and seek funding and additional planning support from WRCOG, SCAG, and CVAG. Additional partnership opportunities shall be explored prior to the development of any new trail segment, following the guidance of the Trail Partners section of this plan.

**Policy 2. Collaboration Strategy:** The County will actively seek out partnerships

neighboring trail managing entities to ensure consistent trail management and maintenance, reduce user conflict, and transitions between types of trails and other non-trail facilities. The County will also utilize these relationships to leverage funding for projects.

**Policy 3. Conserve Resources:** The County will strive to practice resource conservation through its trail system, future development, and programs established for the trail system.

**Policy 4. Habitat Conservation Plans:**

The County shall evaluate each trail plan where the intent is to cross specific areas of environmental protection. Buffers will be considered for the protection of listed species to ensure overall sustainability of the system, while meeting the demands of trail users in Riverside County.

**Policy 5. Identify Benefits and**

**Opportunities:** The County will make efforts to highlight opportunities to connect developments into the Trail System. This will be performed by meeting with developers and discussing opportunities in collaboration with other trail managing stakeholders. The County will consider all types of trails in related discussions.

**Objective 1.** Produce a benefits report: Whenever feasible and at a reasonable timeframe between documentation, the County shall work with volunteers, area stakeholders and the appropriate entities to produce a benefits report of the jurisdiction's trail system. This report will emphasize the health, social, cultural, economic, transportation and other quality of life impacts the systems development has contributed to the County's residents and visitors.

**Policy 6. Proactive Communication:** The County shall undertake a proactive strategy to update stakeholders on the development of the trail system by providing an annual update on capital improvements, and user needs and trends, and other issues.



**Policy 7. Promote Stewardship:** Trail management will make every effort to sustain the system and the surrounding environment through sound management practices and community involvement. Stewardship workshops shall be offered by the County and other with the appropriate authority to speak to trail operations, maintenance and other procedures.

**Policy 8. Risk Management:** The County shall create, in cooperation with trail maintenance and planning, a trail condition classification system based on their location and anticipated use. The standard for repair and level of maintenance will be associated with each individual trail type, dependent upon its terrain, use and location, and thresholds for closure will be established. Reasonable effort will be made to eliminate trail hazards in a timely manner. Signs and notices may be posted if hazards are severe and, if necessary, trail closure may be required until corrective measures can be completed.

**Policy 9. Uniform Procedures:** The County shall produce a management practices and protocol manual to promote consistent management and maintenance practices between facilities.

**Policy 10. Unit Management Plans:** The County will develop a Unit Management Plan for tracts of land or trails that suffer from excessive or over-capacity uses within the system, management needs based off trail related issues, user group conflict and other management related concerns.

#### GOAL

### DELIVER AN ACCESSIBLE SYSTEM OF TRAILS

Riverside County will work toward bringing a unique visitor experience to every trail user by providing safe, scenic, and high-quality outdoor opportunities. Riverside shall identify the appropriate experience for each trail corridor context and desired outcomes. The County shall produce rules

and expectations for users to provide for a predictable trail that accommodates as much experience as possible for a diversity of uses.

#### Trail User Policies

**Policy 1. Trail System Uses:** The County shall identify the appropriate locations for multiple modes of trail users, including people walking and running, bicycling, horseback riding, and other users identified in strategic planning efforts. Where feasible, trail uses shall be conceptualized as individual experiences within the same right-of-way. Where limited right-of-way does not allow a separation of trail uses, buffers, appropriate signage, and other measures shall be taken to enhance the overall trail experience.

**Policy 2. Prohibited Uses:** When necessary, the County may deem necessary to permanently or temporarily close a trail to particular uses or all uses along the trail. In this instance, the County shall publish information on the reason for the closure, expected re-opening of the trail and desired improvements or conditions the trail will need to be maintained to ensure the appropriate uses within an area.

**Objective 1.** The County will identify the appropriate user type for each trail and post restrictions as necessary, in the appropriate locations.

**Policy 3. Equitability Analysis:** The County may consider conducting an equity analysis that identifies areas of consideration for future investments. The analysis shall look at areas within the County that do not have access to trails, or are unable to use trails due to extenuating systemic circumstances.

**Objective 1.** Create an Equity Analysis Map in coordination with municipalities, private developers, significant right-of-way owners, and others to identify areas of opportunity local trails that support or connect to the regional trail system.



**Objective 2.** Update the Equity Analysis Map every three years, or as deemed necessary by County Staff or Board of Supervisors.

**Objective 3.** Generate additional policies.

**Policy 4. Innovative Technologies and Uses:**

The County may consider the use of low-speed electric vehicles, autonomous low-speed electric vehicles and other uses on County-maintained trails if the appropriate right-of-way is located within the corridor, uses will be appropriately separated, and the system is maintained in a safe manner.

**Objective 1.** The County is strongly encouraged to review the design speed of facilities and post appropriate speeds for facilities to create a safe, comfortable trail experience for all users.

**Policy 5. Trail System Rules:** The County may produce a set of rules consistent with the design guidelines, adopted plans, and other documents to protect and maintain natural and human environments while encouraging a courteous, safe and consistent trail system.

**GOAL**

**PROMOTE THE SAFE USE OF ELECTRIC BICYCLES ON TRAILS**

The County shall allow for certain electric bicycles on trails, providing they are consistent with state rules and the desired user experience on each individual trail.

**Electric Bicycle Policies**

**1. Consistency with California Law:**

The County shall abide by the State of California's Electric Bicycle Laws and Regulations (Assembly Bill No. 1096, amending relevant statutes and sections). The State has adopted the following classification system for bikeway access by classification of facility levels 1, 2, 3, and 4.

**2. Off-roadway Trail Restrictions:**

The County may enforce speed restrictions for electric bicycles on facilities not listed

**CALIFORNIA ELECTRIC BICYCLE POLICY**



VEHICLE TYPE	VEHICLE		USER				BIKEWAY ACCESS			
	PEDAL OPERATED	MAXIMUM MOTOR-ASSISTED SPEED (MPH)	MINIMUM AGE (YEARS)	DRIVER'S LICENSE	LICENSE PLATE	HELMET	CLASS I BIKE PATH	CLASS II BIKE LANE	CLASS III BIKE ROUTE	CLASS IV PROTECTED LANE
BICYCLE	YES	N/A	N/A	NO	NO	17 AND UNDER	YES	YES	YES	YES
TYPE 1 E-BIKE*	YES	20	N/A	NO	NO	17 AND UNDER	YES	YES	YES	YES
TYPE 2 E-BIKE*	NO	20	N/A	NO	NO	17 AND UNDER	YES	YES	YES	YES
TYPE 3 E-BIKE*	YES	28	16	NO	NO	YES	NO	YES	YES	YES
MOPED	NO	N/A	16	YES	YES	YES	NO	YES	YES	NO

\*PENDING AB-1096





in state law such as natural surface paths with equestrian and mountain bicycling trails to improve the overall trail experience if deemed necessary by the County.

**3. Trail Use Restrictions:** All trails allowing electric bicycles must be identified, approved, and signed for their use.

#### GOAL

### LEVERAGE VOLUNTEER GROUPS AND COMMUNITY SUPPORT

**1. Adopt-a-Trail Program:** The County may continue its Adopt-a-Trail Program, to support the County in the maintenance, management and ongoing operations of the existing trail system. The County will also consider the expansion of the program to include sponsorships from organizations that choose to enter into agreements as a welcomed component in the County's mission to manage a high-quality trail system. Funds collected from this program shall be exclusively available to the County for

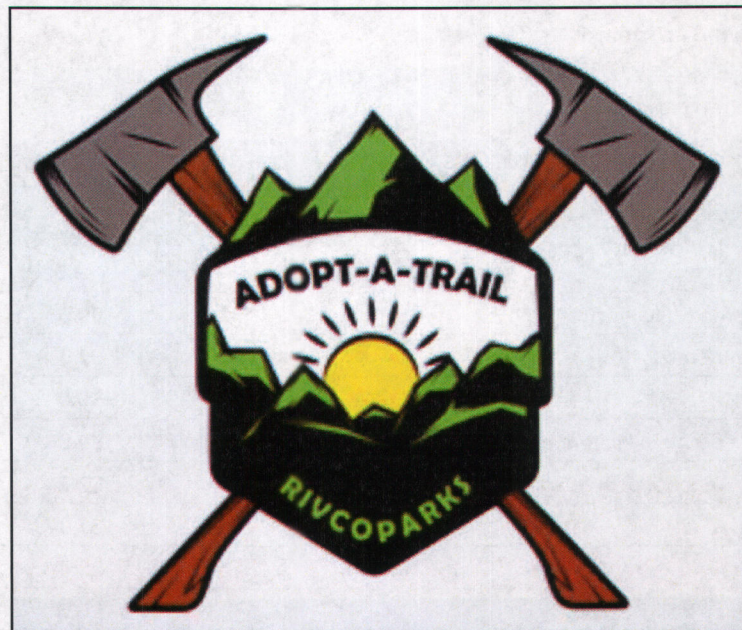
programming, maintenance, management and other activities directly related to the trail.

#### **2. Community Support Organizations:**

The County may investigate the implementation of partnerships with individual not-for-profit organizations that will serve as a voluntary organization as an extension of the County. These organizations shall receive training, offer indemnity to volunteers within the system meeting proper qualifications and certifications to perform functions on behalf of staff.

**3. Trail Ranger Program:** The County may consider the use of Trail Rangers as a means to encourage trail etiquette, work with law enforcement to improve safety conditions, and ensure areas where persistent issues arise are addressed in an efficient manner.

**4. Youth Engagement:** The County may enter into agreements with partners in the area to provide area youth, especially youth in areas with deficient access to the outdoors with programs to expose children to the County's natural areas and trails.





## The Backbone Trail Network

### COUNTYWIDE TRAIL PLANNING

The current planning effort undertakes primarily a reductive process, refining previous trail planning efforts in the service of creating a regional trail network that can be prioritized and is feasible for implementation by the County. In order to preserve previous planning work while giving a path forward, trails have been classified into three tiers.

**Tier 1** trails form the backbone trail network and represent the highest priority and greatest connectivity for the County. It incorporates historical alignments, regional trails with dedicated plans, those currently existing or under construction, and those with long-distance connectivity. Tier 1 trails proposed in this document do not enter MSHCP areas. These trails are intended to be major thoroughfares and are not appropriate for sensitive ecological areas. Where possible, backbone trails provide connections to trails within MSHCP areas.

Additional criteria used to evaluate backbone trails include:

- Population adjacency;
- Connection to destinations;
- Connection to other jurisdictions;
- Available right-of-way;
- Land ownership;
- Ability of the trail to close gaps in the regional network.
- Historic/cultural significance

The tier 1 backbone trail network is depicted in the map on the opposite page.

**Tier 2** trails are those which the County has previously identified as regional but do not provide regional connectivity. Many of these trails are destinations unto themselves, but often form networks internal to parks, without providing external connectivity.

**Tier 3** trails are local and community trails, which generally either fall outside County jurisdiction or are comprised of trail networks with only local connectivity. These trails are important as connections to local destinations, and often must be relied upon for connection between regional trails.

### THE UPDATED BACKBONE TRAIL NETWORK

The current planning effort undertakes a reductive process, emphasizing trails previously classified as regional and/or historic, those existing as part of a previous plan or currently on-the-ground, those with long-distance connectivity, and those providing connections to smaller-scale trail networks (See Table 4-1). Additional criteria used to evaluate backbone trails include:

- Population adjacency;
- Connection to destinations;
- Connection to other jurisdictions;
- Available right-of-way;
- Adjacent and underlying land-owners;
- Ability of the trail to close gaps in the regional network.

The following pages detail each of these backbone trails.

Table 4-1.

Backbone Trail Mileage	
Backbone	Miles
Bradshaw Trail	129.5
California Riding & Hiking Trail (CRHT)	89.0
Colorado River Trail	37.5
CV Link	50.0
Juan Bautista de Anza Historical Trail	84.9
Pacific Crest National Scenic Trail (PCT)	82.2
Salt Creek Trail	37.8
Salton Sea Trail	32.4
Santa Ana River Trail	25.7
Southern Emigrant Trail/Butterfield Overland Trail	66.8
Total	635.8

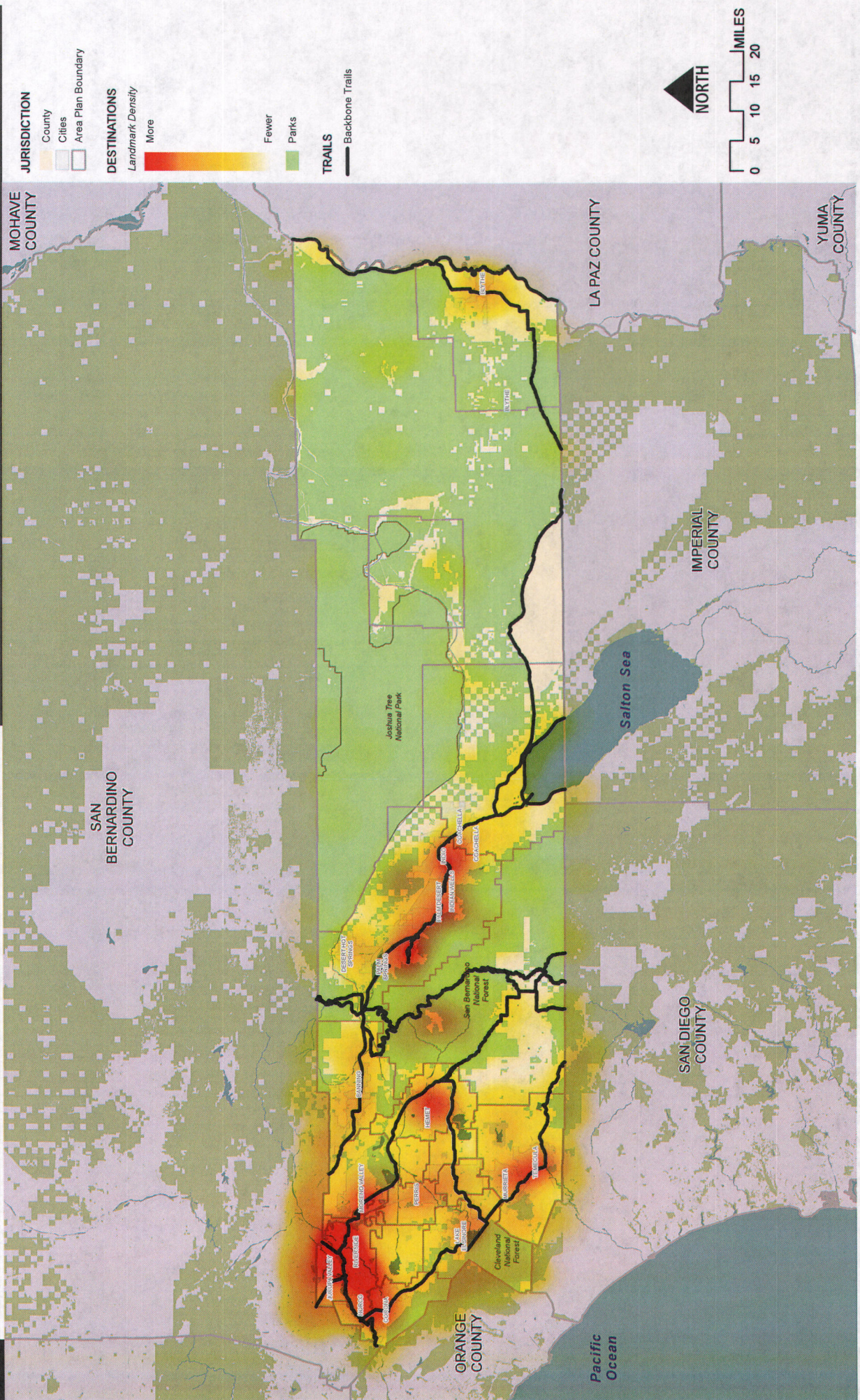






# COUNTY DESTINATIONS

# RIVERSIDE COUNTY TRAILS MASTER PLAN

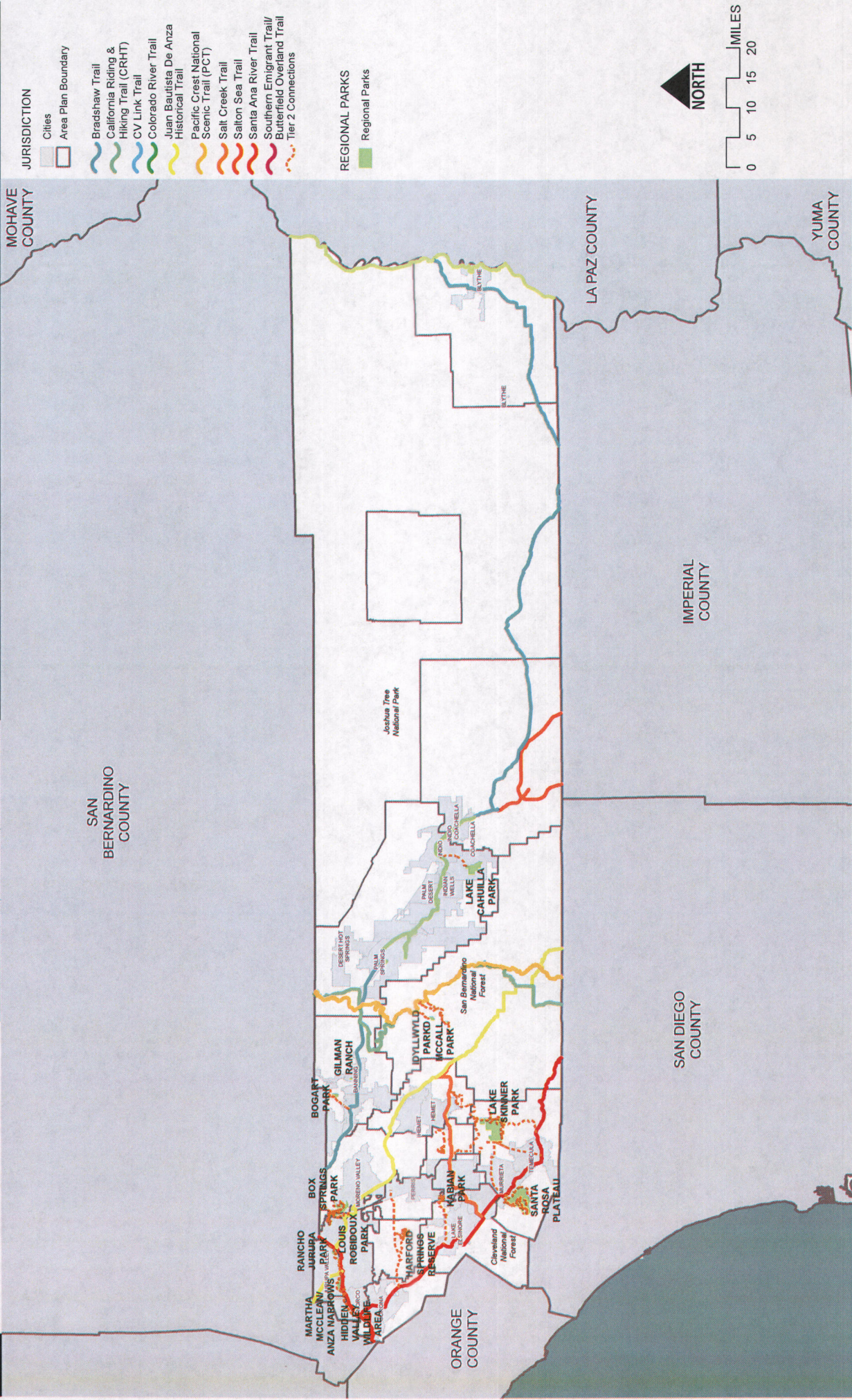


\* Map is conceptual in nature. Cities or destinations are points of reference only, the map is not intended to be complete.

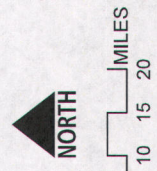


# BACKBONE NETWORK & REGIONAL PARKS

# RIVERSIDE COUNTY TRAILS MASTER PLAN



- JURISDICTION**
- Cities
  - Area Plan Boundary
  - Bradshaw Trail
  - California Riding & Hiking Trail (CRHT)
  - CV Link Trail
  - Colorado River Trail
  - Juan Bautista De Anza Historical Trail
  - Pacific Crest National Scenic Trail (PCT)
  - Salt Creek Trail
  - Salton Sea Trail
  - Santa Ana River Trail
  - Southern Emigrant Trail/Butterfield Overland Trail
  - Tier 2 Connections
- REGIONAL PARKS**
- Regional Parks



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## Backbone Network and Regional Park Connectivity

The backbone trail network is intended to serve both transportation and recreation purposes, and must strike a balance between these two goals. Wherever feasible, backbone trails connect directly to regional parks. As large regional parks tend to be in somewhat remote, undeveloped areas, connecting to them does not always provide for desirable regional connectivity (see Table 4-2). In these cases, Tier 2 trails are identified which make

connections from backbone trails to regional parks. A listing of regional parks that feature trails is below. The table includes the primary backbone trail serving the park, how direct access to the park is achieved if the backbone does not directly connect, and how far the park is from the backbone alignment.

*Bogart Park has over 400 acres of open space in the foothills below Mt. San Geronio.*



*McCall Memorial Park near Idyllwild has many trails and open space areas with equestrian specific campgrounds.*

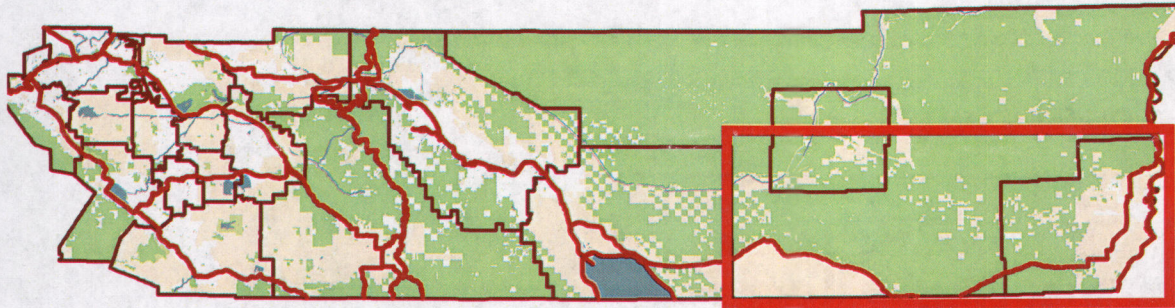




Table 4-2.

Regional Parks and Backbone Connections				
PARK_NAME	Acres	Primary Backbone(s)	Access to Backbone Via	Miles from Backbone
BOGART PARK	387.0	Bradshaw Trail	Noble Creek: partially existing Class 1 Bike Path in City of Beaumont	1.4 Miles
BOX SPRINGS PARK	3320.0	Juan Bautista de Anza	From de Anza: Morton Rd/Gemert Rd/Poarch Rd/Rail ROW	From de Anza: 4.2 Miles to center of park
GILMAN HISTORIC RANCH AND WAGON MUSEUM	170.2	Bradshaw Trail	N 16th St (on-street connection)	0.5 Miles
HARFORD SPRINGS RESERVE	528.6	Butterfield/Southern Emigrant Trail	Cajalco Rd (WRCOG Project)/Gavilan Rd	11 Miles
HIDDEN VALLEY WILDLIFE AREA	1510.1	Santa Ana River Trail	Direct Access	0 Miles
HURKEY CREEK PARK	120.3	PCT/CRHT	San Bernardino National Forest Trails	6.6 Miles
IDYLLWILD PARK	183.6	PCT/CRHT	San Bernardino National Forest Trails	3.6 Miles
KABIAN PARK	640.4	Butterfield/Southern Emigrant Trail	Riverside St/Mauricio Ave/San Jacinto River	9.1 Miles
LAKE CAHUILLA PARK	1887.9	CV Link	Proposed Class 1 Bike Path along drainage channel through City of La Quinta	6.1 Miles
LAKE SKINNER PARK	6817.5	Butterfield/Southern Emigrant Trail and Salt Creek Trail	Multiple routes, proposed by WRCOG and County Planning	Approximately 8 Miles (multiple routes)
LOUIS ROBIDOUX PARK	63.8	Santa Ana River Trail	Proposed Class 1 along north bank. Within City of Jurupa.	2.5 Miles from south bank SART backbone
MARTHA MCLEAN/ ANZA NARROWS	296.8	Santa Ana River Trail	Direct Access	0 Miles
MCCALL PARK	88.3	PCT/CRHT	San Bernardino National Forest Trails	10.4 Miles
RANCHO JURUPA PARK	350.5	Santa Ana River Trail	Proposed Class 1 along north bank. Within City of Jurupa.	2.3 Miles
SANTA ROSA PLATEAU	6929.5	Butterfield/Southern Emigrant Trail	Murrieta Creek Trail	1.9 Miles





### THE BRADSHAW (ROAD, TRAIL, ROUTE)

**Length:** 129.5 miles

**Area Plans:** Palo Verde Valley, East County – Desert Area, Eastern Coachella Valley, Western Coachella Valley, The Pass, Reche Canyon/Badlands

**Jurisdictions:** Riverside County, BLM, Cities of Indian Wells, Banning, Indio, Palm Springs, Palm Desert, and Coachella

**Destinations:** Salton Sea, Coachella Valley, Banning, Blythe

**Nearby Significant Trails:** Vista Santa Rosa, Morongo Wash, Bogart County Regional Park

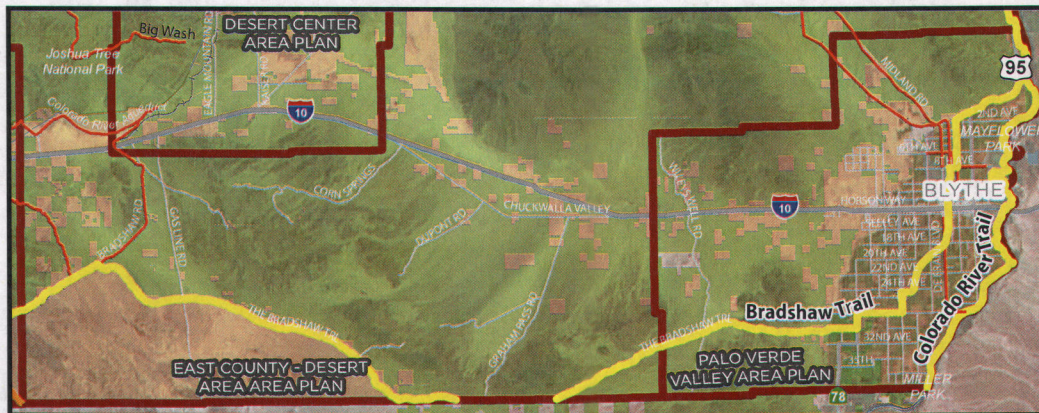
**Percent Existing:** 50%

**Management Status:** Partially maintained

The Bradshaw would follow an historic trail extending from southeast to northwest Riverside County. There is an existing trail functional for over 100 miles, in mostly in the Eastern portion of the County, from Blythe to the Coachella Canal, near the Salton Sea. Heading west from the Coachella Canal there is no existing functional trail. The trail would need to develop a connection to the CV Link trail as it heads north towards Palm Springs.

Heading east from the Coachella Canal to the trail’s proposed terminus in Palo Verde Valley, the trail would use a well-defined dirt road for nearly 70 miles. This well maintained road is periodically graded by the Riverside County Transportation Department. Due to soft sand portions of the road it is primarily for

The Bradshaw Inset 1

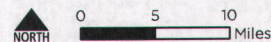


**Trail Network**

- Tier 1 (Backbone)
- Tier 2
- Tier 3

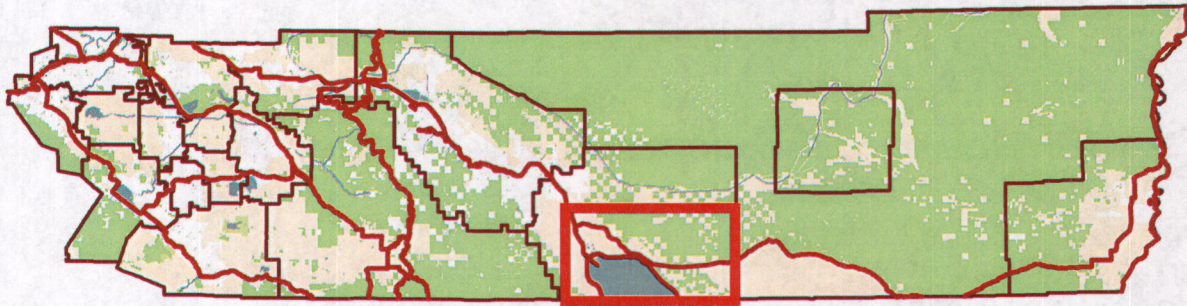
**Boundaries**

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary





## THE BRADSHAW (ROAD, TRAIL, ROUTE) (CONT'D)



four-wheel drive or off-highway vehicles. It is quite isolated, with no nearby populated areas, traveling through mostly public land half of which is on county land (right at the edge of BLM land), and the other half on BLM land. spectacular views of the Chuckwalla Bench and the Orocopa, Chuckwalla and Mule Mountains.

Note that within the Palo Verde Valley Area Plan boundary, the existing trail diverges

from the mapped regional trail (labeled as Bradshaw). At this point the regional trail angles northeast and connects to Blythe. In actuality, the existing Bradshaw runs nearly straight east and is not continuous into Blythe.

West of the Coachella Canal there is a 23-mile gap—all on county land—as the proposed trail passes north of the Salton Sea (inset 2). From there it would connect to an

The Bradshaw Inset 2

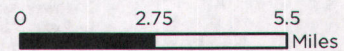


### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

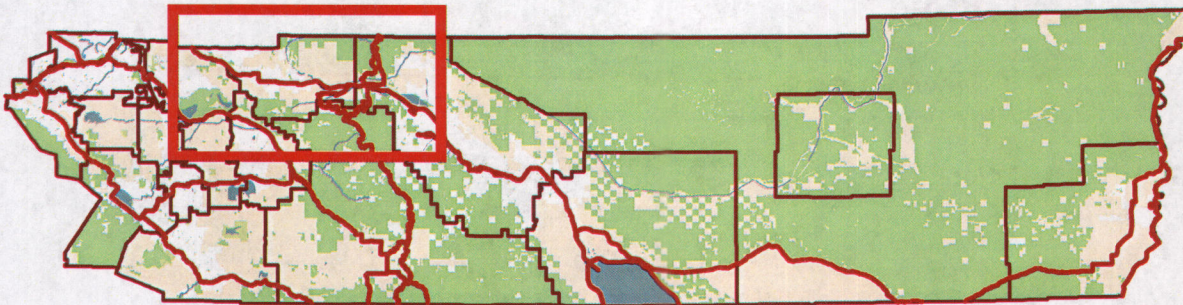
### Boundaries

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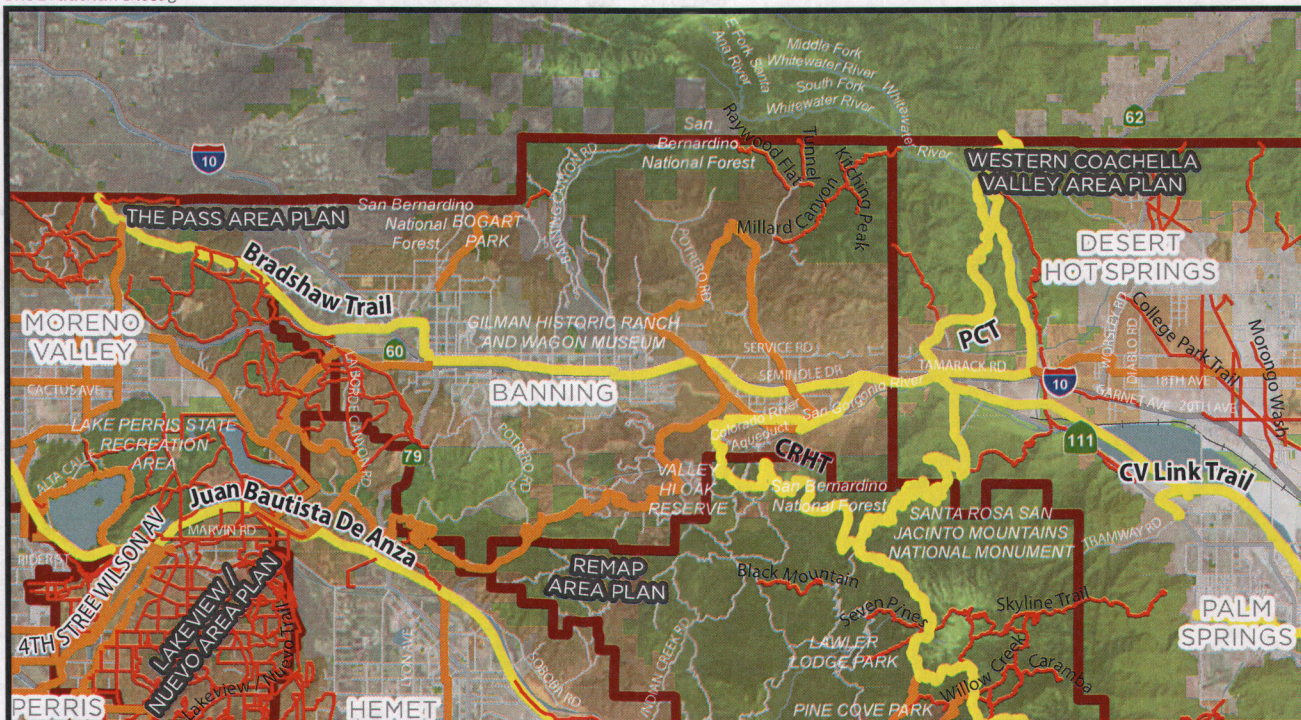
### THE BRADSHAW (ROAD, TRAIL, ROUTE) (CONT'D)



existing, though unmaintained, trail along the Whitewater River. After approximately 4 miles along the Whitewater River, the proposed CV Link Trail begins. CV Link is within various city jurisdictions, and represents the most feasible route. As such, it will constitute 60 miles of the Bradshaw alignment.

At the northwest terminus of CV Link, the proposed Bradshaw continues. At this stage the proposed trail traverses County Jurisdiction, as well as the City of Banning. The proposed route does not align with any unmaintained trails and no plans exist to define a feasible alignment.

The Bradshaw Inset 3

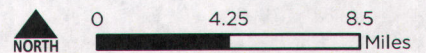


**Trail Network**

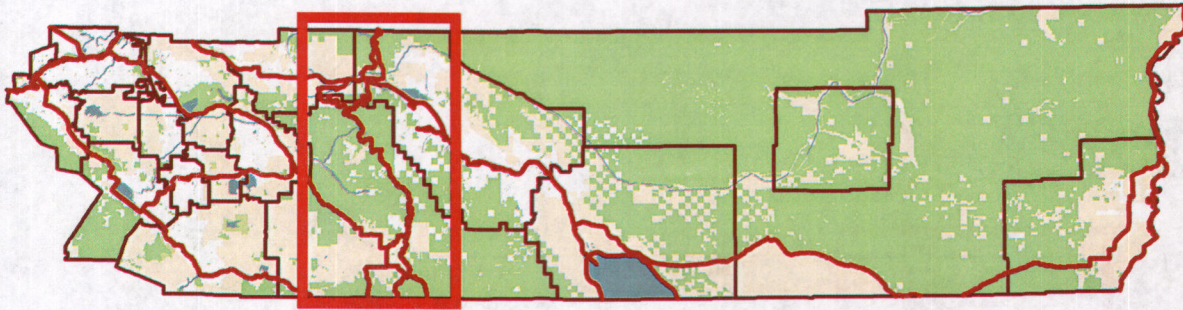
- Tier 1 (Backbone)
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**Boundaries**

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### CALIFORNIA RIDING & HIKING TRAIL (CRHT)

**Length:** 89.0 miles

**Area Plans:** The Pass, REMAP, Western Coachella Valley

**Jurisdictions:** Riverside County, BLM, Cities (Palm Springs), National Forest, State Park

**Destinations:** San Bernardino National Forest, Anza-Borrego Desert State Park, San Jacinto Mountains

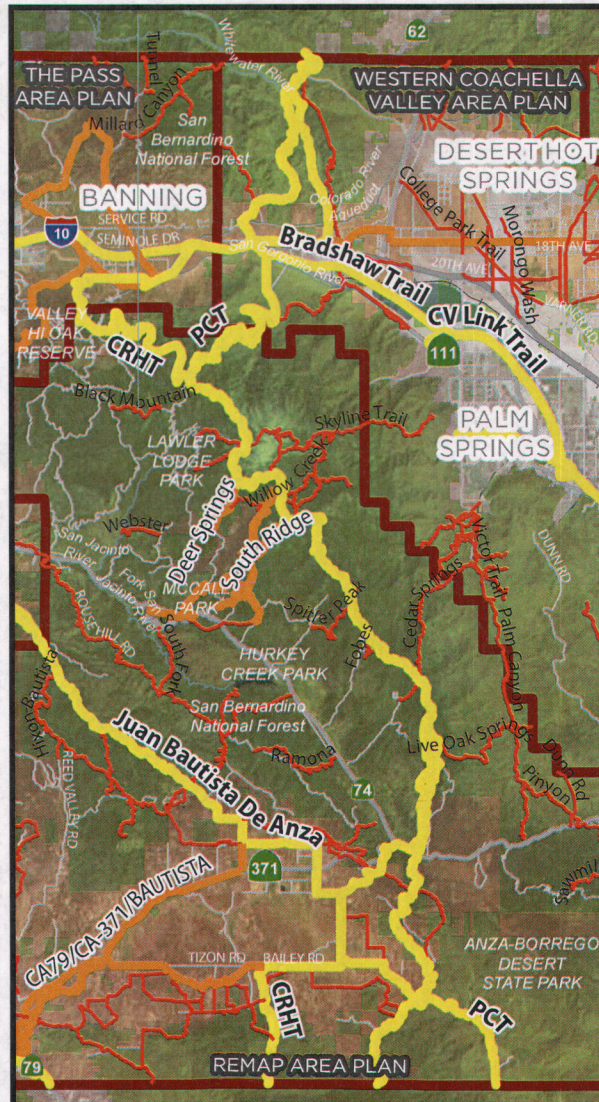
**Nearby Significant Trails:** Southern Emigrant Trail, Juan Bautista de Anza Historical Trail, Pacific Crest National Trail, Bradshaw Trail

**Percent Existing:** 90%

**Management Status:** Unmaintained, state-planned. Some easements in place.

The Proposed California Riding & Hiking Trail (CRHT) would extend nearly 90 miles along dirt roads and backcountry trails. The CRHT alignment shown here is a combination of state and county sources. It primarily follows easements and alignments recorded by California State Parks. Where gaps exist, alignments previously recorded by Riverside County Planning are used. In many areas it runs parallel and near the Pacific Crest Trail. These trails are not interchangeable, however, as the PCT does not allow the use of bicycles, while the CRHT does.

California State Parks has expressed interest in divesting its easements within Riverside County. This provides the Riverside County Parks and Open Space District with an opportunity to help fill in missing pieces of this statewide trail. As much of the alignment falls onto National Forest land, it also presents an opportunity to form partnerships in the development of the trail. At the northern and southern ends of the CRHT in Riverside County, opportunities exist to connect the trail to other backbone and local trails.

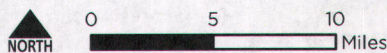


**Trail Network**

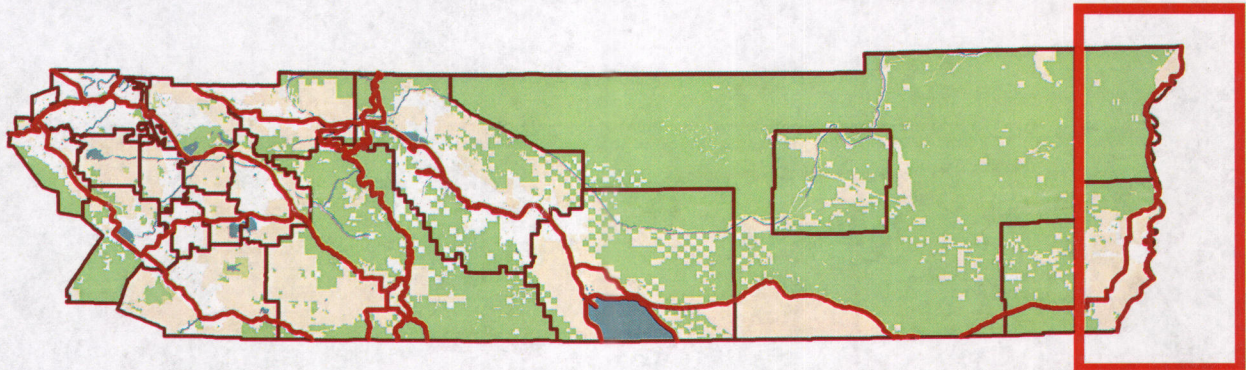
- Tier 1 (Backbone)
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**Boundaries**

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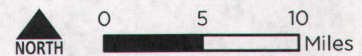
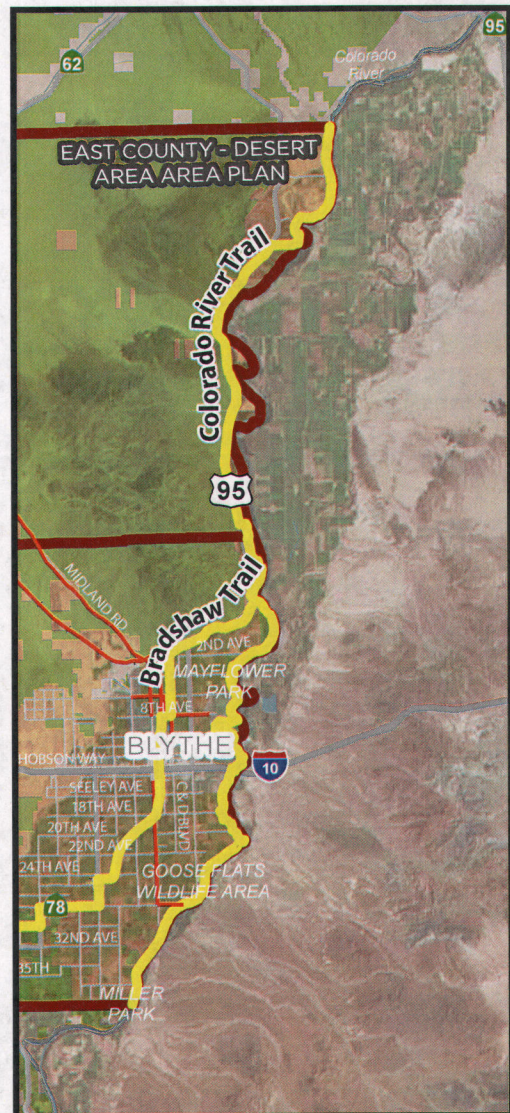
### COLORADO RIVER TRAIL

- Length:** 37.5 miles
- Area Plans:** Palo Verde Valley, East County – Desert Area
- Jurisdictions:** Riverside County, BLM
- Destinations:** Colorado River, Blythe
- Nearby Significant Trails:** N/A
- Percent Existing:** 15%
- Management Status:** Only dirt roads along the alignment are maintained.

The proposed Colorado River Trail would extend from Imperial County to San Bernardino County. For most of the route, the trail would be within unincorporated County jurisdiction.

Within Blythe, the proposed trail would follow a dirt road adjacent to the Colorado River. Mayflower County Park, with extensive camping, recreation, and picnic opportunities, abuts the river, and has great potential to serve as a regional trailhead. From there, the trail would follow available right-of-way alongside the river and canal. Satellite imagery indicates the possibility of using Rancho Not So Grande Road and other dirt roads such as Cotton Tail Lane, for a potentially continuous trail to Aha Quin.

North of Blythe, the trail passes through a series of agricultural areas with minimal populations before reaching the San Bernardino County line. At the Lost Lake Resort there is a greater opportunity for a path within the public right-of-way.



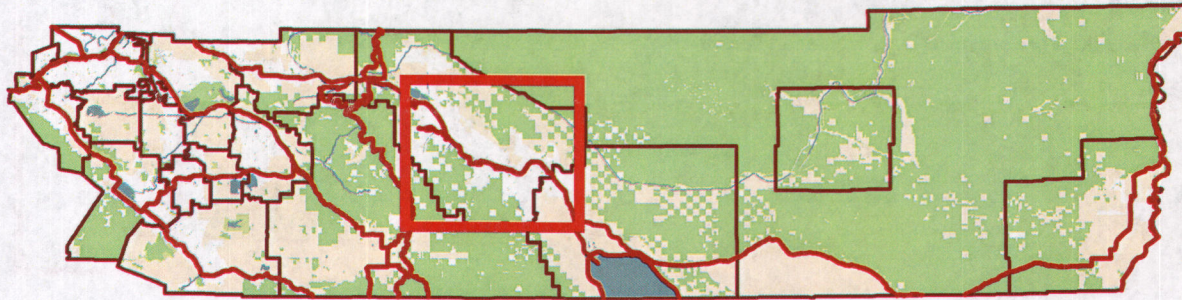
#### Trail Network

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- Tier 2
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#### Boundaries

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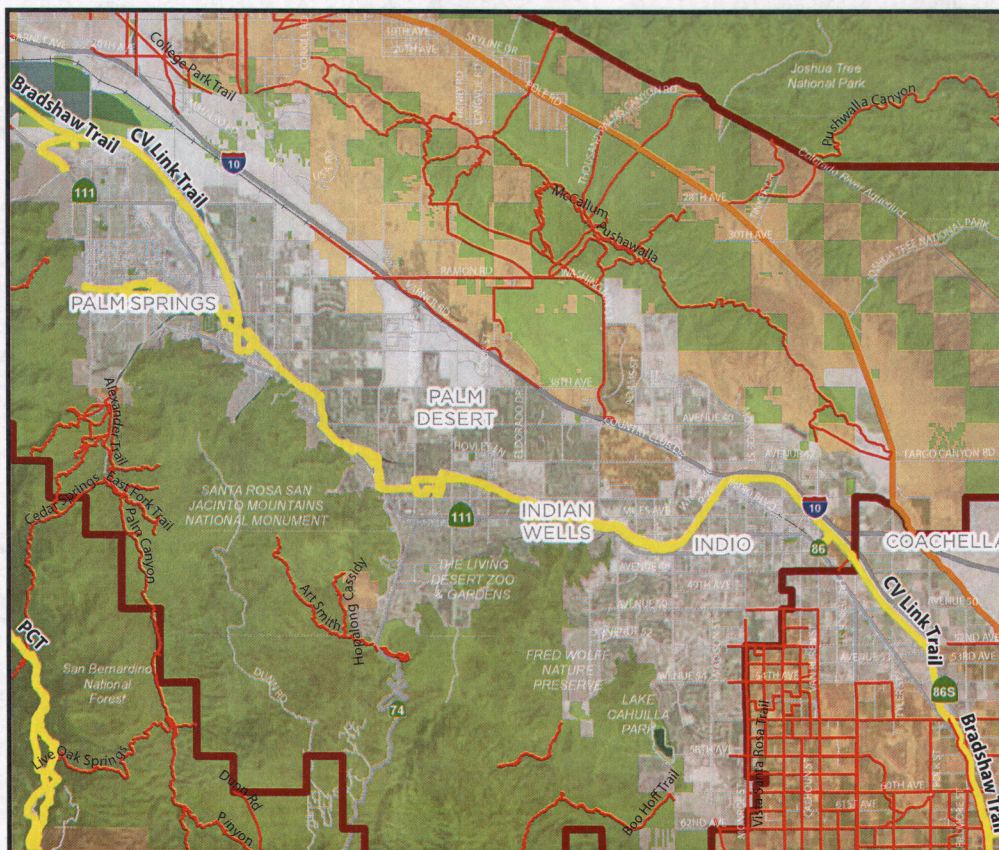


### CV LINK

- Length:** 50 Miles
- Area Plans:** Eastern Coachella Valley, Western Coachella Valley
- Jurisdictions:** Riverside County, Cities of Indian Wells, Indio, Palm Springs, Palm Desert, and Coachella
- Destinations:** Coachella Valley
- Nearby Significant Trails:** Vista Santa Rosa, Morongo Wash
- Percent Existing:** 0% (portions under construction)
- Management Status:** The entire corridor is planned, with a phased implementation plan.

The proposed CV Link will be a 50 mile long Class I (paved) path. It will operate primarily within urban areas, connecting to destinations in Palm Desert, Indio, and Palm Springs. Much of the route will follow a dry creek bed, and will, once constructed, serve a wide range of users, including hikers, bicyclists, equestrians, and neighborhood electric vehicles (NEVs).

See the project fact sheet on the following page for more information.

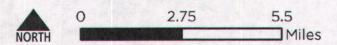


#### Trail Network

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#### Boundaries

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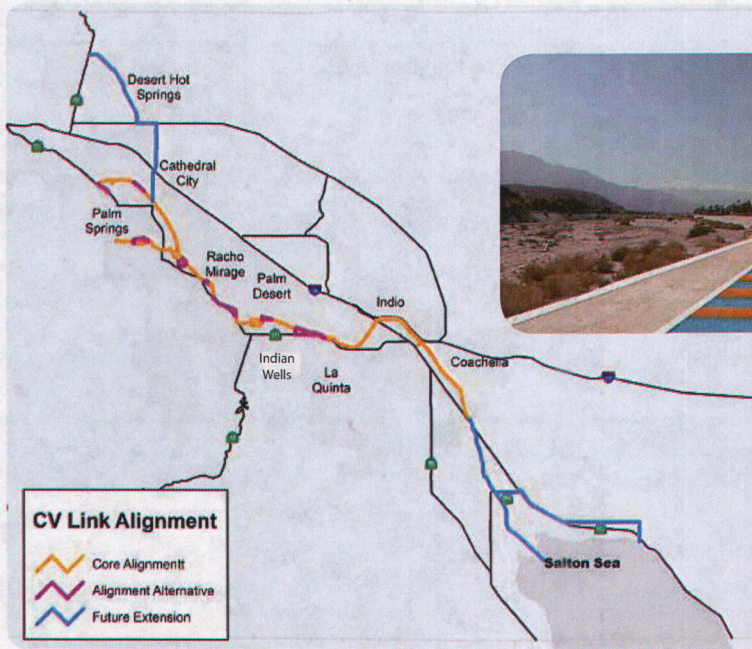
Informational flyer for CVLink

# CVLINK

CONNECTING THE COACHELLA VALLEY



## Project Overview



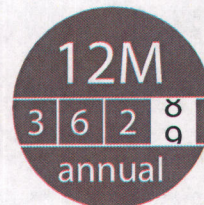
total length



in funding secured



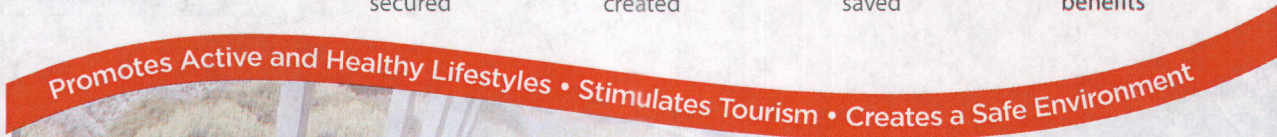
permanent jobs created



vehicle miles saved



in economic benefits



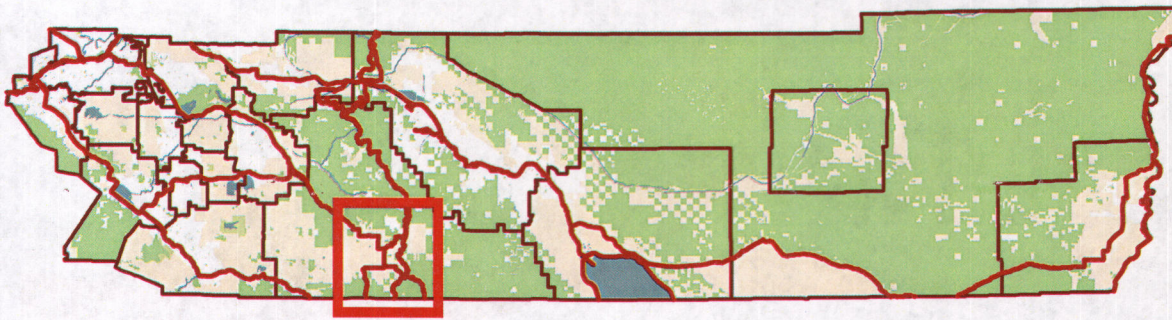
CV Link is a transformative, multi-modal facility that creates a new spine for alternative transportation through the entire Coachella Valley. It will provide significant environmental, health, and economic benefits to generations of current and future residents and visitors.

The route largely follows the Whitewater River Channel. Future paths are planned to extend CV Link to Desert Hot Springs, the Salton Sea and other destinations throughout the desert. Ultimately spanning more than 50 miles across nine cities and three tribal governments, CV Link is the largest, most ambitious project of its kind in the region, the state and the nation.

CV Link will connect users to employment centers, shopping, schools, friends and recreational opportunities. Dual paths are planned to accommodate bicycles and low-speed electric vehicles, and pedestrians. Low-speed electric vehicles include golf carts and neighborhood electric vehicles (NEVs).

This alternative transportation corridor will enable healthier lifestyles, spur economic innovation, and make the Coachella Valley a more sustainable and appealing place to live, work and play. It will bring national recognition to the Coachella Valley as a leader in environmentally friendly transportation.





## JUAN BAUTISTA DE ANZA HISTORICAL TRAIL

**Length:** 84.9 miles

**Area Plans:** REMAP, San Jacinto Valley, Reche Canyon/Badlands, Cities of Riverside and Norco, Jurupa, March, Mead Valley, Lake Mathews/Woodcrest

**Jurisdictions:** County, BLM

**Destinations:** Riverside, Perris, Moreno Valley, Jurupa Valley, Anza-Borrego State Desert Park

**Nearby Significant Trails:** Pacific Crest National Scenic Trail, Alessandro Trail, Lakeview/Nuevo Trails, Santa Ana River Trail

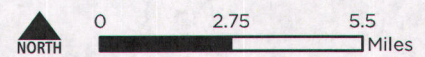
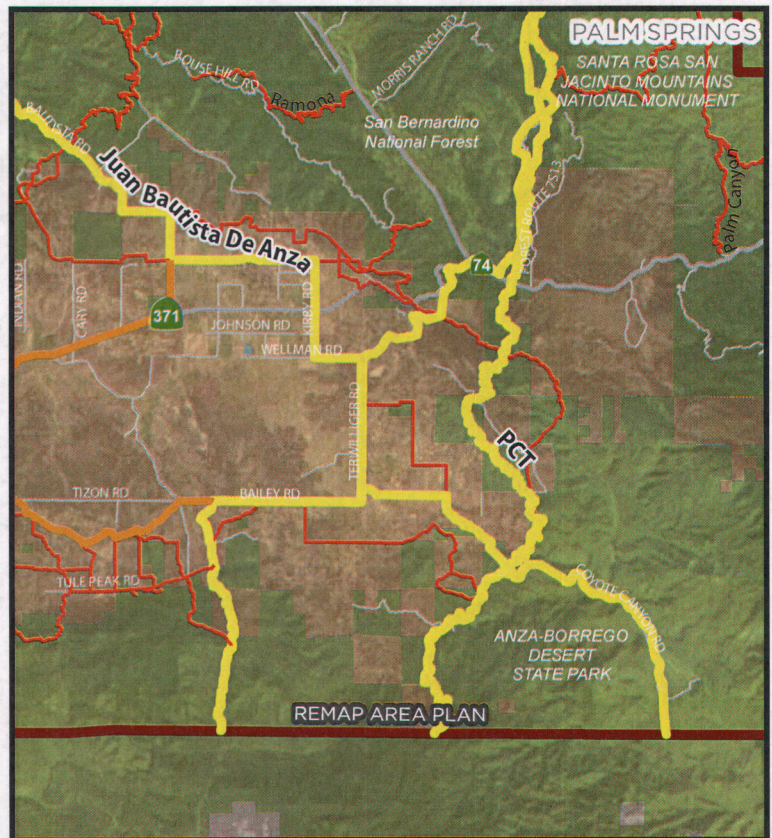
**Percent Existing:** 50%

**Management Status:** Portions within Moreno Valley existing. Planned at a high level by NPS.

The Juan Bautista de Anza Historical Trail would run northwest from the southern border of the Riverside County along the western side of the San Bernardino National Forest, towards Jurupa Valley in the northwestern corner of the county.

At the southern county border, there are two routes labeled as the Juan Bautista de Anza Historical Trail. The western route would use Cooper Cienega Truck Road (dirt road) within BLM land. After descending the Iron Spring Mountain range, the route would run directly east, either on or parallel to Bailey Road. The eastern of the two would use Coyote Canyon Road and a series of minor trails through the canyons (Anza-Borrego State Desert Park land) to Borrego Springs. The trails connect to each other in the Cahuilla area. From there, the proposed trail is unbuilt, and follows roads (inset 1).

Juan Bautista de Anza Trail Inset 1



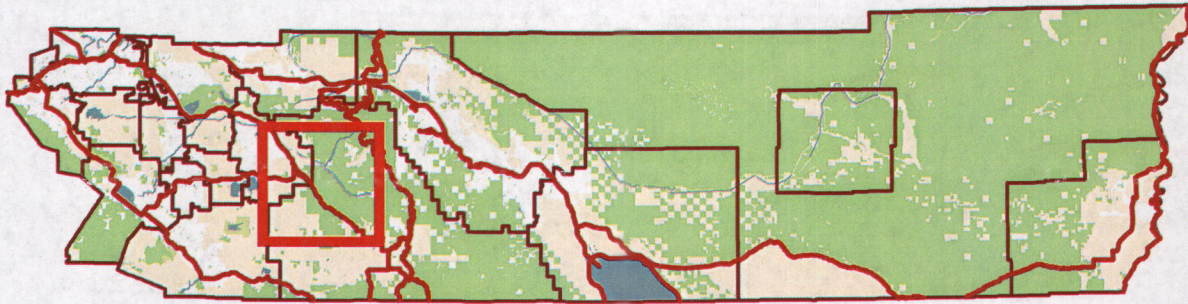
### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

### Boundaries

- Water Bodies
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From Cahuilla, the trail would follow Bautista Road through a series of canyons. The trail could either use the dirt road, or a poorly-maintained canyon trail parallel to Bautista Road (inset 2). Along this segment, the trail would pass to the Alessandro Arroyo Trail. As the trail emerges from the canyons into Hemet, it would follow the Bautista Creek alignment. Bautista Creek is a paved culvert, without a proper trail, and is largely within County jurisdiction.

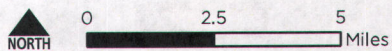
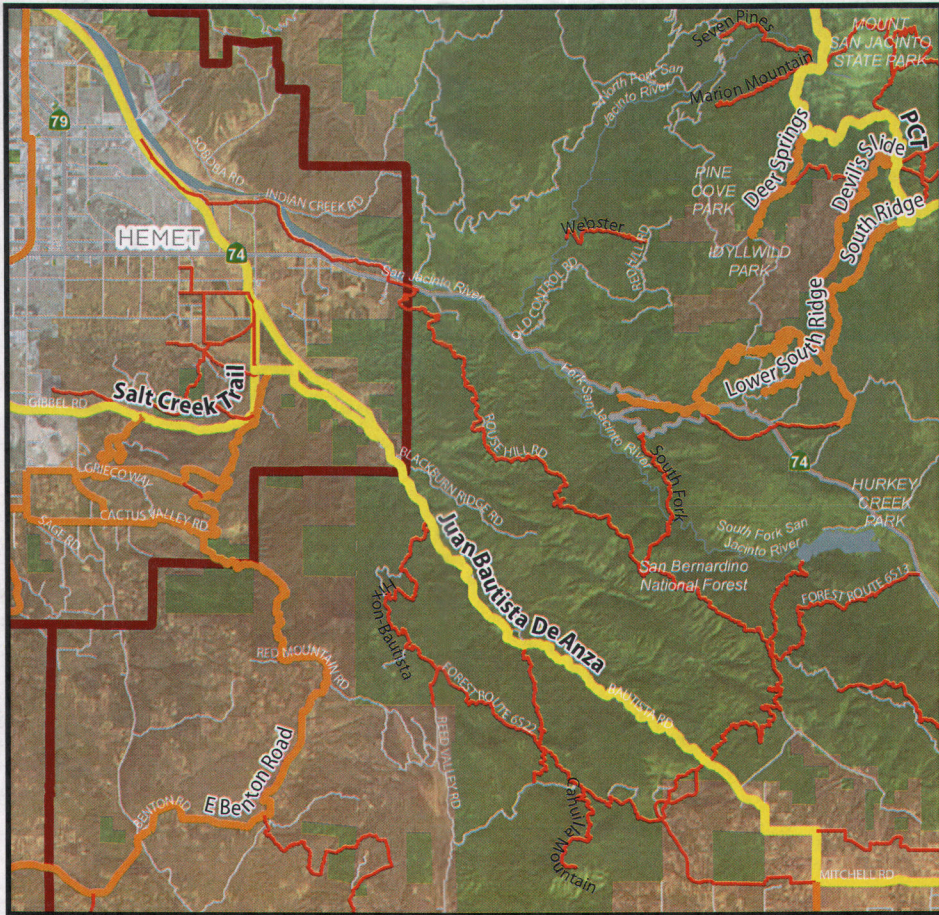
**Trail Network**

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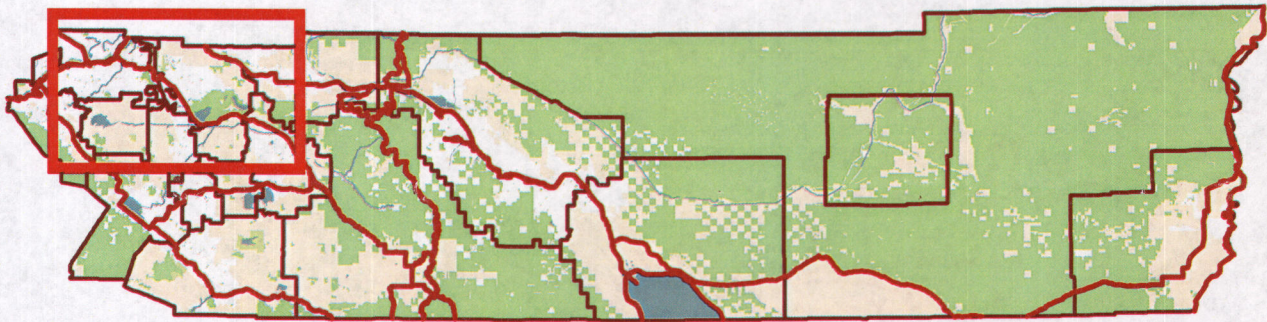
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Juan Bautista de Anza Trail Inset 2



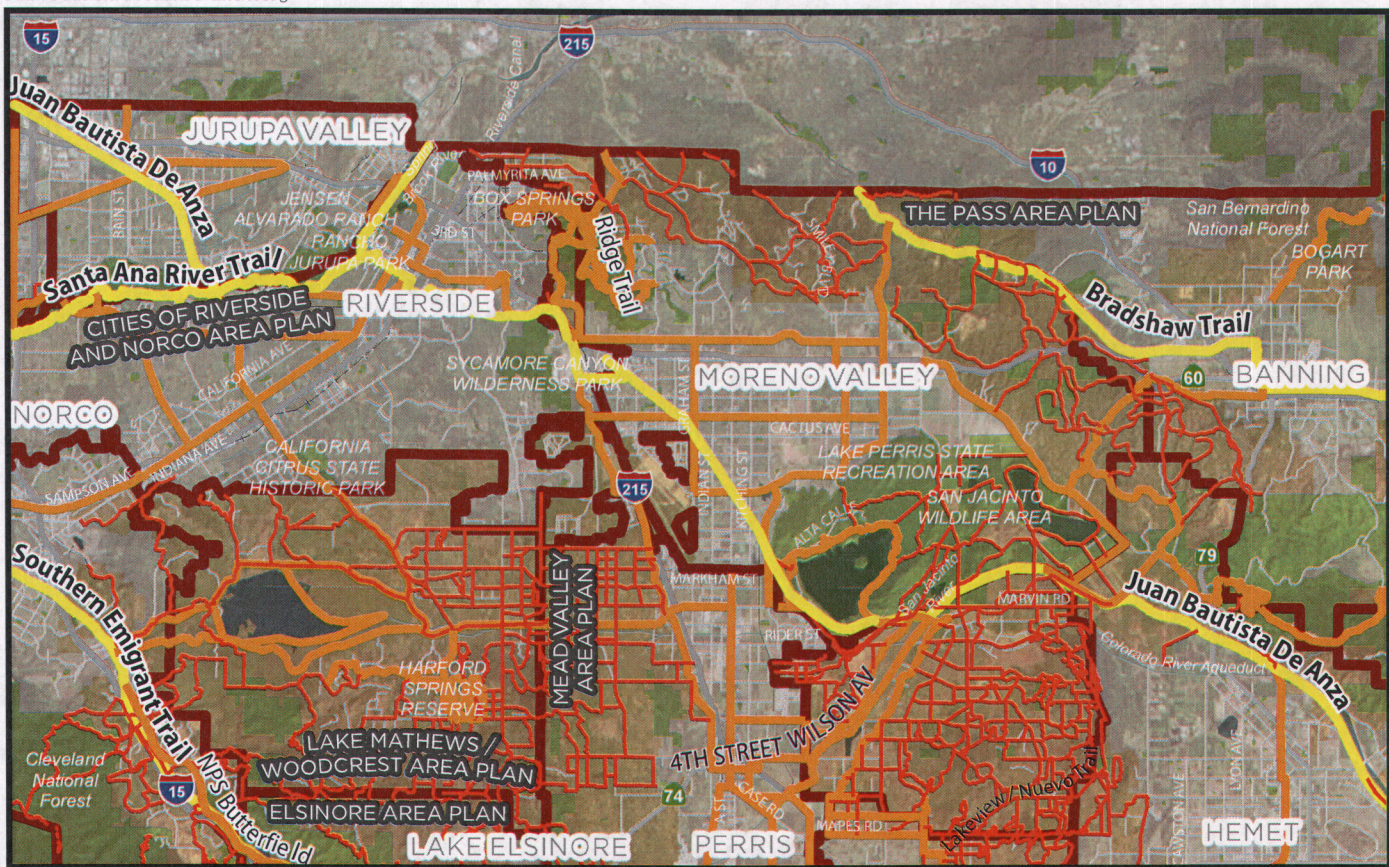




Northwest of Hemet, the de Anza trail would connect to the northern edges of the Lakeview/Nuevo trail network, and the southern edges of the trail network around Lake Perris (Upland Game Hunting Area) (inset 3). The trail would wrap tightly around the south shore of Lake Perris, and from there follow an aqueduct through Perris, Moreno Valley, and Riverside. The map alignment is an approximation of the aqueduct, and will

require further review to determine the precise alignment and to fill in gaps. Between Perris and Riverside, the trail would pass through the Sycamore Canyon Wilderness Park, which contains a small network of existing trails. The trail would use a portion of the Sana Ana River Trail within the city of Riverside before turning north into Jurupa Valley and following Van Buren Blvd to the county line.

Juan Bautista de Anza Trail Inset 3

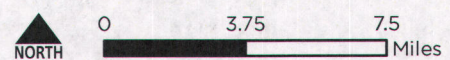


**Trail Network**

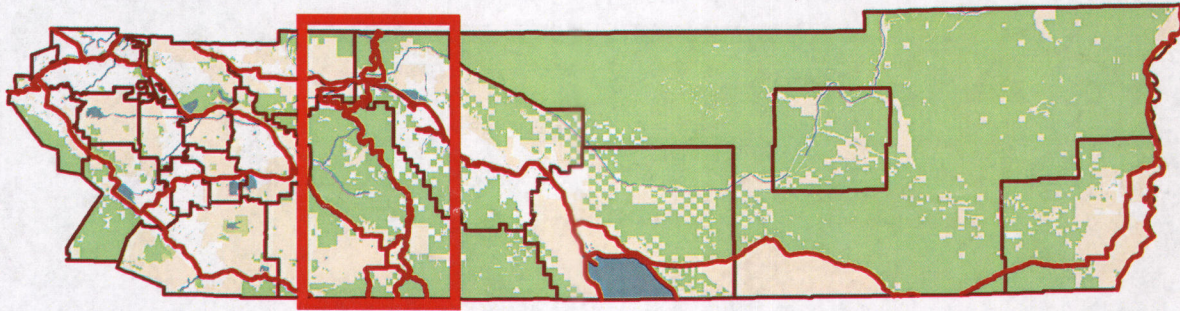
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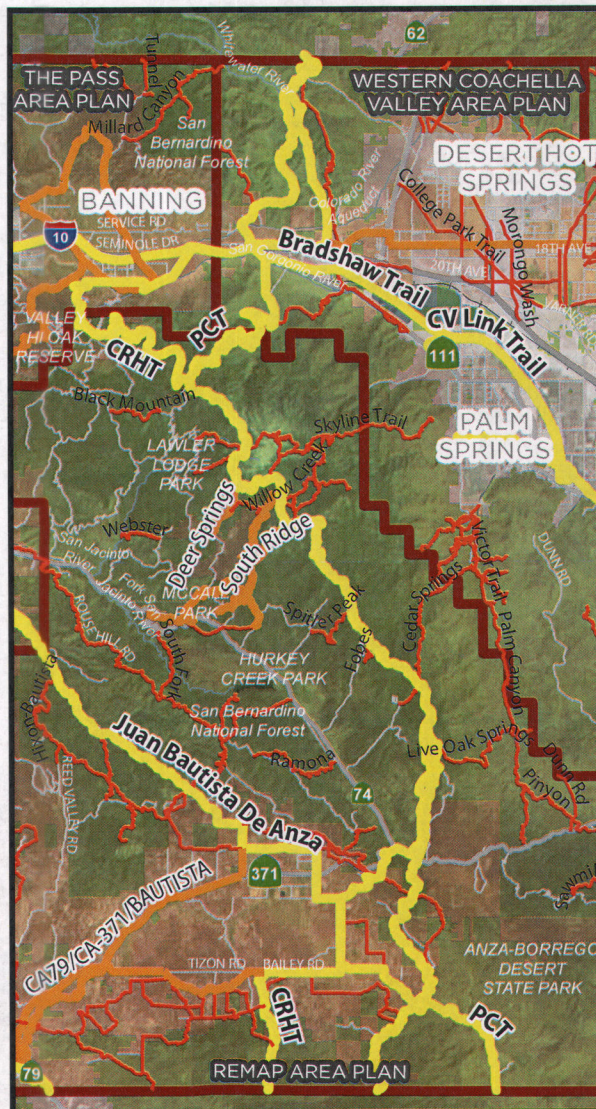


### PACIFIC CREST NATIONAL SCENIC TRAIL (PCT)

**Length:** 82.2 miles within Riverside County  
**Area Plans:** Western Coachella Valley, REMAP  
**Jurisdictions:** Riverside County, BLM, City of Palm Springs, National Forest, State Park  
**Destinations:** San Bernardino National Forest, Anza-Borrego Desert State Park,  
**Nearby Significant Trails:** Juan Bautista de Anza Historic Trail, Mission Creek Trail, Little Morongo Canyon Trail, California Riding and Hiking Trail  
**Percent Existing:** 100%  
**Management Status:** Maintained

The PCT runs north/south near the center of Riverside County, primarily within the San Bernardino National Forest. It does not pass directly through populated areas. It runs parallel to much of the California Riding and Hiking Trail, and intersects the Juan Bautista de Anza Historical Trail and the Bradshaw Trail.

The PCT is maintained by the Pacific Crest Trails Association (PCTA), and does not currently require intervention by the County for maintenance or operations. However, the County should maintain contact with the PCTA as future trails are developed, in order to provide connectivity between new trails and the PCT.

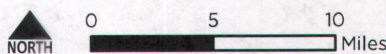


**Trail Network**

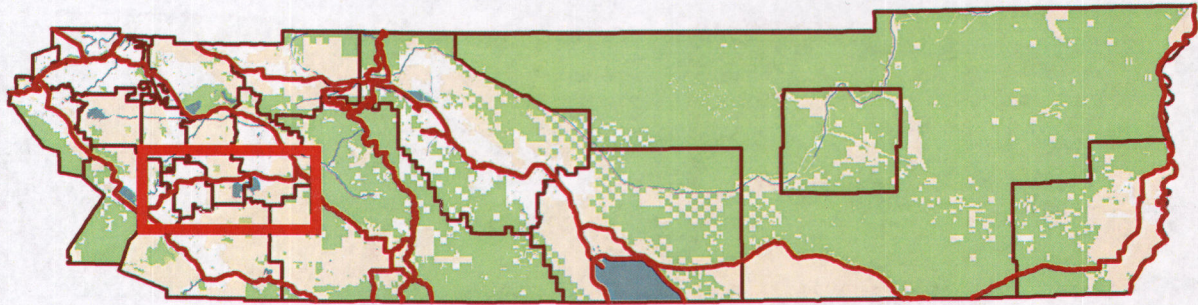
- Tier 1 (Backbone)
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**Boundaries**

- █ Water Bodies
- █ County Jurisdiction
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## SALT CREEK TRAIL

**Length:** 37.8 miles

**Area Plans:** Elsinore, Sun City/Menifee Valley, Harvest Valley/Winchester, San Jacinto Valley  
**Jurisdictions:** Riverside County, Cities of Hemet, Lake Elsinore, and Menifee

**Destinations:** Diamond Valley Lake, Menifee Lakes Country Club

**Nearby Significant Trails:**

**Percent Existing:** 70%

**Management Status:** Planned (with unplanned gap closures). 16 miles undergoing design.

The Salt Creek Trail, when complete, will run east/west adjacent to the Domenigoni Parkway north of Diamond Valley Lake Reservoir. The trail will provide a connection between two backbone trails: Juan Bautista de Anza Historic Trail at the east, and Southern Emigrant Trail / Butterfield Overland Trail at the west.

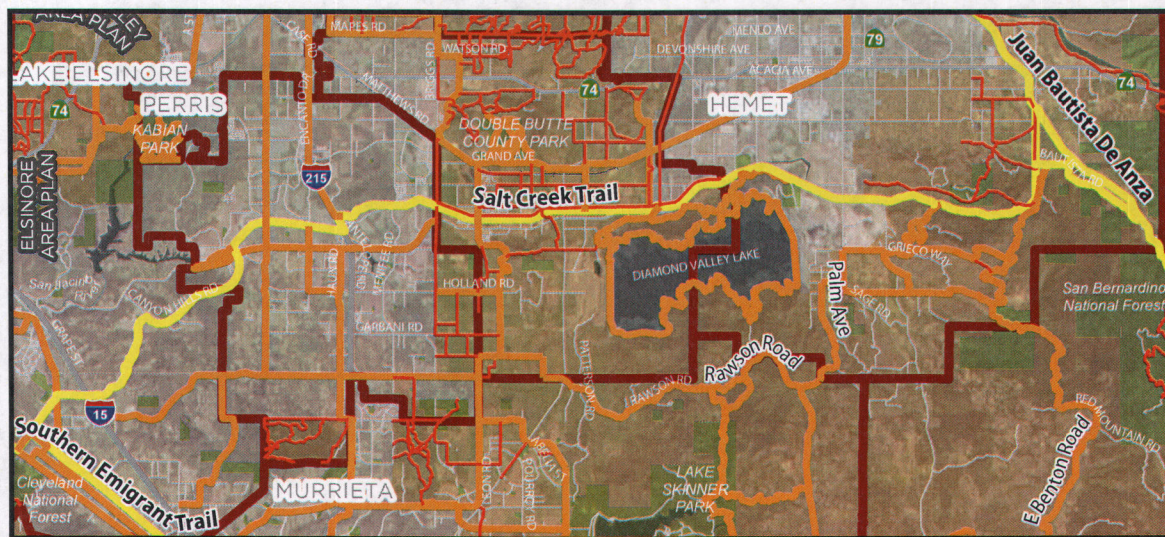
While the alignment of the Salt Creek Trail has been fully planned (and will utilize an existing dirt road that runs along a dry creek bed), the route as planned does not connect with the two backbone

trails mentioned above. These gaps can be filled using other proposed and existing trails.

On the east end of Salt Creek, the gap can be partially filled with the proposed Fairview/Gibbel Class I Bike Path. This proposed path would be partially within county jurisdiction. While not within county jurisdiction, the proposed alignment would be on the Parkway. East of South State Street, it would utilize Gibbel Road through Avery Canyon, and then Avery Canyon Road (which dwindles to a dubiously-maintained trail). After that, the proposed trail would connect to a dirt road, which is possibly private, and head north to the de Anza Trail.

On the west end of Salt Creek, the planned trail has two alternate routes through the Menifee Country Club. West of the country club, there is potential for the trail to utilize a newly-built Class I bike path, then a Class II bike lane (Canyon Hills development), before connecting to a dirt road (Lost Road). Additional planning is needed to finalize the proposed routes.

The western half of the Salt Creek backbone would not be within county jurisdiction, passing through Menifee.

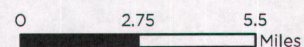


**Trail Network**

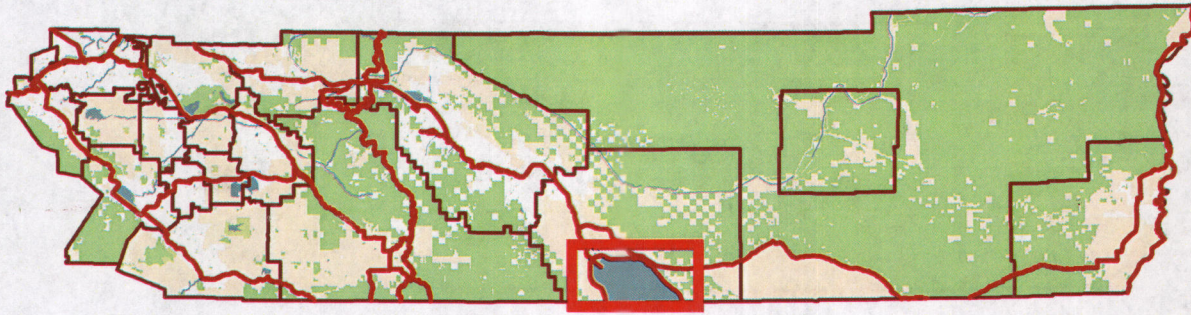
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- Parks and Public Lands
- Area Plan Boundary







### SALTON SEA TRAIL




**Length:** 32.4 miles  
**Area Plans:** Eastern Coachella Valley  
**Jurisdictions:** Riverside County, BLM, US Bureau of Reclamation  
**Destinations:** Salton Sea, Mecca, The Bradshaw  
**Nearby Significant Trails:** N/A  
**Percent Existing:** 0%  
**Management Status:** No active management or detailed plans.

The Salton Sea Trail is a proposed trail that would run along the northern perimeter of the Salton Sea from the county boundary on the east side of the sea to that on the west. As such, it would connect to campgrounds and a number of small populated areas of North Shore and Oasis, and provide recreational access to the Salton Sea. The trail would extend north from the Salton Sea to intersect with the The Bradshaw and CV Link.





The land is a mix of federal and county holdings, and is generally flat and undeveloped.

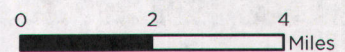


**Trail Network**

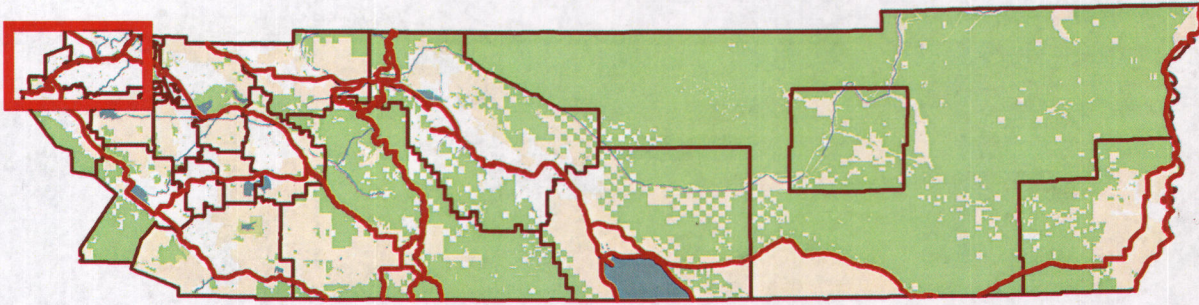
-  Tier 1 (Backbone)
-  Tier 2
-  Tier 3

**Boundaries**

-  Water Bodies
-  County Jurisdiction
-  Parks and Public Lands
-  Area Plan Boundary







### SANTA ANA RIVER TRAIL

**Length:** 25.7 miles

**Area Plans:** Cities of Riverside and Norco, Jurupa, Eastvale, Temescal Canyon

**Jurisdictions:** Riverside County, Cities of Norco, Riverside, Corona, and Jurupa Valley

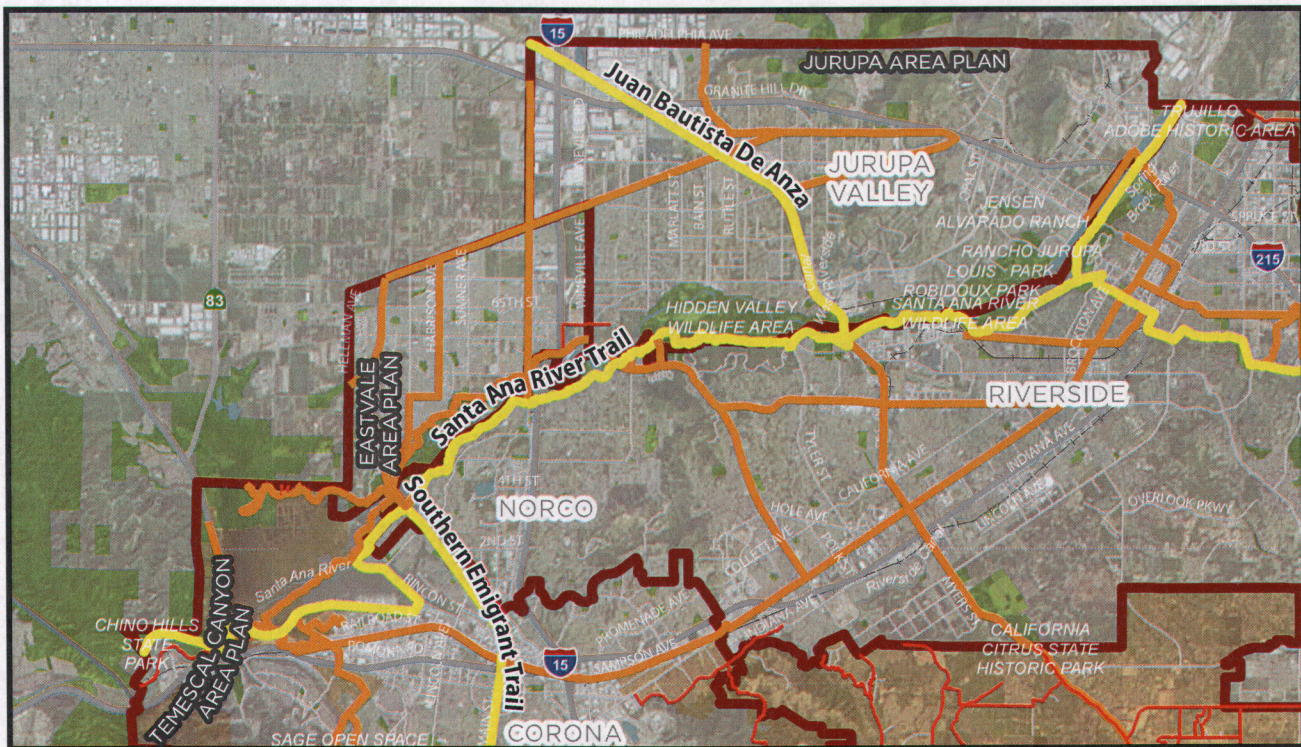
**Destinations:** Riverside, Corona  
Nearby Significant Trails: Southern Emigrant Trail, Juan Bautista de Anza Historic Trail

**Percent Existing:** 57%

**Management Status:** Portions existing and operated by RCPOSD. Extensions to existing portions are planned.

The Santa Ana River Trail is a partially-completed Class I Bike Path that runs adjacent to the Santa Ana River (on the south side). It extends beyond Riverside into Orange and San Bernardino Counties as well. The proposed trail will connect to two other proposed backbone trails: Southern Emigrant and de Anza. At present, 13 miles of the Santa Ana River Trail have been completed, with additional segments being constructed regularly. The trail will have parallel trails: a soft surface trail for equestrians and hikers, and a paved trail for pedestrians and bicyclists.

Segments of the unbuilt western portion of the planned route can use adjacent roadways until the river path is complete. When fully completed, the Santa Ana River Trail will be nearly 100 miles long, connecting from Huntington Beach to the San Bernardino Mountains.

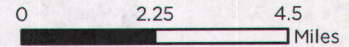


**Trail Network**

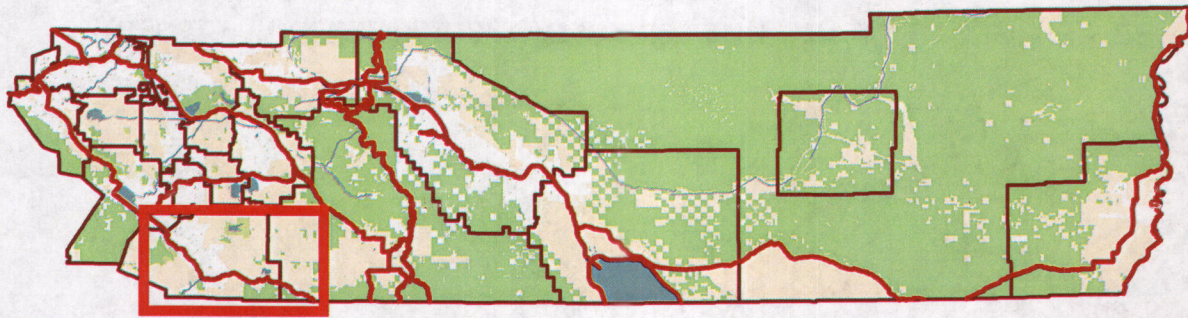
- Tier 1 (Backbone)
- Tier 2
- Tier 3

**Boundaries**

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary







### SOUTHERN EMIGRANT TRAIL / BUTTERFIELD OVERLAND TRAIL

**Length:** 66.8 miles

**Area Plans:** Elsinore, Temescal Canyon, Southwest Area, REMAP

**Jurisdictions:** Riverside County, Cities of Corona, Murrieta, Lake Elsinore, and Temecula

**Destinations:** Corona, Murrieta, Lake Elsinore, Temecula, Cleveland National Forest, Salt Creek Trail

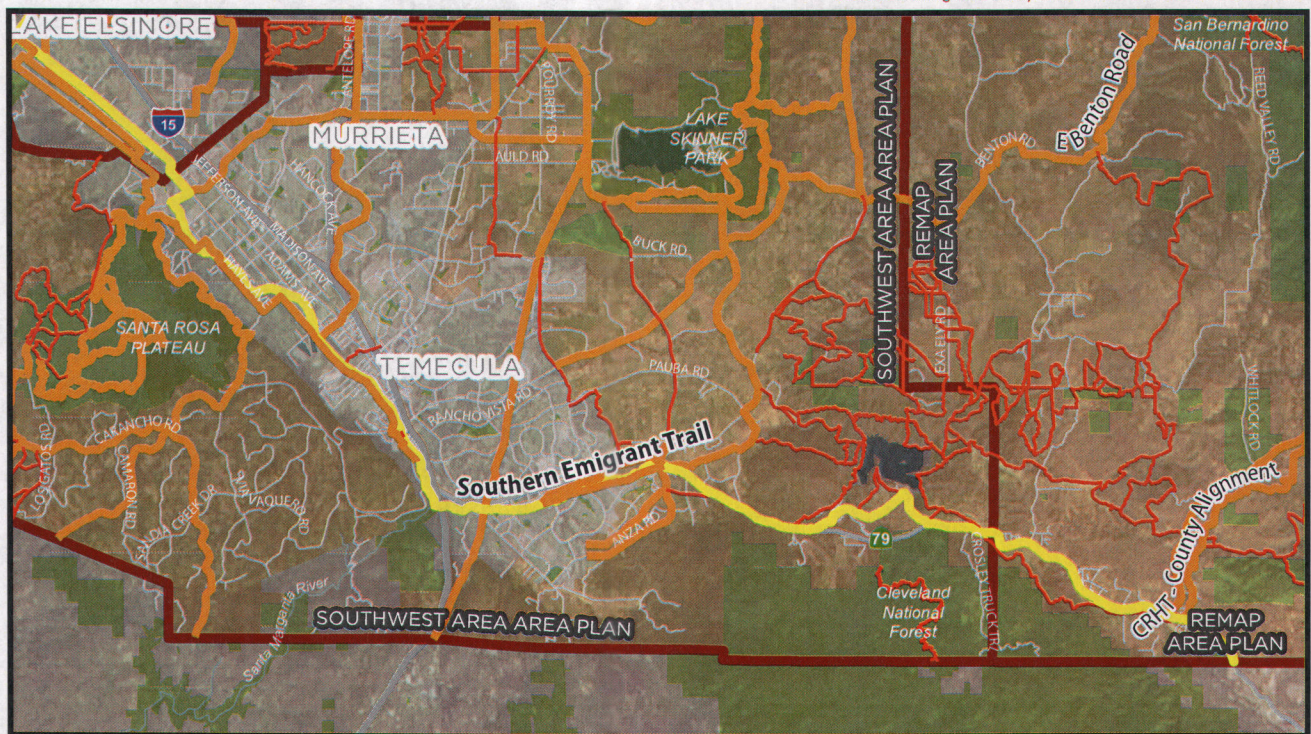
**Nearby Significant Trails:** Temecula Wine Country, Santa Rosa Plateau Walking Trails, Santa Ana River Trail, Murrieta Creek Trail

**Percent Existing:** 0.5%

**Management Status:** Portions planned in detail by the National Parks Service (Butterfield and Murrieta Creek).

The Southern Emigrant Trail and Butterfield Overland Trail are historical corridors without existing current trails. Through Riverside County, both proposed trails generally follow the same alignment. The Butterfield Overland Trail recently underwent a thorough planning process (see Appendix E). The result is the most reasonable alignment for the planned trail. The backbone trail follows the historical route where feasible. Where infeasible, the trail veers from the historical alignment onto alignments developed in consultation with WRCOG through the WRCOG Active Transportation Plan. The indicated route maintains the overall historic trajectory of the Butterfield and Southern Emigrant Trails. For most of the route, it is within populated areas, and largely follows existing roads. Through the city of Murrieta, a portion of this trail will be comprised of the Murrieta Creek Regional Trail. The southern end of the route is in county land, as is the area around Lake Elsinore.

*Southern Emigrant Trail/Butterfield Overland Trail Inset 1*

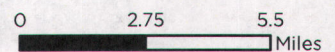


**Trail Network**

- Tier 1 (Backbone)
- Tier 2
- Tier 3

**Boundaries**

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary

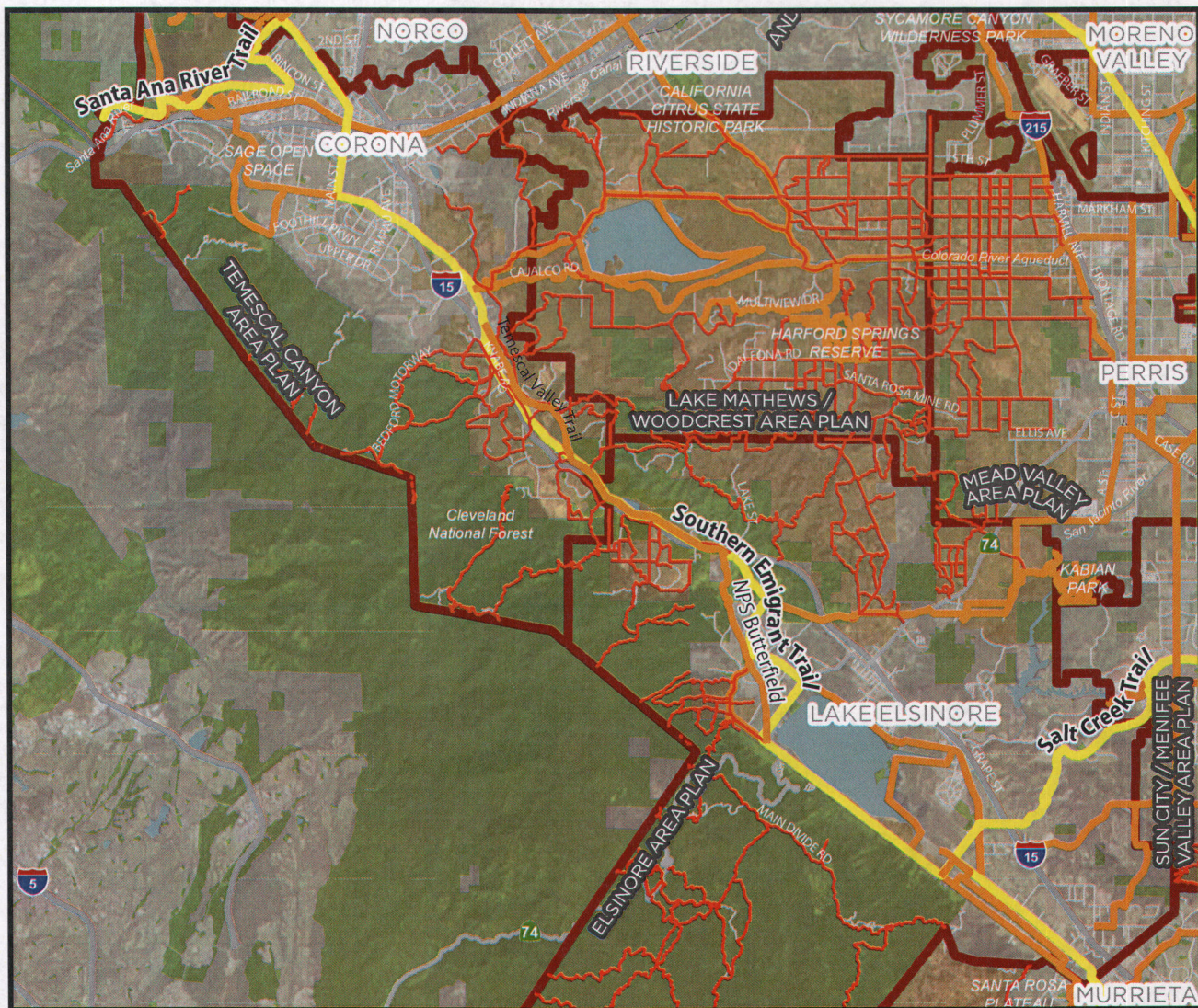




### SOUTHERN EMIGRANT TRAIL / BUTTERFIELD OVERLAND TRAIL (CONT'D)



Southern Emigrant/Butterfield Trail Inset 2

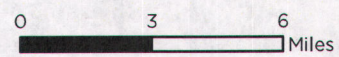


**Trail Network**

- Tier 1 (Backbone)
- Tier 2
- Tier 3

**Boundaries**

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary





## Backbone Trails Mapped by Area Plan Boundary

The current planning effort undertakes primarily a reductive process, refining previous trail planning efforts in the service of creating a regional trail network that can be prioritized and is feasible for implementation by the County. In order to preserve previous planning work while giving a path forward, trails have been classified into three tiers. These three tiers of trails are described below and presented by Area Plan in the following maps.

**Tier 1** trails form the backbone trail network, and represent the highest priority and greatest connectivity for the County. It incorporates historical alignments, regional trails with dedicated plans, those currently existing or under construction, and those with long-distance connectivity.

Additional criteria used to evaluate backbone trails include:

- Population adjacency;
- Connection to destinations;
- Connection to other jurisdictions;
- Available right-of-way;
- Land ownership;
- Ability of the trail to close gaps in the regional network.
- Historic/cultural significance

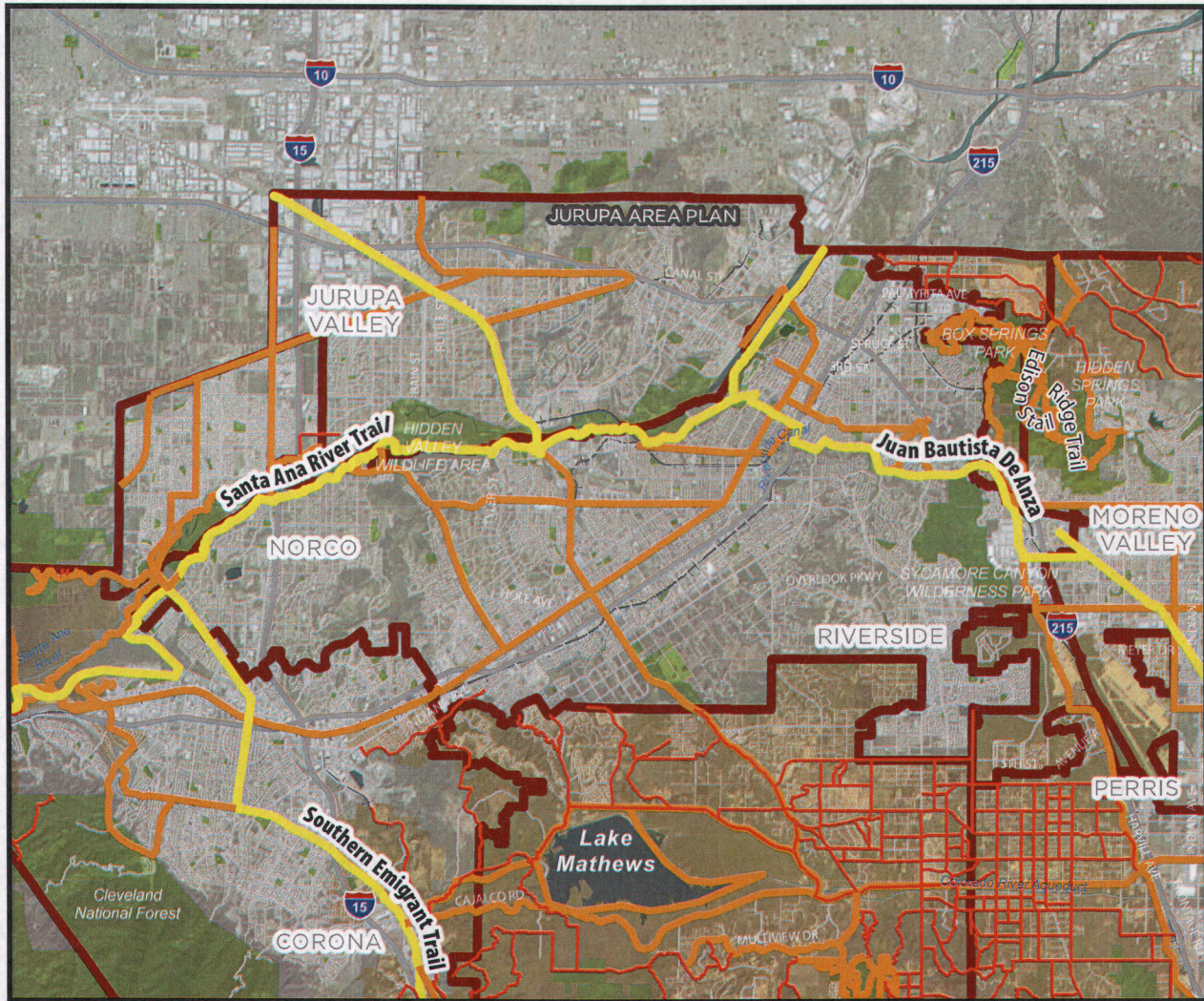
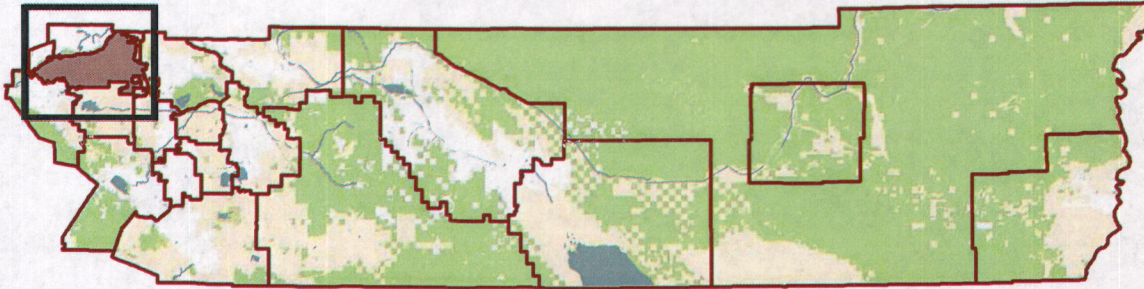
**Tier 2** trails are those which the County has previously identified as regional, but do not provide regional connectivity. Many of these trails are destinations unto themselves, but often form networks internal to parks, without providing external connectivity.

**Tier 3** trails are local and community trails, which generally either fall outside County jurisdiction, or are comprised of trail networks with only local connectivity. These trails are important as connections to local destinations, and often must be relied upon for connection between regional trails.





### AREA PLAN: CITIES OF RIVERSIDE AND NORCO

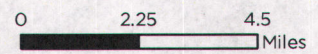


#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

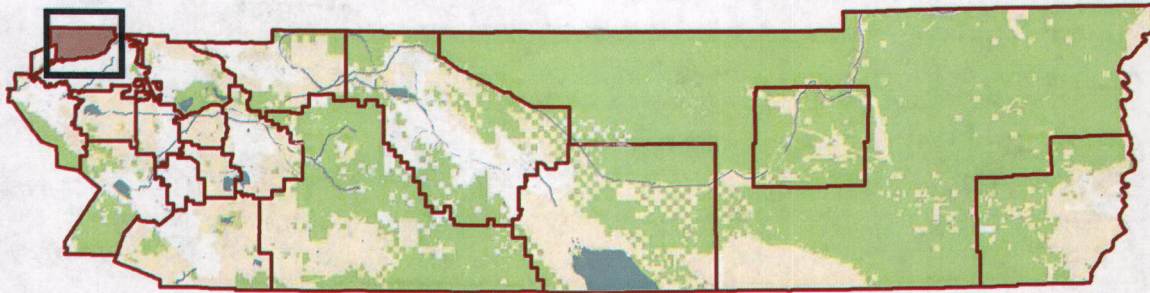
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary





### AREA PLAN: JURUPA



#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

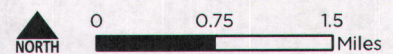
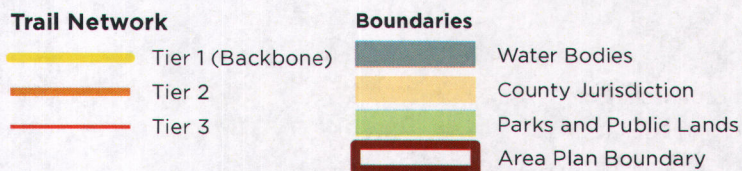
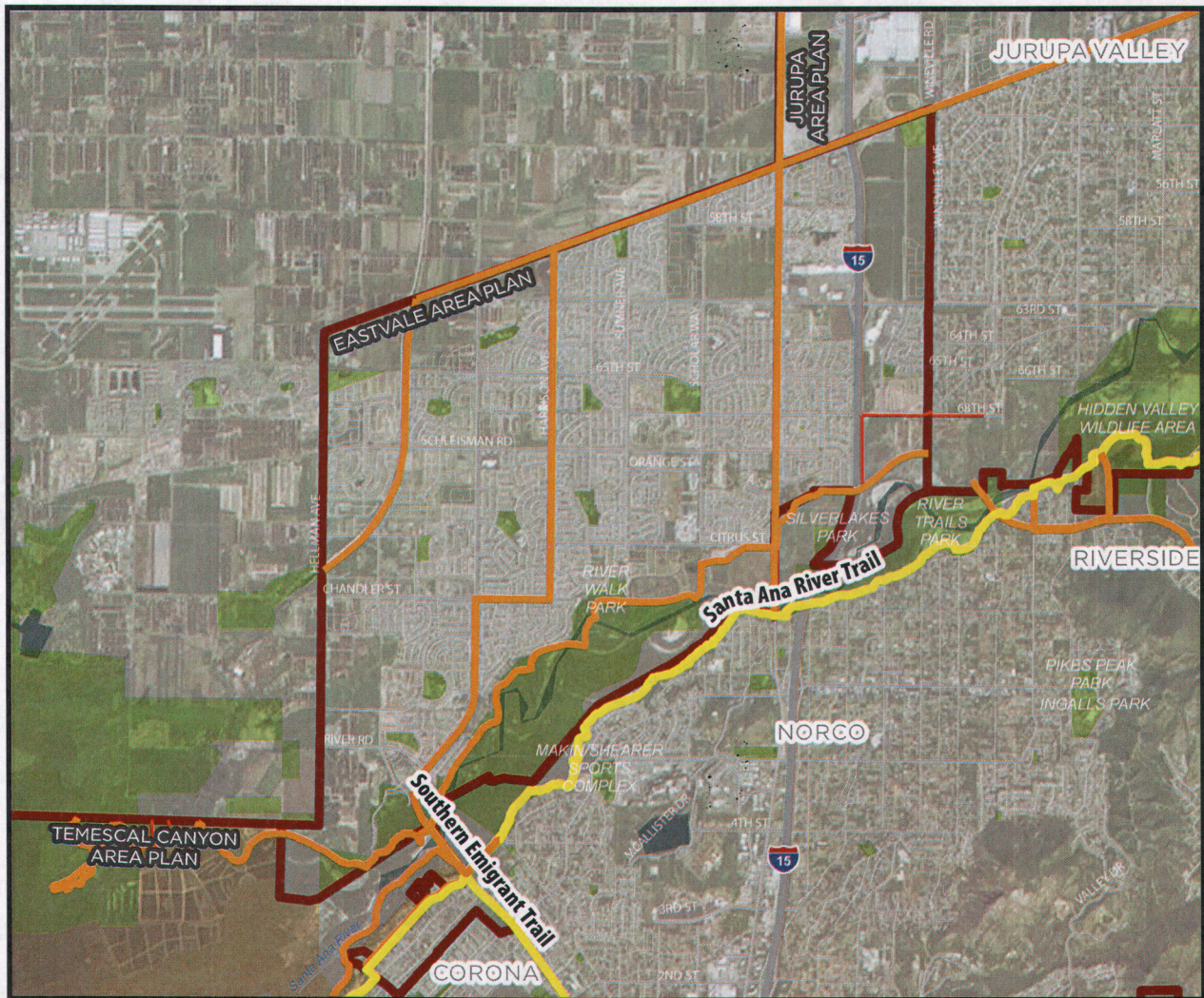
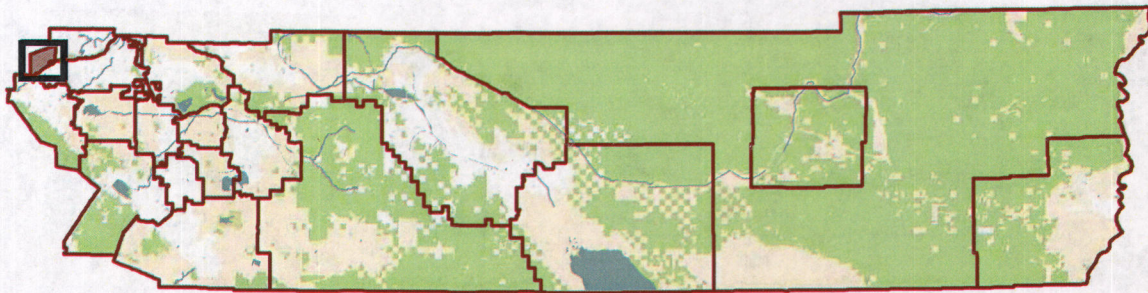
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary



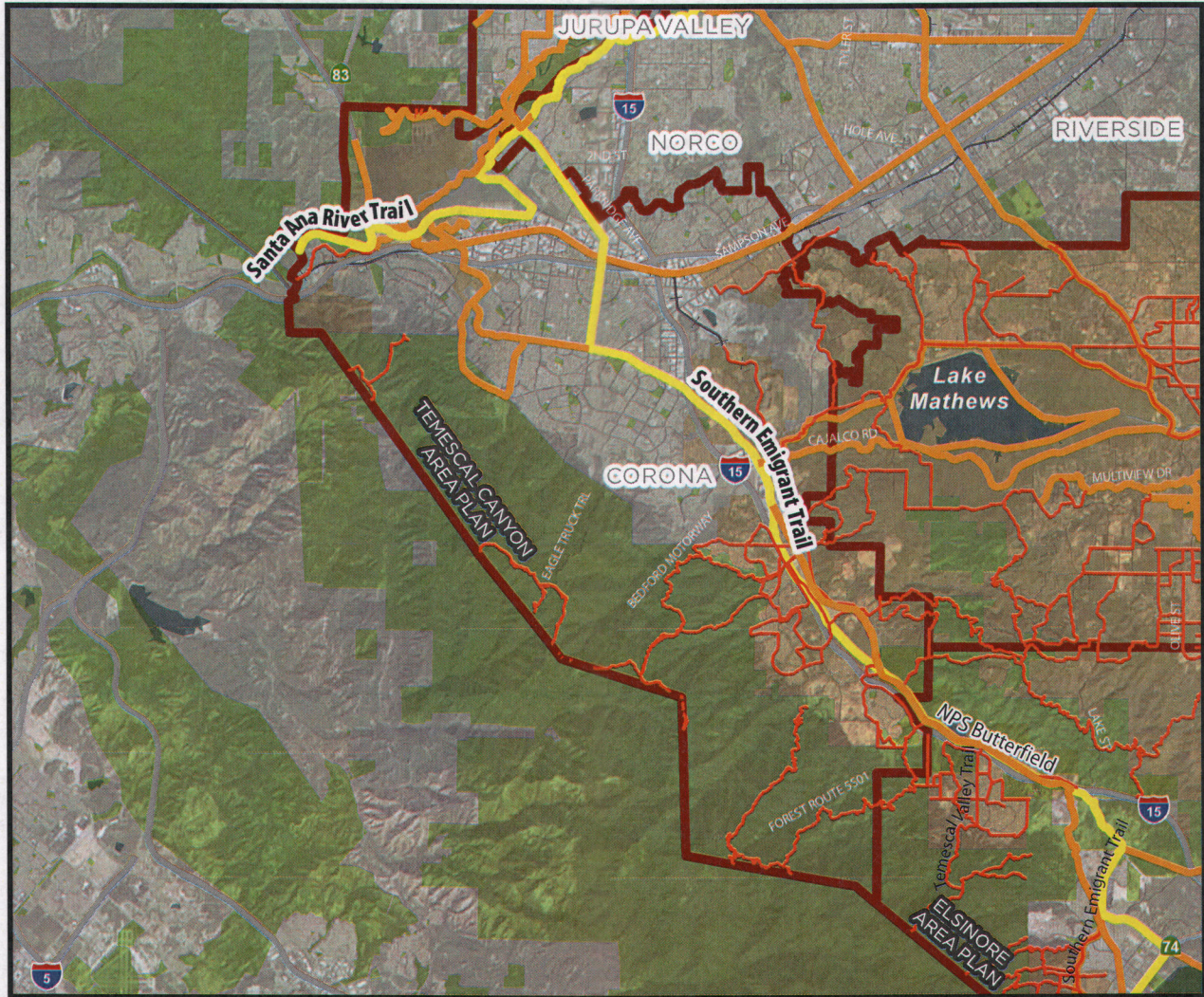
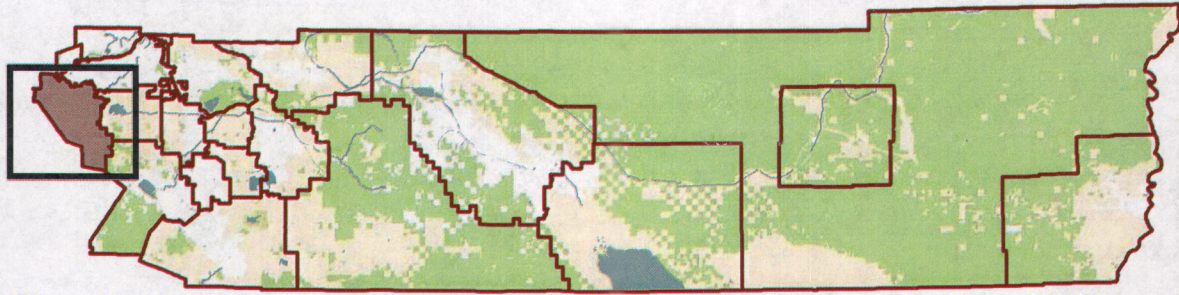


### AREA PLAN: EASTVALE





### AREA PLAN: TEMESCAL CANYON

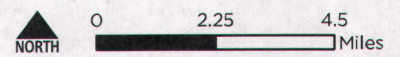


#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

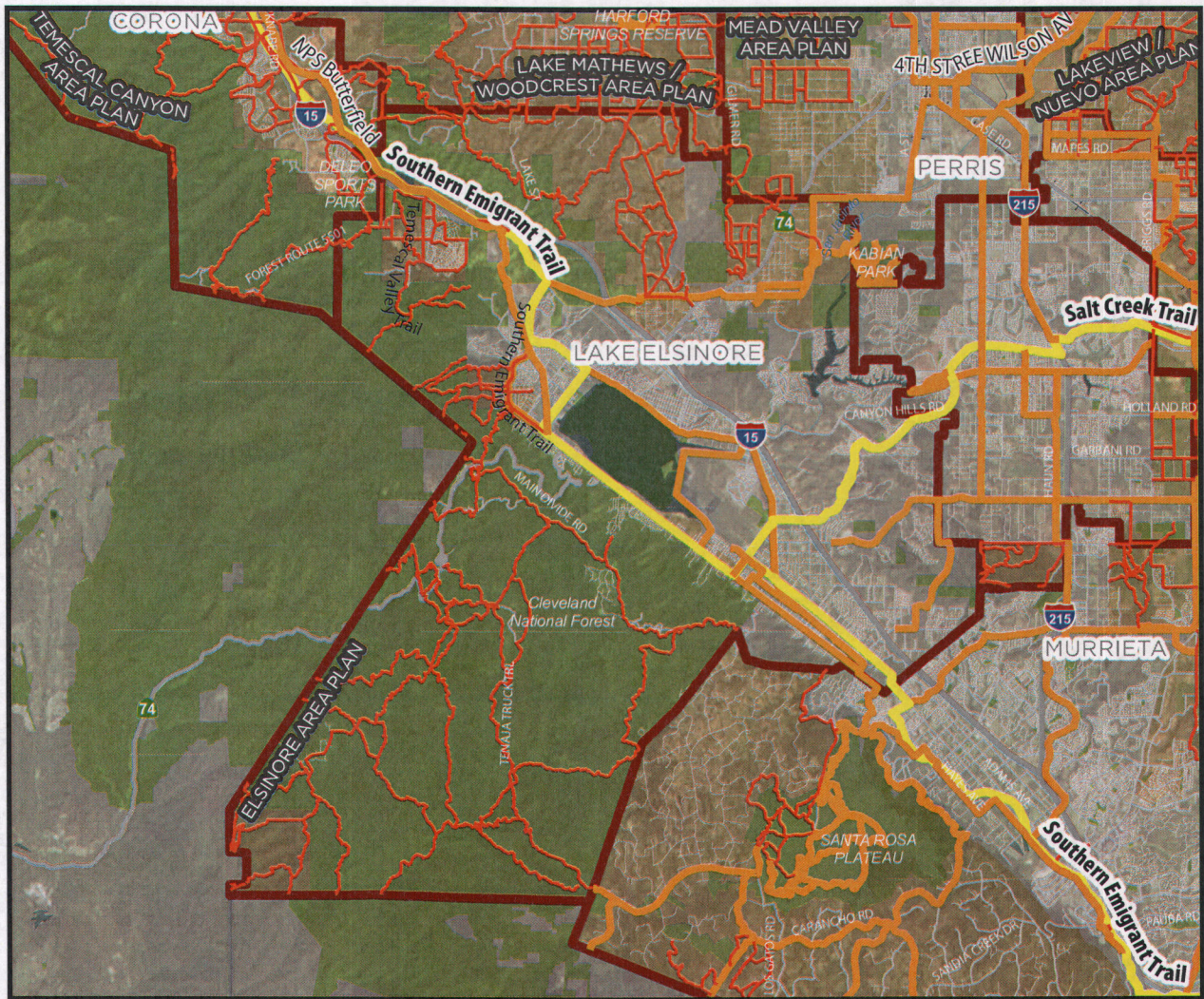
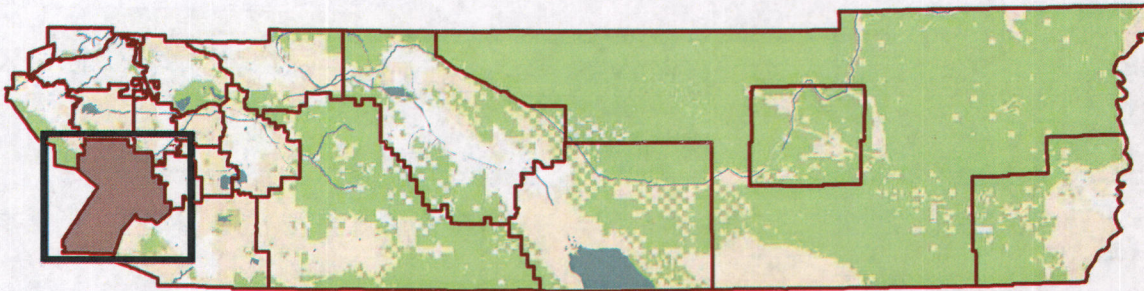
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary





### AREA PLAN: ELSINORE

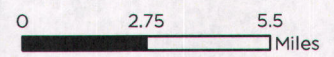


#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

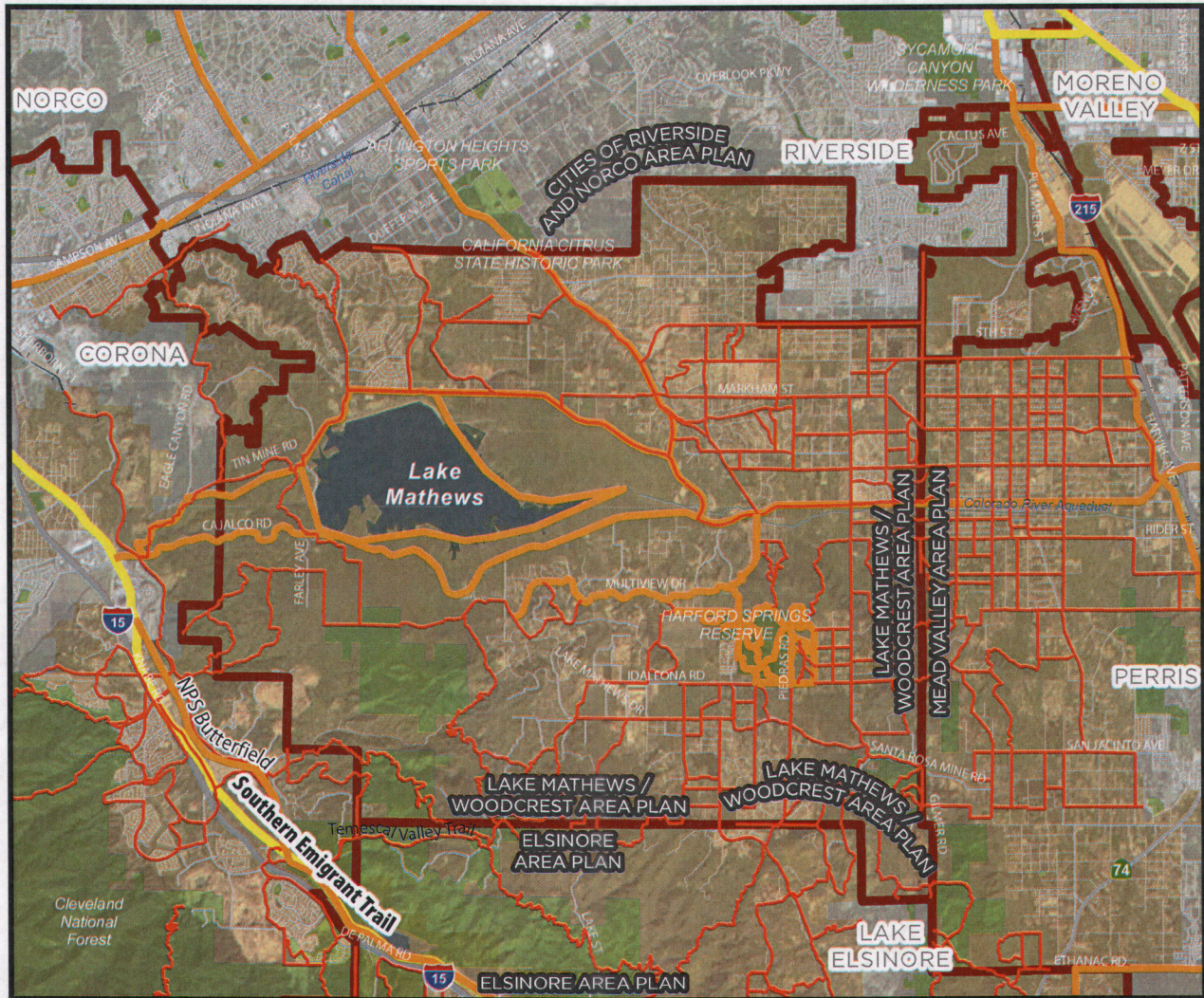
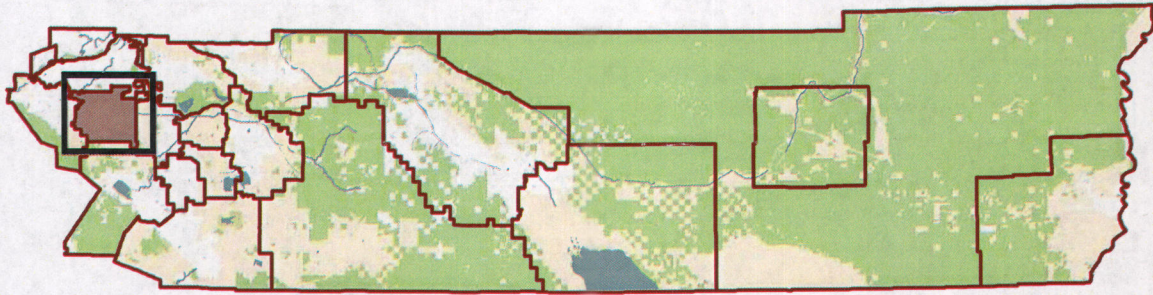
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary

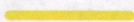










### AREA PLAN: LAKE MATHEWS/WOODCREST

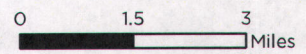


**Trail Network**

-  Tier 1 (Backbone)
-  Tier 2
-  Tier 3

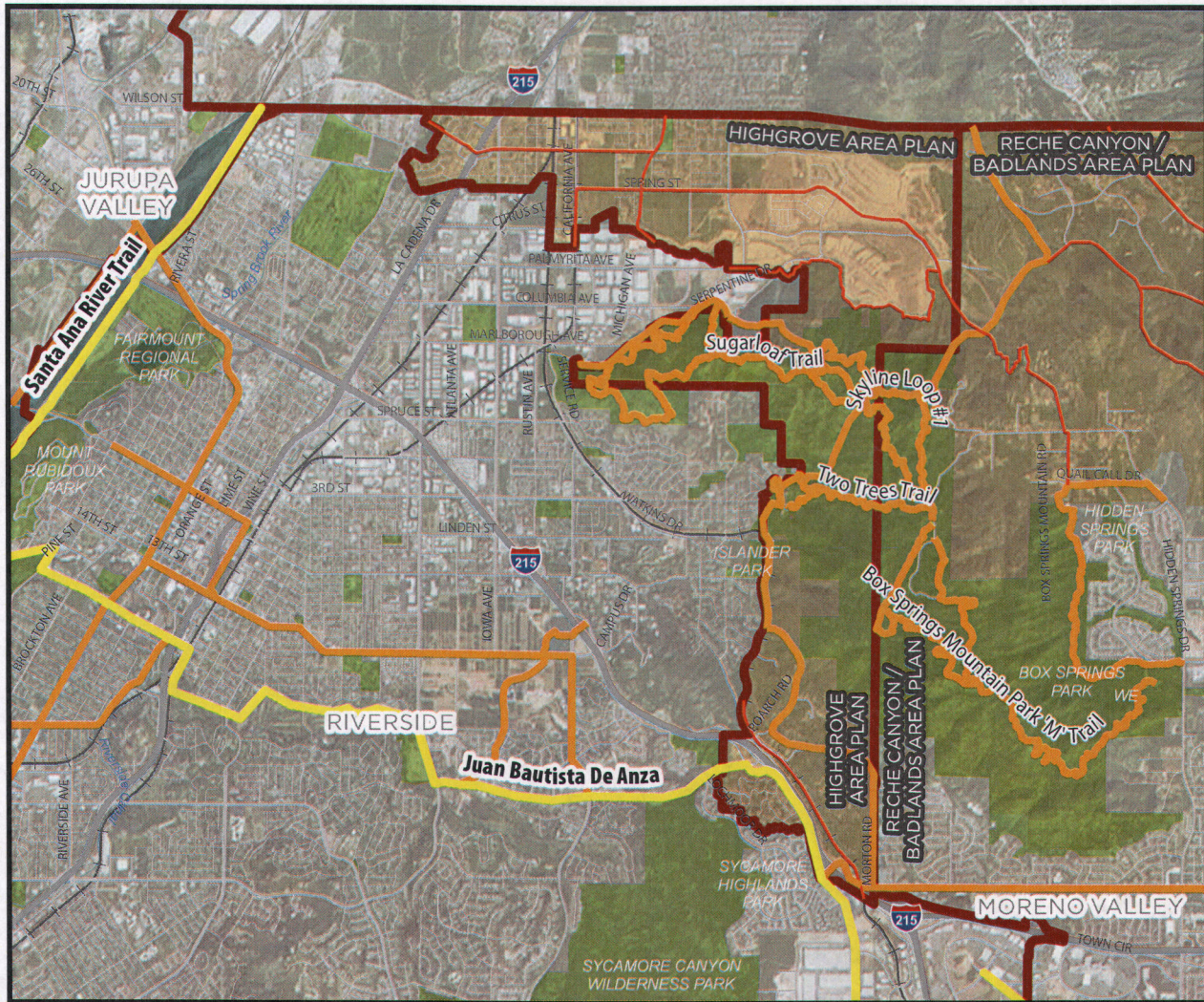
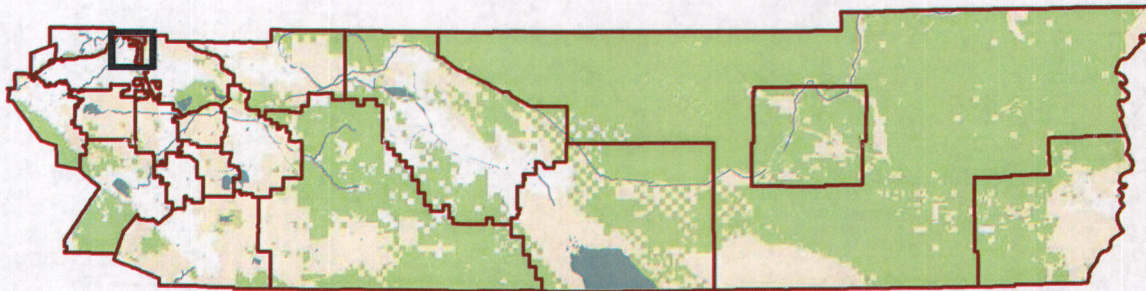
**Boundaries**

-  Water Bodies
-  County Jurisdiction
-  Parks and Public Lands
-  Area Plan Boundary





### AREA PLAN: HIGHGROVE

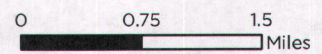


#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

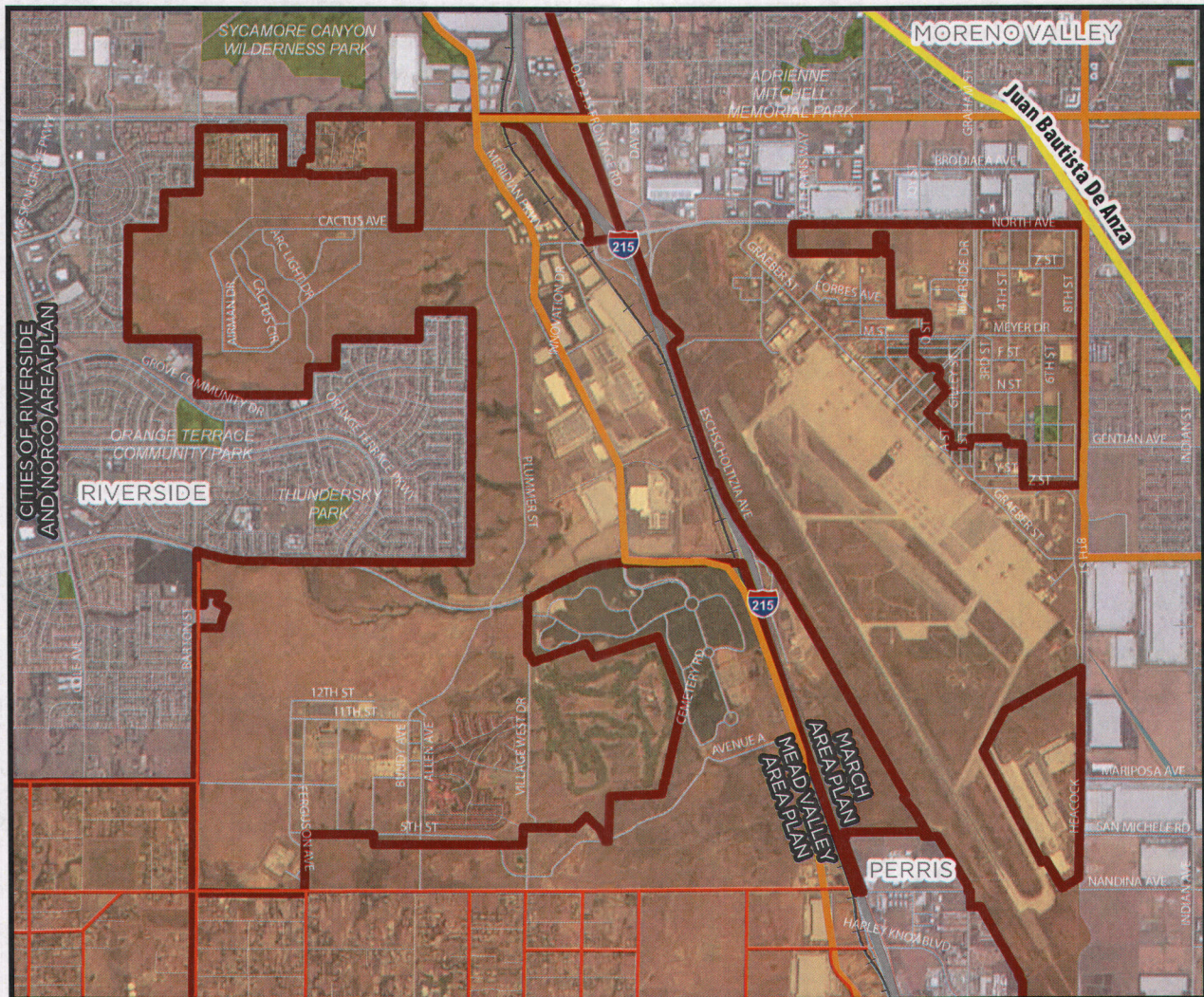
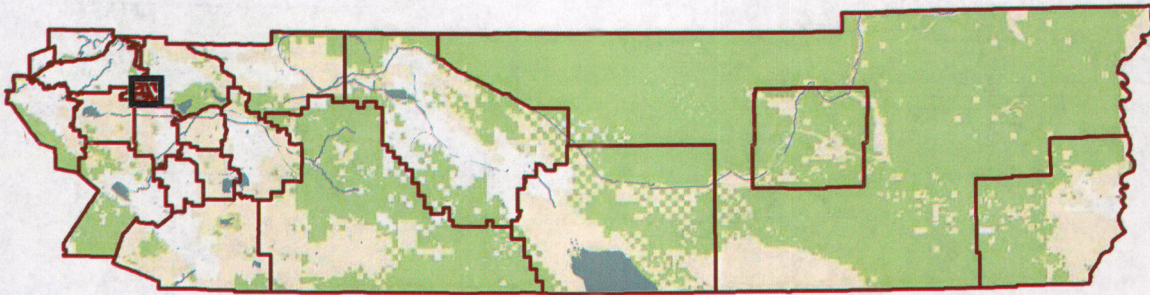
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary












### AREA PLAN: MARCH

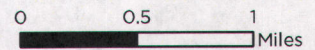


**Trail Network**

-  Tier 1 (Backbone)
-  Tier 2
-  Tier 3

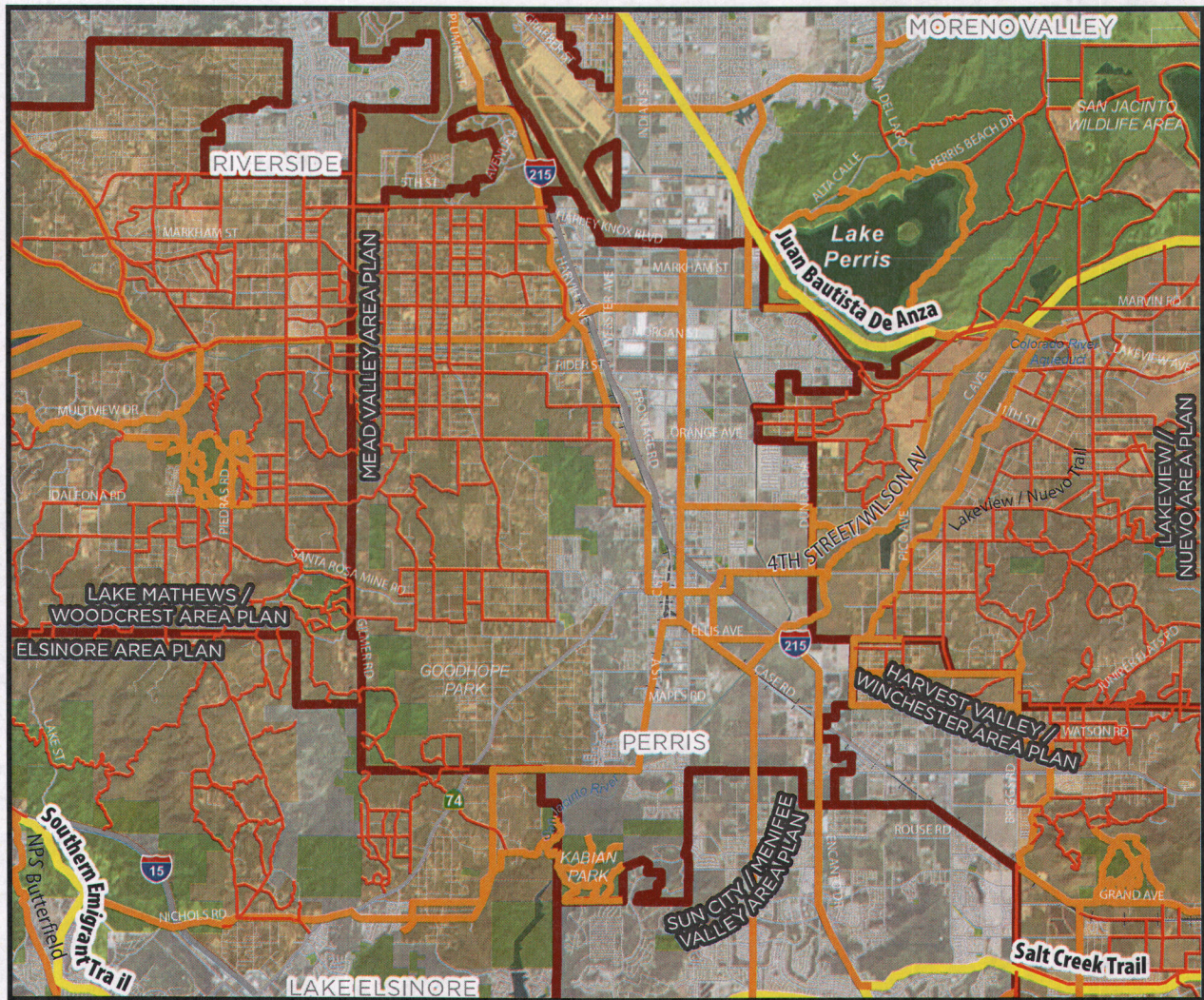
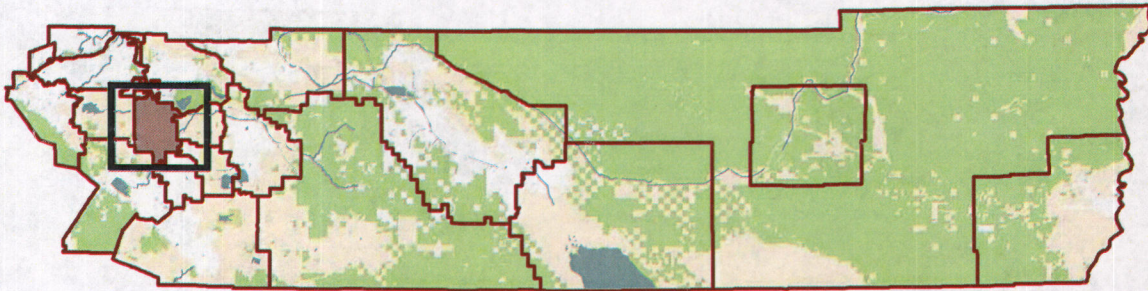
**Boundaries**

-  Water Bodies
-  County Jurisdiction
-  Parks and Public Lands
-  Area Plan Boundary





### AREA PLAN: MEAD VALLEY

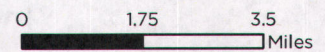


#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

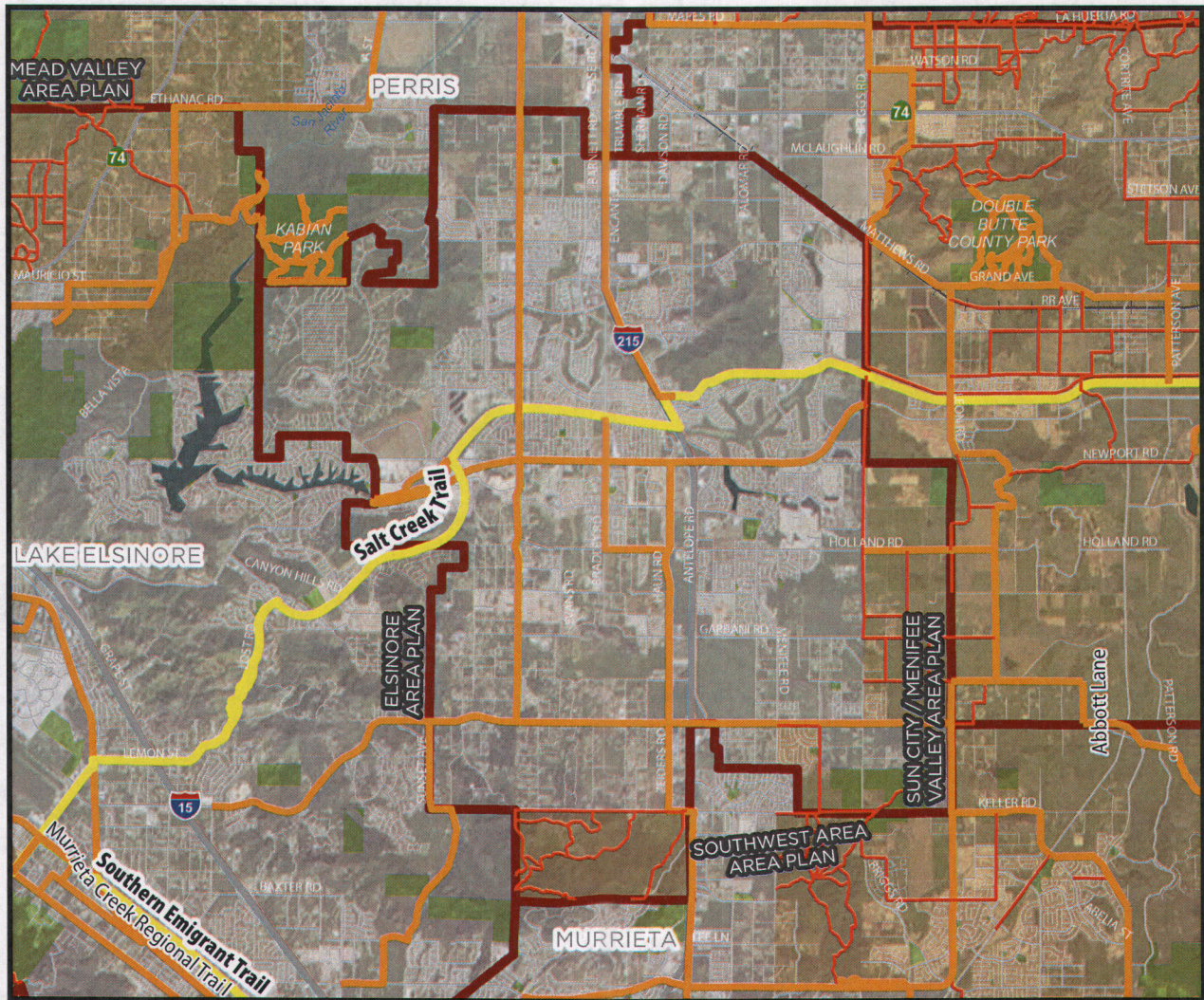
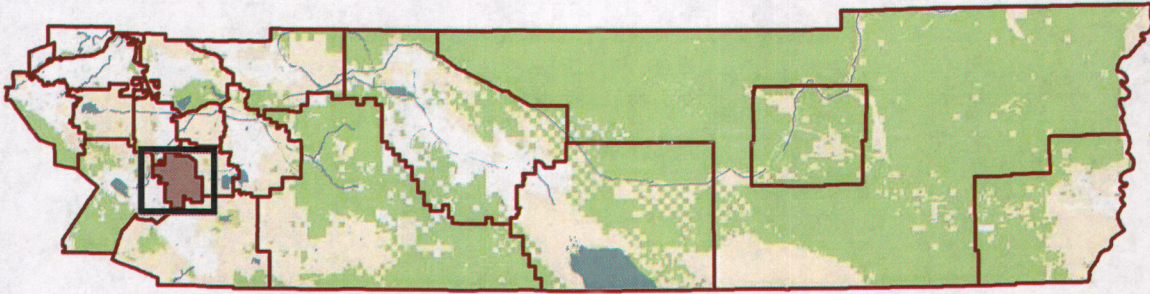
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary

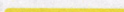






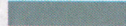



AREA PLAN: SUN CITY/MENIFEE

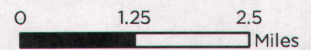


Trail Network

-  Tier 1 (Backbone)
-  Tier 2
-  Tier 3

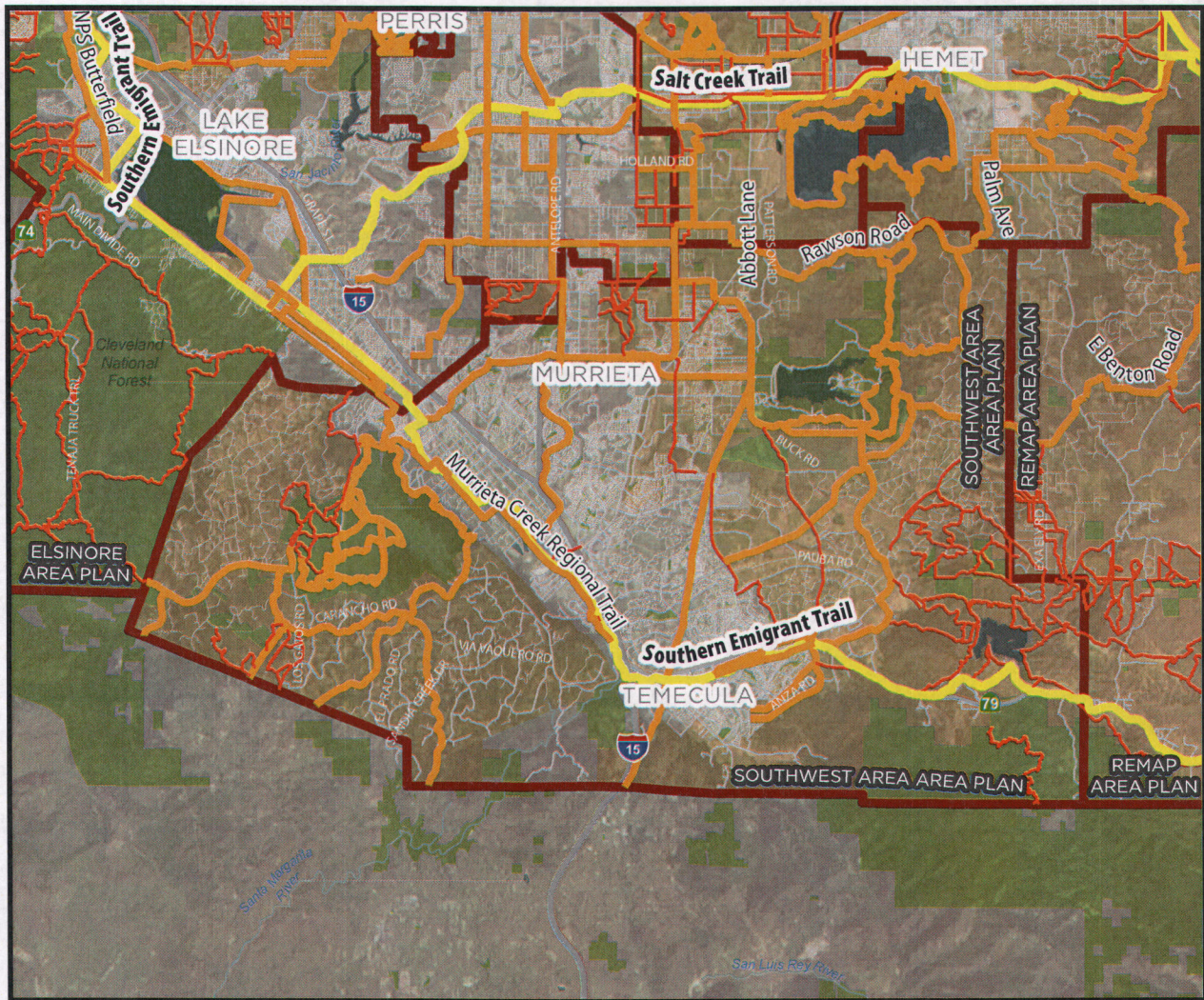
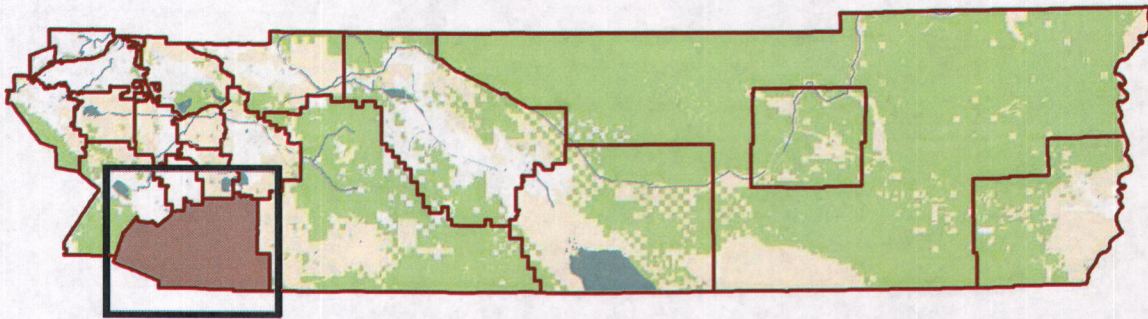
Boundaries

-  Water Bodies
-  County Jurisdiction
-  Parks and Public Lands
-  Area Plan Boundary





### AREA PLAN: SOUTHWEST

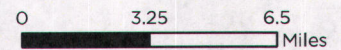


#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

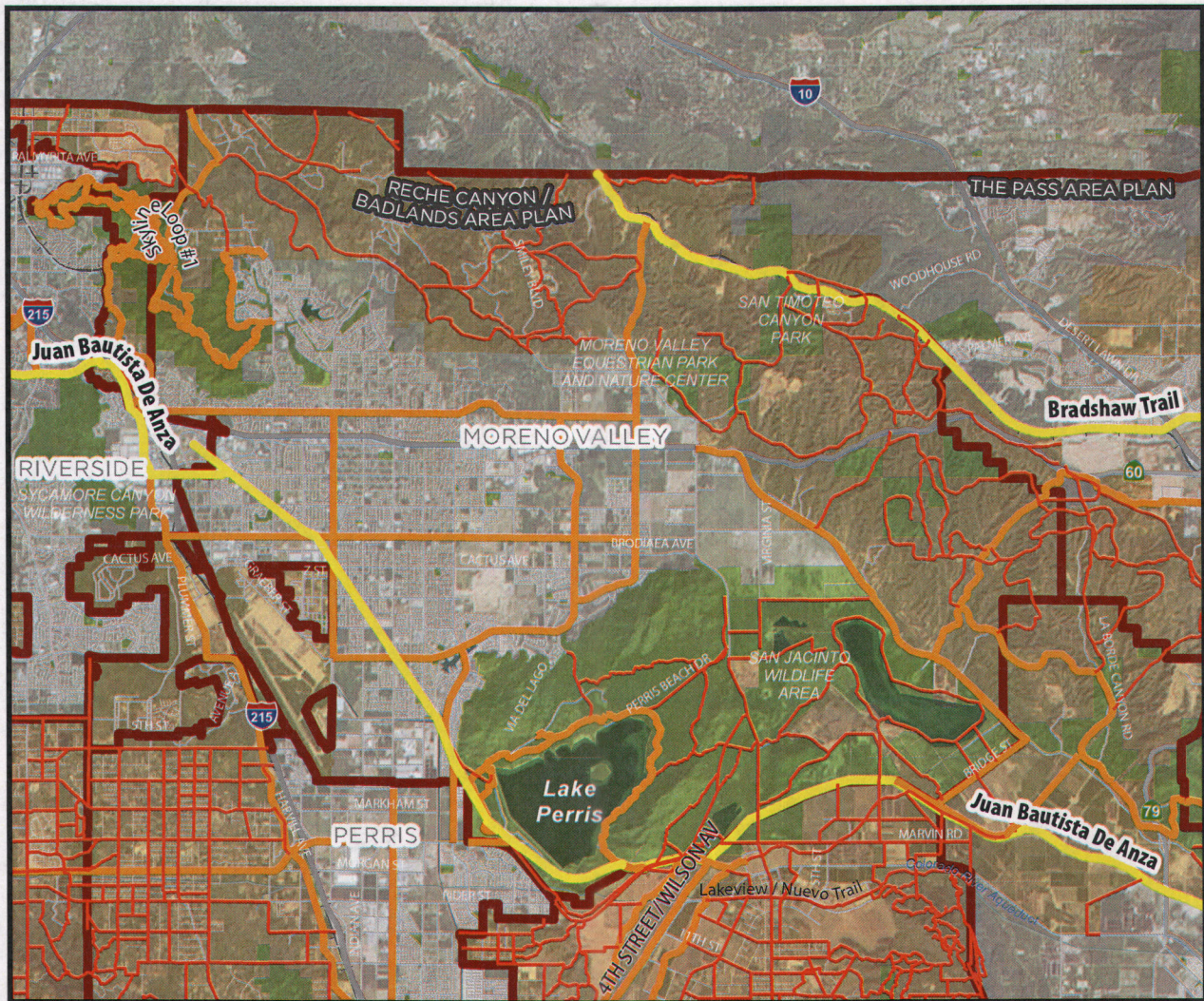
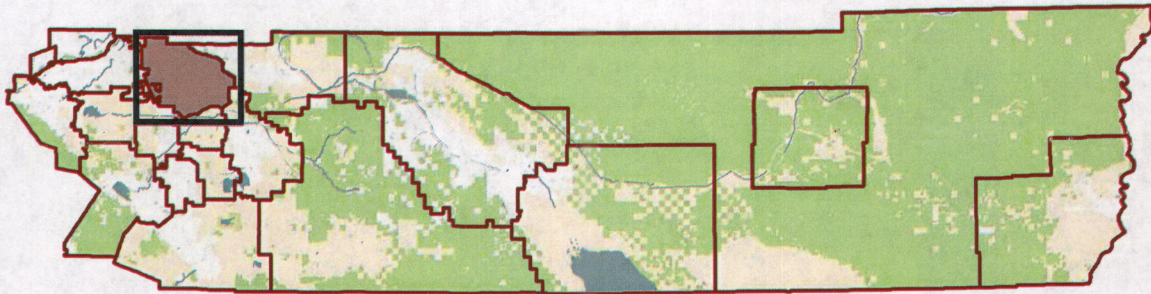
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary

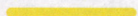






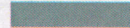



### AREA PLAN: RECHE CANYON/BADLANDS

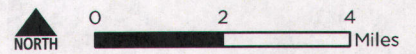


**Trail Network**

-  Tier 1 (Backbone)
-  Tier 2
-  Tier 3

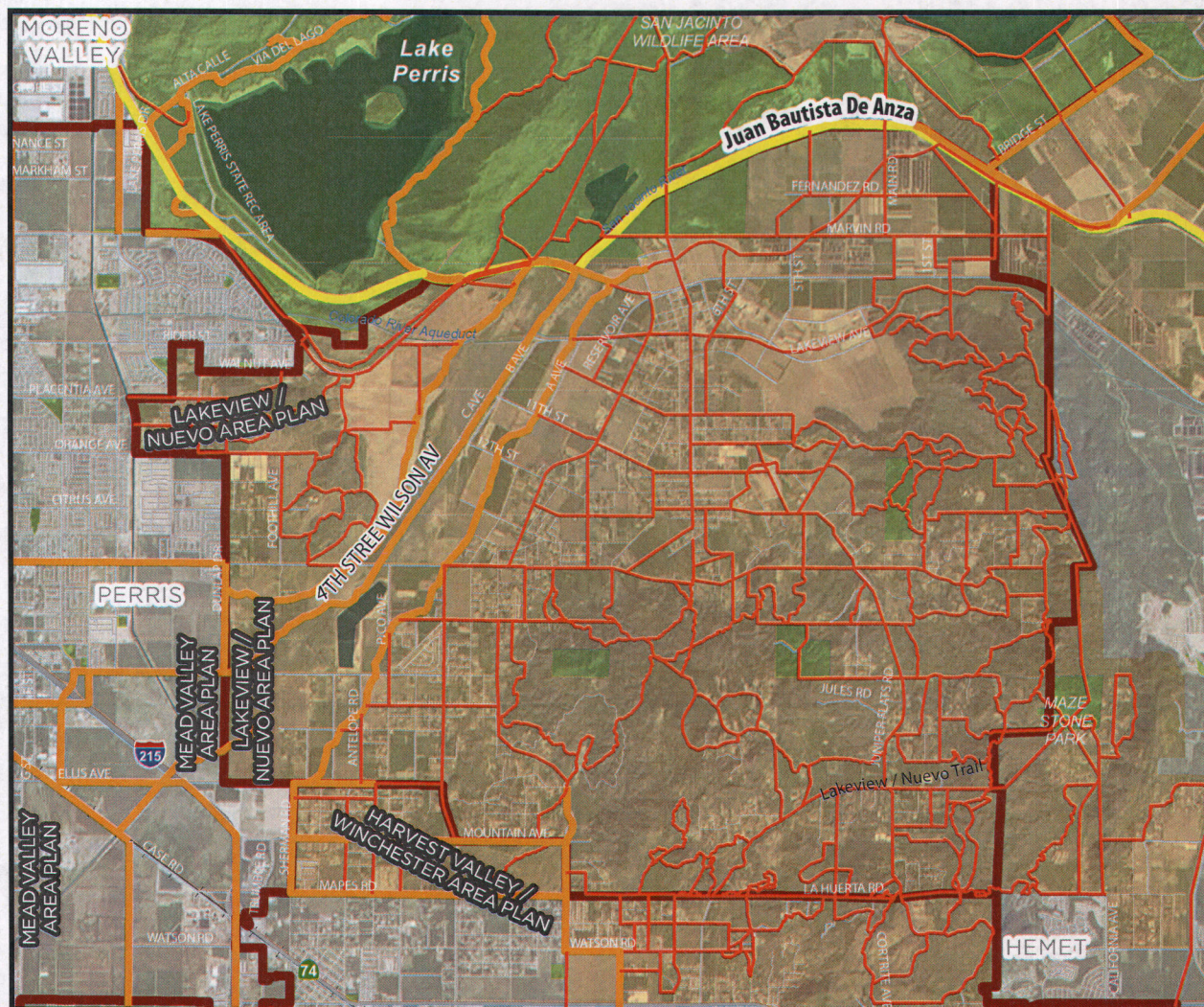
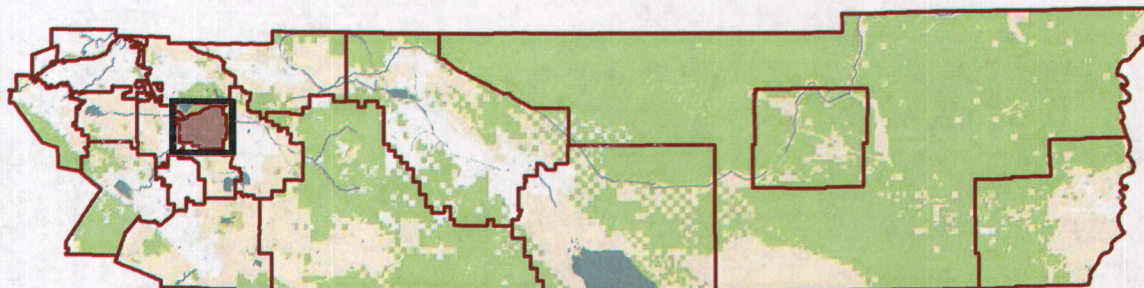
**Boundaries**

-  Water Bodies
-  County Jurisdiction
-  Parks and Public Lands
-  Area Plan Boundary












### AREA PLAN: LAKE VIEW/NUEVO

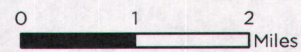


#### Trail Network

-  Tier 1 (Backbone)
-  Tier 2
-  Tier 3

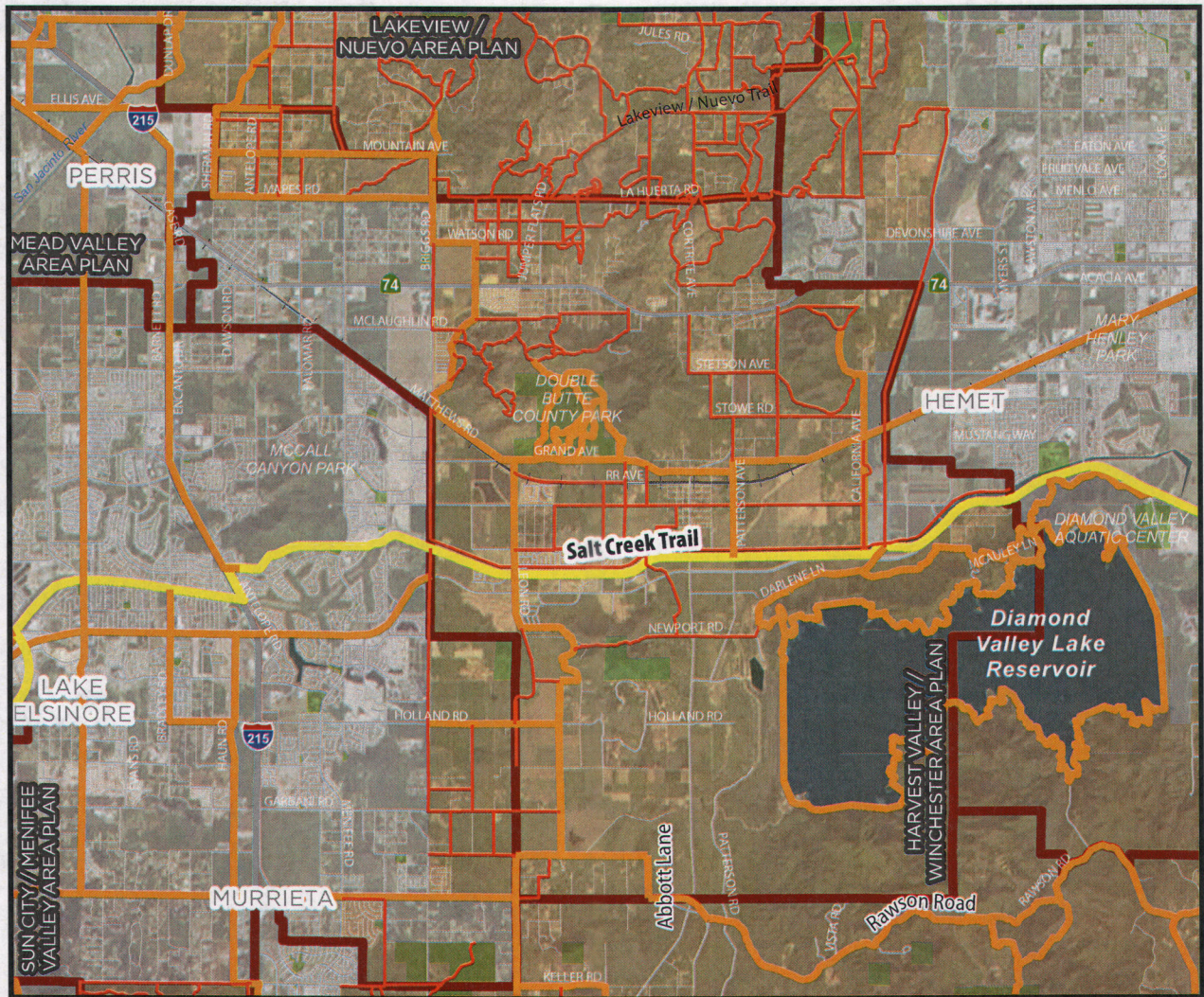
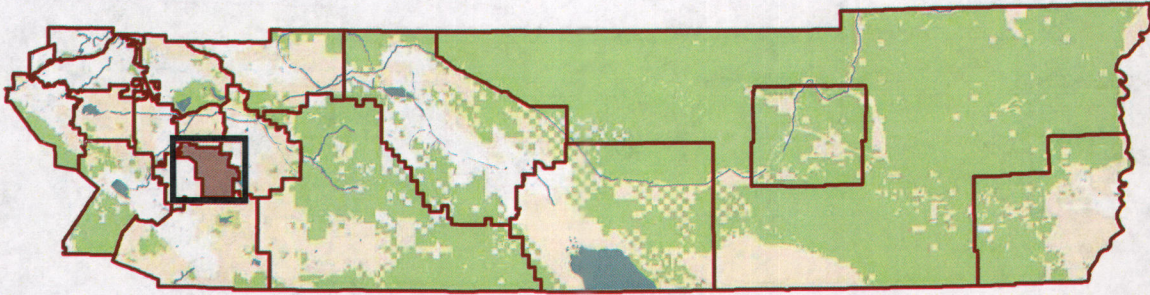
#### Boundaries

-  Water Bodies
-  County Jurisdiction
-  Parks and Public Lands
-  Area Plan Boundary





### AREA PLAN: HARVEST VALLEY/WINCHESTER

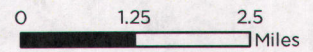


**Trail Network**

- Tier 1 (Backbone)
- Tier 2
- Tier 3

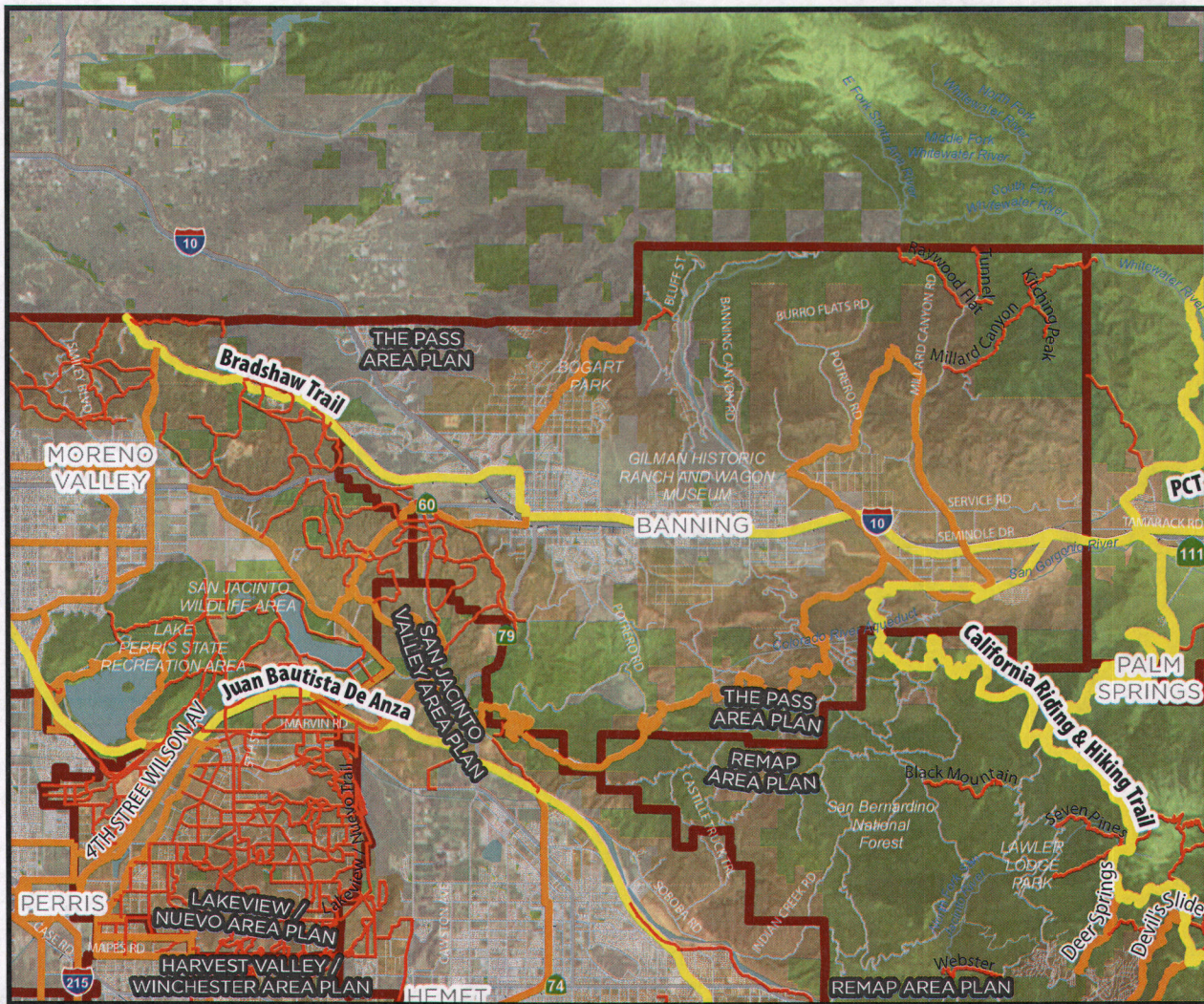
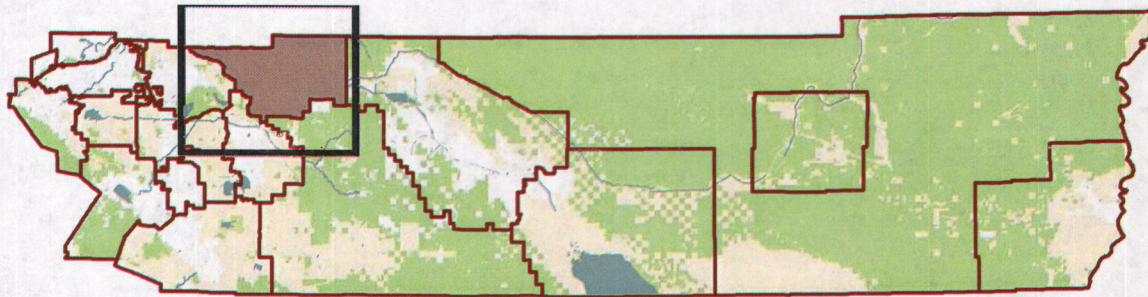
**Boundaries**

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary





### AREA PLAN: THE PASS

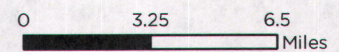


#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

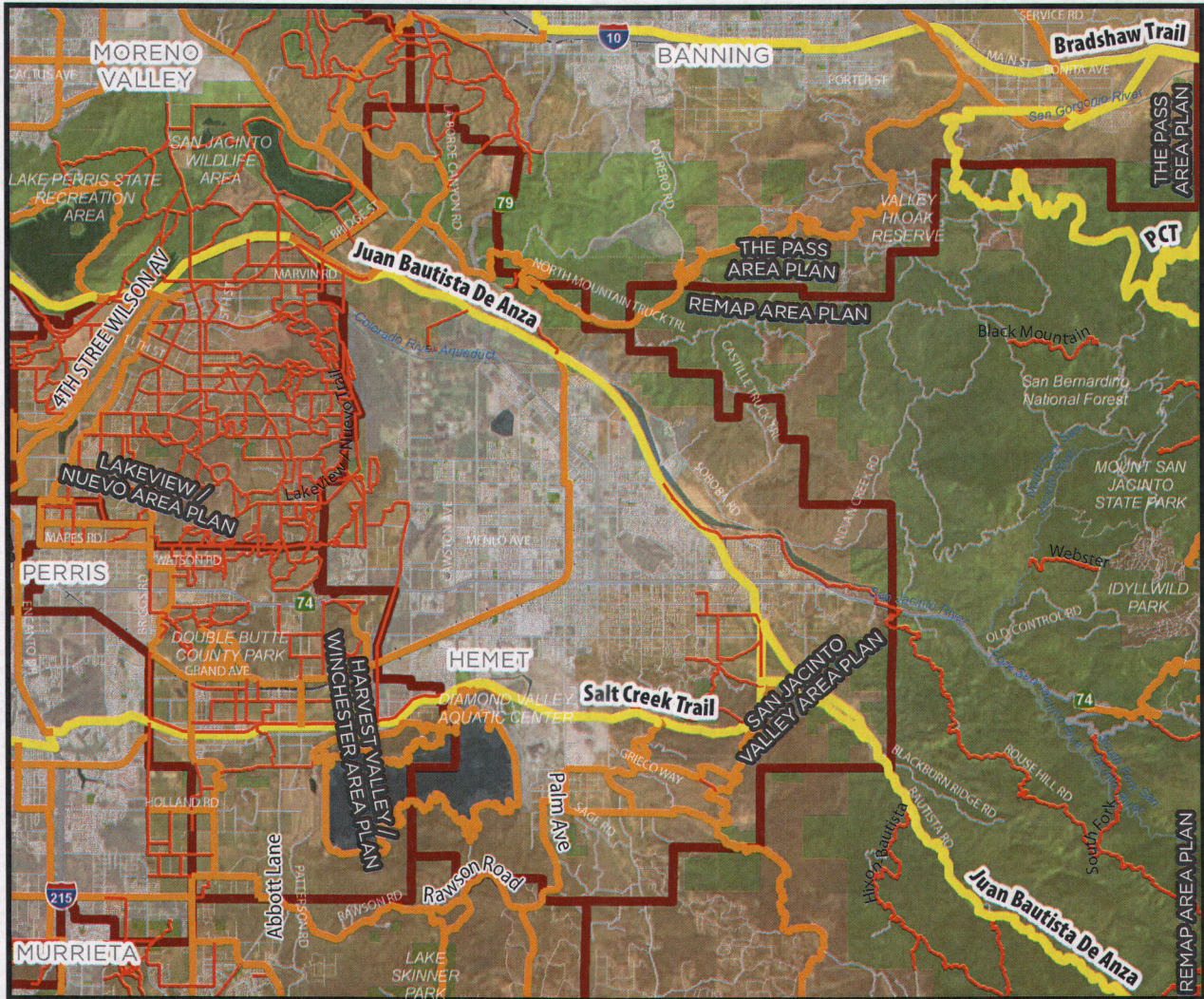
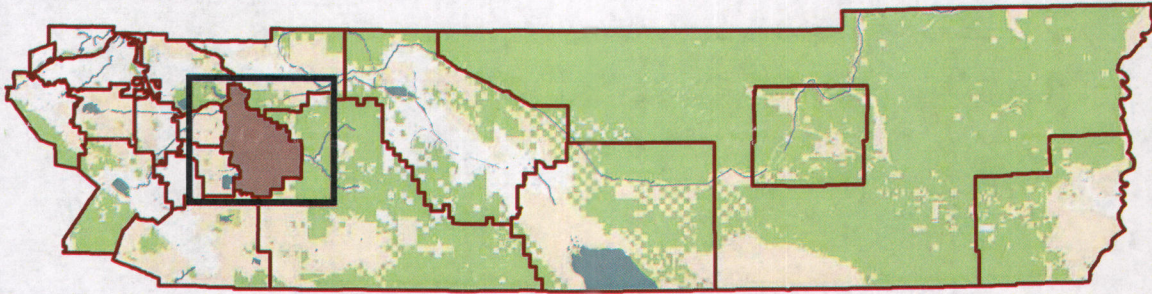
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary





### AREA PLAN: SAN JACINTO VALLEY

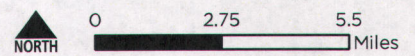


**Trail Network**

- Tier 1 (Backbone)
- Tier 2
- Tier 3

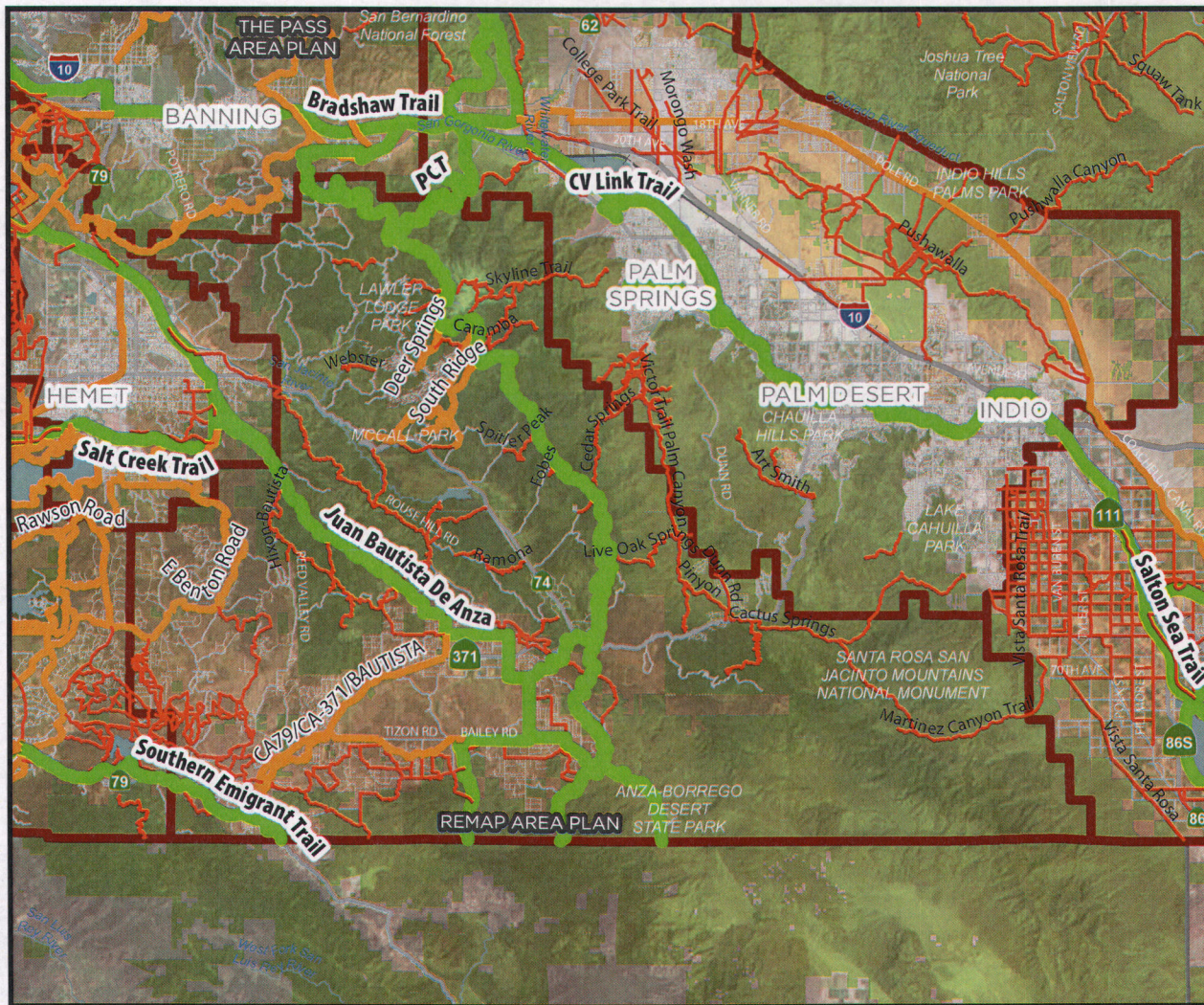
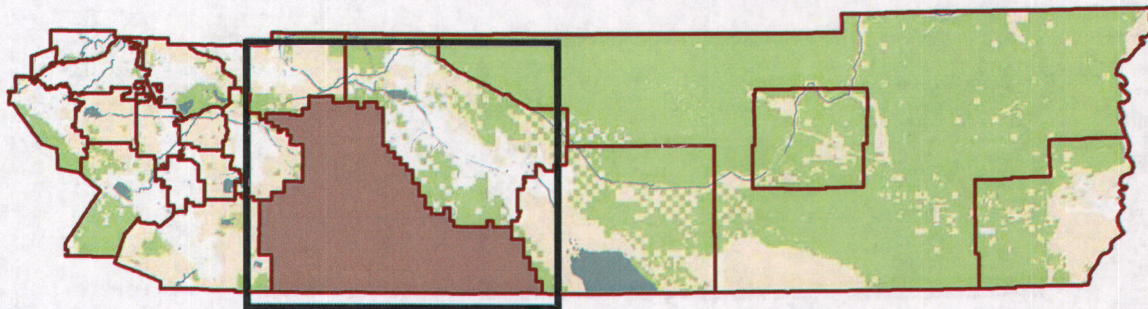
**Boundaries**

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary





### AREA PLAN: REMAP

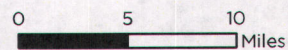


#### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

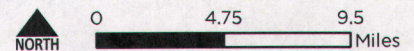
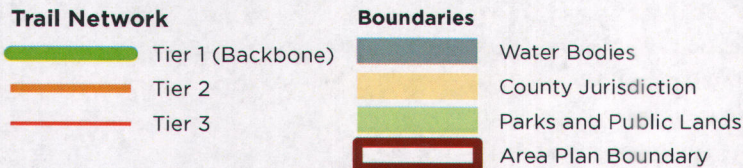
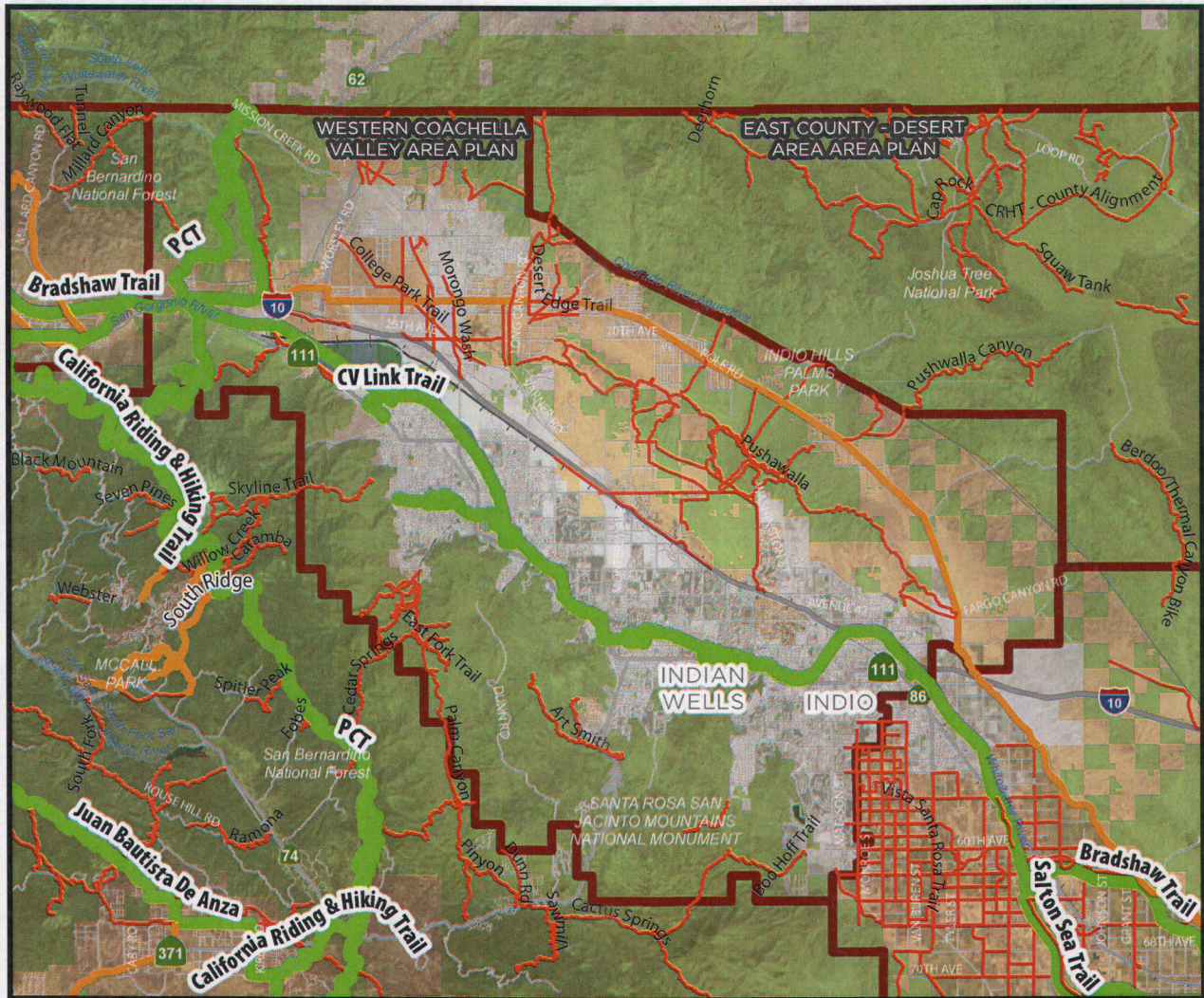
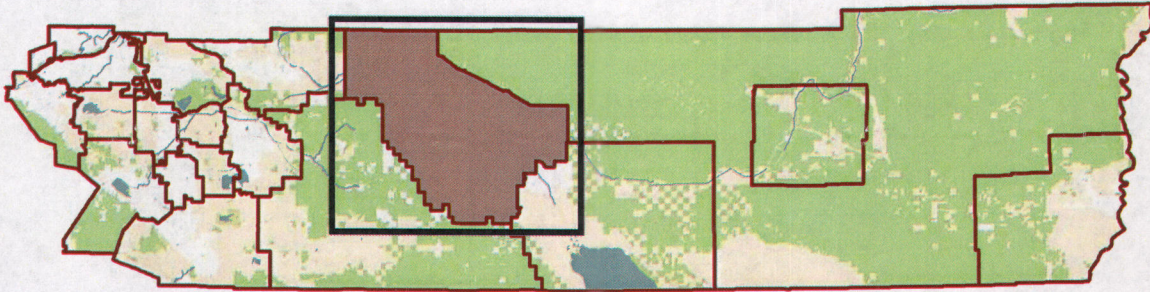
#### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary



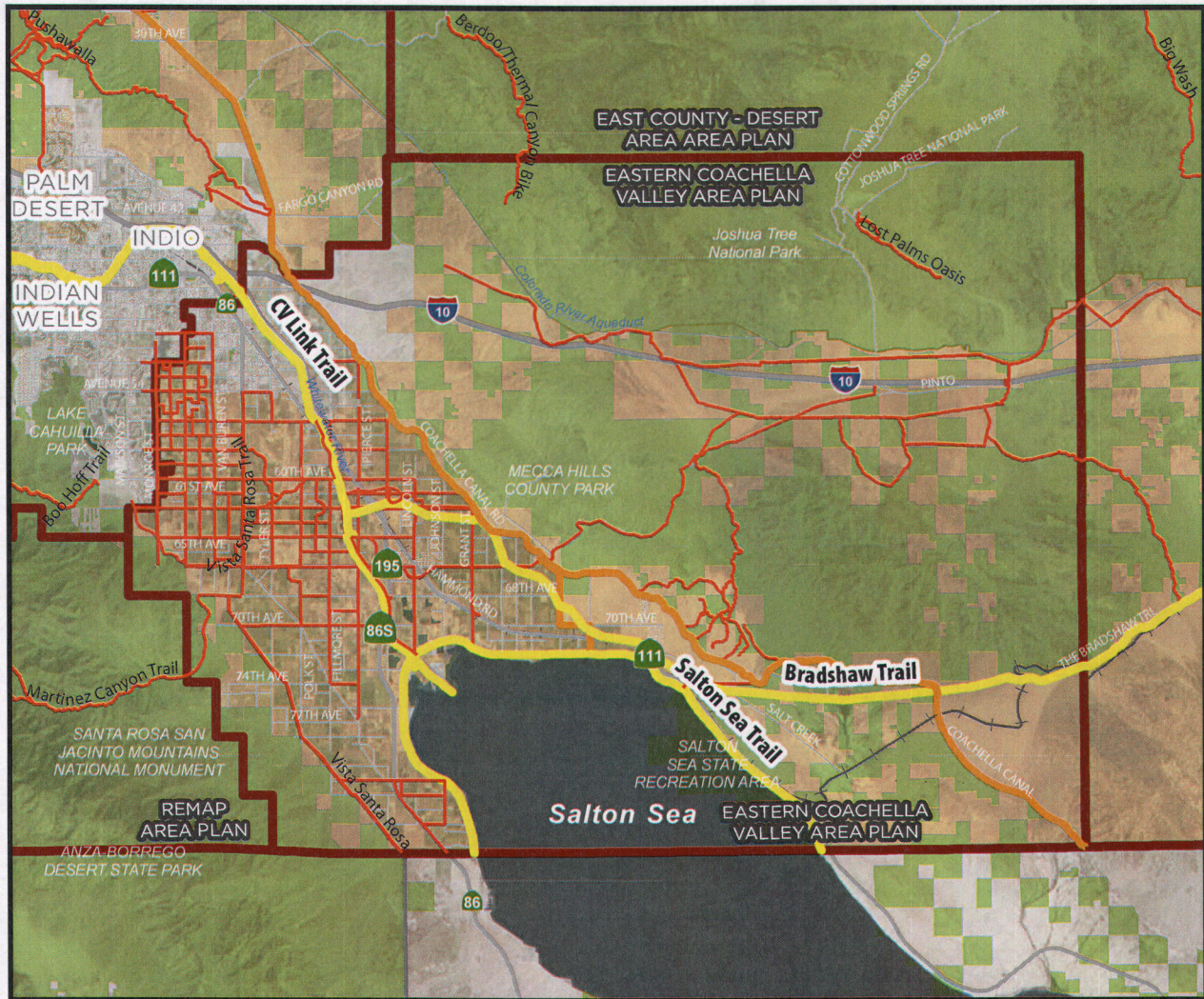
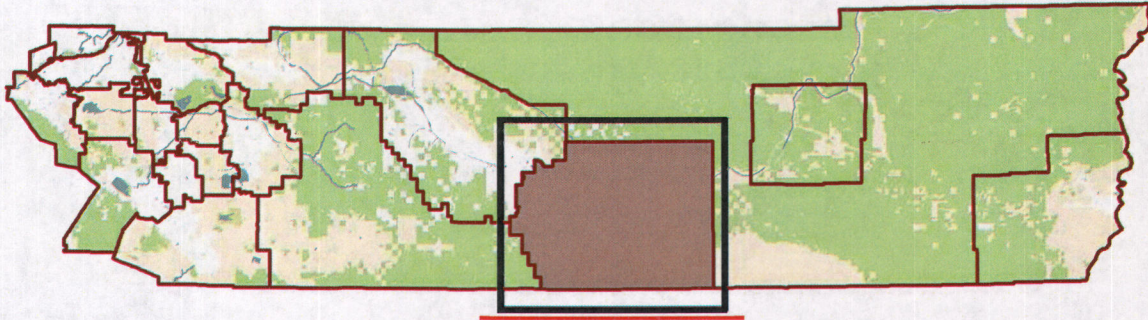


### AREA PLAN: WESTERN COACHELLA VALLEY





# AREA PLAN: EASTERN COACHELLA VALLEY

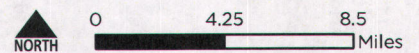


### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

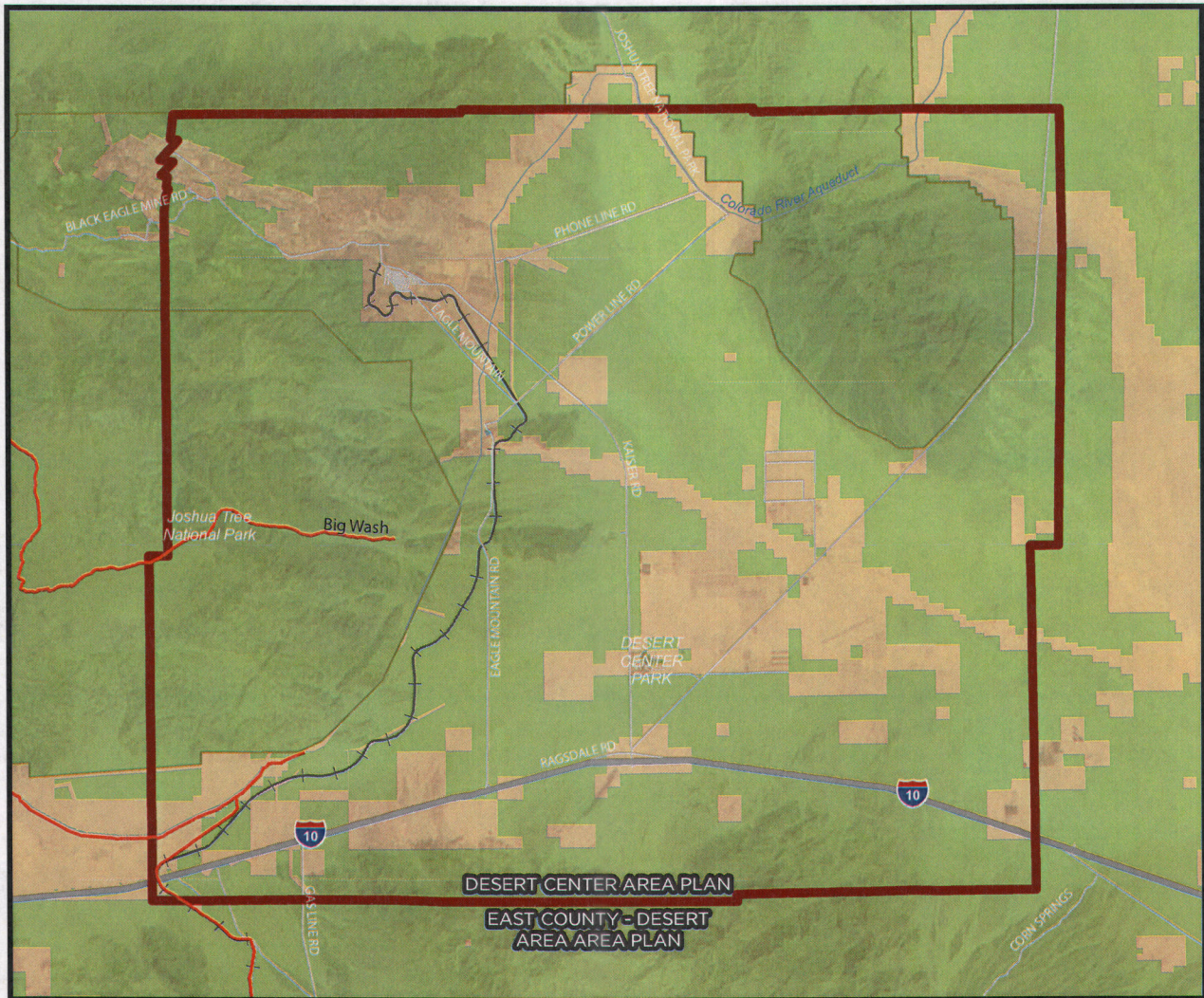
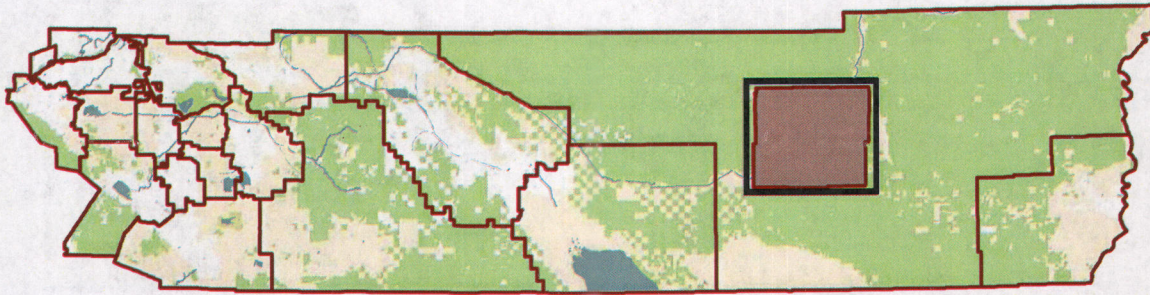
### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary

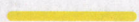










### AREA PLAN: DESERT CENTER

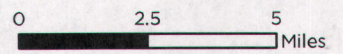


**Trail Network**

-  Tier 1 (Backbone)
-  Tier 2
-  Tier 3

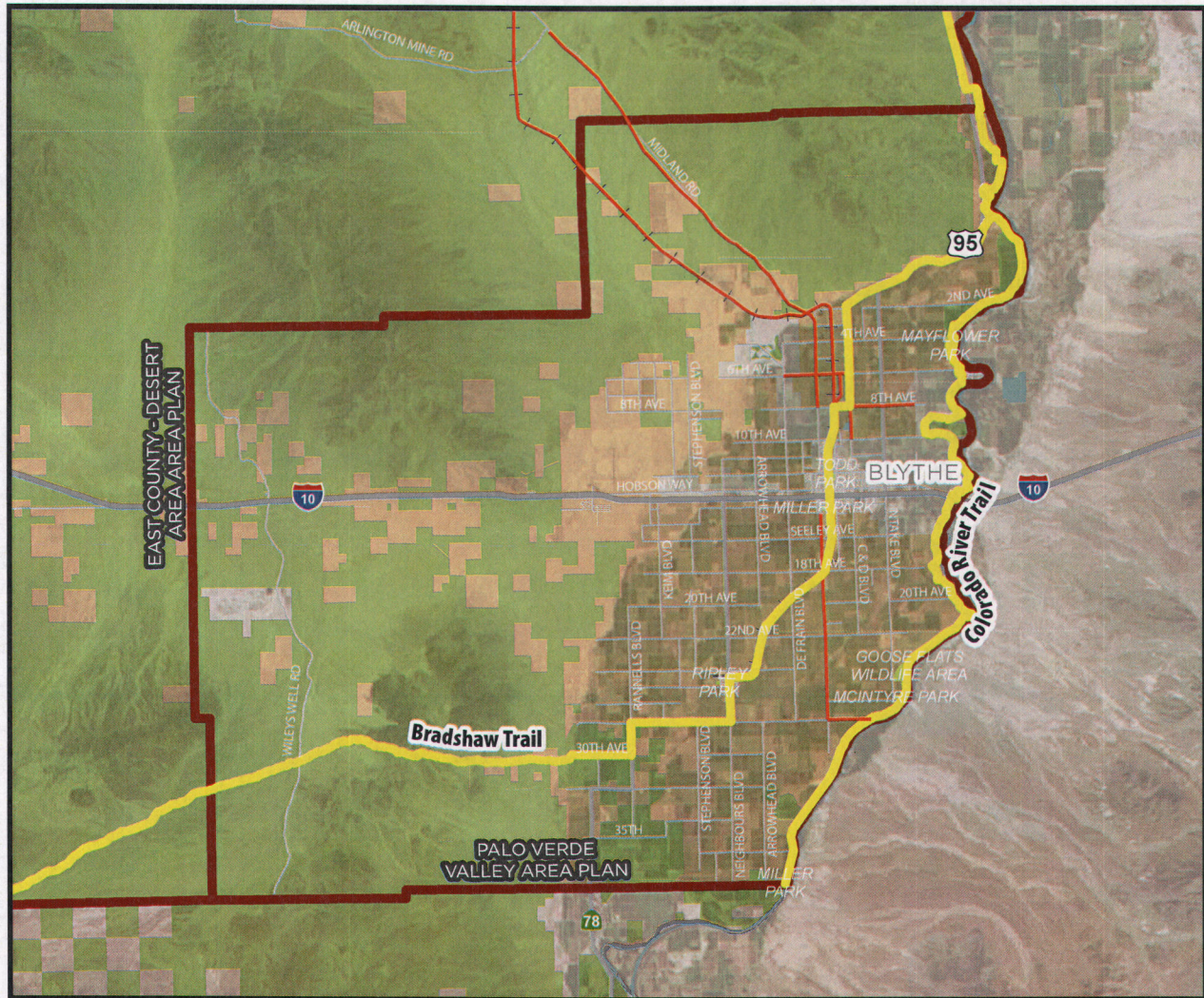
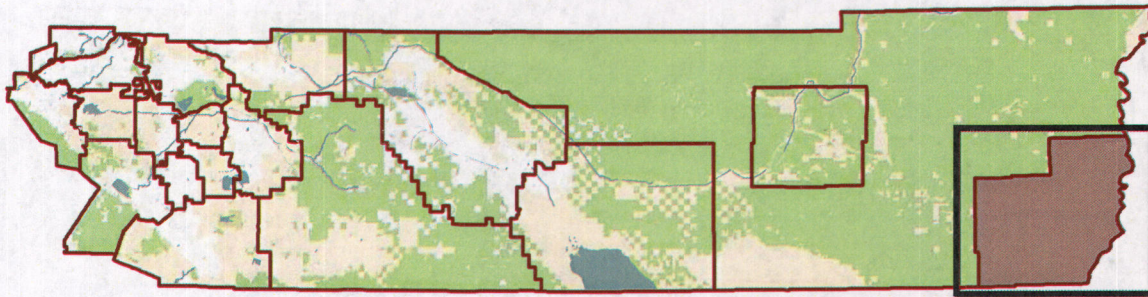
**Boundaries**

-  Water Bodies
-  County Jurisdiction
-  Parks and Public Lands
-  Area Plan Boundary





# AREA PLAN: PALO VERDE VALLEY

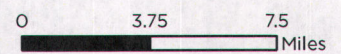


### Trail Network

- Tier 1 (Backbone)
- Tier 2
- Tier 3

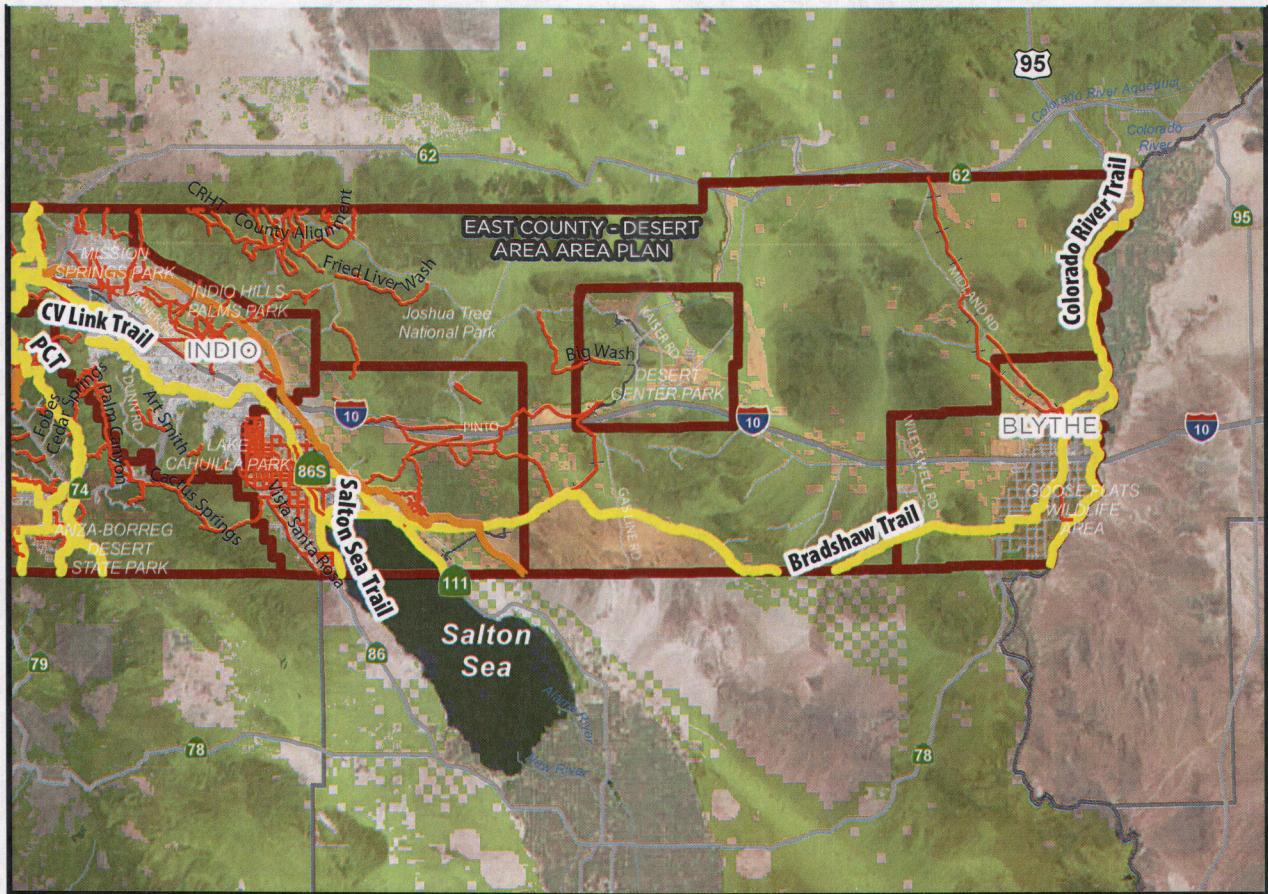
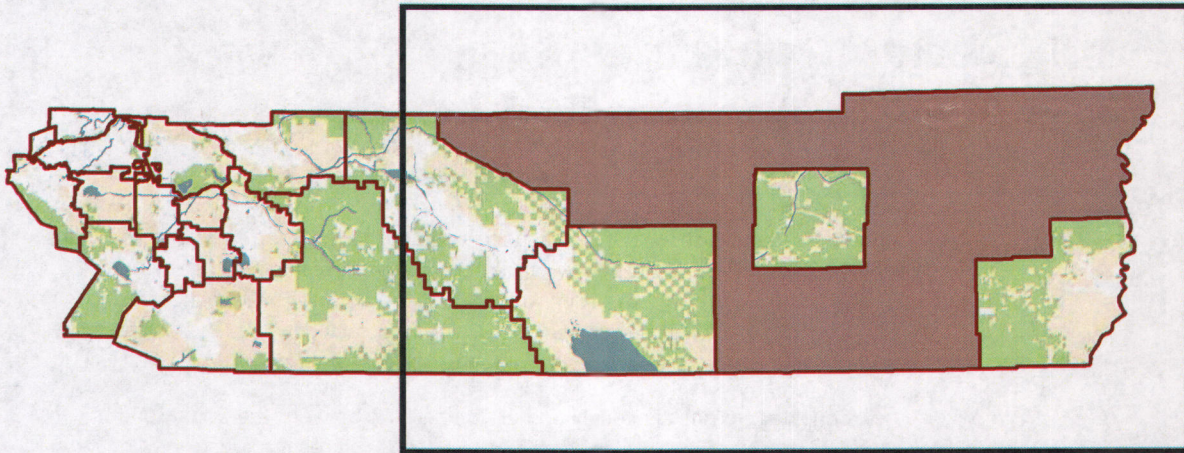
### Boundaries

- Water Bodies
- County Jurisdiction
- Parks and Public Lands
- Area Plan Boundary












### AREA PLAN: EAST COUNTY/DESERT

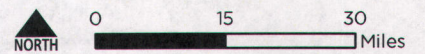


**Trail Network**

-  Tier 1 (Backbone)
-  Tier 2
-  Tier 3

**Boundaries**

-  Water Bodies
-  County Jurisdiction
-  Parks and Public Lands
-  Area Plan Boundary





## Design Guidelines

The following guidelines and cross sections are recommended as updates to the County's existing trail design standards. These sections cover a variety of available easement widths and are intended to serve as the standards upon which the backbone trail network is built. These guidelines will serve as standards for all backbone trails within the County's jurisdiction and are not all-inclusive for every trail-type within the county. Trails which are not backbone trails will continue to be guided by the General Plan when in the County's jurisdiction, or otherwise by the guidelines of the local jurisdiction. The County recommends backbone trails in local jurisdictions adhere to these guidelines but cannot require them to do so. When ROW is too narrow to meet these guidelines, the default option is to use a natural trail. Where trails enter MSHCP areas, trail design is to follow the guidelines developed by MSHCP.

Wherever a backbone trail crosses the county border, compatibility between the County and other jurisdiction's trail design must be evaluated. If the other trail is currently existing, the County should blend from the backbone section into the standard to which the other trail was built. Where possible, the County should recommend the abutting trail is upgraded to match the backbone standard. When meeting another jurisdiction without an existing trail, the County should confer with that jurisdiction on the applicable design standards and encourage the jurisdiction to acquire adequate easements and funding to construct a trail which meets the backbone standards.

### TRAIL FACILITIES

#### Type 1: Class I Bikeway & Side Trail

This facility provides two parallel trails. The Class I bikeway provides bi-directional, off-street bicycle use. It may also be used by electric bikes, rollerbladers, skateboarders, and other modes that require a paved surface. The side trail provides unmarked bi-directional space for pedestrians, wheelchair users, equestrians, and other non-motorized users that may not require a paved trail.

Off-street parallel facilities for bikes and pedestrians provide a high-quality off-street experience for both transportation and recreation, accommodating multiple modes, abilities, and skill levels. A separated bikeway it minimizes potential conflicts between bicyclists and pedestrians / equestrians.

The parallel trails move in tandem and include the following features:

- Frequent access points from the local road network.
- Directional signs to direct users to and from the path.
- User signs to clarify the appropriate modes for each path.
- A limited number of at-grade crossings with streets or driveways.

- Amenity areas located adjacent to the side trail
- Planting or stormwater buffers between the trails

#### Type 2: Shared-Use Path

A shared use path allows for bi-directional, off-street bicycle use and may also be used by pedestrians, skaters, wheelchair users, and other non-motorized users. The shared-use path is less desirable than a Class I bikeway & side trail, but may be appropriate where demand is low or easement is limited. Shared use paths provide a safe off-street facility for recreation and users of all skill levels preferring separation from traffic.

These facilities are frequently found in parks, along rivers, beaches, and in greenbelts or utility corridors where there are few conflicts with motorized vehicles. Path facilities may also include amenities such as lighting, signage, and fencing (where appropriate).

Key features of shared use paths include:

- Frequent access points from the local road network.
- Directional signs to direct users to and from the path.
- A limited number of at-grade crossings with streets or driveways.



## Guidance

### Width

#### Shared-use path / Class I bikeway

- 8 feet is the minimum allowed for a two-way shared use path and is only recommended for low traffic situations.
- 10 feet is recommended in most situations.
- 12 feet is recommended for heavy use situations with high concentrations of users.

#### Side Path

- 4 feet is the minimum allowed for a side path and is only recommended in situations with low traffic or constraints upon right of way.
- 10 feet is recommended where space allows, and will be adequate for moderate to heavy use.

### Lateral Clearance

- A 2 foot or greater shoulder on both sides of all paths should be provided. An additional two foot of lateral clearance to the edge of any vertical object and 5 foot shoulder are recommended by the MUTCD for the installation of signage or other furnishings.

### Overhead Clearance

- Clearance to overhead obstructions should be 10 feet minimum for non-equestrian trails and 12 feet minimum for equestrian trails

### Striping

- When striping is required, use a 4-inch dashed yellow centerline stripe with 4-inch solid white edge lines.
- Solid centerlines can be provided on tight or blind corners, and on the approaches to roadway crossings, curb cuts, and ramps

## Materials and Maintenance

Facility		Easement	Path Width			Path Materials
		Min	Min	Typical	High-use	
Class I Bikeway & Side Trail	Bikeway	20'	8'	10'	12'	Asphalt / concrete
	Side Trail		4'	10'		Compacted native soil / decomposed granite
Shared Use Path		12'	8'	10'	12'	Asphalt / concrete

*Design guidelines for trail types*

#### Shared-use path / Class I bikeway

- Asphalt is the most common surface for bicycle paths. The use of concrete for paths has proven to be more durable over the long term. Saw cut rather than troweled joints improve the user experience.

#### Side Path

- Compacted native soil or decomposed granite are both low-impact materials suitable for most non-bicycle trails. Decomposed granite is the preferred surface for trails with high activity and equestrian activity.

### Amenity Areas

- Where easement width allows, amenity areas may include seating, picnic tables, water fountains, interpretive signage, shade structures, and bike racks and repair stations

### Separations

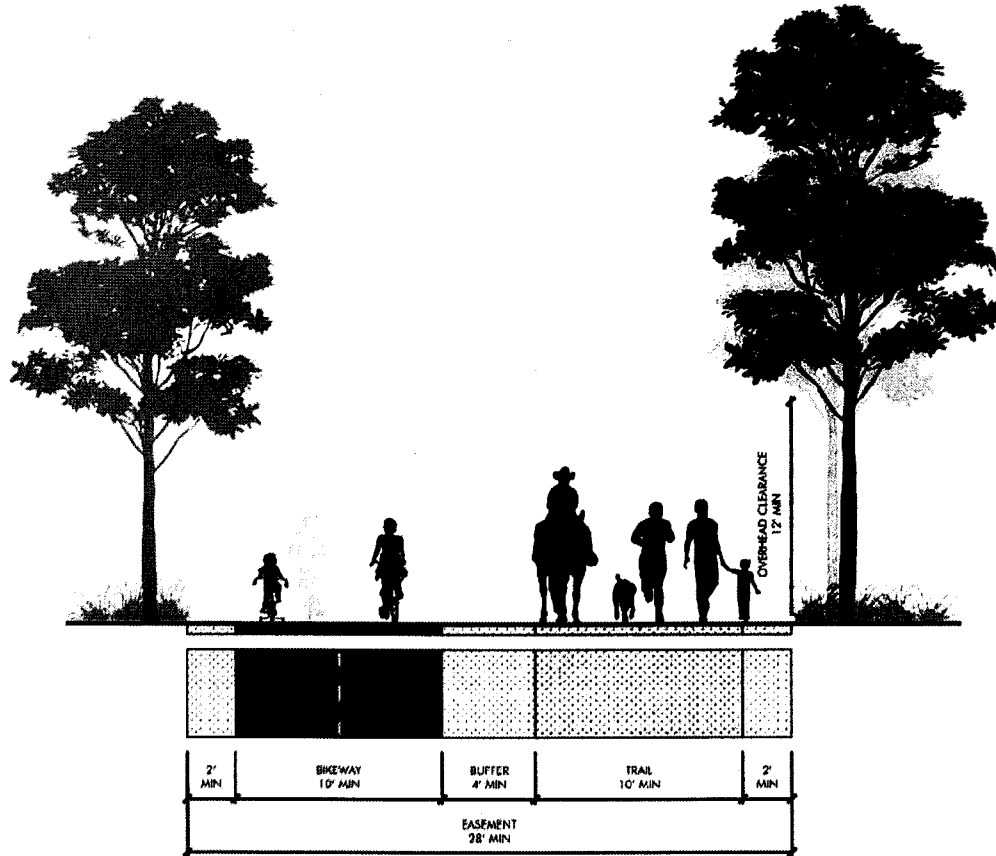
- Fencing should be used between the paved and unpaved portions of a trail wherever the unpaved portion of trail falls below 10' or where the trail is adjacent to a roadway.
- Vinyl rail or split rail lodgepole fencing should be used where needed, and as fitting the context. Vinyl rail tends to be better suited to developed areas, while lodgepole is more suitable for remote and natural areas.

### Discussion

- Terminate the path where it is easily accessible to and from the street system, preferably at a controlled intersection or at the beginning of a dead-end street.



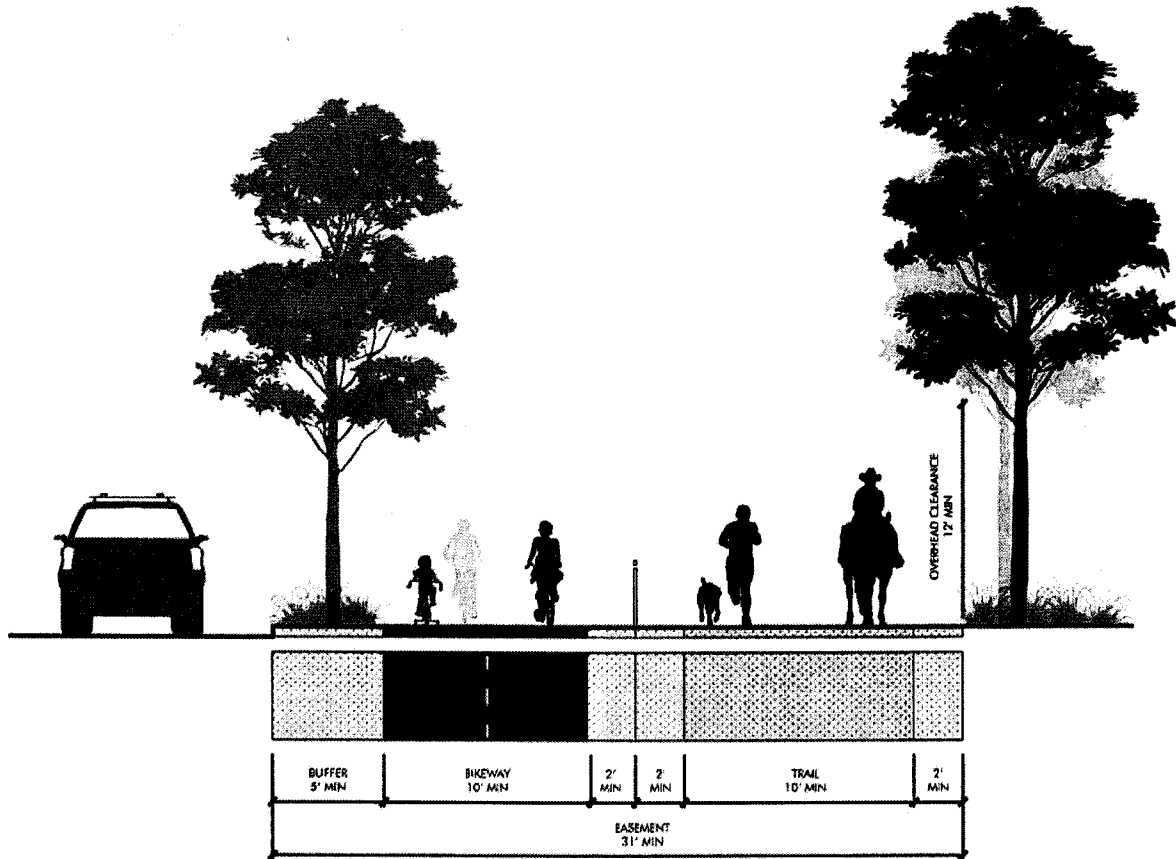
### BACKBONE TRAIL CROSS SECTIONS



*Typical Section: Class I Bikeway w/ Adjacent Side Trail*

- Minimum Easement:** 28'
- Bikeway Surface:** Asphalt Concrete or Portland Cement/Aggregate Mixture
- Bikeway Width:** 10'
- Bikeway Shoulders:** 2' Min
- Bikeway/Trail Separation:** 4' Min, Unpaved
- Trail Surface:** Compacted native soil or decomposed granite
- Trail Width:** 10'

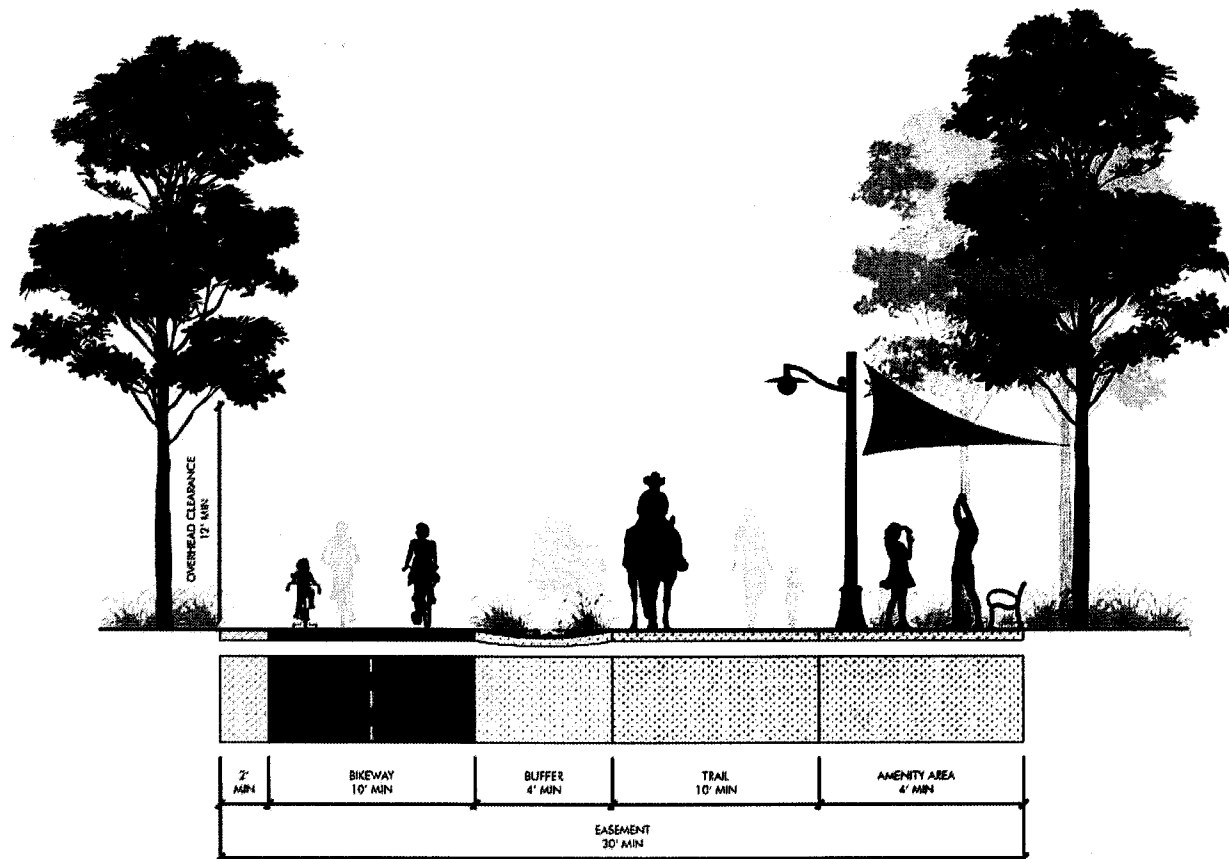




*Typical Section: Class I Bikeway w/ Adjacent Side Trail, Roadway Adjacent*

<b>Minimum Easement:</b>	31'
<b>Bikeway Surface:</b>	Asphalt Concrete or Portland Cement/ Aggregate Mixture
<b>Bikeway Width:</b>	10'
<b>Bikeway/Road Separation:</b>	5' Min
<b>Bikeway/Trail Separation:</b>	4' Min, Unpaved
<b>Trail Surface:</b>	Compacted native soil or decomposed granite
<b>Trail Width:</b>	10'

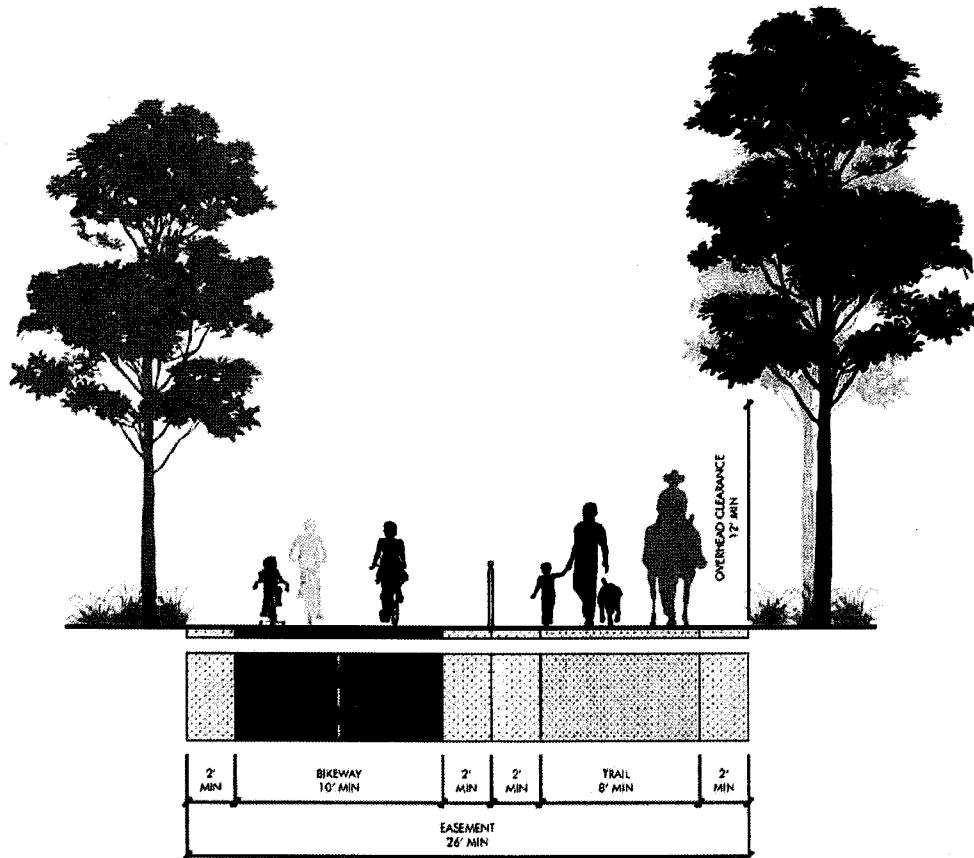




*Section: Class I Bikeway w/ Buffered Side Trail, Wide Easement*

- Minimum Easement:** 30'
- Bikeway Surface:** Asphalt Concrete or Portland Cement/Aggregate Mixture
- Bikeway Width:** 10'
- Bikeway/Trail Separation:** 4' Min, Unpaved
- Trail Surface:** Compacted native soil or decomposed granite
- Trail Width:** 10'
- Amenity Area:** 4' Min

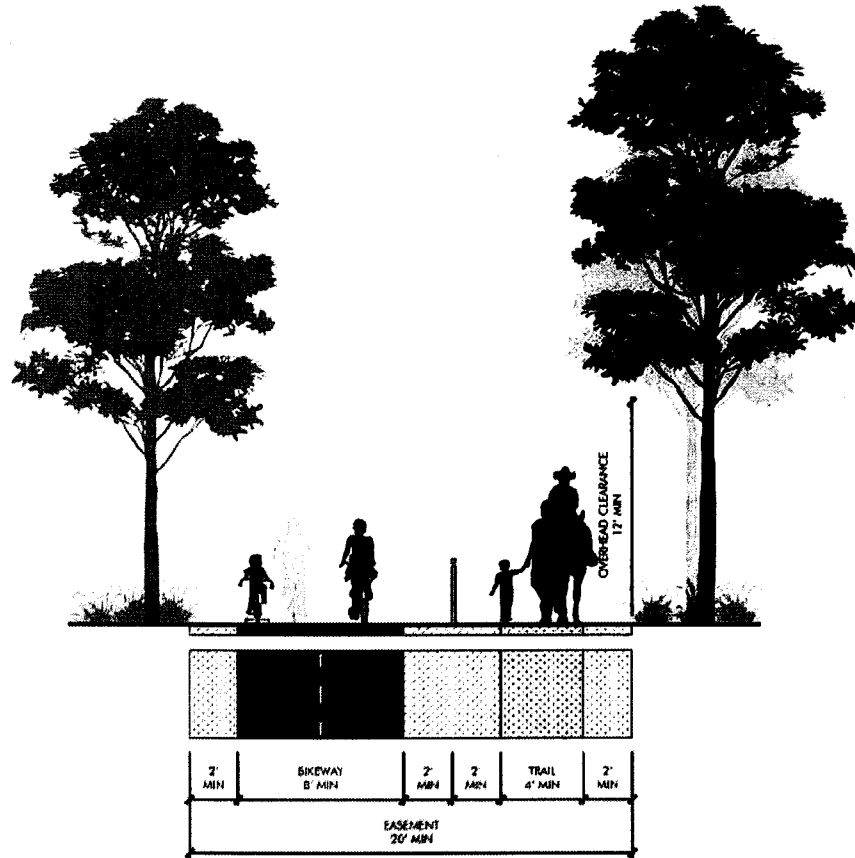




*Typical Section: Class I Bikeway w/ Side Trail and Fence*

<b>Minimum Easement:</b>	26'
<b>Bikeway Surface:</b>	Asphalt Concrete or Portland Cement/ Aggregate Mixture
<b>Bikeway Width:</b>	10'
<b>Bikeway/Trail Separation:</b>	2' Minimum with split rail fence
<b>Trail Surface:</b>	Compacted native soil or decomposed granite
<b>Trail Width:</b>	10'

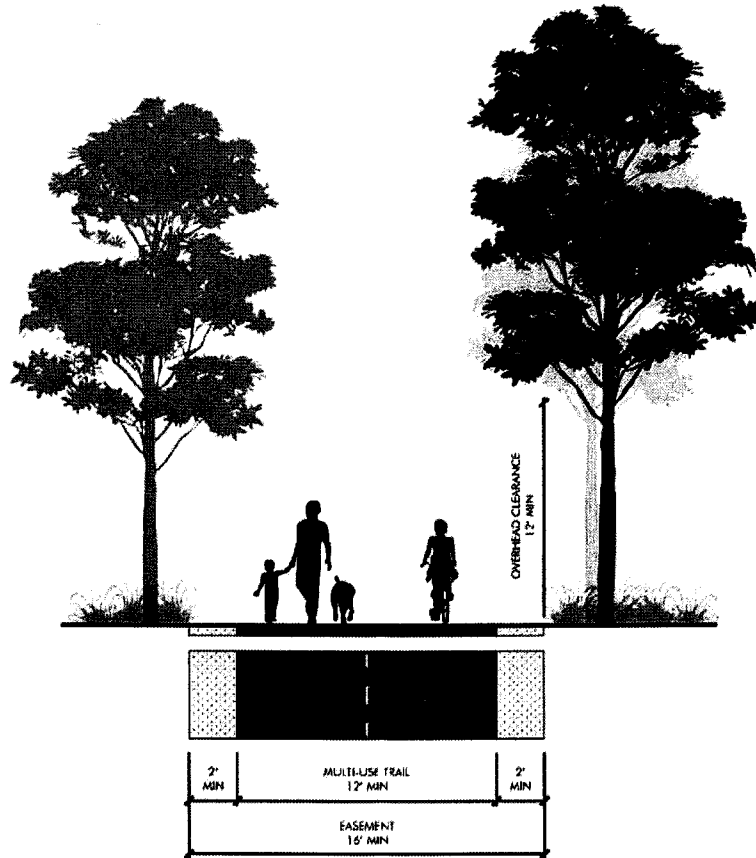




*Typical Section: Class I Bikeway w/ Side Trail and Fence, Constrained*

- Minimum Easement:** 20'
- Bikeway Surface:** Asphalt Concrete or Portland Cement/Aggregate Mixture
- Bikeway Width:** 10'
- Bikeway/Trail Separation:** 4' Min, with split rail fence
- Trail Surface:** Compacted native soil or decomposed granite
- Trail Width:** 4' Min





*Typical Section: Shared-Use Path, Constrained*

- Minimum Easement:** 14'
- Bikeway Surface:** Asphalt Concrete or Portland Cement/Aggregate Mixture
- Bikeway Width:** 10'
- Bikeway Shoulders:** 2' Minimum



## INTERSECTIONS AND CROSSINGS

At-grade roadway crossings can create potential conflicts between trail users and motorists. However, well-designed crossings can mitigate many operational issues and provide a higher degree of safety and comfort. In most cases, at-grade trail crossings can be properly designed to provide a reasonable degree of safety and can meet existing traffic and safety standards. Typically, trail facilities for bicyclists require additional considerations due to the higher travel speed of bicyclists versus other trail users.

Special consideration must be given when delineating at-grade trail crossings. The sign types, pavement markings, and treatments will vary based on the roadway type. Proper signage and pavement markings alerting trail users of at-grade crossings must also be utilized.

### At grade crossings:

- Type 1: Marked/Unsignalized - Unprotected crossings include trail crossings of residential, collector, and sometimes major arterial streets or railroad tracks.
- Type 1+: Marked/Enhanced – Flashing beacons and other treatments can provide additional visibility at unsignalized crossings.
- Type 2: Direct Users to Existing Intersection - Trails that emerge near existing intersections may be routed through those intersections, provided that the crossing provides sufficient protection for nonmotorized users.
- Type 3: Signalized/Controlled - Trail crossings that require signals or other control measures due to traffic volumes and speeds.

### Grade-separated crossings:

- Bridges or under-crossings provide the maximum level of safety but also generally are the most expensive and have right-of-way, maintenance, ADA accessibility, and other public safety considerations.

### Discussion

While at-grade crossings create a potentially high level of conflict between path users and motorists, well designed crossings have not historically posed a safety problem for path users. This is evidenced by the thousands of successful paths around the United States with at-grade crossings.

Evaluation of path crossings involves analysis of vehicular and anticipated path user traffic patterns, including:

- Vehicle speeds.
- Street width.
- Sight distance.
- Traffic volumes (average daily traffic and peak hour traffic).
- Path user profile (age distribution, destinations served).

Crossing features for all roadways include warning signs both for vehicles and path users.

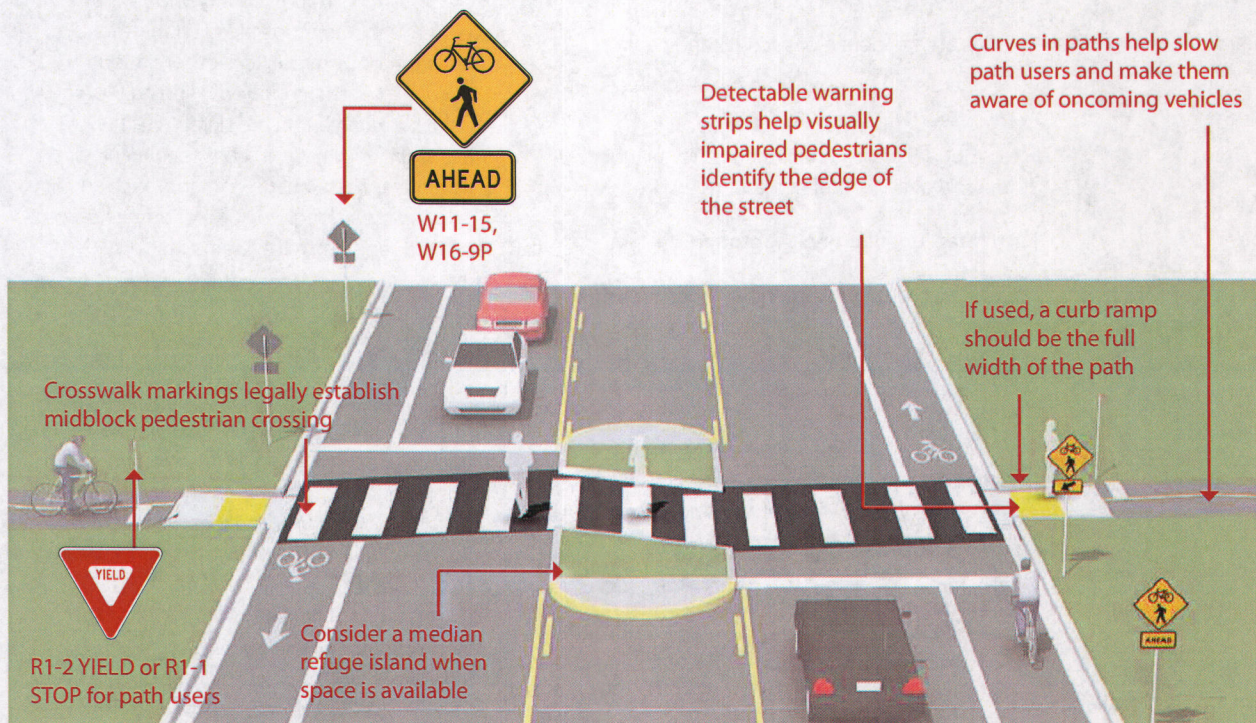


Consideration must be given for adequate warning distance based on vehicle speeds and line of sight. Catching the attention of motorists may require additional alerting devices such as a flashing light, roadway striping or changes in pavement texture. Signing for path users may include a "STOP" or "YIELD" sign and pavement markings. Care must be taken not to place too many signs at crossings lest they begin to lose their visual impact.

A number of striping patterns have emerged over the years to delineate path crossings. A median stripe on the path approach will help to organize and warn path users.

Crosswalk striping is typically a matter of local and State guidelines, and may be accompanied by pavement treatments to help warn and slow motorists.

*Additional Reference: Highway Administration (FHWA) Report, "Safety Effects of Marked vs. Unmarked Crosswalks at Uncontrolled Locations."*





## Wayfinding

Comprehensive and innovative map, marking, and signing systems (collectively “wayfinding”) helps to make trail networks more accessible and desirable. An overarching signage and directional system for the Riverside County trails system will inform and educate users to help them find their way to, from, and along trails. A good wayfinding plan requires an accurate understanding of the regional trail system: its routes, trail types, jurisdictions, destinations, origins, users, and the needs and abilities of those who maintain, manage, and provide emergency services for the trail.

The benefits of a county-wide wayfinding system include:

- Improved awareness of the trail networks
- Enhanced legibility for the public to find and follow the trail
- A greater sense of security and comfort
- Increased numbers of bicycle and walking trips for transportation and recreation
- Better agency and inter-agency planning, coordination and management

### An Inter-jurisdictional Approach

The wayfinding system for Riverside County does not replace or superseded local or regional trail wayfinding systems. Instead, it is generic enough to serve as an overarching framework that can be adopted by local municipalities. The design of the signage is intended to provide space for both county-wide and local branding.



In this Section:

- **Principles:** overarching fundamentals to a strong wayfinding network
- **Existing Plans & Signage:** an inventory of existing wayfinding signage within Riverside County
- **Precedents:** a review of California systems to provide guidance
- **Elements:** type of signs that comprise the wayfinding network
- **Design:** mock-ups and discussion of graphic design elements for signage

## PRINCIPLES



**1: COHESION.** Though county-wide trails pass through numerous jurisdictions, the user experience needs to feel continuous and cohesive. The county wayfinding system should be reliable and predictable with a standardized format that transcends municipal boundaries. The signage and approach should be adaptable enough to integrate the many local and regional trail systems that already exist. It should serve as a guide for all local municipalities to follow or as a template to adopt.



**2: CONNECT PLACES.** The fundamental function of the wayfinding system is to connect people to routes and destinations. Without the use of the Internet, visitors and locals should be able to navigate to major regions, cities, trails, destinations, businesses, and neighborhoods, along the trails. The wayfinding needs to properly communicate current locations, entry into new areas, distances, directions, amenities, and historical/cultural information (where appropriate). Effective wayfinding has the added ability to improve local economic well-being by increasing visitor traffic to key business areas.





3: IDENTITY. By being a reflection of local community values, wayfinding elements can cultivate a sense of pride in one's community resulting in a deeper connection to place. A strong wayfinding identity makes the trail system more recognizable and memorable to locals and visitors alike. The wayfinding system should include custom designs and graphics that celebrate and differentiate the Riverside County trails system.



4: BE PREDICTABLE. When information is predictable, it can be quickly understood. Predictability should relate to all aspects of wayfinding information, from the placement of a sign, to the design and its contents. This allows users to quickly understand new situations. Once users trust that they will encounter consistent and predictable information, their level of comfort is raised. This helps promote an arrival and navigational experience that is welcoming and low-stress.



5: KEEP IT SIMPLE. Information should be presented in a clear and logical form. Wayfinding signage should be both universal and usable for the widest possible demographic. The use of branded trails or color coded districts and destination specific symbology is encouraged. Too much information can become challenging to interpret and can unnecessarily complicate a journey. The longer it takes a user to interpret the information presented, the less likely it is that they will use or rely on the system in the future. **Wayfinding in Municipal Plans**

## EXISTING WAYFINDING ANALYSIS

A county-wide wayfinding plan does not supersede or replace local jurisdictional wayfinding plans. It serves to complement,

integrate, or (when no wayfinding plan is provided) provide local guidance.

A survey of general, transportation, parks, and trails plans in Riverside County illuminates a minimal consensus and a general lack of guidance when it comes to wayfinding. Aside from select larger cities (such as Riverside and Perris), most municipalities do not provide any specifics. Those that do focus on internal trails and do not provide guidance for inter-jurisdictional wayfinding. They also tend not to stray too far from MUTCD standards, though some provide a bit of local flair.

The following municipal / regional trail plans contain brief sections on wayfinding:

- Box Springs Mountain Reserve Comprehensive Trails Master Plan (2015)
- Coachella Valley Association of Governments Non-Motorized Transportation Plan (2010)
- Pacific Crest National Scenic Trail Comprehensive Plan (1981)
- Santa Ana River Parkway Minimum Sign Guidelines (2011)
- City of Calimesa Multi Use Trail Manual (2006)
- City of Moreno Valley Bicycle Master Plan (2014)
- City of Perris Trail Master Plan (2013)
- City of Riverside Bicycle Master Plan (2013)

The county plans identify the need for cohesive trail signage and wayfinding, but lack concrete details. The most comprehensive regional wayfinding plan within the County is for the Santa Ana Trail, which provides a basic strategy, types of signs, and some design / graphics.



*Some jurisdictions incorporate extensive local "branding" in their signage design. Shown: wayfinding concepts for City of Perris*



## PRECEDENT ANALYSIS

### Ventura County Regional Bikeway Wayfinding

Similar to Riverside County, Ventura County trails cross a variety of jurisdictional boundaries. In order to maintain a consistent wayfinding language, the plan proposes that each sign feature a bike symbol with the County name. On top of each sign, "supplemental plaques" with names and logos of specific routes may be added to add clarity. Though predominantly for on-road use, the plan proposes types of signs: decision (destination) signs, confirmation signs, and turn signs. Each sign is a slightly modified MUTCD standard.

Best Practices:

- Based upon MUTCD standards with the addition of the County logo
- A hierarchal system with three types of signs: decision, confirmation and turn signs
- Supplemental plaques allow each sign to have local identifiers as well

### San Diego Regional Bike Network Wayfinding

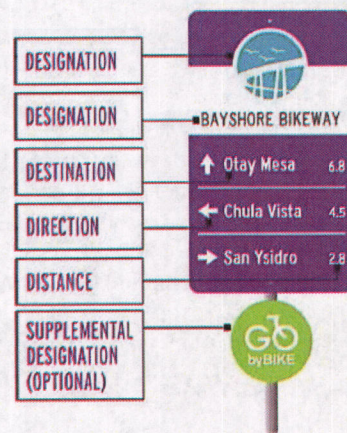
The San Diego Regional Bike Network Wayfinding system covers all on-street bikeways and four major regional trails. It uses a heavily modified MUTCD standard, with a custom purple color, fonts, and iconography specific to San Diego County. All signs feature a central circular graphic. General bikeway signs feature a SANDAG "Go by Bike" emblem while specific trails have their own circular icon and label. All icons share a color scheme and graphic style.

Best Practices:

- Use of color scheme (only five) that is both simple and recognizable
- Strong identity through playful and engaging icons and graphics
- Simple sign layout that allows for specific trail designation within a family of signs



The Ventura County Regional Bikeway signage is simple and affordable, but still provides inter-jurisdictional customization.



The San Diego County bike wayfinding system has a simple but unique color scheme and unique graphics logos.



### Razorback Greenway, Arkansas Wayfinding

The Razorback Greenway sign family creates a custom and cohesive identity for a 32-mile trail system. The colors, fonts, symbology, and design of each sign improve navigation, encourage use, and enhance identity for the trail. Sign types include regulatory information, regional and cultural details, identification markers, walk and bike timing, and geographical references. The large sign family is adaptable and suitable to a wide range of unique trail conditions in order to provide access and comfort to the diversity of people who use the trail system.

#### Best Practices:

- Hierarchical system with eight types of signs: trailhead identification, mile marker, maps, destination, interpretive, kiosk, and community guide signs
- Strong identity through region-specific colors and branding

### Santa Barbara County Bicycle Sign System

The Regional Bikeway Signage Program for Southern Santa Barbara County was created in 1996. Two years later, 500 bicycle wayfinding signs were installed. The sign system functions by guiding bicyclists along named primary routes and supplemental north/south connectors. The custom sign shape is successfully branded with a simple logo of a bike rider centered in a yellow sun. Supplemental directional signs provide arrows and mileage to local destinations.

#### Best Practices:

- Custom shape
- Simple design with identifiable branding and graphics
- Decision and distance information is combined, reducing the number of signage types needed

*The blue color, simple graphics, and unique form of the Razorback Greenway mile markers dot the 32 mile trail system*





## WAYFINDING ELEMENTS

### Gateway/Monument Sign

Gateway signs serve as trail landmarks, placed at with major trail access points. They enhance the visibility of the trail network for both current and prospective users. The trail name is the focus of the sign content and is supported with local destinations that can be reached along that particular trail.

### Trailhead Kiosk

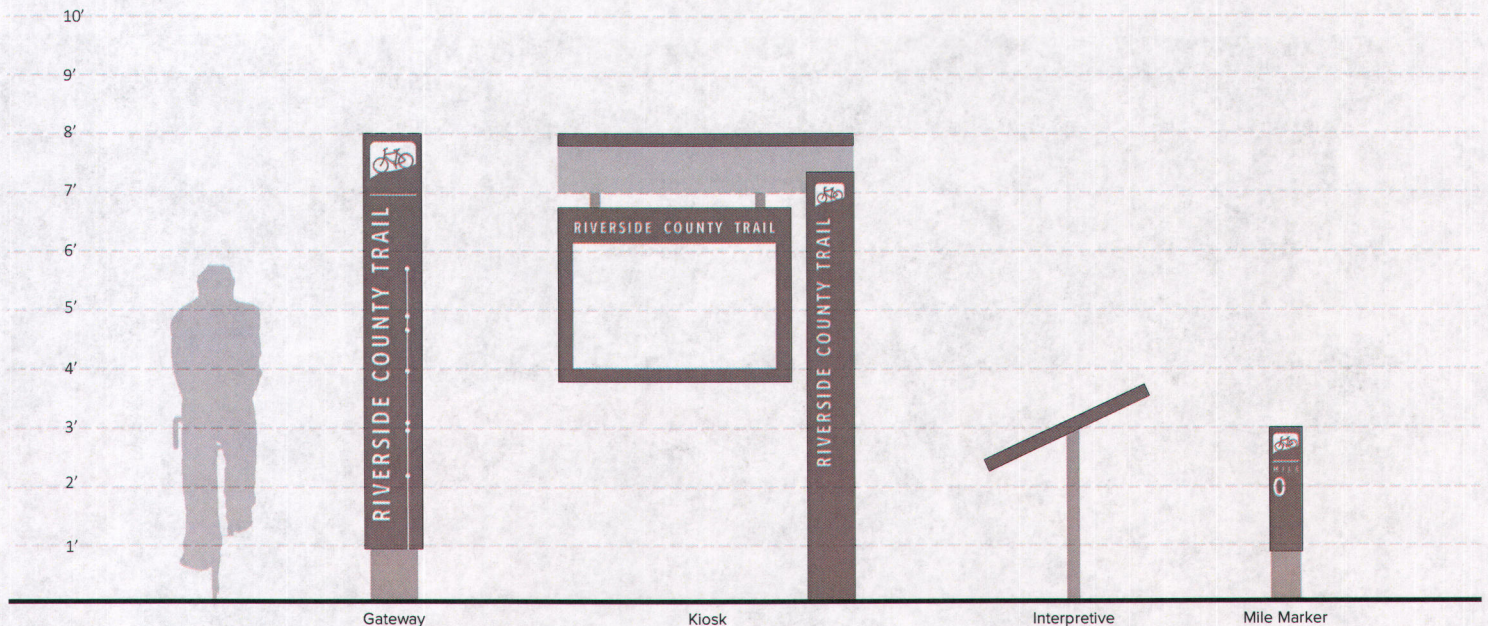
Trailhead kiosks are placed at access points. They are the first point of orientation and the large scale provides space for a trail map and regulatory information. The kiosk sign also provides riding and walking times to local destinations. When adding travel time to signs, a pace of 10 mph or six minutes per mile is typically used for bicyclists and 20 minutes per mile for walking pedestrians.

### Interpretive Sign

Interpretive signs provide information at key natural, historic, or cultural sites along trails. They are typically larger signs angled towards a point of interest. They typically include large graphic material.

### Mile Marker

Mile markers are a small feature with large significance and are an important element of wayfinding along trails. They allow users to track how far they have traveled and help people put their location in context by matching the marker to a map. Most trail users identify strongly with distance from home, distance from their favorite place, or simply with knowing a certain location based on its relationship to a mile point. Knowing one's location on a trail is critical to assisting emergency responders trying to locate a person in distress. Mile markers should be placed every  $\frac{1}{4}$  to  $\frac{1}{2}$  mile along a trail network.





### Confirmation Marker

Confirmation markers provide en route reassurance of trail identity and inform users they are on a designated Riverside County trail system route. They display the Riverside County brand and trail name. The confirmation markers also provide space for supplemental directional arrows, use icons, and can double as mile markers.

### Direction Sign

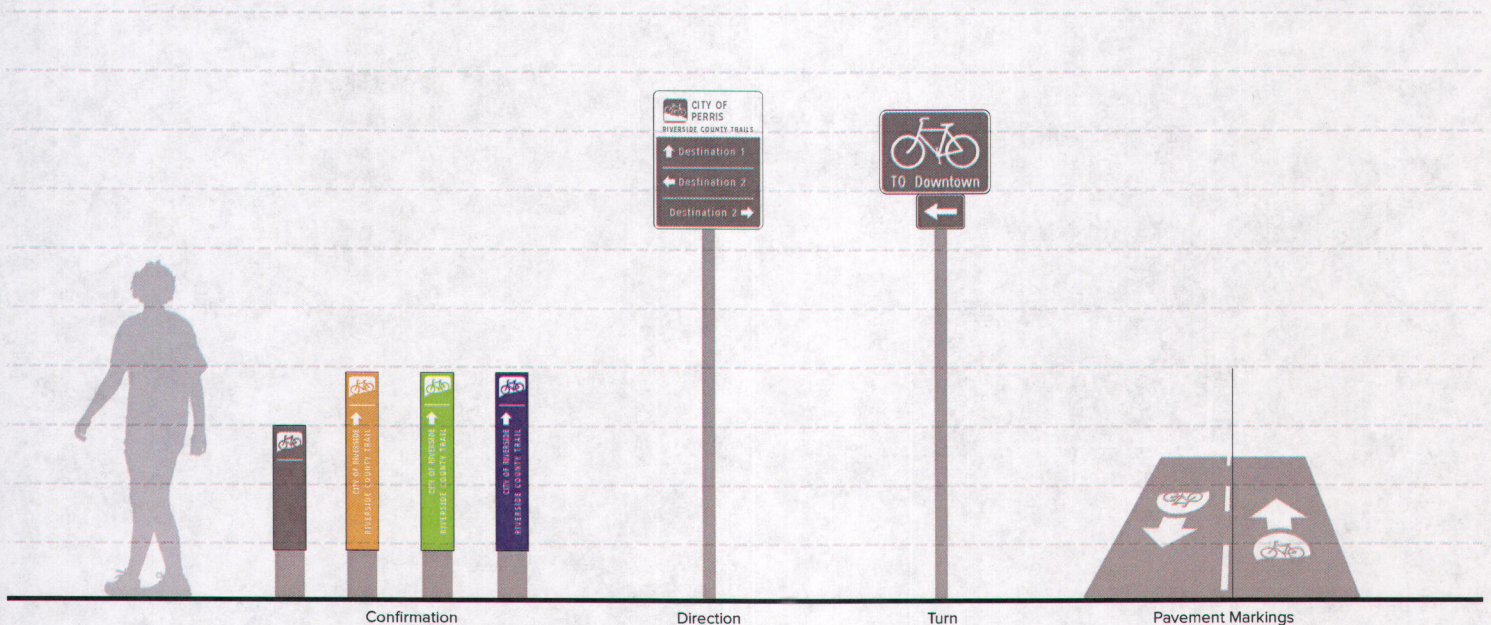
Direction signs provide directional and distance information to major destinations and trail amenities. Direction signs contain the local trail name in the header plaque and list destinations. They should be placed along trails to indicate upcoming destinations and junctures.

### Turn Sign

Turn signs inform riders about an upcoming intersection. They contain the destination or alternate route name and an arrow. They should be placed along trails shortly before trail junctures. Utilized off-route, they can help get cyclists to the trails.

### Pavement Markings

Pavement markings are a cost effective and low profile way to supplement or replace aspects of confirmation markers, mile markers, direction signs, and turn signs. They may also include interpretive elements, such as call-outs to views or points of interest.





## DESIGN

This document provides conceptual wayfinding design guidelines. Additional levels of input and outreach are required to develop a final design.

Though the signage elements have different scales and dimensions, they should be instantly recognizable as part of the same wayfinding family. This is achieved through a uniform design style, including graphics and icons, colors, fonts, materials, shapes, and proportions.

### Accessible Design

Wayfinding should be highly accessible, regardless of spoken language or cognitive ability. In areas with high concentrations of non-English speakers, consider having multi-lingual signs. Avoid relying exclusively on text, and utilize consistent icons and colors throughout.

### Fonts & Text Hierarchy

Aside from fonts used in logos, a single sans-serif font family should be used across all wayfinding. A hierarchy of size, bold, and italics should be used to communicate tiers of detail. This font hierarchy includes (from big/bold to small) municipal designations, general county designations, trail specific designations, destinations, and distances.

### Color

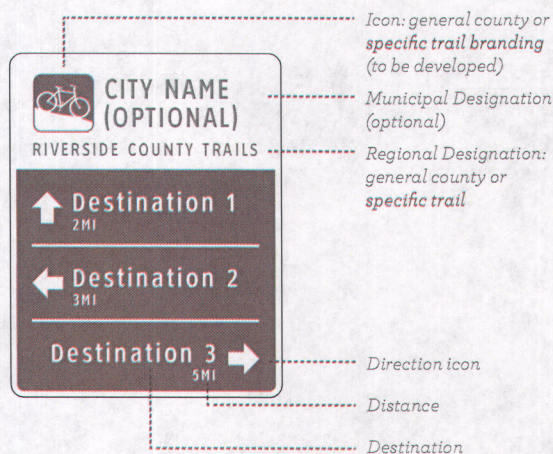
A small color palette should be used across all signs. Utilize no more than five colors with a wide range of contrast. Consider colors that reflect the character of Riverside County, drawing from the natural landscape, existing trail system signage, and municipal colors. As a general rule, maintain standard background / logo / text colors across the County trails. Emphasize wayfinding with specific municipalities or along specific regional trails by modifying background or logo colors.



Natural color palette, based on the colors of the Riverside County landscape



Municipal color palette, based on the colors of the Riverside County seal



A conceptual mock-up of design elements for a typical destination sign



### Branding & Iconography

Successful wayfinding systems utilize branding to create an identity for the entire system as well as for specific trails. Creating a family of icons could include an overall "Riverside County Trails" icon as well as specific icons for each of the backbone trails. Strong icons feature the following characteristics:

- **Consistency:** Most times a single icon will not work. Consistent styles, sizes, fonts, colors, and shapes should be used for destination, trail, municipal, or county icons.
- **Simple & Reproducible:** Icons should be easy on the eyes and perceivable from the speed of a bike. They need to scale from a mile marker to a gateway sign, and should be legible as a stencil or gray-scale graphic.

### Signing Distances

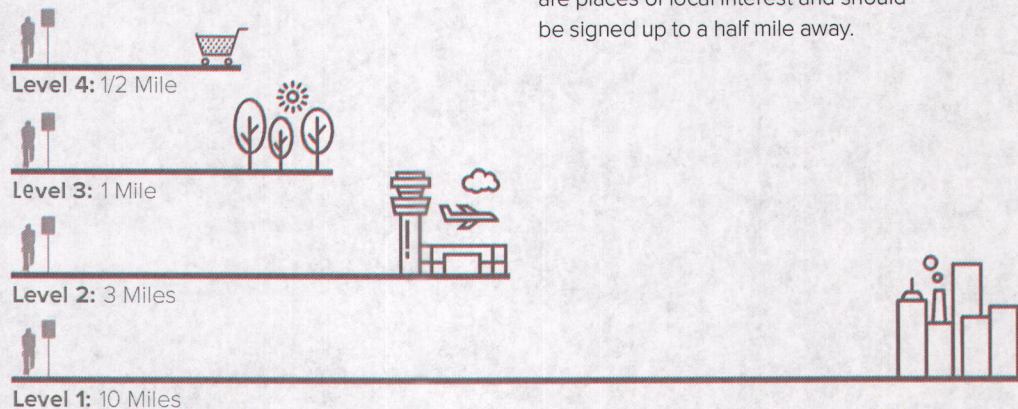
Signing distances suggest the maximum distance that destinations should appear on directional signs. This process ensures that information is spread along the journey in manageable amounts according to a cyclist's immediate needs.

Level 1 destinations include downtowns, cities, towns, and national parks. These areas typically have a well-defined edge and thus should be measured to boundary lines. They provide navigational guidance to the widest spectrum of system users and should be prioritized on signs. As a priority, Level 1 destinations should appear on signs up to ten miles away.

Level 2 destinations include transportation centers, airports, neighborhoods, colleges, regional landmarks, and state parks. They are less defined in terms of their boundaries and thus should be measured to their centers. They appeal to a broad spectrum of users and should be included on signs up to three miles away.

Level 3 destinations include other trails, high schools, hospitals, and regional parks. They are places of regional interest and should be signed up to one mile away.

Level 4 destinations include other trails, community centers, and local parks. They are places of local interest and should be signed up to a half mile away.



*Levels dictate how far from a destination that it needs to appear on a destination sign.*





# 05

## IMPLEMENTATION

“

*A sustainable trail system is one which can be planned and constructed, but also maintained...”*





---

*Implementation of a countywide trail system requires extensive planning and coordination, and multiple stages of effort, from initial corridor feasibility through funding, design, construction, and eventually maintenance. This chapter details these steps, and makes recommendations for a trail system that is sustainable financially and maintainable.*



## Implementation Framework

There are a number of steps related to the implementation of a countywide trail system. This framework outlines the necessary components for trail development and leadership within Riverside County. The framework is provided based on the practices of numerous external agencies, including cities, counties, regional and other plans of greater scale.

The primary steps involved with trail development are:

### Planning

*Concepts*

*Trail Corridor Master Plan*

*Coordination*

*Technical leadership*

*Regional corridor integration*

### ENVIRONMENTAL REVIEW

*Initial Study*

*Negative Declaration/ Negative Declaration with Mitigation Measures/ Environmental Impact Report (EIR)*

*Mitigation Monitoring*

### Design and Construction

*Engineering and Landscaping Plan*

*Construction*

*Inspection*

### Management and Maintenance

*Maintenance of trail amenities and surface*

*Management of trail as a public asset*

### Promotion

*Event Planning*

*Marketing*

### Enforcement

*Ranger Programs*

*Safety and Law Enforcement*



## PLANNING

Riverside County is well positioned with many potential leadership organizations able to plan trail projects and corridors. Activities related to this implementation phase include the identification of a project sponsor or lead agency, conceptual development of trail systems, and planning the remaining segments within the system.

The geographic and demographic context of Riverside County requires an entity to manage an overall regional trail vision, provide technical leadership and coordination, and ensure that conceptual development of corridors is fulfilled to a certain standard. Planning entities such as the Riverside County Regional Park and Open Space District (RCRPOSD) should continue to play a strong role in planning the regional trail system and identifying leadership to maintain and carry projects through to implementation and management.

### Planning related activities in this role typically include:

- Planning overall trail and regional trail systems
- Coordinating between area stakeholders
  - » WRCOG, CVAG, Riverside County Department of Transportation, Riverside County Transportation Commission (RCTC), Flood Control, County Departments
- Identifying project sponsoring agency, project leadership, management
- Facilitating and plan parks and trails capital and non-capital improvements
- Identifying partners and funding mechanisms for all trail and park related improvements
- Creating and updating plans for backbone trails, implements plans.

### Potential Planning Leadership Organizations

- RCRPOSD (Including Advisory Committees)

- » Trails within regional parks, trails outside of parks and outside incorporated cities, federally- and state-managed lands.
- » Riverside County Planning Department
- » Trails within community and specific plans.
- Riverside County Transportation Commission
  - » Regional Class 1 trails and on-street facilities.
- Riverside County Department of Transportation
  - » On-street facilities that fill gaps within and provide connections to regional routes.
- Western Riverside, Southern California and Coachella Valley Councils of Governments
  - » Trail projects specifically within the coalition's boundaries
  - » Trail projects in the Desert and Jurupa Valley Recreation District
  - » Trail projects within recreation district boundaries
- State and Federal agencies
  - » Any project concerning the use of federal or state lands, respectively

## ENVIRONMENTAL REVIEW

Building trails in Riverside County will require environmental reviews through the California Environmental Quality Act (CEQA). The review will identify potential significant environmental impacts that may arise as a result of the project and how to mitigate those impacts. The process will include the following:

- Initial Study
- Negative Declaration/ Negative Declaration with Mitigation Measures/ Environmental Impact Report (EIR)
- Mitigation Monitoring



## DESIGN AND CONSTRUCTION

The proper design and construction of trails is paramount to the development of long lasting facilities. The design and construction of trail facilities is more of a technical exercise on an identified route that has been established through the identification of a route, acquisition of right-of-way, and appropriate permitting. Various entities must undertake this that have proficient staffing capabilities and certifications to build these facilities.

### Potential Design Leadership Organizations

- Desert, Jurupa Area, and Valley-Wide Recreation and Park Districts
- Riverside County Habitat Conservation Agency
- RCRPOSD
- Riverside County Planning Department
- Riverside County Transportation Commission
- Riverside County Department of Transportation
- Western Riverside, Southern California and Coachella Valley of Governments
- State and federal agencies

## MANAGEMENT AND MAINTENANCE

Trail maintenance refers to the long-term well-being of the trail and its facilities. Generally, trails and trail amenities have a life cycle considered during design and construction. If well maintained, facilities should meet this life cycle. Thus, care should be given to the facilities maintenance and inspection activities to detect defective pieces in a system. This could be as simple as monitoring potentially hazardous situations on the trail as risk becomes more apparent to trail users, or generating a repaving schedule as a paved trail's life cycle ends. Generally, maintenance is completed by trail managers or planners.

Trail management relates to the ongoing efforts of an entity to ensure a safe, user-friendly facility. Management and maintenance are closely related, but management refers to the operational context and necessity associated with keeping a trail in good working order. This is an ongoing technical and resource based task from the outset of a trails design. Trails require not only managers who will serve as the "boots on the ground" but also administrators who can perform managerial tasks behind the scenes. Managers often help assist with the maintenance of facilities, ensuring the life-cycle of the facility is maintained.

### Potential Management and Maintenance Leadership

- Western Riverside, Southern California and Coachella Valley of Governments
- Desert, Jurupa Area, and Valley-Wide Recreation and Park Districts
- Riverside County Habitat Conservation Agency
- Riverside County Parks and Open Space District
- Riverside County Department of Transportation
- Riverside-Corona Habitat Conservation District
- Riverside Land Conservancy
- US Fish and Wildlife Service
- Western RC Regional Conservation Authority
- Wildlands Conservancy
- Center for Natural Lands Management
- State and federal agencies

## PROMOTION AND ADVOCACY

The promotion and advocacy of trails in the county becomes increasingly important as the user base and system expands. As demand grows on a trail system, so too must the operations, management and maintenance of



a system. Advocacy plays an important role through informing individuals with decision making authority about the role the system plays. Promotion of the system is more directly related to the advertisement and education of the system's users. Some user groups participate in promotion and advocacy efforts while others focus their efforts on one task.

While it is not the County role to advocate for the trail system, they are responsible for reporting accurate information to different groups. The County must also ensure the proper use of the system in collaboration with private groups through public-private partnerships.

#### **List of potential Advocacy and Promotion Leadership**

- Inland Valley Mountain Bike Association
- Wildlands Conservancy
- Friends of the CV Link
- Friends of the San Jacinto Mountain County Parks
- Inland Empire Waterkeeper
- Friends of Hidden Valley
- Friends of Riverside Hills
- Riverside Community Health Foundation
- Riverside County Health Coalition

#### **ENFORCEMENT**

Enforcement on trails relates to the overall safety and welfare of users. Crime prevention falls on the jurisdiction in which the crime may be taking place: municipalities, federal and state lands, and county land. Within county jurisdiction, law enforcement will be the responsibility of the county sheriffs department. Within parks, rangers often take on preliminary law enforcement roles, and are responsible for regular patrols, issuing warnings, citations, and enlisting the assistance of other law enforcement agencies when needed. Ranger programs are operated at the county, state, and federal levels.



## Funding Best Practices

### SURVEY RESPONDENTS

The Riverside Park and Open-Space District Peer Agency Survey gathered information from county agencies in the Western United States regarding their management of trails. Ten county agencies participated in the survey, representing a mix of rural, suburban, and urban counties (see Figure 5-1). Agencies were asked to provide information related to their trail management practices, including their funding sources, maintenance practices, and usage of their facilities.

The survey was conducted in August and September 2016. The survey, consisting of 24 multiple choice and short answer questions, was administered by Cambridge Systematics using an online survey tool. Participants provided detailed responses to questions, giving valuable insight into trail management practices around the country and in the surrounding region. Respondents indicated a wide range of funding sources for trail construction and maintenance, including federal, state, and local sources. These funding sources are indicated in Table 5-1 on page 168.

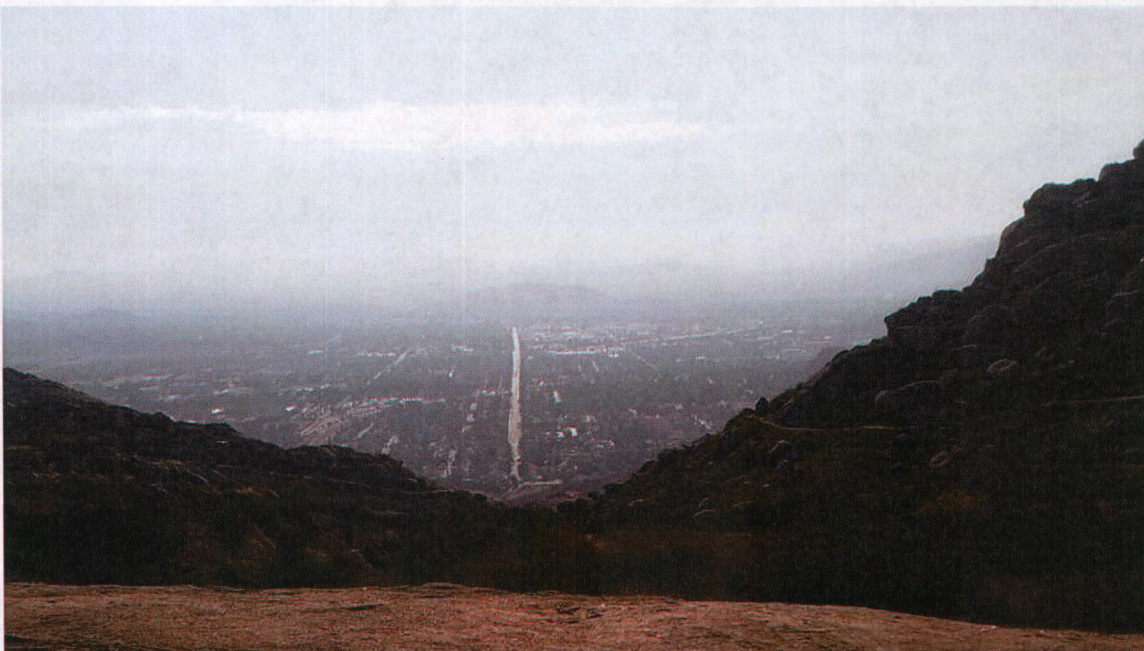
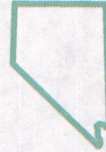




Figure 5-1 Participating Agencies

**ARIZONA**

- Maricopa County Parks and Recreation

**NEVADA**

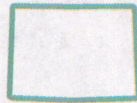
- Washoe County Regional Parks & Open Space

**CALIFORNIA**

- El Dorado County
- Los Angeles County Department of Public Works

**NEW MEXICO**

- Los Alamos County Parks Recreation and Open Space

**COLORADO**

- Jefferson County Open Space

**OREGON**

- Lane County Parks
- Metro Parks and Nature Department
- Tualatin Hills Park & Recreation District

**MONTANA**

- Missoula County Parks, Trails & Open Lands





## SURVEY RESULTS

Much of the funding for trail construction comes from federal sources passed through to state agencies or local agencies. These funds are often dispersed through competitive grant programs. At the County or local level, municipal bonding, sales taxes, general funds, and fee programs are common sources for sustained and dedicated trails funding. Often, local agencies are opportunistic about the source of funds available for specific trails. The Missoula County Parks noted that trails that go through property owners associations or special taxing districts may have dedicated funds for on-going maintenance, freeing up general funding sources for other trails.

Six agencies responded that they have a developer impact fee program, or similar. Three of those six agencies indicated their developer fee funds could be used for trail construction, but no agency specified that they could use the developer impact fee funds for maintenance. Jefferson County Open Space clarified that in order to use the developer fee funds for trail construction, the trail would need to be identified as a transportation improvement (eg. a sidewalk along a major roadway). Lane County Parks noted that Community Service Districts “can only be used for projects that increase capacity or planning, not for maintenance.”

Funding for capital projects comes from diverse set of federal, state, and local sources. However, funding for trail maintenance is almost exclusively local. As described above, many agencies rely on volunteers to perform maintenance. When local staff performs the work, the funding generally comes from local government sources. In the RTC survey, they found that municipal governments were the leading funder of trail maintenance (42% of respondents). While maintenance is eligible for federal funding under the Recreational Trails Program, trail maintenance often has to compete with capital projects in competitive grant programs. In California, the Recreational Trails Program funding is allocated through two

state run application processes, the Active Transportation Program and Recreational Trails Program. Given the lack of dedicated funding sources, trail maintenance often competes with municipal funding needs, and therefore trail managers have indicated a need for dedicated federal and state funding for trail maintenance.

## Costs & Funding



This section is meant to inform the Riverside County Trails Plan process on trail development and maintenance costs, and provide funding strategies for short and long term solutions. Due to funding source changes some sources are more reliable than others. For example, Safe Routes to Schools at the Federal level was funded from 2005 to 2012, but some states have budgeted for the program at state departments of transportation. Therefore, this snapshot of cost estimates, funding strategies and programs are meant to be updated from time to time to better reflect changes in the market, cost of resources and materials, and organizations.

## SUMMARY OF TRAIL NETWORK COSTS

Trail networks are typically created with a number of individual trails that are connected over time, with gaps strategically being closed using innovative methods of funding and design. These networks can connect trails to one another and other supporting facilities such as sidewalks, and on-road bicycle facilities. This section will discuss the development of both paved and unpaved trails, giving information for pre-construction, construction, maintenance and other costs.



Table 5-1. Capital and Maintenance Funding Sources

FUNDING CLASS	CAPITAL FUNDING SOURCES	MAINTENANCE FUNDING SOURCES
Federal	Safe Routes to Schools Congestion Mitigation and Air Quality Improvement (CMAQ) Program Surface Transportation Program (STP) Regional Surface Transportation Program (RSTP) Recreational Trails Program Public Lands Highway Discretionary (PLHD) Transportation Investment Generating Economic Recovery (TIGER) Grants Other Federal Grants (unspecified)	Youth Conservation Corp Unspecified grants
		
State	Active Transportation Program (ATP) State Departments of Transportation Mobile Source Air Pollution Reduction Review Committee (MSRC) Oregon Lottery Oregon Recreational Trail Grant Metropolitan Transportation Improvement Plan Funds State Grants State Parks Recreational Trail Program	Transportation Development Act (TDA)
		
County	Bonds County Capital Improvement Program Funds Dedicated Sales Tax Donations General Fund Tax Parks & Trails Bond Program Funds Parks Funds Parks System Development Charge Capital Improvement Program Road Discretionary Fund Regional Parks and Open-Space District Funds User Fees Regional Call for Projects	Dedicated sales tax General Fund Tax Donations Parks Department Operating and Maintenance Budget County General Fund User Fees
		
Local	Municipal Bonds Community Service Districts (CSD) Developer Impact Fees Partnerships with Local Jurisdictions Park System Development Charge Tax Increment Financing Donations Friends-of Groups Impact Fee Programs	Property Taxes Local Option Levy Volunteer Labor and Resources Non Profit Organizations
		



## TYPICAL TRAIL RELATED PROJECT PHASE COSTS

Paved trails are often referred to as multi-use or shared-use paths. These paths are often used for not only recreation, but also as a commuting alternative to on-street facilities. Paved trails often cost substantially more to construct than natural surface trails due to the materials used and pre-construction permitting and design work. However, the life cycle of paved trails is typically longer than natural surface trails. Conversely, natural surface trails often have smaller budgets to design and construct and often serve more recreational than transportation users.

### Acquisition

Trails can be built on natural lands without much impacts, along roadways, or on abandoned railroad or utility corridors. Acquisition cost will depend on local market conditions, location of the project, and other factors. Establishing a diverse strategy for acquiring trail right-of-way is an important piece of developing an integrated trail network.

### Feasibility Study

A feasibility study for trails aims to find the most feasible alignment for a trail and identify the obstacles to developing a preferred alignment. This type of study

often considers multiple alignments for a potential trail route, environmental and social considerations, right-of-way availability and cost considerations. This part of the study often sets the expectations for other phases of trail development, maintenance, and how the trail has the potential to impact the communities it touches. Economic studies often are considered in feasibility studies as a means to determine economic impacts of the trail.

Feasibility studies for natural surface trails are less intensive than paved trails. Nevertheless, environmental impacts should be considered in this phase, with the inclusion of a biologist and other relevant personnel.

### *Feasibility Study Cost*

Feasibility studies are often the most cost efficient means of identifying a trail alignment and ensuring feasibility in accomplishing trail construction. Depending on the nature of a feasibility study, costs vary widely for this phase. An example is the Doodle Rail Trail Feasibility Study in South Carolina which cost \$40,000 for a 7.5-mile rail-to-trail project (Sanders, 2016).

### Permitting and Design

Every trail should have professional design incorporated into its development to ensure the longevity of the plan and safety in the ultimate build out for users. The type of





trail design and complexity of a project will influence the project cost, namely the location of the trail, surface type, and environmental considerations. Organizations have become more able to lend their assistance to create natural surface trails, such as the International Mountain Bike Association, the American Hiking Society or equestrian groups. Natural surface trails often can be designed at lower cost than paved or decomposed granite trails, due to the use of existing on-site materials.

Trail permitting is often required for trail designs. Environmental engineers or landscape architects are able to effectively consider impacts on the environment and complete the permitting process. Local trails that are smaller in length and have minimal environmental and social impacts generally can go through permitting processes faster than longer trails, or those which traverse areas which would trigger greater levels of permitting. For example, a rail-with-trail project that resides on

already disturbed land and is located entirely on city owned land would likely have less permitting than a trail traversing national park lands with wetland impacts. Due to the varying cost of design and permitting from project to project, cost estimates have not been provided. Variables that typically affect design and permitting are ecologically or historically sensitive areas, bridges, topographically variable terrain, and other elements.

### Construction

Construction costs can vary widely depending on the context and required trail elements. Consideration should be given to any required bridges, tunnels, or other structures, for any potential mitigation for trails in or near sensitive ecosystems, roadway and rail crossings, and the inclusion of elements such as trailheads, rest stops and interpretive materials or signs. Table 5-2 provides insight into a range of national trail construction costs.

Table 5-2. National cost estimates for trail construction on a per-mile basis.

PROJECT NAME	COST PER MILE	NOTES
Doodle Rail Trail	\$283,000	Project located in South Carolina and connects cities of Easley and Pickens.
Florida Park Service, Office of Greenways and Trails	\$500,000	Statewide system consisting of multiple projects with different implementation agencies
Milwaukee County, Wisconsin Trail Construction Cost Estimates	\$149,000 - \$301,000	Milwaukee County Trails Network Plan, 2007
City of Redlands Class I Bike path	\$800,000	Paving, Striping and Signage

Notes: Costs per mile are estimated and based off of best information available at time of writing. Sources for each table available in document bibliography.

Table 5-3. National cost estimates for trail maintenance on a per-mile, per year basis

PROJECT NAME	MILEAGE	COST PER MILE, PER YEAR	NOTES
East Central Regional Rail Trail	50.8	\$3,500	Located in Volusia County, FL
Florida Park Service, Office of Greenways and Trails	Varies	\$5,000-\$6,500	Statewide System, 10 Trails
Memo Attachment 1 cost information	Varies	\$2,000-\$5,000	Many locations throughout the country
City of Redlands, CA (Class I Bike Paths)	33	\$8,500	Estimate provided for Class I Bike Paths in this area
City of Redlands, CA (Class II & III Bikeways)	148	\$2,000	Estimate provided for Class II & III Bike Paths



## Maintenance

In developing a trail system, ongoing maintenance must be considered to ensure the full life cycle of the facility and enjoyment by users. Maintenance cost for trail facilities depend on the trail type, user-features, trail context, and community support. These tenets make the development of a cost estimate for trail system maintenance difficult.

Information is provided above on other trail maintenance costs from around the nation (see Table 5-3). These cost estimates are provided on a per-mile basis. The context and amenities on each trail will vastly impact the cost of trail maintenance.

## Funding Resources

This section discusses the types of funding strategies available to the County. To complement this research, a survey of peer agencies' was conducted; many institutions responded that funding sources for trails generally originate through the federal government and are distributed through competitive grant programs.

### SHORT TERM FUNDING

Short term funding strategies typically occur within a narrow timeframe, such as one to three years. In targeting funding resources for trail projects, managers should generally prioritize discretionary or competitive grant programs. However, these funds are distributed nationwide and sometimes only result in marginal gains on an annual basis. Other successful funding pursuits in the near term could come from a diverse set of funding available from not-for-profits, local government funds, and others. Because of the limited timeframe associated with most funding resources, each funding resource may be in a state of flux and requires attention to ensure opportunities are not missed.

To complement these grant and other assistance programs, numerous local and state funding resources have been identified as potential trail development resources. At the county and municipal level, municipal bonding, sales taxes, general funds, and fee programs are common sources for sustained and dedicated trails funding.

While there are substantial capital sources available at the federal, state and local level, for trail design and construction, trail managers often cite the need for dedicated federal and state funding for trail maintenance (Rails-to-Trails Conservancy, 2015).

Of the ten respondents to the survey, six indicated a developer impact fee program was in place. Three of the six respondents had the option to utilize those funds for construction, but none of the respondents indicated these fees could be used for maintenance once the trail is built.

### Grant Programs

There are a number of programs throughout the nation which provide discretionary and competitive grants for projects. These resources often require an in-kind donation of capital or labor as part of the project. Grant programs are mostly funded through the federal government, but passed through to states and other organizations to administer, such as State Departments of Transportation. There are also many not-for-profit organizations that provide grants for capital improvement and maintenance.

In the short term, these grants are generally available to applicants on an annual, recurring basis.

Additional information on California specific grants is provided below.



- California Department of Parks and Recreation: This department oversees a number of grant programs directly relevant to Riverside County. The Office of Grants and Local Services (OGALS) manages a number of grants for consideration in the short term (California Department of Parks and Recreation).
  - » The Recreational Trails Program (RTP): This national program, managed by the aforementioned office is a national program providing funding for trail projects in various phases. This program is an annual funding source that should be frequently utilized to develop trails.
  - » Land and Water Conservation Fund: Similar to the RTP program, this federal program managed by the state provides annual funding for projects that aim to implement projects.
  - » Habitat Conservation Fund: This fund is geared toward habitat conservation, but can still be used toward conservation projects that have trail elements.
- California Department of Transportation (Caltrans): There are numerous programs that are under the Active Transportation Program (ATP) including the State Safe Routes to Schools Program, Bicycle Transportation Account, and Transportation Alternatives Program (California Department of Transportation (Caltrans), 2016). This program combines state and federal funding sources under the jurisdiction of the Office of Active Transportation and Special Programs. While this program is not directly geared towards recreation it can be used for most trail projects.

### **Not-for-Profits and “Friends of” Groups**

These organizations and collections of individuals often support very localized trail initiatives and take ownership of trails in a meaningful way. In this context, Not-for-profits often have a very specialized desire to develop, maintain, or protect trails. They often take steps to ensure the longevity and community support for trails, funding economic benefits studies, trail collaboration meetings, and other projects. Volunteer or “Friends of” groups often take a more trail or park specific approach in lending their support. These groups are often geared towards action for their community trail or area. Friends groups generally don’t have much capital, but often raise funds for smaller capital projects in certain areas to fulfill needs for the overall trail user base. These groups are often relied upon to maintain trails in some areas as local governments often have funding shortfalls in trail maintenance. At the statewide level, the California Trails and Greenways Foundation provides an excellent example of a partner that advocates for trails, provides educational programming, and hosts volunteer events. This organization provides for a number of grants, and volunteer recognition awards, and works frequently with trail managers to better understand needs of the statewide system (California Trails and Greenways Foundations, 2010).



## Local Funding Sources and Tools

City, county and other regional governments have a substantial amount of latitude in developing and maintaining trail systems. Not only have governments adopted ordinances, codes and policies to ensure trail funding through tax bases, but can also require developers to construct or turn over land for trail connectivity if undeveloped land is considered for development. This section discusses some of the more prominent strategies available in developing these lands.

### *User Fees*

Many parks and trails require users to pay for the use of the facility. In larger parks, there is generally an entry gate which enables the park to collect entry fees. Some parks and trails do not collect user fees, but allow for the local volunteer group to place a donation box at trailheads to raise funds for trail capital projects. User fees are regulated by the managing entity and can be used for the Parks District's general fund, or put back into the County's general fund.

### *Community Service Districts (CSDs)*

CSDs were developed to provide basic services to populated, yet unincorporated areas of counties within California. These entities often form and are based off of property taxes to provide a wide portfolio of services depending on the district and needs of the communities. These entities report to a local board to respond more

immediately to community needs and services are based directly off of need rather than desire (California Tax Data).

### *Capital Improvement Plan (CIP)*

CIPs are a short term budgetary process where local jurisdictions identify and prioritize projects. Generally, these plans are geared towards infrastructure improvements rather than maintenance. These plans aim to identify and collate the projects over the next few years.

### *Other Tax Funds*

A series of tax funds can be implemented locally to ensure funding for trail projects. This includes sales taxes, income taxes, property taxes, and others. These taxes must often be approved by a referendum, board, commission or state ballot depending on the nature and type of taxation. Some regional organizations base their operations off of taxes, such as the Community Service Districts.

While there are substantial capital sources available at the Federal, State and Local level, trail managers often cite the need for dedicated federal and state funding for trail maintenance (Rails-to-Trails Conservancy, 2015). Of the respondents to the survey, six indicated a developer impact fee program was in place. Three of the six respondents had the option to utilize those funds for construction, but none of the respondents indicated these fees could be used for maintenance once the trail is built. Many strategies are used throughout the country to secure funding, the next two sections of this memo will address long and short term strategies for securing funding.

## LONG TERM STRATEGIES

In developing a long-term plan for funding trail network development, many considerations should be made for contingency planning; who will be the implementing and maintaining agency, and where will additional funding





come from down the road in case of needed capital. These are important questions and are susceptible to change over time.

### Assessment Fees

The County may choose to implement an annual assessment in the support of trail maintenance and development. This fee is to be included as an assessment on annual property taxes. The amount of this assessment is subject to County Council approval, and is recommended to be a minimum of \$25.00. The assessment may require a vote as a measure to be scheduled during the County's election cycle.

### Transportation Uniform Mitigation Fees (TUMF)

Voters approved an innovative program for western Riverside County in 2002: the Transportation Uniform Mitigation Fee or TUMF. Western Riverside County's TUMF was patterned after a program by the same name in the Coachella Valley. Under the TUMF, developers of residential, industrial, and commercial property pay a development fee to fund transportation projects that will be required as a result of the growth the projects create. The Western Riverside Council of Governments administers the TUMF.

The TUMF funds both local and regional arterial projects. Eligible projects include nonmotorized transportation - which includes both on- and off-street bikeways and trails.

### Trail Master Plan

In the long term, a locally credible trail implementation entity should execute a planning process with the assistance of local stakeholders to define their trail network vision. This vision within a master plan or trail implementation plan ensures that the existing conditions, and desired future conditions align not just with the expectations of the community, but also the realities within a given timeframe.

Trail master plans should contain an implementation component that describes the remaining phases of trail implementation, expected cost of each phase, including annual maintenance costs, and a targeted funding source for such improvements. Without this vital implementation component, many organizations are left wondering where resources should be prioritized, how funding will be acquired, and who the lead agency will be to develop the trail.

### Parks and Trail Management Plans

Many organizations who manage large units of parks or trails have adopted management plans. These plans vary in timeframe and size, but generally are set at five to twenty years for trails that transect jurisdictional boundaries, and parks with diverse needs. Management plans often have a number of elements that help to guide how management entities respond to certain needs. Plans are written to understand the current needs of the managed unit from an ecosystem and human systems which are connected by the unit. These plans also identify future needs of the unit, including aging infrastructure that will require capital for repair.





## Local Funding Sources and Tools

In addition to the short-term funding resources available for trails, long term solutions are also available to local implementing jurisdictions.

### *Developer Impact Fee Program*

An impact fee program is meant to cover the cost of development and the impacts it has on public services impacted by it. Impact programs are developed only in relationship to new developments and have been identified as an allowable source of funding for trail capital projects. The County currently excludes commercial and industrial development from this program, which has been identified as a missed opportunity for trail funding.

### *Municipal Bonds*

Municipal bonds are loans that governments borrow to pay for capital projects over a given period of time. Because of how these debts are incurred, they are typically not able to be used for day to day trail maintenance. These bonds are largely used for capital projects, such as recreational trails and trail elements.

## ACQUISITION

Acquisition of right-of-way for trails requires not only funding, but also a strategic approach to acquiring lands. The best strategy for acquiring right-of-way is to ensure utilize a diverse set of approaches to different pieces of property. The strategies listed below should be considered as practices which could be utilized, rather than one singular approach to each parcel. A combination of strategies may be considered, depending on the parcel of land, market conditions, and other variables.

## Fee Simple and Easement Purchases, and Donations

Fee simple acquisitions are the purchase of land and all rights therein, while easements are acquisitions of the right to use land for a given purpose, such as developing a trail. Donations are also another form of receiving land through creative means. Donations are provided to governments by individuals or organizations with a philanthropic sense to develop a trail. These acquisition approaches can be taken to acquiring land, but there are also certain measures cities and counties can take to ensure that land will be acquirable, once it is on the market.

## Zoning and Development Regulations, and Developer Contributions

Cities and counties often require land to be constructed in a certain way, or to have certain elements constructed depending on the nature of a development. These types of regulations can also be used to protect certain types of ecosystems from development, or acquiring land from developers before they are developed. Governments can require that developments incorporate trails into plans and construction, but generally have to have established plans in place to negotiate with the developer. This also benefits the community as it provides a recreational and transportation amenity (City of Monroe Parks and Recreation Dept., NC, 2015).

## Right of First Refusal

In some instances, land may not be available in the near term and agencies must become creative in their approaches to closing gaps. One of the approaches to closing gaps which is a long-term approach is to enter into a right of first refusal agreement. This type of agreement enables an organization to make the first offer on a tract of land once it becomes available for purchase. Landowners are not required to accept the offer, and it enables the organization in question to have a "first claim" if the offer is acceptable to all parties.



## Dual Easements

Many times, agencies throughout governments are working towards the same goals but not collaborating on projects. Many agencies at the County level, such as school boards, water managers, or other public utilities should consider negotiating easements for water, schools, and other types that also include trail elements. This provides governments and other institutions with the power to negotiate many items, and leverage multiple resources.

## Condemnation

The use of eminent domain or acquisition of parkland or trail property is used when it cannot be obtained through other means. This is generally a last resort for institutions to take as it can sometimes result in costly litigation, and generally removes the ability to negotiate payment at anything less than full market value.

# Management

## TRAIL MANAGEMENT SURVEY

Trail management practices vary considerably depending on the structure of the agency, regional characteristics, and types of trails that are maintained. This report summarizes the key findings from a survey conducted to support the Riverside Park and Open-Space District's development of a Comprehensive Trails Plan. The purpose of this document is to summarize the trail maintenance, funding, and usage patterns for trail agencies in the Western United States (see Figure 5-2). Findings from the surveys are presented here, and where necessary, data from external best practices surveys was used to fill in the gaps.

## TRAIL USAGE

Survey respondents were asked if and how they monitor trail users. Most agencies declined to answer this question, likely signifying the lack of available data. Two agencies noted that they use automated counters, and one agency noted the use of manual counts. One agency responded that they estimate their annual users. Of those who responded, most agencies suspected that greater than 70% of the usage was recreational. Weekday and weekend usage was common for most agencies, and while there was some seasonal variation, most agencies see fairly consistent usage throughout the year.

However, quantifiable trail usage data was limited and therefore does not allow for useful comparisons to Riverside County. The limited data on trail usage is a challenge for many agencies nationwide. A 2014 study conducted by the Rails-to-Trails Conservancy (RTC) found that over half of management agencies did not track users, and roughly a quarter simply estimate or guess their trail usage.

## MANAGEMENT

### Trail Planning Documents

All agencies surveyed have completed at least one trail planning document. Nine out of ten agencies indicated they had a park master plan, six indicated they had a regional park/open-space district master plan, and five agencies indicated they had a trail master plan. A breakdown of planning documents prepared by the responding agencies is shown in Figure 5-2.



## Trail Ownership and Management Responsibilities

Most of the responding agencies are responsible for maintaining between 35 and 80 miles of trails, though some maintain as little as 10 and as many as 600 trail miles. Most agencies manage trails in a variety of land uses, with the exception of 100% rural (Lane County and El Dorado County) or 100% suburban (Tualatin Hills). Under normal circumstances, the trails are open seven days a week. Half of the agencies noted that their trails are open from dawn until dusk, including one agency that opens their trails an hour before dawn, and closes an hour after dusk.

The other half of the agencies responded that their trails are open twenty-four hours per day.

The responsibility for maintaining a trail depends on the location, the owner of the right-of-way, and the type of trail. Four agencies noted that they maintain trails on land not owned by the agency. Three of those agencies maintain trails on Federal lands and one maintains trails on State land. Ninety percent of the agencies have at least one regional or inter-county trail which intersects the agency's jurisdiction. Depending on the parties involved, there are different inter-agency agreements for trail management:

- The Los Angeles County Department of Public Works and the Tualatin Hills Park & Recreation District indicated they are solely responsible for maintaining regional trails in their jurisdiction.
- Missoula County Parks Trails & Open Lands noted that for trails or paths on State land, the County has a memorandum of understanding outlining maintenance responsibility. The County's responsibility includes snow sweeping, snow plowing and weed control, and the state is responsible for pavement management and maintenance. However, they noted that each trail may have a unique set of circumstances.
- The Metro Parks and Nature Department does not maintain any trails on land owned by other agencies, but other agencies maintain trails on their land.
- Jefferson County Open Space noted that the City and County of Denver owns some land in Jefferson County; Jefferson County builds and maintains the trails on this land. Furthermore, some of regional trails are constructed using County funds, however, ongoing maintenance is the responsibility of local jurisdictions.
- Some agencies mentioned that they share regional trail maintenance responsibilities with the US Forest Service or local homeowner's associations.
- Within the Los Alamos County Parks, Recreation, and Open Space Division, the Parks division manages paved trails and the Open Space division manages the unpaved trails.

In some cases, trail management responsibilities vary by trail:

- The Los Angeles County Department of Public Works noted that they maintain paved paths, while unpaved paths are maintained by the Department of Parks and Recreation.
- The Tualatin Hills Park & Recreation District specified that soft surface trails are maintained by natural area staff, while the maintenance for hard surface trails is split between park maintenance and trail maintenance staff.
- The Metro Parks and Nature Department noted that on-street facilities are managed by the Transportation division, while off-street paths and trails are under the Parks department purview



- El Dorado county noted that Class II and III bike lanes are maintained by their Transportation Division, while Class I trails are maintained by their Facilities Division and volunteers.
- Jefferson County Open Space shared that the County has a transportation and engineering division charged with maintaining commuter trails.

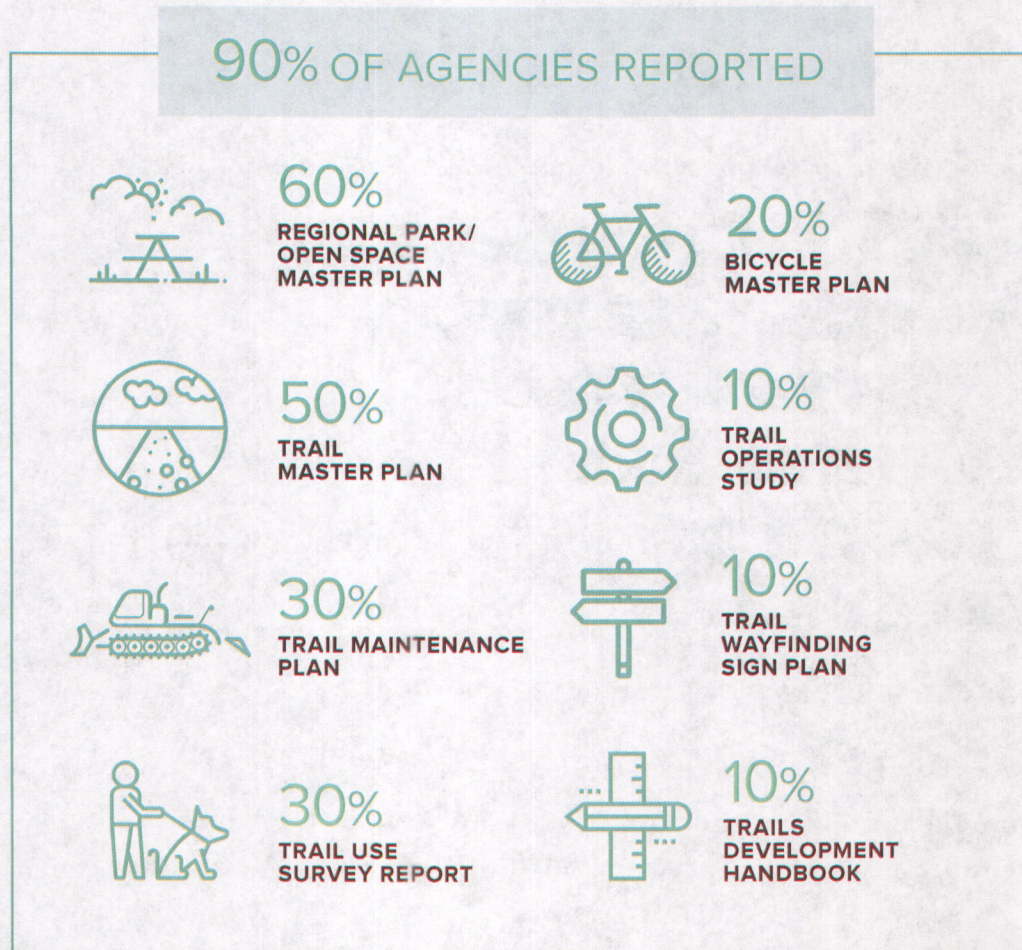
Trail usage is considered a hazardous sport in only two of the responding counties, and four agencies have liability insurance for trail usage. Metro Parks in Oregon noted they

are protected by a recreational immunity clause in their Public Use of Lands Act.

### Trail Maintenance Practices

Trail agency maintenance needs depend on the surrounding land uses, regional climate, and permitted uses. Trail maintenance practices range from physical repair or resurfacing of the trail surface, to surface clearing and vegetation control, to maintenance of parking facilities, restrooms, and other amenities. While resurfacing and repairs are time consuming and costly

Figure 5-2. Trail Agency Planning Documents Completed by Survey Respondents





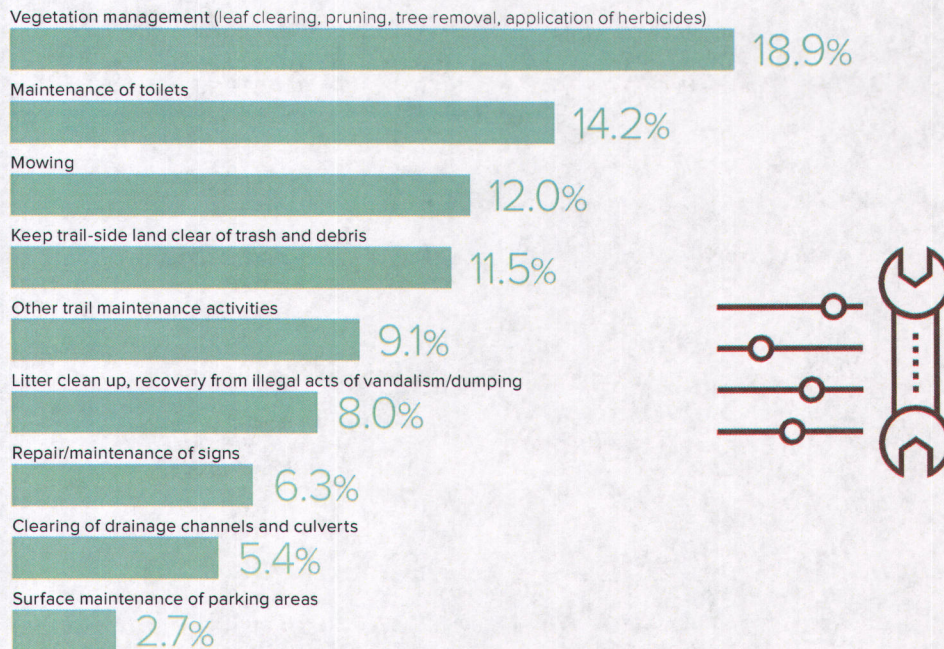
processes, they represent a small amount of what agencies normally spend on trail maintenance. Given the widely different sizes of the surveyed agencies, there was a significant range in the maintenance budgets; annual maintenance budgets ranged from \$2,000 to \$5,000,000. Agencies with limited resources available, such as Washoe County Parks, have no specific funding for trail maintenance in their budget, and therefore rely heavily on the efforts of active volunteer groups to maintain trails. Seven of the ten agencies rely on volunteers or non-profit organizations for some of the trail maintenance, however, many agencies contract with private firms or have paid staff that performs maintenance duties. The importance of volunteers for trail maintenance responsibilities is consistent with national trends; the 2014 RTC study found that 58% of trails benefited from volunteer groups

performing maintenance tasks, up from 46% in their 2005 study, and 43% of agencies utilize paid municipal staff (see Figure 5-3).

Of the responding agencies, vegetation maintenance and surface clearing were the most costly maintenance tasks. This is also consistent with the RTC study, which found that vegetation maintenance, including mowing, makes up about 30% of average maintenance budgets, while litter clean up and clearing the trail and surrounding land clear of debris and trash is close to 20%.

The 2014 RTC survey found that the average annual maintenance costs for paved asphalt trails were \$1,971 per mile and \$1,006 for crushed stone trail, excluding major repairs.

Figure 5-3. Percent of Maintenance Budget by task: Rails-to-Trails Conservancy Survey



Source: Rails-to-Trails Conservancy, Maintenance and Cost of Trails, 2014



## Trail Partners in Riverside County

A number of partners throughout Riverside County provide trail users with a high-quality trail experience by operating, maintaining and otherwise supporting trail systems in their respective areas or managed lands. These partners range from the federal to the community level, with varying operational context from each organization. This section describes how each partner can provide support to the development of a comprehensive trail network in Riverside County. The following partner organizations are presented as potential partners for the County and represent opportunities for future partnerships.

A table has been provided at the end of this section that classifies each agency's potential methods of assistance. These categories are, advocacy, design, planning, funding assistance, planning, marketing and promotion, and land or trail management, operations and maintenance.



### FEDERAL

Riverside County consist of approximately 61% federally owned lands (see existing conditions). Thus, federal partners have a heightened sense of importance in creating high quality trails and functional trail systems. While these partners have a large amount of land, they also have heavy environmental restrictions placed on these lands to ensure protection of ecosystems and the most appropriate use of the land. Thus, the use of federal lands to connect trail systems is complicated. Furthermore, the remote location of some of these sites make for connecting and managing trail systems highly inefficient.

Major federal land partners are housed within the US Department of the Interior, National Park Service, and Bureau of Land Management. Each federal land is managed differently according to the manager for each unit of land. For example, federal park lands generally have a more intense recreation or use element than preserves. The major partners in the County include:

**National Park Service (NPS):** The NPS operates Joshua Tree National Park. This park offers a large amount of recreational facilities within the unit. The park serves as a preserve to the Colorado and Mojave Deserts, both are rare ecosystems that provide for scientific study and interpretation. Due to the park's remote location, it is generally unable to connect to outside recreational facilities as part of a trail network. The park is home to a number of different trails, with over twenty miles of hiking trails and long distance equestrian trails.

Additionally, the NPS has provided assistance to the County in the past. Other grants have been provided to municipalities in the past to develop local parks and historic places. The NPS overall administers a number of grant programs for communities and natural areas that offer significant potential in developing trails. These additional grant programs vary in scope, but are generally focused around preservation, interpretation, and development of natural areas for recreation.

**US Department of the Interior: Bureau of Land Management (BLM):** The BLM is responsible for the oversight of many different land managers at the federal level. The departments mission is to sustain the health, diversity, and productivity of America's public lands for the use and enjoyment of present and future generations. In Riverside County, the BLM manages the Dos Palmas Preserve, which has multiple types of natural surface trails, although the focus for the unit is not recreation in nature.





The BLM is another federal agency responsible for the execution of federal funding assistance to communities that wish to develop land and increase recreational opportunities within communities. The BLM executes right-of-way assistance grants for federal lands, the Oregon and California Lands appropriation, and other initiatives passed down by the federal government.

**US Fish and Wildlife Service:** This department's mission is focused primarily to conserve, protect and enhance fish, wildlife and plants and their habitats. Thus, the scope of this entity is limited to the Coachella Valley National Wildlife Refuge is fairly limited in the trails that are provided for the County or its visitors. The only trail located on this property allows equestrian use, but does not allow for general parking. The trail is primarily a connection for other trails in the area.

**US Department of Agriculture: US Forest Service (USFS):** The purpose of the USFS is to sustain the health, diversity, and productivity of the nation's forests and grasslands to meet the needs of present and future generations. In Riverside County, the USFS manages the San Bernardino and Cleveland National Forest operations. These operations include the Front Country Ranger and San Jacinto Ranger District. On USFS lands, hiking, mountain biking and equestrian trails are present.

**Military Installations and Bases:** The US Military has a presence in and around

Riverside County. The US Army Corps of Engineers is responsible for the administration of the Prado Dam, while the dam and its recreational facilities are managed by San Bernardino County. The March Air Reserve Base provides for some recreational activities in the County, but is limited by operations of the base itself and tickets must be acquired by the Ticket and Tour Office.



## STATE

The two agencies with responsibilities for trails in Riverside County are the CA Departments of Parks and Recreation, and CA Department of Fish and Wildlife. Similar to the federal partners in the County, these are primarily sources of potential funding for grants and other technical assistance.

### **University of California at Riverside:**

The University is a statewide system of higher institutions with the purpose of postsecondary education. The campus is home to the botanical gardens, a 40-acre site which is home to hiking trails on the campus. The campus is responsible for the maintenance of the trails.



**CA Department of Parks and Recreation:**

The CA Department of Parks and Recreation operates similarly to the NPS, in that they provide for state recreational areas on tracts of land that may also help to protect natural and cultural resources. This partner is able to provide valuable funding assistance and is able to assist in making trail connections.

**CA Department of Fish and Wildlife:**

The CA Department of Fish and Wildlife serves a similar function to the US Fish and Wildlife Service, but at a state scale of governance. This partner also has limited capacity to manage infrastructure within the County and has primarily been a funding resource.

**PRIVATE**

Many private groups operate within Riverside County that support either individual trails, trail systems or parks containing trails. These groups range from friend's groups, to health foundations, environmental protection groups, and many others.

**Greater Riverside Chamber of Commerce:**

The Greater Riverside Chamber is committed to helping local companies grow their business by taking the lead in programs and efforts that help create a strong local economy and make our community a great place to do business.

This non-conventional trail partner could be approached to assist with the marketing and promotion of Riverside County Trails and Natural Areas. Other chambers have been very helpful in promoting businesses and places to live in areas with established trail systems.

**Environmental Protection****Center for Natural Lands Management**

**(CNLM):** The CNLM manages preserves throughout Washington and California. Their focus is to protect threatened, endangered or rare species and habitats throughout these areas. Each preserve is given its own stewardship staff, who provides for the care of the land and species. In Riverside County, the CNLM manages the following preserves:

Bogart Wash, CVAG I-10, Dos Palmas, Four Seasons, Johnson Ranch, Lincoln Ranch, Mockingbird Canyon, Roripaugh Ranch, Skunk Hollow, Smoke Tree, Summerhill, Thousand Palms Oasis, Warm Springs, Wilson Creek and Wilson Valley. Since the focus of these tracts are preservation, it is unlikely that a diverse range of trail uses would be provided in the areas managed by the CNLM.

In Western Riverside County, the CNLM works to provide for trail maintenance, cleanup and landscaping, and some habitat restoration through seed planting. The organization frequently works with local schools who wish to expose primarily school aged students to the outdoors. In the Thousand Palms Oasis Preserve, the CNLM also provides for similar activities, along with docents in the visitor's center. Volunteers in this preserve also help to lead trail hikes.

**Inland Empire Waterkeeper:**

Inland Empire Waterkeeper's mission is to protect and enhance the water quality of the Upper Santa Ana River Watershed through programs of advocacy, education, research, restoration, and enforcement in San Bernardino and Riverside County.

The Inland Empire Waterkeeper's small staff have worked to mobilize community efforts since 2005. They are a grassroots environmental movement that supports the restoration and enjoyment of the Santa Ana River by its neighbors. The group collects samples water, conducts research, restores habitats and conducts outreach efforts. The group hosts corporate cleanups and other volunteer days.

**Riverside County Parks Foundation:**

The Riverside County Parks Foundation is an organization of parks and recreation enthusiasts dedicated to improving lives through parks. The Foundation promotes volunteerism to support parks, advocates on behalf of the parks, and provides educational programming to help connect people to parks and healthy living. Major areas of focus are environmental education, health and recreation, and public lands stewardship.



**Sierra Club:** The Sierra Club is an environmental non-profit organization whose aim is to protect natural and wild places. In Riverside County the San Geronio chapter of the Sierra Club works towards a variety of goals including conservation-oriented land use policy, the partnership of clean energy and conservation in the desert, as well as the preservation of trails and open spaces. This organization also plans and leads hiking and camping excursions as well as clean ups across Riverside County.

### Friends Groups

Friends groups typically provide trails with a diverse range of support. This support can come in the means of providing for ongoing trail and trailside cleanup, constructing trail facilities such as restrooms, improving the character of the trail, volunteer work days, fundraisers, and many other activities. These groups provide trail managers with much needed capacity to ensure a trail or park facility functions well as a recreational facility. Generally, friends groups align with a geographic area or specific facility to ensure that resources are not spread thin.

**Friends of Hidden Valley Preserve:** This friends group mission is to maintain and protect Hidden Valley Wildlife Preserve and enhance the educational opportunities it provides for present and future generations. The group accomplishes its goals through providing advocacy, operational support, educational programming, resource management and technological support.

This friends group works to fill in resource gaps where the Hidden Valley Nature Center may not have the capacity to fill itself. The site has access to over 25 miles of equestrian and hiking trails. The group helps to staff the center and provide for events.

**Friends of Riverside Hills:** The Friends of Riverside Hills focus is to support the protection, expansion and preservation of open spaces and natural habitats. This group is focused on providing Box Springs Mountain Park with needed amenities for multiple types of paths. The group has also lobbied extensively to entities to provide necessary elements to the park.

### Friends of the Coachella Valley (CV) Link

**Trail:** The Friends of the Coachella Valley (CV) Link has organized around the mission of encouraging a healthy lifestyle and building the CV Link Trail. This trail is a fifty-mile facility that will incorporate multiple new technologies to encourage an active lifestyle. The group organizes trail cleanup and maintenance, operational and educational support, resource management, and advocates for the trail. The CV Link is located in eastern Riverside County.

The Friends of the CV Link Trail work to advocate, fundraise, provide office support, create web resources, event planning and other actions to ensure that the trail is built and utilized. The group host numerous events in support of the trail, and frequently work with the County to establish partnerships between groups.

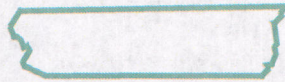
### Friends of the San Jacinto Mountain County

**Parks:** The goal of this friends group is to support the work of the Riverside County Regional Parks & Open-Space District in the San Jacinto Mountain Area. In doing so, the group directs equipment purchases, educational and interpretive programming and other assistance to the parks directly.

**Inland Valley Mountain Bike Association:** The Inland Valley Mountain Bike Association aims to develop and care for sustainable, multiple-use trails while fostering a community that participates in healthful activity, preserving the environment and stimulating the local economy through advocacy, education and participation. The group utilizes volunteer time towards the development of trail systems, community meetings and bicycling encouragement events.



**Riverside Recreational Trails:** Founded in 1987 by a group of trail enthusiasts, this group is dedicated to protecting the local trail system and offers many activities throughout the year. The organization provides both equestrian and non-equestrian events. The organization has adopted trail segments for management, constructed facilities and donated money to parks for maintenance and equipment purchases.



## REGIONAL AND COUNTYWIDE

**Coachella Valley Association of Governments (CVAG):** CVAG is responsible for the intergovernmental coordination of governments in eastern and central Riverside County. The regional coalition aims to provide better quality of life and balanced growth by providing for resources across agencies in community, energy and environmental, and transportation resources.

The County is able to utilize CVAG as a partner in coordinating actions by multiple entities through its committees. The Transportation Department of this group also manages the transportation program for the County, which could act as a funding source for the overall development of the trail system.

**Desert, Jurupa Area and Valley-Wide Recreation and Park Districts:** Recreation districts have been established in these areas to provide for parks and recreational facilities in the respective places. Having these districts in place creates a special incentive for local users to have access to parks, trails and other facilities funding through special provisions.

**Riverside Community Health Foundation:** The Riverside Community Health Foundation aims to improve the health status of Riverside County. The foundation provides funding, developing and operating partnerships and collaborations that provide expanded



### Adopt-A-Trail

The RCRPOSD operates an Adopt-A-Trail program (AAT) to engage community members and groups in the maintenance and protection of County trails and playgrounds. The County provides training for trail volunteers as well as the necessary tools to perform work. Currently the County operates the program in 8 parks and open spaces, and for four urban trails.

## Adoptable Trails

PARK/OPEN SPACE	ADOPTABLE TRAILS
Bogart Park	Bogart Loop, Meadow Loop, Playground
Box Springs Mountain Preserve	M Trail; Edison Trail; C Trail; Skyline Loop # 1; Skyline Loop #2; Sugarloaf Trail; Two Trees Trail; Two Trees Trailhead; Hidden Springs Trailhead
Idyllwild Park	Perimeter Trail, Hillside Trail, Summit Trail
Idyllwild Nature Center	Nature Center Trail, Equestrian Trail
Lake Cahulla	Cove to Lake Trail
Lake Skinner Recreational Area	Shoreline Trail #1; Shoreline Trail #2; Playground #1; Playground #2
McCall Memorial Equestrian Park	Interior Equestrian Trail
Gilman Historic Ranch & Wagon Museum	Bobcat Trail
Hidden Valley Wildlife Area	Native Plant Gardens
Harford Springs Reserve	Trails #1-6; Mockingbird Trail
Urban Trails	Jim Real Trail; Rider Street Trail; Santa Ana River Trail; Mary Tyo Equestrian Staging Area



access to high quality health care services and education. The foundation provides grants, organizes community groups, and utilizes educational programs to improve community health. As a health foundation, there is likely little direct involvement in the ongoing operations and maintenance of trails, but much support is provided in the support of trails as a community health tool.

**Riverside County Economic Development**

**Agency:** This agency is in place to enhance the economic position of county and county residents, improve quality of life, improve existing communities, provide cultural and entertainment activities, and others. In promoting these activities, many economic development agencies have found that the ability to commute greatly contributes to their goals. Agencies have provided funding for design, construction, planning and other marketing for trails.

**Riverside County Healthy Coalition:** The coalition was formed to promote, improve and sustain social and physical environments for healthy eating behaviors and active lifestyles for wellness through policy development and advocacy, environment change and community empowerment in Riverside County. The coalition has been providing grants, organizing meetings, and conducting environmental health programs which all are in line with trail efforts.

**Riverside County Sheriff:** Safety and law enforcement are a large concern for many trail users and adjacent property owners. The Sheriff's Department is responsible for many areas that have trails and utilize non-automotive means to patrol these areas. Careful coordination with law enforcement and other emergency responders is key to ensuring the overall safety of trail users in rural areas. The Sheriffs department have mapped off-highway vehicle trails to support this initiative, but it could be expanded to future regional trails since enforcement for these facilities contain similar issues.

**Riverside County Transportation Commission:**

The Riverside County Transportation Commission is responsible for planning and implementing transportation and transit improvements, assisting local governments with money for local streets and roads, helping to smooth the way for commuters and goods movement, and ensuring that everyone has access to transportation.

The commission is largely responsible for coordinating investments into the transportation system. The commission could serve as a resource in coordinating trail investments and maintenance efforts.

**Riverside County Transportation Department:**

The County transportation department is the lead agency for maintenance of roadways, contract services, land development and other typical functions within the unincorporated area of Riverside County. This department is integral to the development of the overall transportation system within the County, including diverse types of transportation leading into the future.

**Southern California Association of**

**Governments (SCAG):** SCAG aims to provide a high-quality of life for Southern Californians by fostering the realization of regional plans. The organization aims to develop long range regional transportation goals, including sustainable communities, forecast population and transportation growth, housing needs, and improve air quality. This organization will be helpful in the establishment of communications across the region.

**Western Riverside Council of Governments**

**(WRCOG):** WRCOG enables this area to speak with a collective voice on important issues. The focus on this entities collective governance is on transportation, environment, energy, economy and health.



## Environmental Protection

### Riverside County Habitat Conservation

**Agency (RCHCA):** The RCHCA was formed in 1990 for the purpose of planning, acquiring, and managing habitat for the Stephens' Kangaroo Rat and other endangered, threatened, and candidate species. The RCHCA provides for management of many different preserves within the County, but does not necessarily provide for direct trail resources throughout the area.

The RCHCA provides an opportunity to organize coalitions around the development of natural surface trails in areas within the entities jurisdiction. It also enables entities within the area to identify feasible areas for trail development.

### Riverside-Corona Habitat Conservation

**District: (RCRCD)** Similar to recreation districts, the RCRCD special district established in San Bernardino and Riverside County aims to conserve the natural resources of areas within in southern California. The RCRCD provides technical advice to land users, educational programs for the community, and conducts on-the-land conservation projects. The focus on this district is centered on resource preservation and education.

### Western Riverside County Regional Conservation Authority (RCA):

The Western Riverside County Regional Conservation Authority acquires land for conservation and habitat protection purposes. The organization also reviews development applications and has some trail space on reserves. The primary role of the organization is to provide land management, facilities maintenance, ecological monitoring, and control for unauthorized access.

## OVERVIEW OF TRAIL PARTNERS

A table of the partners in Riverside County has been provided to illustrate which stakeholder is capable of assisting with certain tasks (see Table 5-4). These stakeholders are categorized by scope of operations and type of tasks conducted. Scope of operations pertains to how large an organization operations from the regional or county level, to the federal level. Types of task are broken down by advocacy, planning, design, management, operations and maintenance, and non-conventional partners. These categories are generalizations and do not reflect the on the ground, every day operations of the organizations.

*The RCHCA offers educational opportunities to the local community like hikes during their Endangered Species Day event.*





Table 5-4. Trail Partner Applicability Matrix

RIVERSIDE COUNTY (RC) PARTNER NAME	SCOPE OF OPERATIONS	ADVOCACY	DESIGN
CA Department of Parks and Recreation	State		●
CA Department of Fish and Wildlife	State		●
RC Flood Control and Water Conservation District	County		●
RC Habitat Conservation Agency	County		●
RC Health Coalition	County	●	
RC Parks and Open-Space District	County		●
RC Planning Department	County		●
RC Sheriff	County		
RC Transportation Commission	County		●
Riverside University's Public Health System	County	●	
RC Transportation Department	County		●
Riverside Economic Development Agency	County	●	
Coachella Valley of Governments	Regional		●
Desert Recreation District	Regional		●
Jurupa Recreation and Park District	Regional		●
Valley-Wide Recreation and Park District	Regional		●
Metrolink	Regional		
Riverside Land Conservancy	Regional		
Riverside-Corona Habitat Conservation District	Regional		●
Southern California Association of Governments	Regional		●
Western Riverside Council of Governments	Regional		●
Western RC Regional Conservation Authority	Regional		
Army Corps of Engineers	Federal		
March Air Reserve Base	Federal		
US Department of the Interior: Bureau of Land Management	Federal		●
US Fish and Wildlife Service	Federal		
US Forest Service	Federal		●
Friends of Hidden Valley	Friends Group	●	
Friends of Riverside Hills	Friends Group	●	
Friends of the CV Link	Friends Group	●	
Friends of the San Jacinto Mountain County Parks	Friends Group	●	
Center for Natural Lands Management	Private		
Inland Empire Waterkeeper	Private	●	
Inland Valley Mountain Bike Association	Private	●	●
Riverside Chamber of Commerce	Private		
Riverside Community Health Foundation	Private	●	
Riverside County Parks Foundation	Private	●	
Sierra Club	Private	●	
Wildlands Conservancy	Private	●	
University of California at Riverside	University		