

Table 3.1-E, Potential Park Uses (Race Track Scenario)

Park	Description	Size	Potential Park Uses
1	Same as Table 3.1-C	9.97 acres	Same as Table 3.1-C
2	Same as Table 3.1-C	4.0 acres	Same as Table 3.1-C
3	Same as Table 3.1-C	9.1 acres	Same as Table 3.1-C
4 – 6	Same as Table 3.1-C	6.4 acres	Same as Table 3.1-C

Trail System

The proposed development is planned with an off-street (pavement) trail system that parallels streets and drainage ways throughout the site. **Figure 3.1-10, Public Facilities Sites** shows proposed trail locations, however, the final location of the off-street trail system may be adjusted to accommodate final engineering of the drainage system and development plans. A conceptual design for drainage swales and pipeline is shown in **Figure 3.1-4, Conceptual Drainage Plan**. Street-parallel trails will be constructed per the designations identified on **Figure 3.1-10, Public Facilities Sites** and illustrated in the cross sections found in **Figure 3.4-18, Tyler Street Trail** and **Figure 3.4-19, Avenue 62 Trail** found in Section 3.4 – Landscape Design Guidelines. The off-street system is supplemented by sidewalks and bike lanes within the paved roadway. In addition to providing access to the local parks, the trail system also provides a pedestrian linkage to the schools and to key commercial sites. The proposed trail system provides connections through the site to implement its portion of the County-planned trail system for the area.

Naturalized Open Space

The system of drainage channels throughout the project site will offer a scenic amenity to residents of the Kohl Ranch Specific Plan. The channels will be graded and will contain native desert vegetation that will also provide a buffer between land uses. The acreage of this natural open space will vary depending upon whether the golf courses or other recreational uses are built. Planning Area D-1 (110.66 acres) will remain in open space due to airport-related restrictions.

Landscape Concept

The landscape concept for the Kohl Ranch Specific Plan provides criteria for the treatment of areas within roads and easements, medians, land use transitions, development edges and project entries. The landscape concept has been conceived to organize the development and to establish a unified landscape framework that provides continuity throughout the project area and supports the community themes. Proposed landscape materials are intended to direct and guide the resident or visitor to the development, screen sensitive views, and frame or create focal points and views as the motorist and pedestrian moves through and around the project.

The landscape concept consists of a hierarchy of recommended plantings that correspond to the roadway classifications and project entry statements. More detailed descriptions of these landscape features are provided in Section 3.4 – Landscape Design Guidelines. The landscape concept is based upon the use of natural desert vegetation both for its visual effect and drought tolerant characteristics. The use of water efficient irrigation practices is encouraged, with canal water used for golf courses and public open space areas.

Five basic streetscape planting schemes are proposed:

- Palm Tree Streetscape;
- Formal Canopy Streetscape;
- Informal Canopy Streetscape;
- Windrow Planting; and
- Expressway Planting.

Project Entries

Major project entries generally are identified by groves of palm trees, which create visual interest through the introduction of a vertical element and a beacon that is visible at a distance across the flat landscape. The palm tree plantings are carried into the site along major project roadways, using a less dense spacing between plantings than the concentrated plantings at project entries. Major project entries are identified at the following locations:

- Avenue 60 at northern project boundary;
- Avenue 62 and Polk Street; and
- "E" Street and Avenue 66.

Minor project entries will be treated with a scaled-down version of the palm tree concept. Entries and intersections will be precisely located as internal roadways are designed.

Streetscapes

In order to achieve a cohesive overall circulation system for the Kohl Ranch Specific Plan and provide a strong community structure, a consistent streetscape treatment is recommended for public and private rights-of-way. The general landscape concept combines the use of palms trees with citrus understory, massed in selected locations, interchanged with formal and informal canopy plantings and use of other desert plant material. The streetscapes are broken down into three major categories with special features and plant forms as indicated.

The proposed plantings along major project roadways are based upon the roadway significance and strategic location within the project site. Because of its importance as a connection and northern project entry, Tyler Street, north of Avenue 62, is proposed to employ the palm tree planting concept, with breaks in the plantings where tall trees are prohibited through the airport safety zones. If the race track scenario is developed, Windrow Plantings will be used along its frontage to screen from residential uses to the west.

Formal canopy streetscape plantings are planned along Avenue 62, "B" Street, "C" Street, "E" Street, "F" Street, Tyler Street (south of Avenue 62) and several local roadways that provide internal circulation between neighborhoods. These streets provide definition to the project neighborhoods. Informal canopy streetscape plantings are recommended for local roads that are more interior to the site.

Buffers and Land Use Transitions

Windrow plantings are recommended to serve as buffers between land use types, along drainage channels and project edges, and within residential neighborhoods to mitigate against high desert winds. Windrows also can be used to provide structure to project neighborhoods and can be integrated with the project trail system, but must be sited to take into consideration airport safety zones and screening of the race track, if developed.

Community Structure Development Standards

- 1) All detailed landscaping programs for planning areas and roadways will be prepared by a qualified landscape architect for review by the County.
- 2) Common open space areas within each planning area may be devoted to passive or active uses, and will, to the extent feasible, be coordinated with any open space in adjoining planning areas to create a continuous network. The exact design and layout of facilities will be accomplished in conjunction with detailed future tract layouts.
- 3) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped common areas shall be submitted to the Riverside County Planning Department for approval. The improvement plans shall include, but not be limited to, the following:
 - Final grading plan;
 - Irrigation plans prepared by a landscape architect;
 - A landscaping plan with specifications for mulching and staking methods; locations, type, size and quantity of plantings;
 - Fence treatment plans; and
 - Special treatment/buffer area treatment plans.
- 4) The applicant developer and/or builder shall be responsible for maintenance and upkeep of all common landscaped areas and irrigation systems within its ownership parcels until such time as these operations are the responsibility of other parties.
- 5) At the time of recordation of any final subdivision map which contains a common open space area, the subdivision shall have those common areas transferred to the master property owners association or an appropriate public maintenance agency who will take responsibility for maintenance.
- 6) Irrigation of common open space areas and golf courses will be accomplished with canal water and/or reclaimed water to the extent possible.
- 7) For further landscape development standards, please refer to Section 3.4 – Landscape Design Guidelines.

3.1.6 Water and Sewer Plan

The project study area is completely within the service boundary of the Coachella Valley Water District (CVWD) which provides water and sewer service to the project area. The water and sewer plan is based on an analysis of water and sewer infrastructure which evaluates availability of services, calculates water demands and sewer generation quantities, and proposes infrastructure facilities which allow for the advancement of development. The purpose of the water and sewer plan is to provide for the backbone improvements which will be capable of serving the ultimate project, in addition to the individual portions independent of their progression.

Existing Water and Sewer Conditions

There are some domestic water distribution facilities within the project boundary. Domestic water provided within the CVWD service boundary comes from ground water which is chlorinated, the Colorado River and the State Water Project. Previous studies conducted in the area indicate that some ground water must be treated for arsenic to meet the State Drinking Water Standards. A Domestic Water, Non-potable Water, Canal Water and Sanitation System Installation and Service Agreement (Document No. 2002-538101) is currently on file at CVWD which provides for mitigation of arsenic when required, through payment of fees for arsenic treatment facilities. CVWD is located at 85-995 Avenue 52 in Coachella, California.

There is an existing tank site southwest of the intersection of Avenue 68 and Harrison Street with a 24-inch waterline that brings water to the project. An 18-inch water line is located in Tyler Street at the intersection of Tyler and Avenue 66 which extends north along the project boundary to Neighborhood I. A 24-inch water line is also located at the intersection of Tyler Street and Avenue 66. This line runs east along the existing school site boundary (Planning Area M-4). An existing arsenic treatment plant is located on the northeast corner of Tyler Street and Avenue 66.

There are some sewer facilities located within the project boundary. An existing sewer line currently services the existing school site (Planning Area M-4) and runs through Neighborhoods J, L, and M to an existing sewage pump station located in Planning Area J-4. An existing 12-inch sewer force main runs north to Avenue 62 and intersects with an existing CVWD force main. It is then pumped to Wastewater Reclamation Plant No. 4.

CVWD operates and maintains its Wastewater Reclamation Plant No. 4 located between Avenue 62 on the north, Avenue 64 on the south, adjacent to the Whitewater River on the east, and approximately 600 feet east of Fillmore Street on the west. This plant has a design capacity of approximately 5.0 MGD, and is estimated to be currently operating at half capacity. The plant currently treats to a secondary treatment level using stabilization ponds for finishing. The ponds are used for flow stabilization by allowing the depth to increase during peak wet water events.

Proposed Water System and Water Conservation Measures

This project will utilize a "dual water system" consisting of potable and non-potable water systems. The potable system will provide water for domestic purposes and provide water for fire protection. The non-potable system will provide water for irrigation purposes.

The proposed potable water system improvements as depicted in **Figure 3.1-6, Conceptual Potable Water Plan** will operate within a single pressure zone serving ground elevations from 88 to 167 feet below sea level. Hydraulic control for the project should be set using a reservoir high water level of approximately 83 feet above sea level. An existing tank site has been constructed at CVWD's recommended pad elevation of approximately 60 feet above sea level.

The potable water system is proposed to be fed by wells to be located within or near the project boundary. Four wells are anticipated to pump peak daily demands through an on-site system from 12-inch to 30-inch diameter transmission lines looped for maximum efficiency and fire flow delivery. Internal water lines feeding individual houses will range from 6-inch to 8-inch. Excess water during low and average flows will be conveyed to reservoirs to be located southwesterly as reflected in **Figure 3.1-7, Off-Site Water Improvements**, of the project. An existing 24-inch diameter transmission main will convey flows to the project when operating from gravity flow. Per CVWD's Master Water Plan, transmission mains are proposed adjacent to, and through the Specific Plan. The Specific Plan water system shall connect to any existing adjacent water facilities that have available capacity.

The potable water system will be designed to deliver gravity flow during high power cost windows. Well pumps will be controlled by level sensing devices and transmitters on the reservoirs. The water system will utilize a certain volume from the tanks, prior to the well pumps cycling, to ensure that adequate circulation of water in the tanks is maintained.

In accordance with the Water Conservation Plan, water conservation measures will be incorporated into the project plan to include water saving devices and systems. Further, the use of surface water will be used for irrigation purposes. Water conservation for the Kohl Ranch project will be achieved through:

- Dual-source water system to reduce groundwater consumption;
- Recycled and/or canal water for irrigation;
- Drip and other efficient irrigation;
- Intelligent irrigation controllers;
- Native and non-native drought-tolerant planting materials;
- Tiered water rates; and
- Use of proposed lake to double as a forebay for the non-potable water treatment.

Groundwater use will be reduced through the initial use of Colorado River water for non-potable water uses as part of a dual-source water system. The design will include a potable water system designed for indoor use only, as well as a separate non-potable water system for outdoor landscaping. The potable water system will be connected to the CVWD domestic water system, which relies upon groundwater. The non-potable system will use Colorado River water delivered by the existing agricultural canal system for landscaping and outdoor use. The majority of the landscaping throughout the project site is designated (in general terms) as "desert landscaping." And, the project's proposed high density residential uses reduce the amount of landscaped open space per unit, limits the number of individual pools, and encourages the use

of public facilities.

This non-potable system will be designed to meet recycled water standards in anticipation of the possible future availability of tertiary water when it becomes available from the nearby Water Reclamation Plant 4 (WRP-4). The project will be sewered and the majority of the demand on the Aquifer from the project will be from indoor use. In the event recycled wastewater from WRP-4 becomes available, project demand on the Aquifer will be further reduced; recycled water can be used to replace Colorado River water or groundwater for agriculture use in the area or on project landscaping. Additional Colorado River water could be freed up for agricultural use or to recharge the Aquifer.

CVWD has three WRP's that produced recycled water for a combined total of 8,289 acre-feet in 2008, which was used to reduce the demand on the groundwater basin. In addition, Desert Water Agency, a partnership with CVWD in the management of the Aquifer, operates a reclamation plant in Palm Springs and produced 3,000 acre-feet of recycled water per year, which also reduces the demand on the groundwater basin. This dual water system is being required of new, larger developments in the Lower Valley, including Kohl Ranch, as a result of SB1557, which finds that the use of potable domestic water for nonpotable uses is wasteful if suitable nonpotable water sources are available.

Additionally, CVWD is working with the cities in its service area to limit the amount of water that can be used for outdoor landscaping. As the result of the adoption of state-wide indoor water conservation measures requiring low flush toilets, shower and faucet flow restrictors, and other devices, the amount of water used inside homes has been significantly reduced. With the large number of new homes constructed, these conservation programs have reduced impacts of new development on the Aquifer.

The non-potable water system as reflected in **Figure 3.1-8, Conceptual Non-Potable Water System**, water source will be diverted canal water that would be delivered into an on-site system of lakes and pond(s). Hydropneumatic booster station(s) will pump water from the lakes and pond(s) throughout the development for irrigation purposes. The on-site water system consists of 8-inch to 42-inch diameter transmission lines looped for maximum efficiency and delivery at appropriate service pressures.

Proposed Sewer

The Specific Plan's sewer improvements as reflected in **Figure 3.1-9, Sewer Plan**, are based on an analysis of sewer infrastructure that elevates availability of services, calculates sewage generation, and proposes infrastructure facilities which allow for the advancement of the development. The purpose of the sewer plan is to provide for the backbone improvements that will be capable of serving the ultimate project, in addition to the individual portions independent of their progression.

The Specific Plan proposed sewer system conveys flows to the existing sewage pump station and ultimately to CVWD's treatment plant. The Specific Plan sewer system shall connect to any adjacent existing sewer facility that has capacity.

Water and Sewer Development Standards

- 1) All water and sewer facilities will be designed and constructed in accordance with the CVWD standards and specifications, American Water Works Association (AWWA), American National Standards Institute (ANSI) and the Standard Specifications for Public Works Construction, (GREEN BOOK).
- 2) The capital cost of on-site and off-site facilities necessary to serve the project site will be the responsibility of the applicant. Such facilities will be dedicated to CVWD, after construction, for the District to maintain and operate. Fee credits may be available for transmission, storage, and wells. As discussed above, a Domestic Water, Non-potable Water, Canal Water and Sanitation System Installation, and Service Agreement (Document No. 2002-538101) is currently on file at CVWD which provides for mitigation of arsenic through payment of fees for arsenic treatment facilities.
- 3) In accordance with the Water Conservation Plan (see Appendix F), water conservation measures will be incorporated into the project plan. The following measures will be utilized to reduce the impact on the underground basin and increase efficiency in water usage:
 - (a) A dual water system (potable/non-potable) will be installed to service landscaped areas.
 - (b) Irrigation lines will utilize canal water and/or treated effluent to irrigate landscape areas.
 - (c) Water conservation plumbing fixtures or water saving devices and systems shall be used in all construction.
 - (d) Where possible, the existing tile drains will be maintained to prevent high salt water from migrating to the groundwater basin.
 - (e) The project development shall conform with state, county, and CVWD regulations regarding water conservation and reclamation.
- 4) All development is required to landscape in accordance with **Table 3.4-A, Kohl Ranch Plant Palette** found in Section 3.4, and CVAG Ordinance 1302.1, Landscape and Irrigation System Design Criteria. This will ensure additional water conservation through the use of desert-appropriate landscape materials.

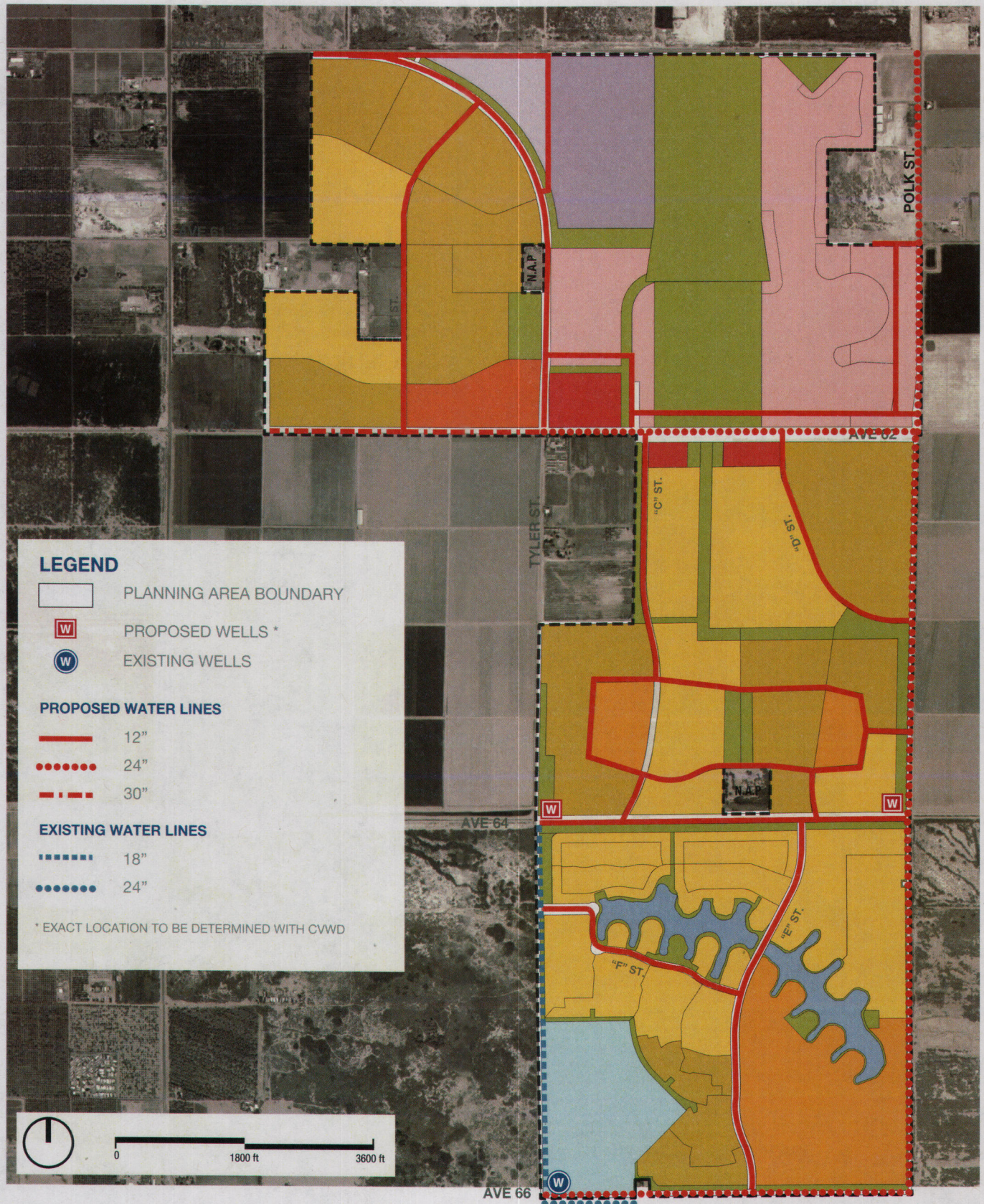
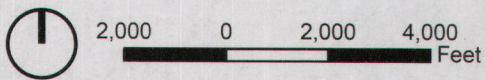
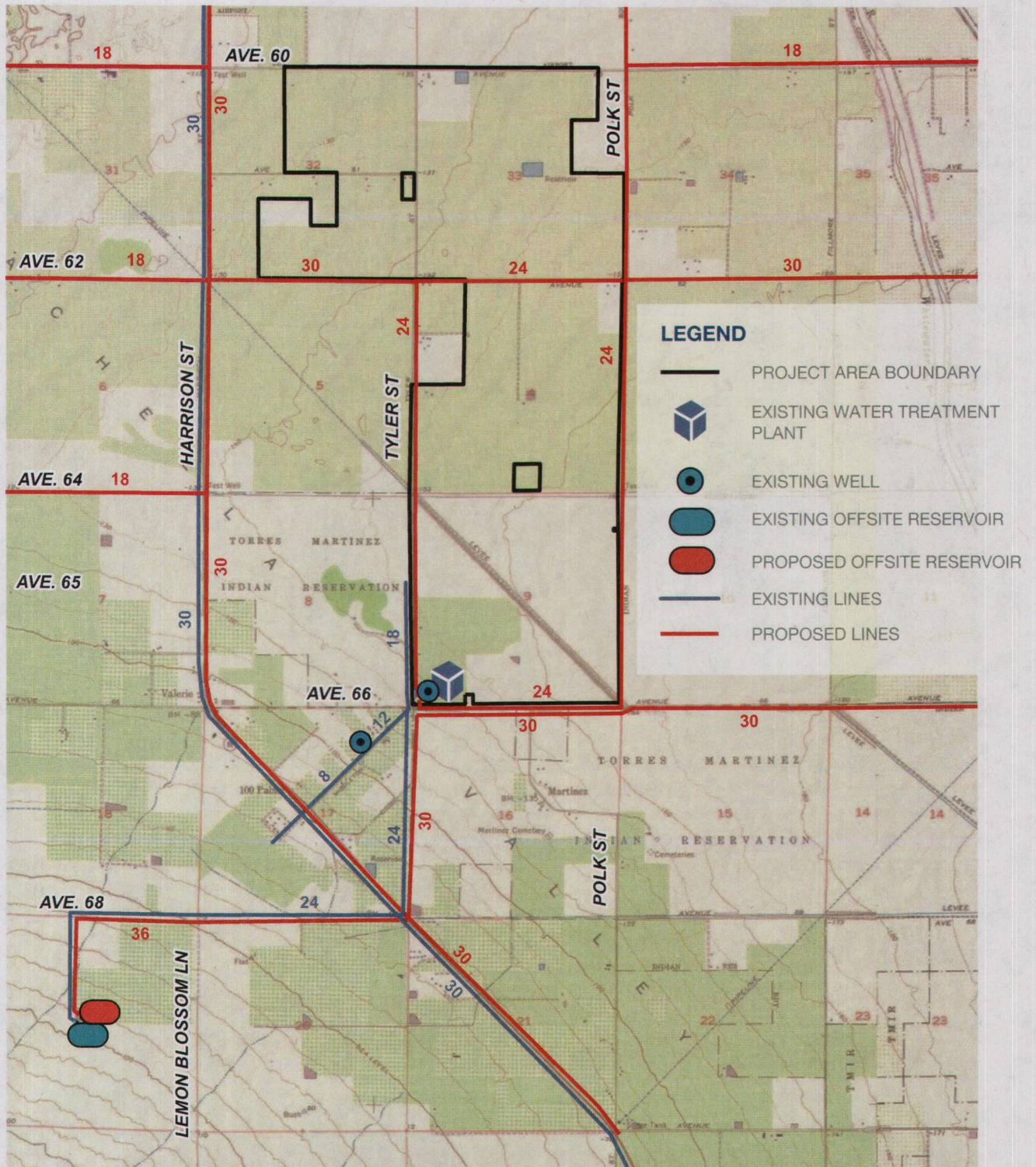
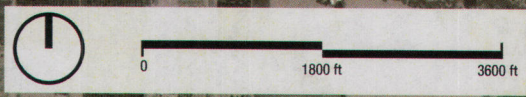
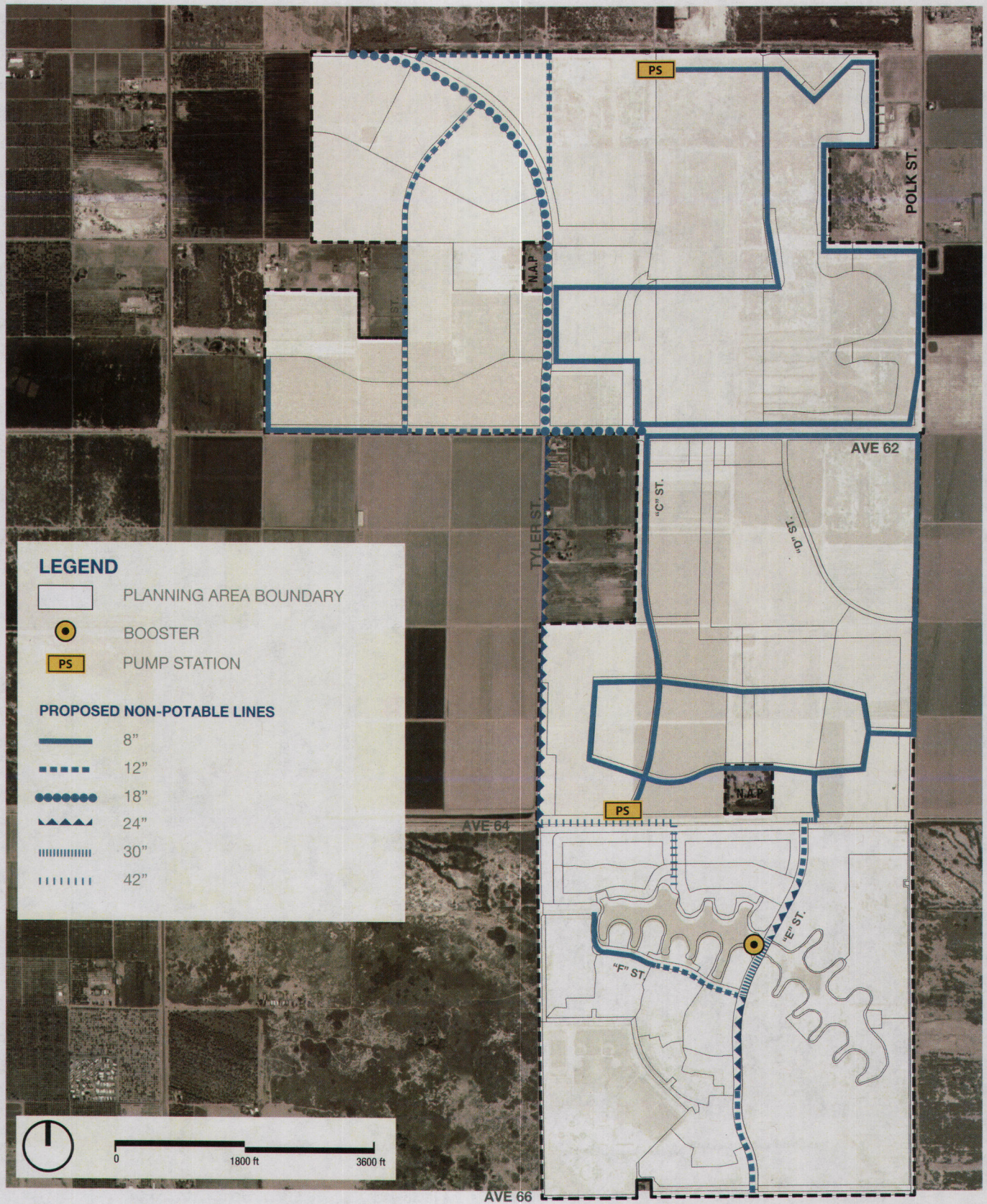
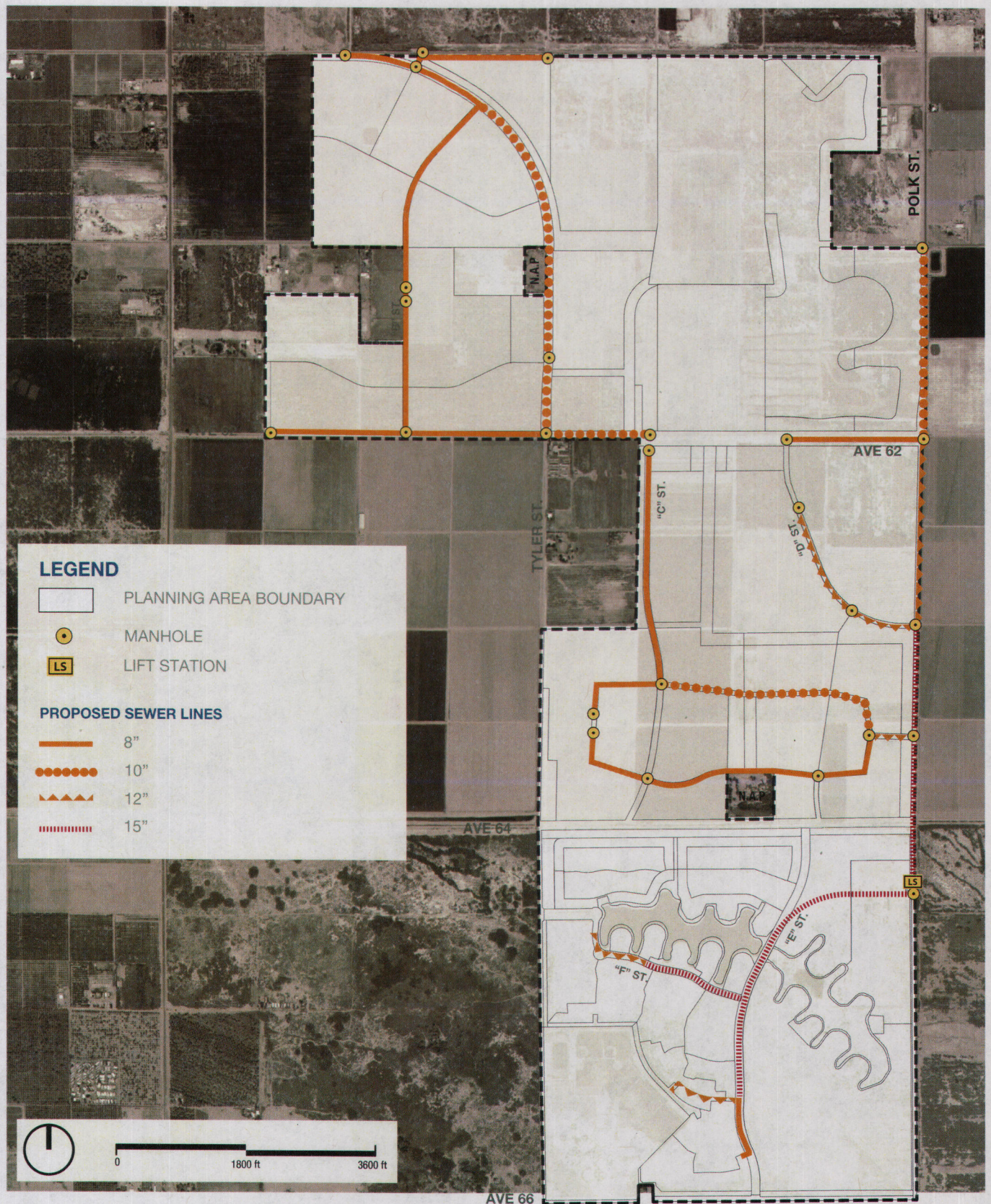


FIGURE 3.1-7 OFF-SITE WATER IMPROVEMENTS







3.1.7 Public Facilities Sites and Project Phasing

In order to insure timely development of public facilities, a phasing plan has been prepared for parks, schools and other public facilities as depicted in **Figure 3.1-10, Public Facilities Sites**.

Project Phasing Plan

Four phasing scenarios have been established for the proposed project as depicted in **Figure 3.1-11, Phasing Plan**. Each scenario represents an area of the site that is likely to develop as a unit. The purpose of a scenario is to establish the infrastructure required to initiate development, assuming that the scenario site is the portion of the total project site to be developed first. However, the scenario numbers do not necessarily indicate the sequence in which the site will be built out. For example, Phasing Scenario 2 and Phasing Scenario 4 could be constructed first. It is anticipated that the project will be developed over a twenty-five year period, in response to market demands, and according to a logical extension of roadways, public utilities and infrastructure. Efforts by local government to stimulate economic development in the project area, through the planned development at Jacqueline Cochran Regional Airport, can be expected to serve as an impetus to development of revenue-producing land uses in the northern portion of the project site. The project phasing scenarios are further described below.

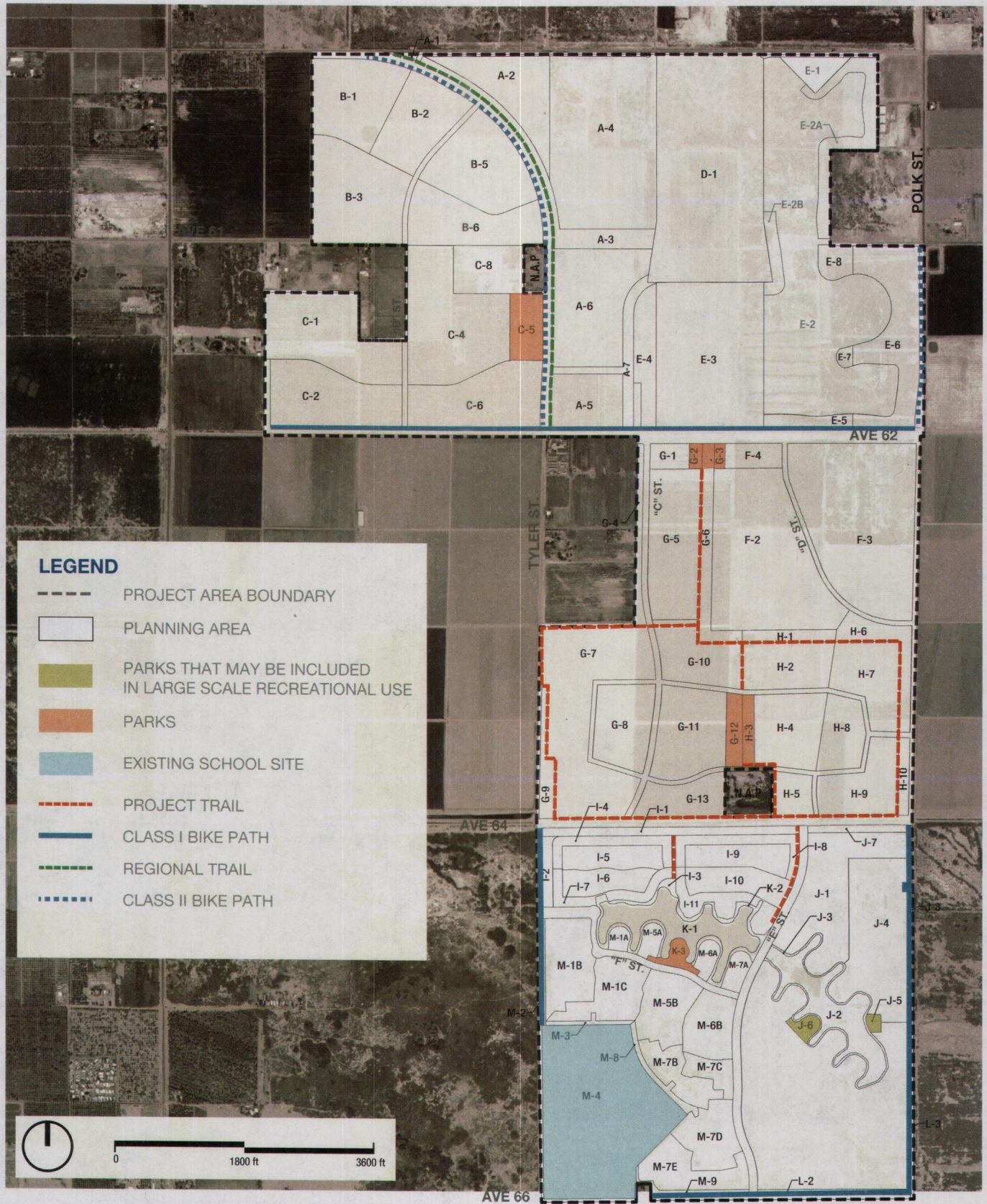
Public Facilities Phasing Schedule

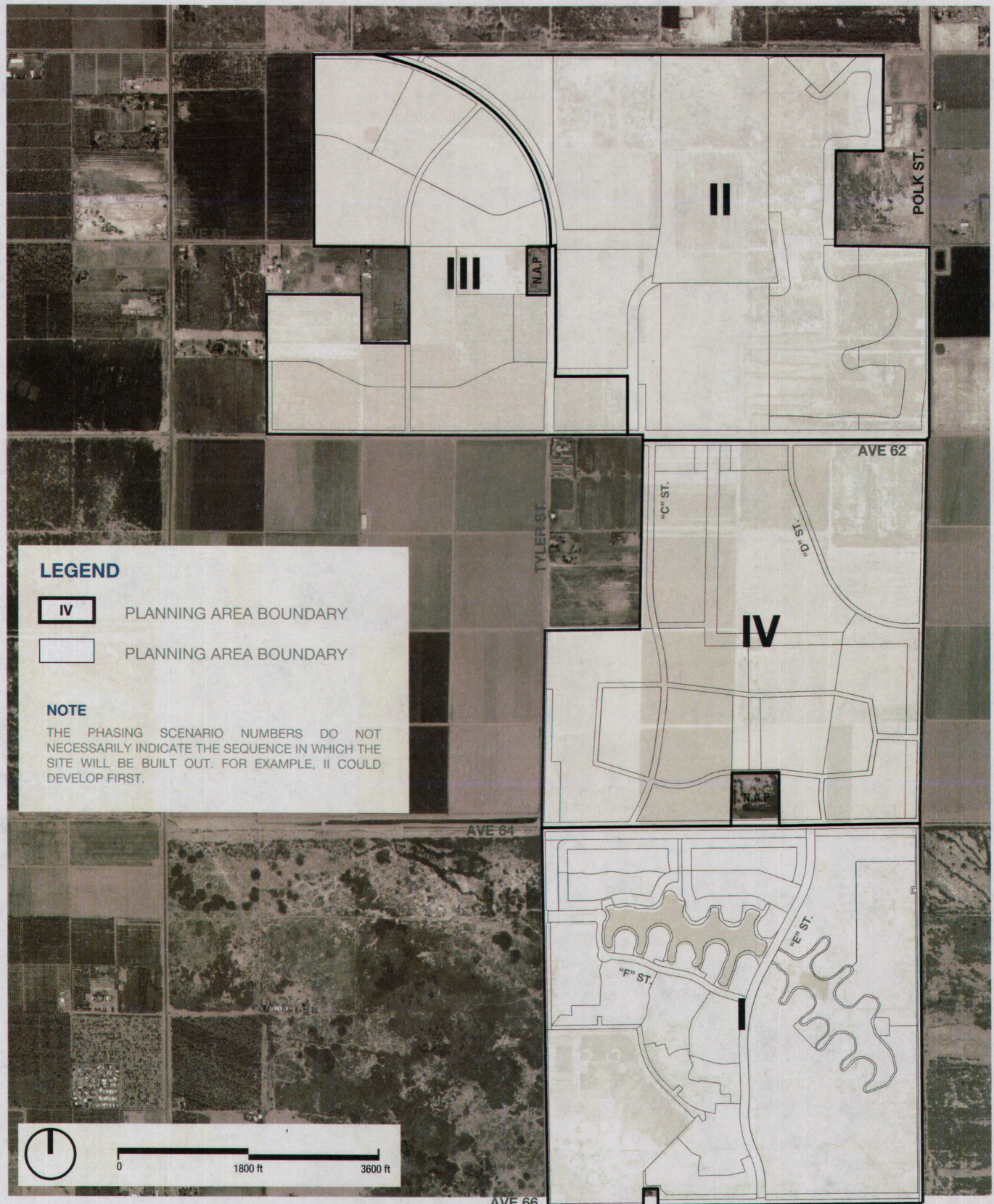
Public facilities shall be phased as indicated in **Table 3.1-F, Public Facilities Phasing Schedule**, below.

Table 3.1-F, Public Facilities Phasing Schedule

Planning Area	Public Facility	Size of Site (acres)	Milestones and Requirements (to be determined)
PHASING SCENARIO 1			
K-1/J-2	Lake	44.4	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
K-3	Park	3.2	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
J-6	Park	2.2	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
J-5	Park	1.0	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
PHASING SCENARIO 2			
-	-	-	Payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or industrial subdivision map is filed for Neighborhood E.
PHASING SCENARIO 3			
C-5	Park	9.97	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood C.
PHASING SCENARIO 4			
G-3	Park	1.19	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G.
G-2/G-6	Park	15.09	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G.
G-12/H-3	Park	9.14	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G or H.

Note: The park acreages assume that the golf course or race track would not be built. Planning Area M-4 is an existing school.





Phasing Scenario 1

Phasing Scenario 1 encompasses the southern segment of the project area. The infrastructure improvements described in this section assume that Scenario 1 would be the first area of the site to develop. Transportation improvements would involve both major and secondary roadways, including partial width of Polk Street from Avenue 66 to Avenue 64; Avenue 64 from Polk Street to Tyler Street; "F" Street between "E" Street and Tyler Street; and "E" Street between Avenue 64 and Avenue 66. Grading activities for this Scenario would be accomplished by stockpiling export on the Scenario 4 site to the north. Drainage for this scenario would require that off-site flows be picked up along the westerly boundary in master planned facilities, conveyed through the site and then released at a controlled rate along the eastern boundary of Polk. Sewer facilities would gravity flow to the easterly portion of the scenario site to the existing sewage pump station. From that location, an existing sewer force main will convey the flows to the existing waste water lines along Avenue 62. Water facilities to support the development of this scenario begin with the extension by others, of the existing 24-inch diameter waterline in Avenue 66 to the east at the intersection of Avenue 66 and Polk Street. A 24-inch diameter waterline will be constructed by others, in Polk Street between Avenue 66 and Avenue 64. A 12-inch diameter waterline will be constructed in Avenue 64 between Tyler Street and Polk Street in "E" Street and "F" Street. Two wells exist to provide adequate water supply and backup (one on the school site and one on Middleton Street just to the southwest of the project area). Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-G, Phasing Scenario 1 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	I-4	7.0	35
	I-5	10.6	51
	I-6	9.6	42
	I-7	6.7	29
	I-8	8.0	40
	I-9	11.5	57
	I-10	12.4	47
	I-11	10.1	37
	J-1	37.52	130
	J-4	43.36	189
	M-1A	2.5	8
	M-1B	18.9	68
	M-1C	16.4	61
	M-5A	4.1	14
	M-5B	15.7	61
	M-6A	3.8	14
	M-6B	13.4	57
	M-7A	4.1	20

Land Use	Planning Areas	Acreage	Units
Medium High Density Residential	M-7B	11	56
	M-7C	5.9	32
	M-7D	15.1	82
	M-7E	15.5	82
High Density Residential	L-1	106.2	1,158
Open Space	I-1	9.6	
	I-2	3.9	
	I-3	0.5	
	J-2	22.0	
	J-3	8.8	
	J-5	1.0	
	J-6	2.2	
	J-7	3.3	
	J-8	1.31	
	K-1	22.4	
	K-2	8.8	
	K-3	3.2	
	L-2	4.1	
	L-3	1.1	
	M-2	1.9	
	M-3	1.6	
M-8	2.9		
M-9	2.7		
Subtotal Scenario 1		490.69	2,370

Note: Planning Area M-4 (84.3 acres) is an existing school site.

Phasing Scenario 2

Phasing Scenario 2 is situated in the northeast corner of the project area. The infrastructure improvements described in this section assume that Scenario 2 would be the first area of the site to develop. Transportation improvements would involve expressway, arterial and secondary roadways, including partial width of Polk Street north of Avenue 62 to project boundary; and Avenue 62 between "C" Street and Polk Street. Grading activities for this scenario would be limited to the Scenario 2 site. There would be limited (if any) import/export required off-site. Drainage for Scenario 2 would require that off-site flows be picked up along the western and northern Scenario 2 boundary and in an interim swale along the westerly boundary of Tyler Street and conveyed to the proposed on-site open space channel areas (Planning Areas A-3 and A-7). Off-site flows from these open space areas would be conveyed along a drainage channel running along the west edge of Scenario 2 daylighting south of site through Scenario 4 that returns to a sheet flow condition, with no additional drainage facilities required. Sewer facilities would gravity flow to the southeast corner of the drainage areas to a gravity sewer presently being constructed by CVWD. Water facilities to support the development of this scenario begin with the extension of the existing 24-inch diameter waterline in Avenue 66 east to Polk Street and north in Polk Street to Avenue 60. A proposed 24-inch waterline in Avenue 62 between Tyler Street and Polk Street is also needed. Connecting to these 24-inch lines will be 12-inch lines in Tyler Street, Avenue 60 and internal to the Scenario 2 area to serve the area. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-H, Phasing Scenario 2 Details

Land Use	Planning Areas	Acreage	Units
Mixed Use/Air Park	A-2	24.45	
Mixed Use	A-6	46.18	
	E-2	127.34	
	E-2A	16.48	
	E-2B	4.92	19
	E-4	12.25	120
	E-5	5.31	5
	E-6	33.99	15
	E-7	1.32	
Heavy Industrial	E-8	4.94	
	A-4	81.17	
Open Space	A-1	0.63	
	A-3	10.77	
	A-7	9.97	
	D-1	110.66	
	E-1	5.61	
	E-3	69.33	

Land Use	Planning Areas	Acreage	Units
Subtotal Scenario 2		565.32	159

Phasing Scenario 3

Phasing Scenario 3 is located at the northwestern corner of the project area. The infrastructure improvements described in this section assume that Scenario 3 would be the first area of the site to develop. Transportation improvements would involve expressway, arterial and secondary roadways, including "A" Street north of Avenue 62; "B" Street between Avenue 62 and Tyler Street; Tyler Street north of Avenue 62; and partial width of Avenue 62. Grading activities would be limited to the Scenario 3 site. Drainage for Scenario 3 would require that off-site flows be picked up along the western project boundary, conveyed through the site and then released to the adjacent Scenario 2 area. Two small off-site retention facilities may be required east of Scenario 3. The retention facilities would have overflow weirs on the downstream side to return the storm flow to its original sheet flow condition. Sewer facilities would gravity flow to the southeast corner of the site to a gravity sewer in Avenue 62 presently being constructed by CVWD. Water facilities to support the development of this scenario begin with either the extension of the existing 18-inch diameter waterline at Tyler Street and "F" Street to the intersection of Avenue 62 and Tyler Street, or a 24-inch line will be extended westerly from Polk Street in Avenue 62 to its intersection with Tyler Street. From the intersection of Avenue 62 and Tyler Street, a 12-inch diameter waterline will be constructed north on Tyler Street to Avenue 60. From the intersection of Avenue 62 and Tyler Street, a 30-inch waterline will be constructed west to the project boundary. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-1, Phasing Scenario 3 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	B-3	39.01	117
	C-1	40.77	122
Medium High Density Residential	B-1	30	210
	B-2	25.24	177
	B-5	32.70	229
	B-6	26.18	183
	C-2	31.16	218
	C-4	49.83	349
Very High Density Residential	C-8	14.96	105
	C-6	30.85	477
Commercial Retail	A-5	17.61	
Open Space	C-5	9.97	
Subtotal Scenario 3		348.28	2,187

Phasing Scenario 4

Phasing Scenario 4 site is located in the central portion of the project area and abuts the southern boundary of the Phasing Scenario 2 and the northern boundary of Phasing Scenario 2. The infrastructure improvements described in this section assume that Scenario 4 would be the first area of the site to develop. Transportation improvements would involve expressway, major and secondary roadways, including partial width of Polk Street between Avenue 62 and Avenue 64; Avenue 64 between Tyler Street and Polk Street; "C" Street from Avenue 64 to Avenue 62; "D" Street from Polk to Avenue 62; Avenue 62 between "C" Street; and Tyler Street between Avenue 64 and the northwest corner of Phase 4. Grading activities for this scenario would be limited to the Scenario 4 site, with the exception of a portion of open space area D-1 in the Scenario 2 site and a portion of open space areas K-1 and J-2 in the Scenario 1 site, which would be used for a borrow/stockpile area or detention/retention areas. Drainage for this scenario would require that off-site flows be picked up in interim swales along the northerly and westerly boundaries, conveyed through the site and then released to the Scenario 1 site immediately to the south. In addition, master planned off-site retention facilities would be required immediately to the south. The retention facilities would have overflow weirs on the downstream side to return the storm flow to its original sheet flow condition. Sewer facilities would either gravity flow to the north to a CVWD gravity line in Avenue 62 or flow through the site to the southeast corner of the scenario to an existing sewer pump station at Polk and Avenue 64. From there, an existing force main will convey the flows to the existing wastewater treatment facility. Water facilities to support the development of this scenario begin with the extension of an existing 24-inch diameter waterline at the intersection of Avenue 66 and Tyler Street to Polk Street and north in Polk Street to Avenue 62. A 24-inch diameter waterline will be constructed in Avenue 62 between Polk Street and "C" Street. A 12-inch diameter waterline will be constructed in Avenue 64 between Tyler Street and Polk Street and within the phase. Two wells will be constructed along the waterline alignment to provide adequate water supply and backup. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-J, Phasing Scenario 4 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	F-2	71.64	158
	G-5	34.6	69
	G-10	18.57	37
	G-11	26.86	70
	G-13	14.17	53
	H-9	20.03	66
Medium High Density Residential	F-3	79.94	635
	G-7	60.38	432
	H-2	18.65	118
	H-4	22.4	141
	H-5	7.83	45
	H-7	20.99	151
High Density Residential	G-8	20.6	282
	H-8	13.55	189
Commercial Retail	F-4	6.23	
	G-1	4.43	
Open Space	G-2	1.44	
	G-3	1.19	
	G-4	5.6	
	G-6	13.65	
	G-9	6.17	
	G-12	5.35	
	H-1	5.12	
	H-3	3.79	
	H-6	11.43	
H-10	3.96		
Subtotal Scenario 4		498.57	2,446

Project Phasing Standards

- 1) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped areas for that stage of development, shall be submitted to the County Planning Department for approval. The improvement plans shall include, but not be limited to the following:
 - Final grading plan.
 - Irrigation plans certified by a landscape architect.
 - A hardscaping plan with location, type and quantity of potential recreational amenities/facilities prepared by a licensed landscape architect.
 - Wall or fencing treatment details consistent with the Specific Plan.Plans for landscaping, grading and irrigation systems shall be submitted to the Coachella Valley Water District for review. This is to ensure efficient water management.
- 2) Construction of the development permitted hereby, including recordation of final subdivision maps, may be done progressively in stages, provided adequate vehicular access is constructed for all dwelling units in each stage of development and further provided that such phase of development conforms substantially with the Specific Plan Phasing Program.

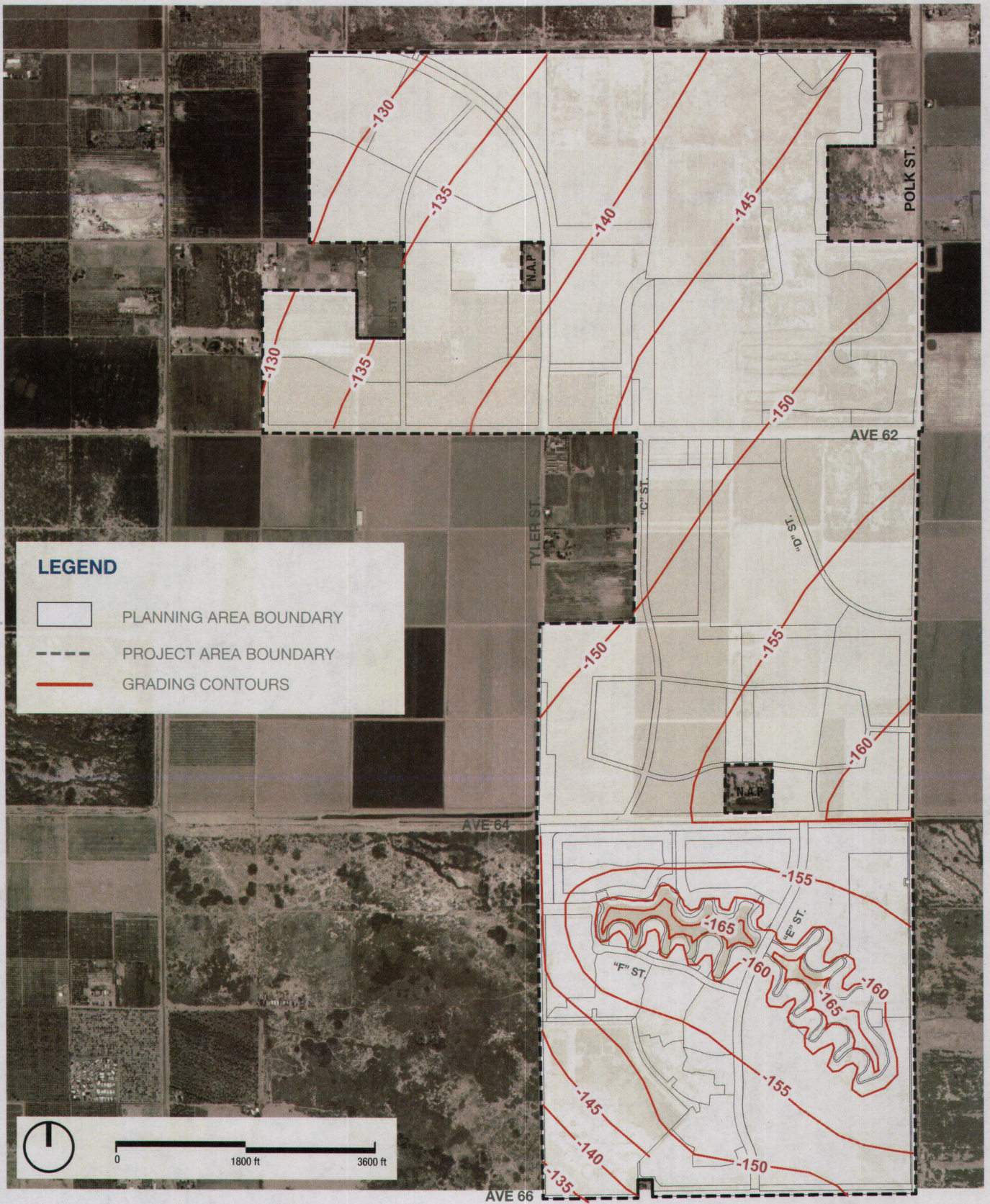
3.1.8 Illustrative Grading Plan

The study area is relatively flat with approximately 45 feet of topographical relief. The existing topography tends to slope from the northwest to the southeast at a nominal rate of between 0.30 percent and 0.40 percent. Earthwork characteristics and grading recommendations as presented in the Geotechnical Feasibility Report were utilized in this study (EIR396 – Appendix B).

The grading concept illustrates site development feasibility and provides a "balanced" earthwork scenario not dependent upon import or export of material. Grading is designed to conform to the drainage conveyance requirements while following existing topographical patterns as reflected in **Figure 3.1-12, Grading Plan**. All development areas are designed with positive drainage towards acceptable drainage conveyances. Prior to the issuance of a grading permit from Riverside County, the developer's engineer shall submit the grading plans to the Coachella Valley Water District for review and approval. This is to ensure that all existing facilities and easements are taken into account prior to construction.

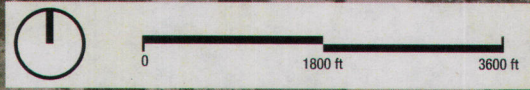
Grading Plan Development Standards

- 1) All grading activities shall be in substantial conformance with the overall Conceptual Grading Plan, the California Building Code and Riverside County Ordinance No. 457.
- 2) Prior to any development within any area of the Specific Plan, an overall Conceptual Grading Plan for the portion in process shall be submitted for Planning Department approval. The Grading Plan for each such area shall be used as a guideline for subsequent detailed grading plans for individual stages of development within that area, and shall include preliminary pad and roadway elevations.
- 3) Unless otherwise approved by the County of Riverside, Building and Safety Department, all cut and fill slopes shall be constructed at inclinations of no steeper than two (2) horizontal feet to one (1) vertical foot.
- 4) A grading permit shall be obtained from the County of Riverside, as required by the County Grading Ordinance, prior to grading.
- 5) Soil stabilizers should be used to control dust as required by SCAQMD Rule 403.
- 6) Erosion control practices shall be implemented during grading activities.
- 7) The grading contractor shall be required to obtain an encroachment permit from CVWD prior to entering any right-of-ways which belong to CVWD or USBR.
- 8) All projects proposing construction activities including: clearing, grading, or excavation that results in the disturbance of at least five acres total land area, or activity which is part of a larger common plan of development of five acres or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the specific plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program.



LEGEND

- PLANNING AREA BOUNDARY
- PROJECT AREA BOUNDARY
- GRADING CONTOURS



THE KOHL RANCH
COACHELLA VALLEY, CALIFORNIA

3.1.9 Comprehensive Maintenance Plan

Maintenance of open space, recreational facilities and roadways is important in establishing the appearance and quality of the Kohl Ranch Specific Plan area. It is anticipated that maintenance responsibilities for the project will be shared by several entities, as outlined below.

Parks and Recreation

Local parks will be dedicated to the Coachella Valley Recreation and Parks District (CVRPD), the new special district created to maintain Open Space as described in 8.b, below, or will remain private. The CVRPD, the new district, or Home Owners Association (HOA) will be responsible for park maintenance and recreation programs associated with these facilities.

Open Space

To maintain project open space (arterial and local streetscapes, trails, drainage courses, and natural open space), a new special district is required, such as a landscaping and lighting district, community services district, or county service area. This decision will be made at a future stage in project development, in concert with County agencies or HOA. The special district would contract with an existing local agency or a private firm for actual services.

Street Lighting

To operate and maintain local street lighting, a new special district is required, such as a landscaping and lighting district, community services district, or county service area. This decision will be made at a future stage in project development, in concert with County agencies. The special district would contract with an existing local agency or a private firm for actual services.

Drainage Facilities

CVWD shall not be responsible for the construction, operation or maintenance of stormwater/drainage facilities within the project area. CVWD shall review the hydrology design of the project and evaluate how drainage from the site could affect regional stormwater facilities. The drainage plan shall be subject to mitigation based upon the findings of the CVWD.

Riverside County and a property owners association or County Service Area are assumed to be responsible for maintaining any storm drains, canals or basins within the project area. Riverside County will not be responsible for drainage maintenance unless specifically indicated by the County.

Project Roadways

All public project roadways and private streets will be designed and constructed to standards acceptable to the County. All public roadways will be entered into the Riverside County System of roads for operation and maintenance as approved by the Board of Supervisors.

3.1.10 Airport Zones and Utility Easement Beltway

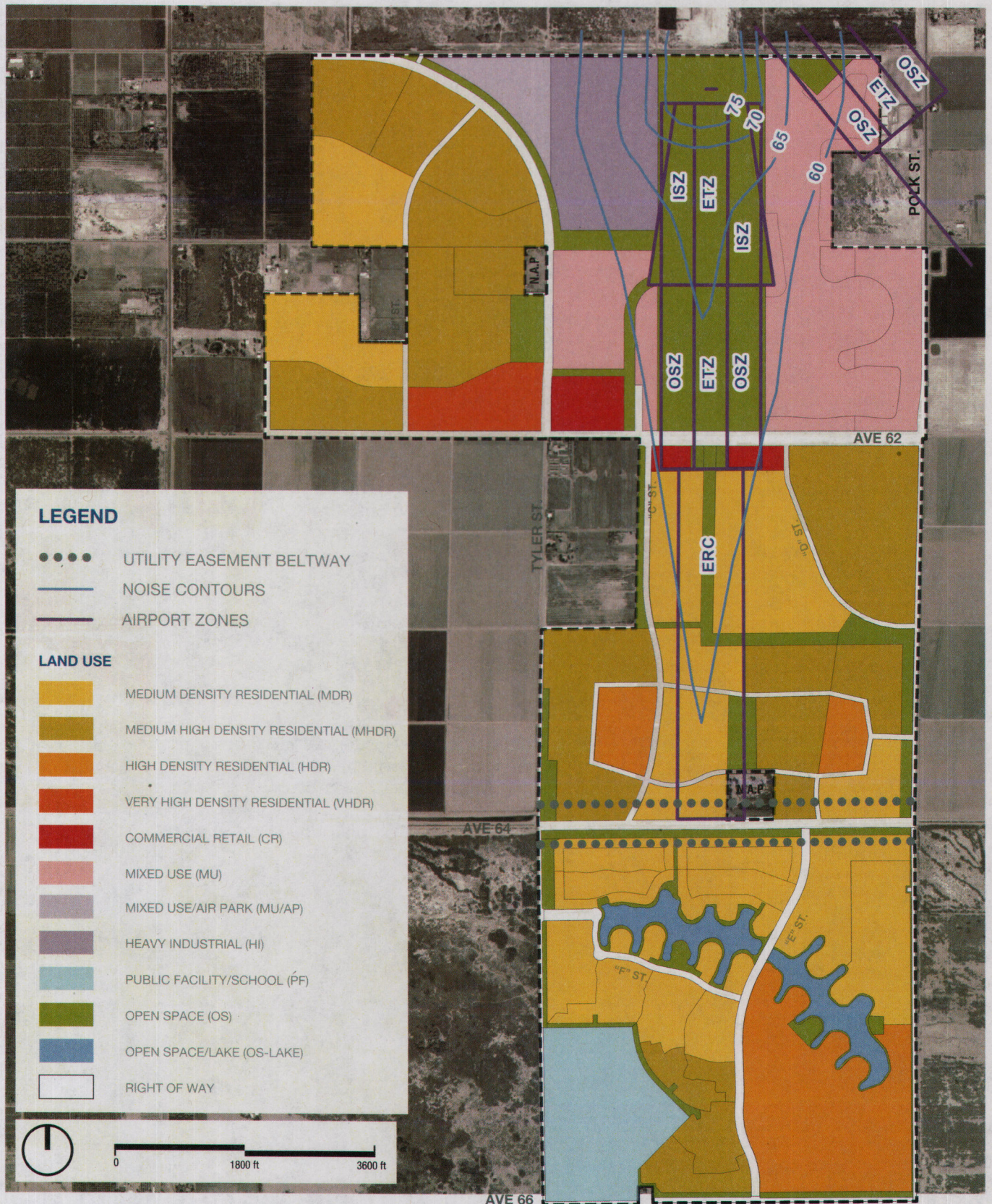
Jacqueline Cochran Regional Airport Land Use Compatibility

The Kohl Ranch site is located immediately south of Jacqueline Cochran Regional Airport, a general aviation transport airport. Based on proximity of the site to the airport, the Specific Plan has been designed to meet the following objectives:

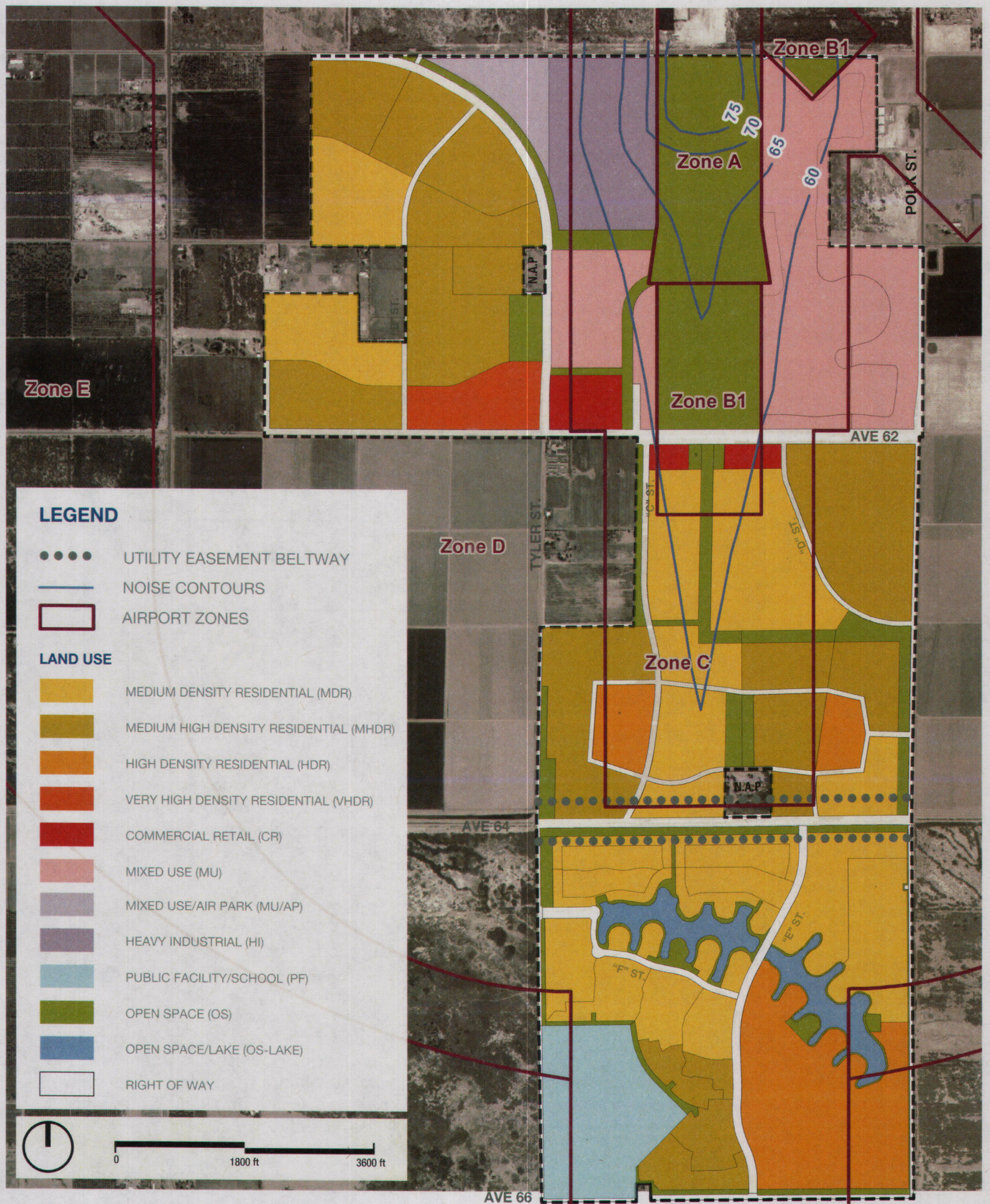
- reflect current use of and future plans for the airport;
- respond to constraints posed by the airport influence area, and ensure land use compatibility with respect to noise, safety and height; and
- maximize opportunities presented by proximity to this resource through careful design of the project circulation system and strategic location of employment-based uses.

In October 1990, the Riverside County Department of Aviation prepared the Thermal Airport Master Plan. In December 2004, the Riverside County Economic Development Agency prepared a new Airport Master Plan for the renamed Jacqueline Cochran Regional Airport. The Master Plan identifies the potential for limited commercial air carrier and air cargo service, and evaluates several alternatives for expansion of the airport to meet current and future demands through the year 2013. Plans for both airside and landside improvements are identified, including: extension of Runway 17-35 across Avenue 60 onto the Kohl Ranch property; retention of Runway 12-30 as the crosswind runway; general aviation terminal through expansion/redevelopment of existing facilities; separate air carrier and air cargo area; additional T-hangars; and heliport. The Master Plan calls out property acquisition of approximately 128 acres south of Avenue 60 for expansion of runway 17-35. The Master Plan also delineates Airport Safety Zones and noise contours related to planned airport operations as reflected in **Table 3.1-K, Airport Safety Zones** and depicted on **Figure 3.1-13, Airport Safety Zones and Utility Easement Beltway**.

In accordance with State requirements, the County Airport Land Use Commission (ALUC) prepared the Comprehensive Land Use Plan (CLUP) for Thermal Airport in August 1992 and updated it in 2005. The CLUP designates an airport influence area and includes land use compatibility guidelines that address airport noise, safety, height restrictions, and general concerns related to aircraft overflight. The "airport influence area" around Jacqueline Cochran Regional Airport is divided into six compatibility zones. Five zones affect the Kohl Ranch Specific Plan, as shown on **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**.



THE KOHL RANCH
COACHELLA VALLEY, CALIFORNIA



THE KOHL RANCH
COACHELLA VALLEY, CALIFORNIA

Table 3.1-K, Airport Safety Zones

Zone		Maximum Density	Maximum Lot Coverage by Structures	Land Use
Symbol	Name			
ISZ	Inner Safety Zone	0 (No structures permitted)	0 (No structures permitted)	No petroleum or explosives storage. No petroleum or natural gas pipelines. No above-ground power lines.
OSZ	Outer Safety Zone	Uses in structures ² : 25 persons/acre Uses not in structures: 50 persons/acre	25% of net area	No residential, hotels, places of public assembly, public utility stations/plants, and facilities that process flammable materials, or that could be damaged in an aircraft accident, among others.
ETZ	Emergency Touchdown Zone	0 (No structures permitted)	0 (No structures permitted)	No structures or other objects that would represent obstructions are allowed, such as but not limited to: large trees, heavy fences and walls, tall and steep berms and retaining walls, non-frangible street light and sign standards, billboards.
ERC	Extended Runway Centerline Zone	3 du/net acre. Uses in structures ² : 100 persons/acre.	50% of gross area or 65% of net area, whichever is greater.	No uses involving as the primary activity, manufacture, storage or distribution of explosives or flammable materials.

Source: Jacqueline Cochran Regional Airport Master Plan, December 14, 2004.

The areas of the Kohl Ranch project amended/modified by SPA4 and CZ07952 were determined by ALUC to be inconsistent with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (JCRA LUCP) as amended in 2006, on the basis that the proposal would result in an exceedance of the permissible numbers of dwelling units within a portion of the property located in Compatibility Zone C, which limits residential density to one dwelling unit per five acres. However, Section 21676.5 of the California Public Utilities Code states that the County of Riverside may overrule the ALUC's inconsistency determination if, after a public hearing, the County makes findings that the project is consistent with the purposes set forth in Public Utilities Code Section 21670.

On November 6, 2018, the Riverside County Board of Supervisors approved the project and found it consistent with the purposes set forth in Public Utilities Code Section 21670 and approved to overrule the ALUC inconsistency determination because, the proposed Project is entirely within Compatibility Zone C and the lots within Compatibility Zone C that would be allowed for overnight occupancy are located on the edge of the zone, adjacent to Compatibility Zone D, where all the existing lots are allowed to have overnight occupancy. The remaining lots in Zone C, on the west side of the racetrack, directly adjacent to Compatibility Zone A, would continue to be prohibited from having overnight occupancy. Therefore, the Project minimizes the potential safety impacts by not allowing overnight stays in the areas closest to the runway thereby reducing the potential for a serious hazard to the public in the event of a potential aircraft accident on the Project site. Indeed, the Racetrack Recreational Units are

located along the perimeter of Thermal Motorsport Racetrack and are consistent and complimentary with surrounding land uses to the east, south, and west that include various amenities and uses that support the racetrack. Therefore, as demonstrated, the site is consistent with Public Utility Code section 21670(a)(1) and (2). Moreover, the Project does not represent a hazard to aircraft in typical flight because the Project does not propose the construction of any new buildings that were not previously considered under the Kohl Ranch Specific Plan, and all buildings are required to comply with all applicable FAA requirements. Therefore, the Project would not result in a safety hazard to aircraft in flight, consistent with Public Utility Code section 21670(a)(1) and (2).

Airport Noise Compatibility Guidelines

The CLUP presents noise compatibility guidelines for the Jacqueline Cochran Regional Airport vicinity, as follow:

- Discourage new single-family dwellings and prohibit mobile homes within the 60 CNEL contour. Where homes are permitted within the 60 CNEL contour, the need for sound insulation should be studied and noise easements should be acquired.
- Within the 65 CNEL, new residential construction should not be undertaken. New hotels or motels are permitted if the need for sound insulation is evaluated.
- Institutional uses should be discouraged within the 65-70 CNEL range. If no alternative location is available, the need for sound insulation should be studied.
- Commercial, industrial and recreational uses are considered compatible with noise levels between the 65 and 70 CNEL.

The Kohl Ranch property is impacted by noise from Jacqueline Cochran Regional Airport. To address airport noise in the Specific Plan, primarily open space uses are located within the 65, 70, and 75 CNEL noise contours. A limited amount of airpark/mixed use and industrial uses are within the 65 CNEL contour, consistent with the guidelines. Land uses within the 60 CNEL contour include open space, industrial and commercial retail uses. Some medium density residential uses also are within the 60 CNEL contour. Cluster provisions have been incorporated into Section 2.0 – Specific Plan Zoning so that residential uses can be located outside areas impacted by airport noise. In addition, common walls within medium high, high and very high density units are anticipated to attenuate airport noise within the 60 CNEL contour.

Airport Vicinity Height Guidelines

The Federal government has developed standards for determining obstructions in navigable airspace. Federal Aviation Regulations Part 77 defines a variety of imaginary surfaces at certain altitudes around airports. The Part 77 surfaces include the primary surface, approach surface, transitional surface, horizontal surface, and conical surface. Collectively, the Part 77 surfaces around an airport define a bowl-shaped area with ramps sloping up from each runway end. The Part 77 standards are not absolute height restrictions, but instead identify elevations at which structures may present a potential safety problem. Penetrations of the Part 77 surface generally are reviewed on a case by case basis. The CLUP uses the Part 77 criteria as the basis for height limitations in the vicinity of Jacqueline Cochran Regional Airport.

Height limitations are not anticipated to pose a development constraint for the Kohl Ranch site. Based on the Part 77 surfaces identified in the CLUP, structures above approximately 85 feet in height in the vicinity of Avenue 62, 150 feet in height in the vicinity of Avenue 64, and 315 feet in height in the vicinity of Avenue 66, could potentially create a problem. Development in these areas is not expected to exceed the relevant height limitations.

Airport Safety Compatibility Guidelines

The CLUP for Jacqueline Cochran Regional Airport establishes land use compatibility guidelines for land use compatibility zones that affect the project site. These guidelines are summarized below in **Table 3.1-L, Basic Compatibility Criteria**.

In addition to the guidelines for the five zones, the CLUP prohibits particularly hazardous land uses in all designated safety zones. These land uses include uses which would impede the ability of the pilot to see the airfield as identified in Footnote 1 to **Table 3.1-L**.

Open space is concentrated within the most restrictive safety zones, the Emergency Touchdown Zone (ETZ) and the Inner Safety Zone (ISZ). Land uses proposed in the Specific Plan within the Outer Safety Zone (OSZ) are limited to open space, commercial, and mixed use land uses. The Extended Runway Centerline (ERC) zone, while less restrictive, limits the density of uses within the zone. Consequently, open space and medium density residential uses have been sited within this area. Residential cluster provisions incorporated into Section 2.0 – Specific Plan Zoning, encourage common open space areas to be located within the ERC, with transfer of residential density to areas outside the safety zone.

Airport Compatibility Design Standards for Mixed Use

Mixed Use structures in Planning Areas E-2 through E-8 of The Kohl Ranch Specific Plan, allowing for overnight occupancy shall incorporate the following features:

- Signage depicting areas allowing overnight occupancy from areas of non-overnight occupancy.

Airport Compatibility Design Guidelines for Lakes

Recreational lakes within The Kohl Ranch Specific Plan shall be designed to incorporate the following features in order to reduce or eliminate the habitat and foraging opportunities for birds in the water areas:

- Use of linear shaped water areas.
- Formed, vertical banks that make access to land at the water's edge difficult.
- Restrict any vegetation that offers shelter and nesting opportunities.
- Do not include islands in water areas.
- Eliminate shallows and muddy areas.
- Control availability of forage – fish, frogs, algae and insects.
- Provide for human activity in or near the water.

Through the incorporation of these features into the design of the recreational lakes, the lakes will not attract the type or concentration of bird activity that would create a hazard to aircraft in flight.

Utility Easement Beltway

A utility easement beltway traverses the site in an east-west direction, roughly along the Avenue 64 right-of-way. This beltway contains easements for CVWD irrigation pipes, the Avenue 64 Evacuation Channel, and a power line easement for an existing 161 kV power line. This area is proposed to remain in street right of way and open space, and to incorporate an element of the project-wide trail system, connecting with parks and public facilities throughout the site. Residential development in the planning areas immediately to the north of the easement beltway would be clustered outside of the beltway, to maintain the residential densities in these areas and preserve open space areas.

Table 3.1-L, Basic Compatibility Criteria

Zone	Locations	Maximum Densities / Intensities				Req'd Open Land ³	Additional Criteria	
		Residential (d.u./ac) ¹	Other Uses (people/ac) ²		Prohibited Uses ⁴		Other Development Conditions ⁵	
		Average ⁶	Single Acre ⁷	with Bonus ⁸				
A	Runway Protection Zone and within Building Restriction Line	0	0	0	0	All Remaining	<ul style="list-style-type: none"> > All structures except ones with location set by aeronautical function > Assemblages of people > Objects exceeding FAR Part 77 height limits > Storage of hazardous materials > Hazards to flight⁹ 	<ul style="list-style-type: none"> > Avigation easement dedication
B1	Inner Approach/Departure Zone	0.05 (average parcel size ≥20.0 ac.)	25	50	65	30%	<ul style="list-style-type: none"> > Children's schools, day care centers, libraries > Hospitals, nursing homes > Places of worship > Bldgs with >2 aboveground habitable floors > Highly noise-sensitive outdoor nonresidential uses¹⁰ > Aboveground bulk storage of hazardous materials¹¹ > Critical community infrastructure facilities¹² > Hazards to flight⁹ 	<ul style="list-style-type: none"> > Locate structures maximum distance from extended runway centerline > Minimum NLR of 25 dB in residences (including mobile homes) and office buildings¹³ > Airspace review required for objects >35 feet tall¹⁴ > Avigation easement dedication
B2	Adjacent to Runway	0.1 (average parcel size ≥10.0 ac.)	100	200	260	No Req't	Same as Zone B1	<ul style="list-style-type: none"> > Locate structures maximum distance from runway > Minimum NLR of 25 dB in residences (including mobile homes) and office buildings¹³ > Airspace review required for objects >35 feet tall¹⁴ > Avigation easement dedication
C	Extended Approach/Departure Zone	0.2 (average parcel size ≥5.0 ac.)	75	150	195	20%	<ul style="list-style-type: none"> > Children's schools, day care centers, libraries > Hospitals, nursing homes > Bldgs with >3 aboveground habitable floors > Highly noise-sensitive outdoor nonresidential uses¹⁰ > Hazards to flight⁹ 	<ul style="list-style-type: none"> > Minimum NLR of 20 dB in residences (including mobile homes) and office buildings¹³ > Airspace review required for objects >70 feet tall¹⁵ > Deed notice required
D	Primary Traffic Patterns and Runway Buffer Area	(1) ≤0.2 (average parcel size ≥5.0 ac.) or ¹⁶ (2) ≥5.0 (average parcel size ≤0.2 ac.)	100	300	390	10%	<ul style="list-style-type: none"> > Highly noise-sensitive outdoor nonresidential uses¹⁰ > Hazards to flight⁹ 	<ul style="list-style-type: none"> > Airspace review required for objects >70 feet tall¹⁵ > Children's schools, hospitals, nursing homes discouraged¹⁷ > Deed notice required
E	Other Airport Environs	No Limit	No Limit ¹⁸			No Req't	<ul style="list-style-type: none"> > Hazards to flight⁹ 	<ul style="list-style-type: none"> > Airspace review required for objects >100 feet tall¹⁵ > Major spectator-oriented sports stadiums, amphitheaters, concert halls discouraged beneath principal flight tracks¹⁸
*	Height Review Overlay	Same as Underlying Compatibility Zone				Not Applicable	Same as Underlying Compatibility Zone	<ul style="list-style-type: none"> > Airspace review required for objects >35 feet tall¹⁴ > Avigation easement dedication

See Chapter 3 for airport-specific additions or exceptions to these policies

NOTES:

- ¹ Residential development must not contain more than the indicated number of dwelling units (excluding secondary units) per gross acre. Clustering of units is encouraged. See Policy 4.2.5 for limitations. Gross acreage includes the property at issue plus a share of adjacent roads and any adjacent, permanently dedicated, open lands. Mixed-use development in which residential uses are proposed to be located in conjunction with nonresidential uses in the same or adjoining buildings on the same site shall be treated as nonresidential development. See Policy 3.1.3(d).
- ² Usage intensity calculations shall include all people (e.g., employees, customers/visitors, etc.) who may be on the property at a single point in time, whether indoors or outside.
- ³ Open land requirements are intended to be applied with respect to an entire zone. This is typically accomplished as part of a community general plan or a specific plan, but may also apply to large (10 acres or more) development projects. See Policy 4.2.4 for definition of open land.
- ⁴ The uses listed here are ones that are explicitly prohibited regardless of whether they meet the intensity criteria. In addition to these explicitly prohibited uses, other uses will normally not be permitted in the respective compatibility zones because they do not meet the usage intensity criteria.
- ⁵ As part of certain real estate transactions involving residential property within any compatibility zone (that is, anywhere within an airport influence area), information regarding airport proximity and the existence of aircraft overflights must be disclosed. This requirement is set by state law. See Policy 4.4.2 for details. Easement dedication and deed notice requirements indicated for specific compatibility zones apply only to new development and to reuse if discretionary approval is required.
- ⁶ The total number of people permitted on a project site at any time, except rare special events, must not exceed the indicated usage intensity times the gross acreage of the site. Rare special events are ones (such as an air show at the airport) for which a facility is not designed and normally not used and for which extra safety precautions can be taken as appropriate.
- ⁷ Clustering of nonresidential development is permitted. However, no single acre of a project site shall exceed the indicated number of people per acre. See Policy 4.2.5 for details.
- ⁸ An intensity bonus may be allowed if the building design includes features intended to reduce risks to occupants in the event of an aircraft collision with the building. See Policy 4.2.6 for details.
- ⁹ Hazards to flight include physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations. Land use development that may cause the attraction of birds to increase is also prohibited. See Policy 4.3.7.
- ¹⁰ Examples of highly noise-sensitive outdoor nonresidential uses that should be prohibited include amphitheatres and drive-in theaters. Caution should be exercised with respect to uses such as poultry farms and nature preserves.
- ¹¹ Storage of aviation fuel and other aviation-related flammable materials on the airport is exempted from this criterion. Storage of up to 6,000 gallons of nonaviation flammable materials is also exempted. See Policy 4.2.3(c) for details.
- ¹² Critical community facilities include power plants, electrical substations, and public communications facilities. See Policy 4.2.3(d) for details.
- ¹³ NLR = Noise Level Reduction, the outside-to-inside sound level attenuation that the structure provides. See Policy 4.1.6.
- ¹⁴ Objects up to 35 feet in height are permitted. However, the Federal Aviation Administration may require marking and lighting of certain objects. See Policy 4.3.6 for details.
- ¹⁵ This height criterion is for general guidance. Shorter objects normally will not be airspace obstructions unless situated at a ground elevation well above that of the airport. Taller objects may be acceptable if determined not to be obstructions. See Policies 4.3.3 and 4.3.4.
- ¹⁶ Two options are provided for residential densities in *Compatibility Zone D*. Option (1) has a density limit of 0.2 dwelling units per acre (i.e., an average parcel size of at least 5.0 gross acres). Option (2) requires that the density be *greater than* 5.0 dwelling units per acre (i.e., an average parcel size *less than* 0.2 gross acres). The choice between these two options is at the discretion of the local land use jurisdiction. See Table 2B for explanation of rationale. All other criteria for *Zone D* apply to both options.
- ¹⁷ Discouraged uses should generally not be permitted unless no feasible alternative is available.
- ¹⁸ Although no explicit upper limit on usage intensity is defined for *Zone E*, land uses of the types listed—uses that attract very high concentrations of people in confined areas—are discouraged in locations below or near the principal arrival and departure flight tracks. This limitation notwithstanding, no use shall be prohibited in *Zone E* if its usage intensity is such that it would be permitted in *Zone D*.

Source: Riverside County Airport Land Use Compatibility Plan Policy Document (adopted October 2004)

3.1.11 Specific Plan Implementation and Administrative Process

Pursuant to Government Code Section 65451 all specific plans must contain a "program of implementation measures including regulations, programs, public works projects, and financing measures" necessary to implement the specific plan. This section outlines the methods by which development in the specific plan will be processed, including infrastructure financing mechanisms and other necessary programs.

As it relates to the implementation and administrative process, Riverside County Zoning Ordinance 348 shall be adhered to at all times.

Specific Plan Time Frames

Riverside County requires the adoption of a phasing plan for each Specific Plan (SP). Each plan must be monitored for reasonable progress toward implementation. The phasing program for the Kohl Ranch Specific Plan is described above in Section 3.1.7 – Public Facility Sites and Project Phasing. The project developer will work with the County to adhere to the phasing plan and to ensure the logical and timely completion of the project.

Financing Plan

The developer shall be responsible for financing construction of the infrastructure improvements required to support the Project, such as perimeter and internal streets, water lines, sewers, and storm drains. All necessary infrastructure improvements shall be developed in conjunction with the roadway improvements. The financing of construction, operation, and maintenance of public improvement and facilities will include funding through a combination of financing mechanisms. However, the developer shall be ultimately responsible for all fair share costs associated with implementing the Project, including but not limited to the costs of providing infrastructure and complying with mitigation measures, conditions of approval, and other requirements of the Project.

Financing may involve a combination of impact fees and exacting, special assessment districts, landscaping and lighting districts, and other mechanisms agreed to by the developer and the County of Riverside as noted below. Developer funded improvements may be subject to a reimbursement agreement or credits against fees pursuant to provisions of a development agreement or conditions of approval. The County and developer will cooperate to ensure that the public facilities are built in accordance with all requirements of the Specific Plan and EIR. A development agreement and conditions of approval may be used to facilitate this process.

Developer Funding

In many cases, certain onsite facilities are tied directly to individual projects. In these cases, it is reasonable to expect the developer (or property owner) to pay the entire cost of the facility in order to secure development rights. Onsite local streets, utility connections from main trunk lines, and drainage facilities are good examples of facilities that are normally required concurrent with development of an individual parcel funded by the developer.

Special Assessment Districts

A special assessment district is a type of benefit district that requires a vote by the property owners to encompass a defined and limited geographic area. The County or other agencies may form a special assessment district under one of several different statutory acts to construct

public improvements such as streets, storm drains, sidewalks, streetlights, sewers, parks landscape, and other similar capital facilities. The special assessment district can issue bonds to finance those improvements and levy a special assessment to pay debt service on those bonds.

A special assessment district may fund improvements within the entire Specific Plan area or smaller areas in the Specific Plan where special improvements are constructed that directly benefit only certain property owners. Special assessments districts may only be used to pay for projects that are of specific and direct benefit to the property owner being assessed. The amount of the assessment must directly relate to the amount of benefit received by the property owner.

Landscaping and Lighting Districts

Landscaping and lighting districts (LLD) may be used for installation, maintenance, and servicing of landscaping and lighting through annual assessments on benefiting properties. LLDs may also provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities.

Other Funding Sources

Other sources may be available to finance improvement projects, such as government grants, private developer coalitions, or various types of bonds not listed above.

Dwelling Unit Tracking System

Each Residential Planning Area within the Kohl Ranch Specific Plan has an identified density range consistent with the County General Plan, Flexibility Range, and a Target number of dwelling units as identified in **Table 3.1-M, Residential Planning Area Flexibility Range**. The Flexibility Range is consistent with a variation of no more than 10% from the allowable density range. The Flexibility Range was determined based on several factors: (1) ultimately the Planning Area acreage may change as a result of final engineering, (2) the flexibility may be necessary to accommodate airport restrictions which continue to change over time, and (3) based on the fact that there is an overall unit cap within the SP. Flexibility can be given to the density range and target number of dwelling units within each Residential Planning Area, under certain circumstances, as described below. Flexibility can be given to the density range and target number of dwelling units within each residential Planning Area, under certain circumstances, as described below.

Additionally, all Planning Area acreages may be revised when more accurate information is available. Up to a 15 percent variation in Planning Area acreage may be allowed without a Specific Plan Amendment or determination of substantial project conformance with adopted specific plan, but will be required to receive administrative approval. A 16 to 30 percent variation in Planning Area acreage may be allowed with a 'determination of substantial project conformance with adopted specific plan,' and any acreage change in Planning Area acreage of over 30 percent will require a Specific Plan Amendment. A Planning Area acreage change would not result in a Flexibility Range change, without requiring a Specific Plan Amendment. This accommodation should cover reasonable changes in planning area size caused by roadway realignments such as the one that precipitated changes reflected in Specific Plan No. 303 Amendment 2 (SP00303A2).

Any developer proposing residential development within the Kohl Ranch Specific Plan, either through individual or multiple Planning Areas, will be required to report how many dwelling units will be developed within each Planning Area, as shown on **Table 3.1-N, Reporting Mechanism**, and if any modifications to the Land Use Table are necessary. This report will need to be submitted to the County of Riverside Planning Department to ensure the total number of dwelling units will not exceed 7,171. **Table 3.1-O, Master Log**, will be the running tabular record of all proposed and developed dwelling units, for use by the Planning Department.

Table 3.1-M, Residential Planning Area Flexibility Range

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range DU/AC	Flexibility Range DU/AC
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0	4.5-8.8
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0	4.5-8.8
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0	1.8-5.5
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0-8.0	4.5-8.8
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0	4.5-8.8
C-1	Medium Density Residential (MDR)	122	40.77	2.0-5.0	1.8-5.5
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0	4.5-8.8
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0	4.5-8.8
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0	12.6-22.0
C-8	Medium High Density Residential (MHDR)	105	14.96	5.0-8.0	4.5-8.8
F-2	Medium Density Residential (MDR)	158	71.64	2.0-5.0	1.8-5.5
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0	4.5-8.8
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0	1.8-5.5
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0	4.5-8.8
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0	7.2-15.4
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0	1.8-5.5
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0	1.8-5.5
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0	1.8-5.5
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0	4.5-8.8
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0	4.5-8.8
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0	4.5-8.8
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0-8.0	4.5-8.8
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0	7.2-15.4
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0	1.8-5.5
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0	1.8-5.5
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0	1.8-5.5
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0	1.8-5.5
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0	1.8-5.5
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0	1.8-5.5

I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0	1.8-5.5
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0	1.8-5.5
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0	1.8-5.5
J-1	Medium Density Residential (MDR)	130	37.52	2.0-5.0	1.8-5.5
J-4	Medium Density Residential (MDR)	189	43.36	2.0-5.0	1.8-5.5
L-1	High Density Residential (HDR)	1,158	106.2	8.0-14.0	7.2-15.4
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0	1.8-5.5
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0	1.8-5.5
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0	1.8-5.5
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0	1.8-5.5
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0	1.8-5.5
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0	1.8-5.5
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0	1.8-5.5
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0	1.8-5.5
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0	4.5-8.8
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0-8.0	4.5-8.8
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0-8.0	4.5-8.8
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0	4.5-8.8
TOTAL		<u>7,003</u>	<u>1,140.29</u>		

Table 3.1-N, Reporting Mechanism

Application Case #							
PA	Acres in PA	Density Range		Flexibility Range		Proposed Number of Dwelling Units	Justification Statement (required if outside dwelling unit range)

Table 3.1-O, Master Log

Planning Case No, TTP, or TPM	Residential PA	Gross Acres	Target Number of SU	Density Ranch DU		Flexibility Range DU		Actual DU Implemented At:			Unit Pool Balance	Unit Balance
								TIM or TTP Approval	Final Map Recordation	Actual Building Permit		

Development within the Dwelling Unit Range

The Density Range is established at the time of the adoption of the Specific Plan. Any proposed development within the Density Range is in conformance with the Specific Plan. When completing the Reporting Mechanism, and if the proposed development is within the Density Range, a justification statement is not required, and a Specific Plan Amendment or Specific Plan Substantial Conformance Report is not required, as well.

Development Below or Above Density Range but Within the Flexibility Range

The Flexibility Range is defined as the range within 10 percent of the allowable Density Range for each Planning Area. The Flexibility Range gives more flexibility to the Density Range for each Planning Area to account for minor modifications, and is permitted under certain circumstances, without requiring a Specific Plan Amendment, but a Specific Plan Substantial Conformance Report is required.

The Developer, in requesting use of the Flexibility Range, shall state those special circumstances in **Table 3.1-N**, which would justify the use of the Flexibility Range. Examples that would justify use of the Flexibility Range include, but are not limited to: a decrease or increase in Planning Area size at the final engineering level; or the decrease of residential development acreage as a result of the development of parks, trails, or public facilities. The Planning staff shall determine if the request for the Flexibility Range is justified, and in granting approval of the Flexibility Range, staff shall find as follows:

- 1) That the Planning Area for the desired Flexibility Range is adequate in size and shape to accommodate proposed development; and
- 2) That the site for the proposed development contains streets and public facilities properly designed to carry the type and quantity of traffic generated by the subject development; and
- 3) That the proposed development will not impair the integrity and character of the Planning Area in which it is located or otherwise have an adverse effect on adjacent property or the permitted use thereof; and
- 4) That the proposed use is not contrary to this Specific Plan.

Development Outside of Flexibility Range

If the proposed development is below or above the existing allowable Density Range and the Flexibility Range, a Specific Plan Amendment will be required.

The Difference of Dwelling Units Developed Compared to the Target

Each residential Planning Area has a designated Dwelling Unit Target, which in total comprises 7,161 dwelling units. When a Planning Area is developed, a Residential Development Report, **Table 3.1-N, Reporting Mechanism**, is submitted to the Planning Department to record the number of dwelling units. Any units over or under the Dwelling Unit Target number of dwelling units will be added or taken away from the remaining balance of units to ensure the 7,171 maximum dwelling unit total for the Specific Plan is not exceeded and added or subtracted from the Unit Pool. When the Unit Pool reaches 50 ± units, a redistribution to/from other planning areas shall be completed by the Master Developer and submitted to the County Planning Department.

3.2 LAND USE, PLANNING & DEVELOPMENT STANDARDS

Development standards for the Kohl Ranch have been established at four levels: Section 3.1 – Project-Wide Planning Standards, Section 3.3 – Design Guidelines, Section 3.4 – Landscape Design Guidelines, and Land Use, Planning & Development Standards, which are provided in this section.

Neighborhood Concept

The development proposal for the Kohl Ranch Specific Plan is founded upon the concept of self-sufficient neighborhoods, each defined by strong edges and an internal circulation system, and identified by a similar orientation, intent, and theme. Where appropriate, design guidelines are tailored to reinforce the unique character and theme of the project neighborhoods. Linking all of the neighborhoods is an extensive open space and trail system, relating to the airport safety zones and drainage ways, which provide residents and workers with active and passive recreational opportunities and scenic amenity. This open space network also serves to buffer incompatible uses and neighborhoods from one another as reflected in **Figure 3.2-1, Neighborhoods**. As described earlier in this chapter, neighborhoods in the northern portion of the site generally have a high concentration of employment-based uses, while neighborhoods in the southern portion are predominantly residential.

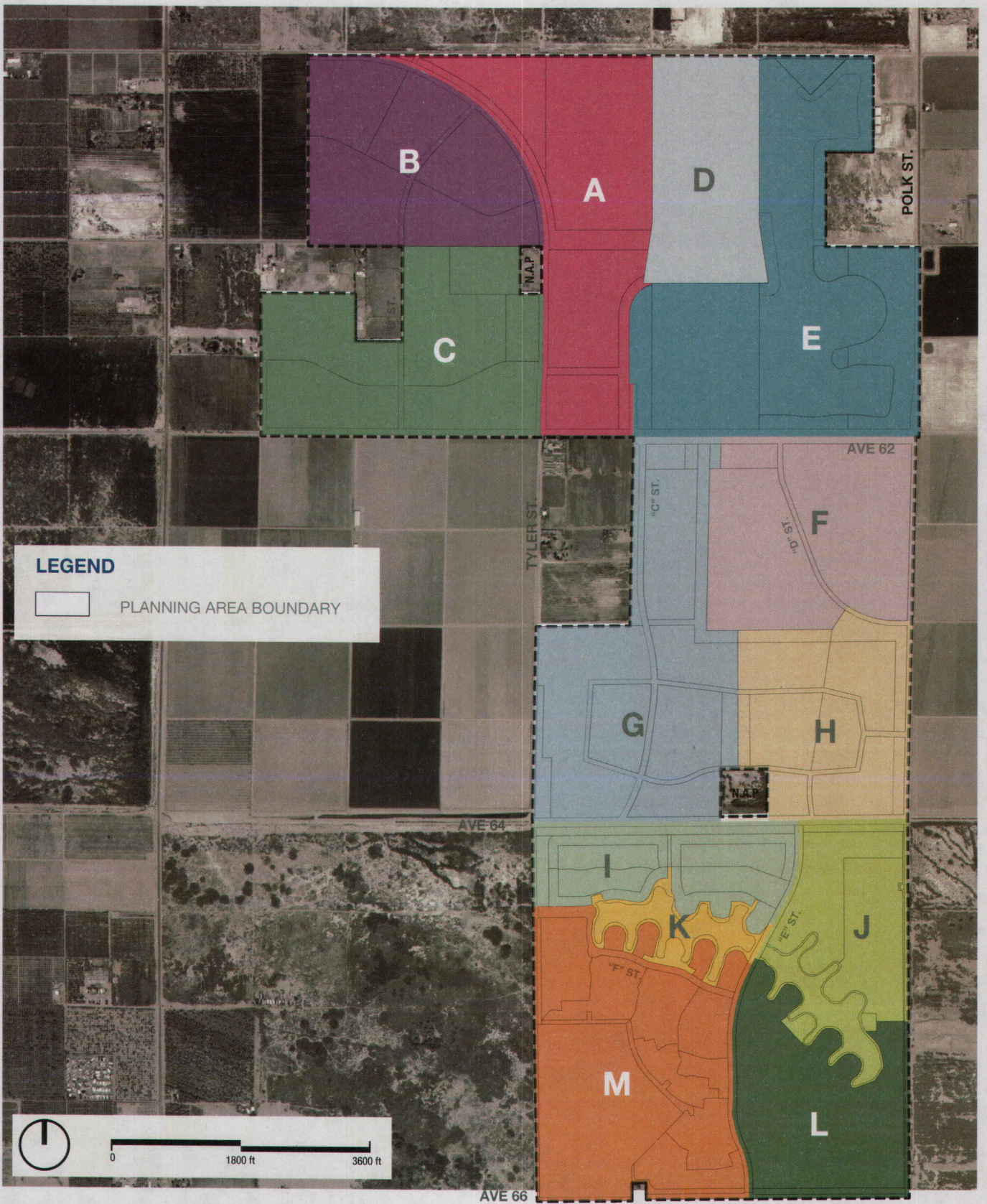
The Kohl Ranch Specific Plan identifies 95 planning areas on the site grouped within 13 larger areas referred to as neighborhoods. Neighborhoods define logical development areas based on land use and the relationship to planned roadways and infrastructure. The thirteen project neighborhoods were conceived to create cohesive, well-balanced communities, upon completion of the project. The neighborhoods relate generally to the phasing plan for the project, so that infrastructure and amenities can be readily provided as each neighborhood is developed. However, the neighborhood boundaries are slightly different than the boundaries of the project phasing scenarios which are based on the logical provision of infrastructure as well as overall market forces.

Definitions of the project planning areas are based upon logical, separate units of development, adjoining land uses, available infrastructure and physical landscape features.

Flexibility of the Standards

The Land Use, Planning & Development Standards described here and in Section 2 – Specific Plan Zoning Ordinance, are designed to provide a high degree of flexibility for future development of the Kohl Ranch site. This flexibility is critical, in order to respond to changing market forces during the life of the project, and to accommodate future conditions related to development of Jacqueline Cochran Regional Airport

This flexibility is reflected in provisions for alternate allowable land uses that are specified in the descriptions of neighborhoods and planning areas below. Such alternate land uses include golf courses and related facilities, other large recreational uses such as race track facilities and lakes. The conditions under which alternative land uses are allowed, along with the planned use of buffers and organization of land uses throughout the project will ensure that development of the project can be managed over time and that incompatibilities between adjacent land uses can be avoided.



Flexibility is also evident in the plan through the incorporation of provisions which allow the clustering of residential uses, through the transfer of private open space to common open space. As a result, overall residential densities remain the same, and a shared amenity is created. The cluster option is particularly well-suited to land within airport-restricted zones, and to planning areas that are adjacent to drainage facilities which may be expanded under a golf course scenario.

Organization of this Section

Detailed descriptions of the neighborhoods are provided below, followed by neighborhood-wide planning standards which describe airport-related constraints, allowable land uses, neighborhood access, and streetscape, entry and intersection treatments. Following the neighborhood descriptions and planning standards are descriptions and standards for each individual planning area. A brief description, summary of land use and development standards specified in Section 2 – Specific Plan Zoning Ordinance, and applicable planning standards are provided for each planning area. For each planning area, these standards describe such features as alternate allowable land uses and conditions, access, and edge and buffer treatments.

A foldout, plan view graphic is provided for each of the thirteen neighborhoods. Each of these graphics is located at the beginning of the corresponding neighborhood section, and should be kept open during review of the planning area descriptions and standards which precede it. These graphics indicate the location of all planning areas, access points, and all landscape features including streetscapes, intersection details, project entries, edges, and buffers.

Appropriate references in the text are made to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines. The graphics for the Design Guidelines are derived from the Community Structure Plan and are intended to convey the project intent with respect to open space, landscaping, site planning and architecture. These elements will create the framework for development of the site. Although development may conform closely to elements of the illustrative plans provided in this section, it is anticipated that actual development will respond to market conditions.

A Specific Plan Zoning Ordinance was prepared in conjunction with this Specific Plan document. The zoning provisions within that ordinance establish allowable uses and development standards for each planning area. The zoning provisions should be used in conjunction with the planning standards for each planning area.

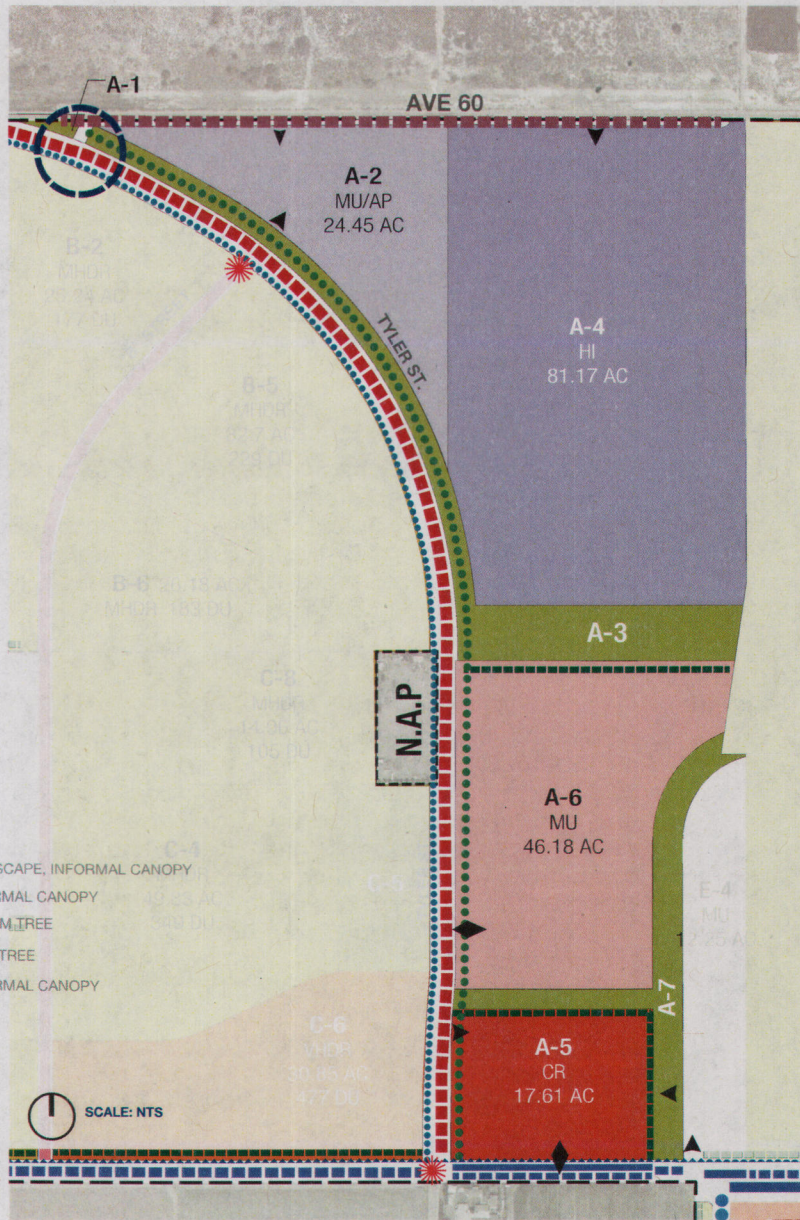
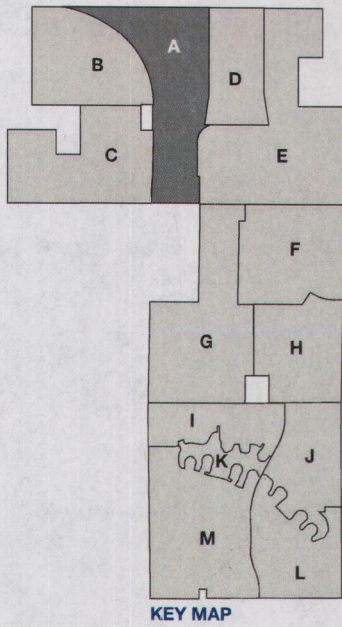
3.2.1 Neighborhood A

Neighborhood A, which consists of seven planning areas, is located in the north-central portion of the project site, just south of Avenue 60 and east of Tyler Street as depicted in **Figure 3.2-2, Neighborhood A**. The neighborhood totals approximately 190 acres including 21.37 acres of open space, 24.45 acres of mixed use/air park, 17.61 acres of commercial retail, 81.17 acres of heavy industrial, and 46.18 acres of mixed use. The neighborhood has a natural outward orientation and response to the regional context. Land uses targeted for Neighborhood A are intended to serve an area larger than the neighborhood itself, providing jobs and services to areas north and west of the site.

The uses provided for within Neighborhood A relate to planned uses at the Jacqueline Cochran Regional Airport located immediately to the north. A mix of commercial, industrial, mixed use, and service uses are intended to support airport development. The open space areas in the neighborhood include elements of the project-wide trail system that are generally coincident with drainage ways and major streets. Primary access to the neighborhood is provided via Avenue 60 and Tyler Street. A major project entry is provided at the intersection of Tyler Street and Avenue 60.

NEIGHBORHOOD A PLANNING STANDARDS

- 1) Portions of the commercial retail, mixed use, and industrial land uses within this neighborhood are located within Zone C of the Airport Compatibility Land Use Plan (CLUP). The mixed use/air park area and portions of the commercial-retail, mixed use and industrial uses are located within Zone D as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with these zones are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- 2) A large-scale recreational use such as a motor sports race track is a permitted use in two of the seven planning areas.
- 3) Tyler Street provides access to Planning Areas A-1 through A-7.
- 4) The northwest corner of Planning Area A-2 at Avenue 60/Tyler Street will be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- 5) The intersections of Tyler Street and Avenue 62 and Tyler Street and “B” Street will be developed in accordance with **Figure 3.4-5, Special Intersection Detail**.
- 6) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**.
- 7) Tyler Street, north of Avenue 62 will be developed in accordance with **Figure 3.4-8, Arterial Streetscape 1**.
- 8) Avenue 60 (along north edge of Planning Areas A-2 and A-4) will be developed in accordance with **Figure 3.4-16, Industrial Collector Streetscape**.
- 9) The east side of Tyler Street will be developed in accordance with **Figure 3.4-19, Tyler Street Trail**.
- 10) The north side of Avenue 62 will be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.



LEGEND

- PROJECT AREA BOUNDARY
- INDUSTRIAL COLLECTOR STREETSCAPE, INFORMAL CANOPY
- EXPRESSWAY STREETSCAPE, FORMAL CANOPY
- EXPRESSWAY STREETSCAPE, PALM TREE
- ARTERIAL STREETSCAPE 1, PALM TREE
- COLLECTOR STREETSCAPE 1, FORMAL CANOPY
- WINDROW PLANTING
- CLASS I BIKE PATH
- REGIONAL TRAIL
- CLASS II BIKE PATH
- FULL ACCESS
- RIGHT IN/RIGHT OUT ACCESS
- INTERSECTION DETAIL
- MAJOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Heavy Industrial (HI)		46.17	
A-7	Open Space (OS)		9.97	
TOTAL			190.77	

NEIGHBORHOOD A PLANNING AREAS**A-1: OPEN SPACE**

Planning Area A-1 provides for development of 0.63 acres of open space. This area is to be used as a collection point for storm flows entering the project site.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-1 is located in Zone D of the CLUP. Development in this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area A-1 is accessed from Tyler Street.
- (3) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (4) Large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (5) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-19, Tyler Street Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

A-2: MIXED USE/AIR PARK

Planning Area A-2 provides for 24.45 acres of mixed use/air park development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-2 is located in Zone D of the CLUP. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-2 is accessed from Tyler Street.
- (4) The edge treatment for the interface between Planning Area A-2's mixed use/air park and the drainage uses in the adjacent planning area, is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-3: OPEN SPACE

Planning Area A-3 provides for the development of 10.77 acres of open space which will be part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-3 is located in Zones C and D of the CLUP. Development in these zones will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-3 is accessed from Tyler Street.
- (4) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The edge treatment for the interface between the drainage uses in this planning area and the heavy industrial and mixed use/air park land uses in the adjacent planning areas are described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (6) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-18, Tyler Street Trail**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

A-4: HEAVY INDUSTRIAL

Planning Area A-4 provides for 81.17 acres of heavy industrial development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-4 is located in Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-4 is accessed from Tyler Street.
- (4) The edge treatment for the interface between Planning Area A-4's heavy industrial use and the drainage uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-5: COMMERCIAL RETAIL

Planning Area A-5 provides for 17.61 acres of commercial development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-5 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area A-5 is accessed from Tyler Street and Avenue 62.
- (3) The edge treatment for the interface between the commercial use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-19, Tyler Street Trail**. A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-6: MIXED USE

Planning Area A-6 provides for 46.18 acres of mixed-use development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-6 is located within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development in these zones will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-6 is accessed from Tyler Street.
- (4) The edge treatment for the interface between the drainage uses and the heavy industrial land uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-18, Tyler Street Trail**.
- (6) An observation tower built within Planning Area A-6 and built as part of a large scale recreational use shall not exceed 70 feet.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 3.3 – Design Guidelines for Mixed Use Development and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-7: OPEN SPACE

Planning Area A-7 provides for 9.97 acres of open space which will be part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-7 is located within Zone C of the CLUP for the airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-7 is accessed from Tyler Street and Avenue 62.
- (4) The edge treatment for the interface between the drainage in this planning area and the heavy industrial and commercial land uses in the adjacent planning areas is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

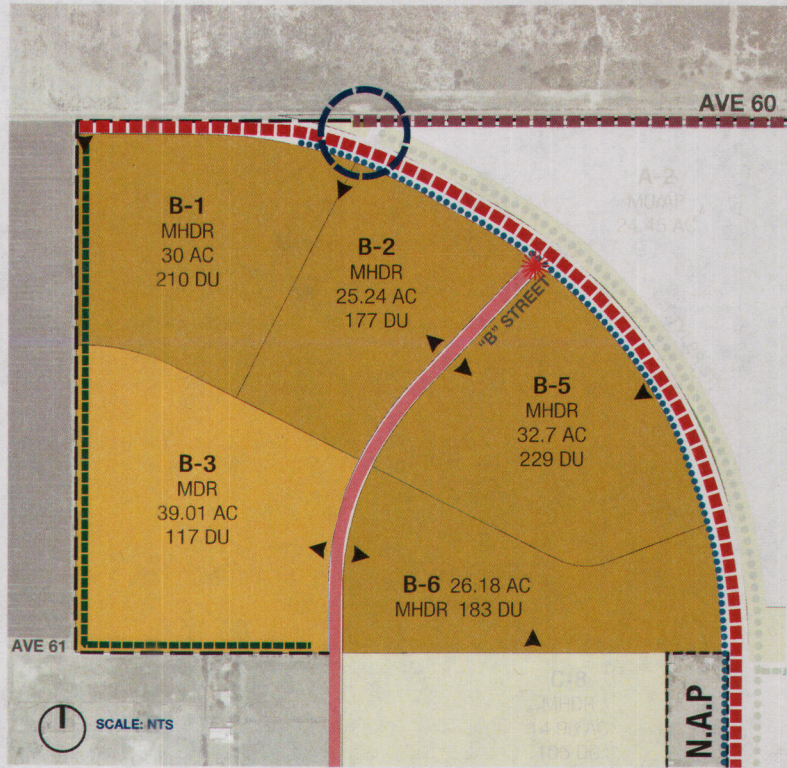
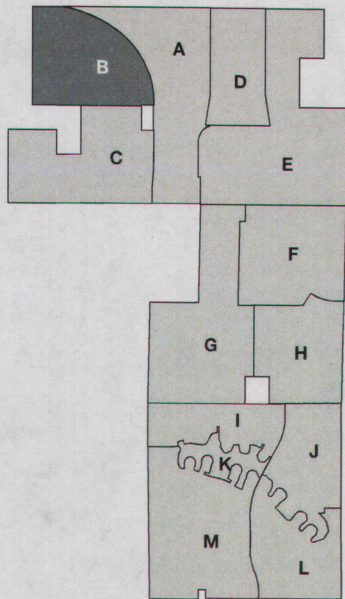
A-8: *Planning Area A-8 has been combined with adjacent planning area E-4 to provide a new Planning Area E-4 described below in Section 3.2.8.*

3.2.2 Neighborhood B

Neighborhood B contains five planning areas and is comprised of 153.13 acres as reflected in **Figure 3.2-3, Neighborhood B**. It is located in the northwestern portion of the project site, just south of "B" Street and Tyler Street. Proposed land uses are focused along Tyler Street, and include Medium Density Residential (39.01 acres) and Medium High Density Residential (114.12 acres). A target of 916 residential units is planned for Neighborhood B. These units are intended to provide housing for employees of the business, commercial and industrial areas in the northern portion of the project site. Primary access to the neighborhood is provided via Tyler Street and "B" Street. A major project entry is provided at the Avenue 60/Tyler Street intersection.

NEIGHBORHOOD B PLANNING STANDARDS

- (1) The entire neighborhood is located within Zone D of the CLUP for the airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1 – Project Wide Planning Standards of this Specific Plan as depicted in **Table 3.1-G, Basic Compatibility Criteria**.
- (2) Avenue 60 provides access to Planning Areas B-1.
- (3) Tyler Street provides access to Planning Areas B-2, B-5 and B-6.
- (4) "B" Street provides access to Planning Areas B-2, B-3, B-5 and B-6.
- (5) A Major Project Entry is located in the northwest corner of the neighborhood at Avenue 60/Tyler Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) A Special Intersection Detail is provided for the intersection of "B" Street and Tyler Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (7) Tyler Street will be developed in accordance with **Figure 3.4-8, Arterial Streetscape 1**.
- (8) "B" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.



LEGEND

- PROJECT AREA BOUNDARY
- INDUSTRIAL COLLECTOR STREETScape, INFORMAL CANOPY
- ARTERIAL STREETScape 1, PALM TREE
- COLLECTOR STREETScape 1, FORMAL CANOPY
- WINDROW PLANTING
- CLASS II BIKE PATH
- FULL ACCESS
- INTERSECTION DETAIL
- MAJOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0-8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0
TOTAL		916	153.13	

NEIGHBORHOOD B PLANNING AREAS**B-1: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area B-1 provides for the development of 30.00 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 210 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-1 is accessed from Tyler Street.
- (2) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (3) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-2: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area B-2 provides for the development of 25.24 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning). A target of 177 dwelling units is planned within a density range 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-2 is accessed from Tyler Street and "B" Street.
- (2) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (3) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-3: MEDIUM DENSITY RESIDENTIAL

Planning Area B-3 provides for the development of 39.01 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 117 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-3 is accessed from "B" Street.
- (2) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer.**
- (3) Please refer to Section 33.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-4: *Planning Area number has intentionally been skipped in the numbering sequence.*

B-5: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area B-5 provides for the development of 32.7 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development. A target of 229 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-5 is accessed from "B" Street and Tyler Street.
- (2) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (3) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-6: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area B-6 provides for the development of 26.18 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 183 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-6 is accessed from "B" Street and Tyler Street.
- (2) This planning area will be developed with the project edge buffer conditions as indicated in in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer** .
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project-Wide Planning Standards.

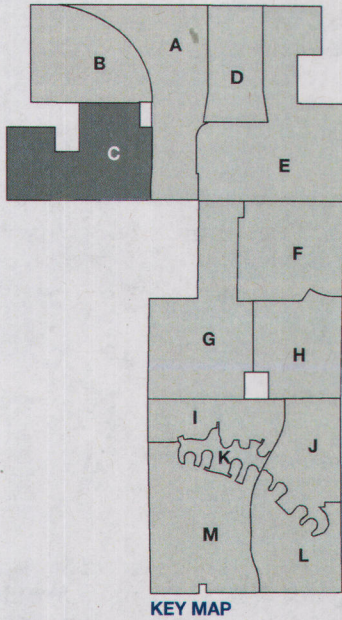
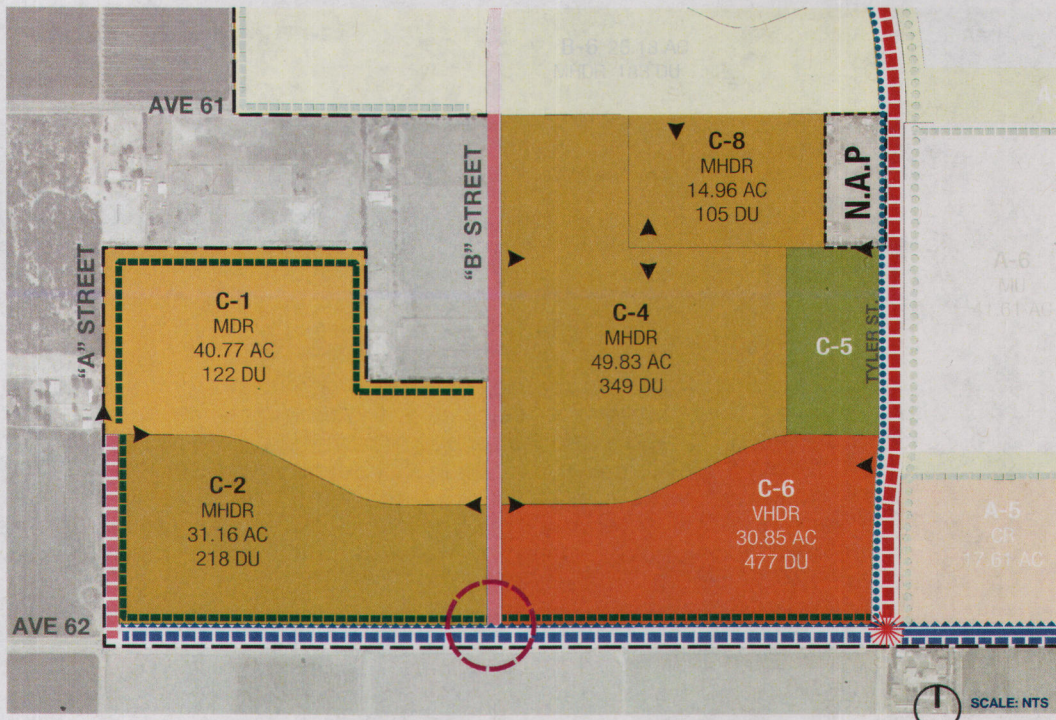
PLANNING AREA B-7: *Planning Area number has intentionally been skipped in the numbering sequence.*

3.2.3 Neighborhood C

Neighborhood C is located just north of Avenue 62 in the northwestern portion of the project site as reflected in **Figure 3.2-4, Neighborhood C**. This 177.54-acre neighborhood contains the following land uses within its six planning areas: Medium Density Residential (40.77 acres), Medium High Density Residential (95.95 acres), Very High Density Residential (30.85 acres) and Open Space (9.97 acres). A target of 1,271 dwelling units is planned for Neighborhood C. The predominantly residential uses are intended to house employees working in the employment-based neighborhoods located nearby. The maximum density and the total number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. The proximity of these residential areas to job opportunities is designed to reduce the number and length of home-to-work trips that would otherwise be generated by these land uses. The higher density residential uses are located in closer proximity to commercial uses, while the lower density residential uses are located at the project periphery. If developed with a golf course, a permitted use under provisions of Section 2 - Specific Plan Zoning Ordinance, residents would be provided with a high-quality recreational and scenic amenity. A number of the neighborhood planning areas would be afforded golf course frontage. In the absence of the golf course, drainage ways would function to provide more natural open space, providing structure and definition to the neighborhood. In addition, a local park would be developed to serve neighborhood residents and act as a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for development of a kart track or driver training facility. Primary access to the neighborhood is from "A" Street, "B" Street, Tyler Street and Avenue 62.

NEIGHBORHOOD C PLANNING STANDARDS

- (1) Neighborhood C is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with this zone are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use is a permitted use in all planning areas as defined in Section 2 – Specific Plan Zoning Ordinance.
- (3) Avenue 62 provides access to Planning Areas C-2 and C-6.
- (4) "A" Street provides access to Planning Areas C-1 and C-2.
- (5) "B" Street provides access to Planning Areas C-1, C-2, C-4, C-6 and C-8.
- (6) Tyler Street provides access to Planning Areas C-5 and C-6.
- (7) A Minor Entry is located at "B" Street and Avenue 62 to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (8) "B" Street will be developed in accordance with **Figures 3.4-17, Collector Streetscape 1**.
- (9) "A" Street will be developed in accordance with **Figures 3.4-18, Collector Streetscape 2**.
- (10) Tyler Street north of Avenue 62 will be developed in accordance with **Figures 3.4-8, Aerial Streetscape 1**.
- (11) Avenue 62 will be developed in accordance with **Figures 3.4-6, Expressway Streetscape**.
- (12) A local park will be provided in Planning Area C-5, to serve neighborhood residents and provide a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for development of a kart track.
- (13) The north side of Avenue 62 will be developed with **Figure 3.4-20, Avenue 62 Trail** as reflected in **Figure 3.1-10, Public Facilities**.



- LEGEND**
- PROJECT AREA BOUNDARY
 - INDUSTRIAL COLLECTOR STREETScape, INFORMAL CANOPY
 - EXPRESSWAY STREETScape, FORMAL CANOPY
 - EXPRESSWAY STREETScape, PALM TREE
 - ARTERIAL STREETScape 1, PALM TREE
 - COLLECTOR STREETScape 1, FORMAL CANOPY
 - COLLECTOR STREETScape 2, INFORMAL CANOPY
 - SECONDARY STREETScape
 - WINDROW PLANTING
 - CLASS I BIKE PATH
 - REGIONAL TRAIL
 - CLASS II BIKE PATH
 - FULL ACCESS
 - INTERSECTION DETAIL
 - MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
C-1	Medium Density Residential (MDR)	122	40.77	5.0-8.0
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0
C-5	Open Space (OS)		9.97	
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0
C-8	Medium High Density Residential (RM)	105	14.96	5.0-8.0
TOTAL		1271	177.54	

NEIGHBORHOOD C PLANNING AREAS**C-1: MEDIUM DENSITY RESIDENTIAL**

Planning Area C-1 provides for the development of 40.77 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 122 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area C-1 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-1 is accessed from "A" Street and "B" Street.
- (4) The Project Edge Treatment for this planning area is illustrated in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

C-2: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area C-2 provides for the development of 31.16 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 218 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area C-2 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-2 is accessed from "A" Street, "B" Street and Avenue 62.
- (4) The Project Edge Treatment for this planning area is illustrated in **Figure 3.4-29, out Parcel/Adjacent Property Buffer**.
- (5) A Minor Entry is planned for the intersection of "B" Street and Avenue 62 as shown in **Figure 3.4-4, Minor Project Entry**.
- (6) A Class I Bike Path is located in this planning area along the north side of Avenue 62 as illustrated in **Figure 3.4-20, Avenue 62 Trail**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

C-3: *Planning Area number has intentionally been skipped in the numbering sequence.*

C-4: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area C-4 provides for the development of 49.83 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional lots and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning). A target of 349 dwelling units is planned within a density range of 5.0–8.0 du/acre).

Land Use and Development Standards

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning).

Planning Standards

- (1) Planning Area C-4 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-4 is accessed from "B" Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

C-5: OPEN SPACE

Planning Area C-5 provides for the development of 9.97 acres of open space that will be developed as a local park and provide a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for mixed-use development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area C-5 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-5 is accessed from Tyler Street.
- (4) This planning area will be developed in accordance with **Figure 3.4-29, Out Parcel/Adjacent Property Buffer.**
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

C-6: VERY HIGH DENSITY RESIDENTIAL

Planning Area C-6 provides for the development of 30.85 acres of very high density residential land uses with minimum lot sizes of 4,000 square feet for conventional lots and 2,000 square feet under cluster development. A target of 477 dwelling units is planned within a density range of 14.0–20.0 du/acre. The maximum density and the total number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area C-6 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area C-6 is accessed from “B” Street, Tyler Street and Avenue 62.
- (3) A Minor Entry is planned for the intersection of “B” Street and Avenue 62 to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) The Commercial Buffer treatment for the interface between the residential uses in this planning area and the commercial uses located across the street is illustrated in **Figure 3.4-31, Commercial Buffer Streetscape**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

C-7: *Planning Area number has intentionally been skipped in the numbering sequence.*

C-8: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area C-8 provides for the development of 14.96 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development. A target of 105 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

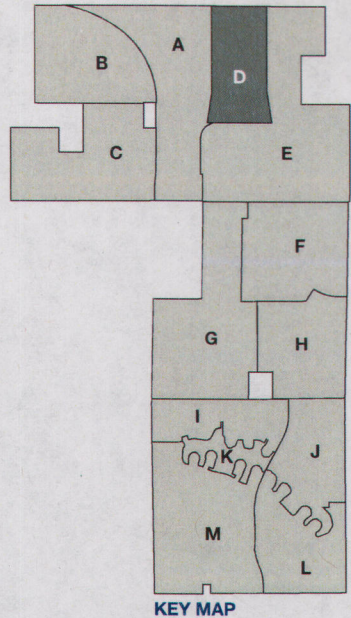
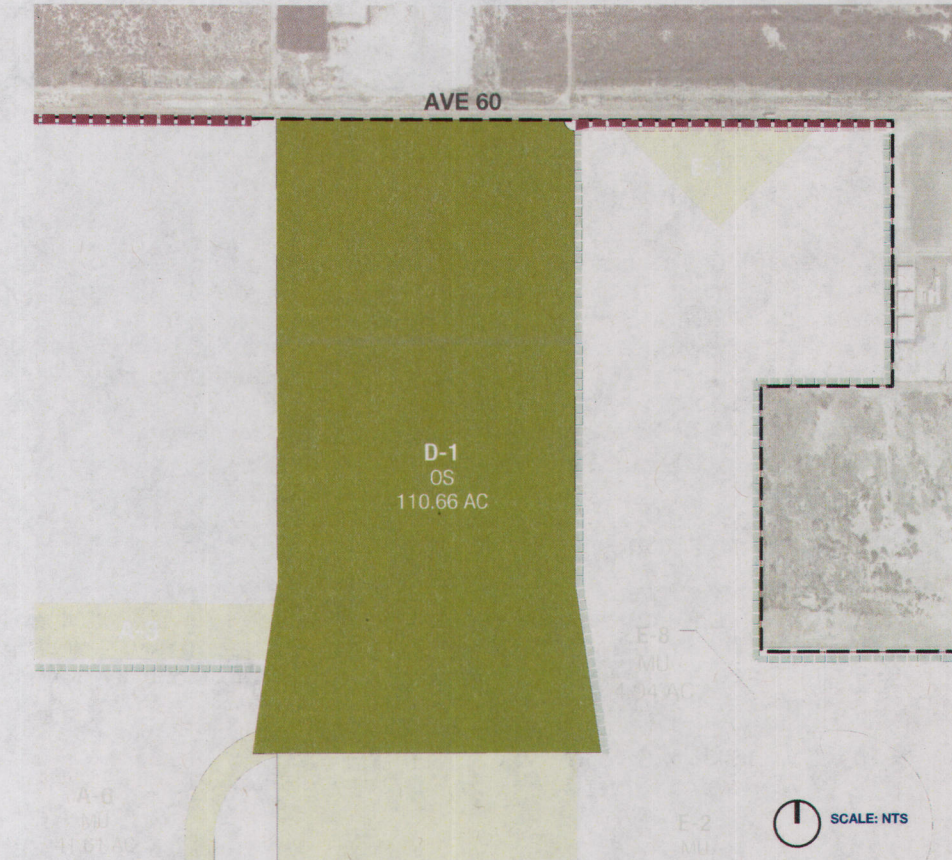
- (1) Planning Area C-8 is located within Zone D of the CLUP for the Desert Resorts Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area C-8 is accessed from "B" Street through interior loop roads.
- (3) This planning area will be developed with the Buffer Condition along the adjacent property to the east which is depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.4 Neighborhood D

Neighborhood D is located in the northern portion of the project site, between Avenue 60 and Avenue 62 as depicted in **Figure 3.2-5, Neighborhood D**. This neighborhood is intended to function as a buffer between the business, commercial, recreational uses and the airport runway to the north. The neighborhood is 110.66 acres in size and consists of open space uses. All of the open space is within airport influenced areas and is designed to handle storm drainage. Primary access to this neighborhood is provided via Avenue 60 from the west or the east, but Avenue 60 will not traverse the northern edge of the Neighborhood due to possible airport expansion.

NEIGHBORHOOD D PLANNING STANDARDS

- (1) The open space neighborhood is constrained by the OSZ, Inner Safety Zone (ISZ) and ETZ airport zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, Neighborhood D is located within Zone A of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Avenue 60 provides access to Planning Area D-1.



LEGEND
--- PROJECT AREA BOUNDARY
■■■■ INDUSTRIAL COLLECTOR STREETSCAPE, INFORMAL CANOPY

NEIGHBORHOOD D PLANNING AREAS**D-1: OPEN SPACE**

Planning Area D-1 provides for 110.66 acres of open space. A portion of this area will be used to handle storm flows. Much of the planning area is described in the Jacqueline Cochran Regional Airport Master Plan as proposed for acquisition for the runway clear zone. This planning area shall remain in open space uses.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area D-1 is located within the Jacqueline Cochran Regional Airport ETZ and ISZ airport safety zones as reflected on **Figure 3.1-13 Airport Zones** and **Table 3.1-F, Airport Safety Zones**. Development within these zones will comply with all applicable restrictions and requirements. Additionally, Neighborhood D is located within Zone A of the CLUP for the Jacqueline Cochran Regional Airport as reflected in reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with this zone are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Planning Area D-1 is accessed from Avenue 60.
- (3) The Buffer Condition between Planning Area D-1 and adjacent industrial uses will be as illustrated in **Figure 3.4-6, Expressway Streetscape**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.5 Neighborhood E

Neighborhood E is situated in the northeastern corner of the project site and is bounded by Jacqueline Cochran Regional Airport and Avenue 60 on the north, Polk Street on the east, and Avenue 62 on the south as depicted in **Figure 3.2-6, Neighborhood E**. This neighborhood has a strong regional orientation as a result of its excellent access to the regional circulation system and to Jacqueline Cochran Regional Airport. The neighborhood also looks eastward by virtue of its potential connection to the proposed interchange at Avenue 62 and the new Highway 86, just a few miles to the east. Primary access is provided by Polk Street, Avenue 60 and Avenue 62. The land uses proposed in this 281.49-acre neighborhood include mixed-use (206.55 acres), and open space (74.94 acres).

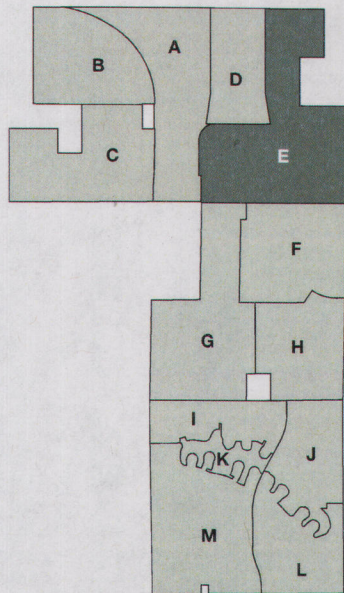
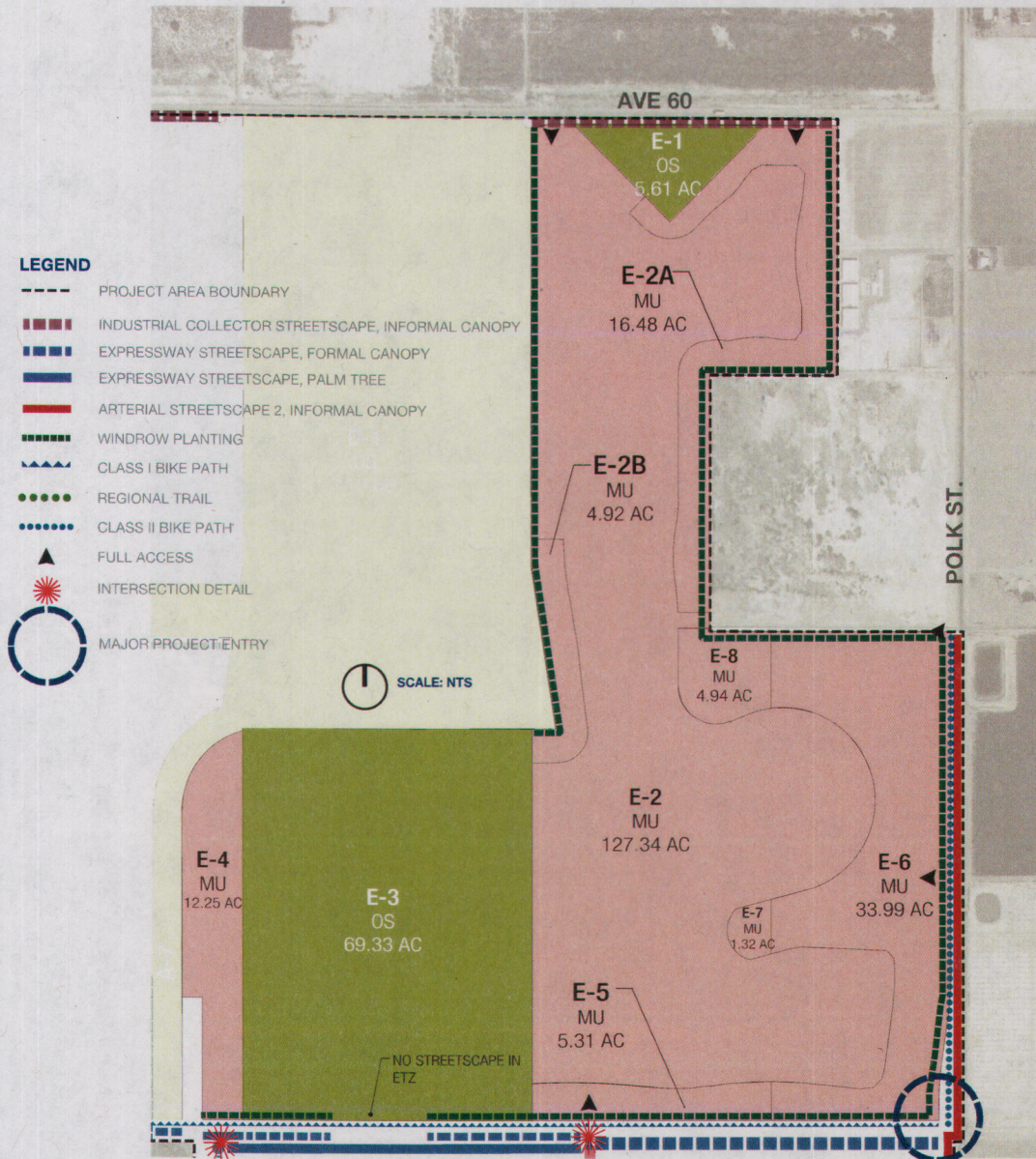
A racetrack has been approved for this neighborhood with its own separate set of conditions of approval and design manual for features specific to its development. The open space in the northeastern portion of the neighborhood responds to airport-related constraints, and will become part of the larger recreational land use.

Land designated for mixed use development abutting the airport property offers the potential for airport uses to extend southward into the site. This area also has potential for rail service to be provided from the north via a rail spur from the Southern Pacific main line. In addition to the jobs that could be created by heavy industry, Neighborhood E allows for a large-scale recreational use such as a motor sports race track which will also create jobs. Agricultural operations to the east of Polk Street offer the potential for development of compatible agriculture-related uses on the property. A Major Entry is located at the intersection of Polk Street and Avenue 62.

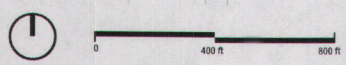
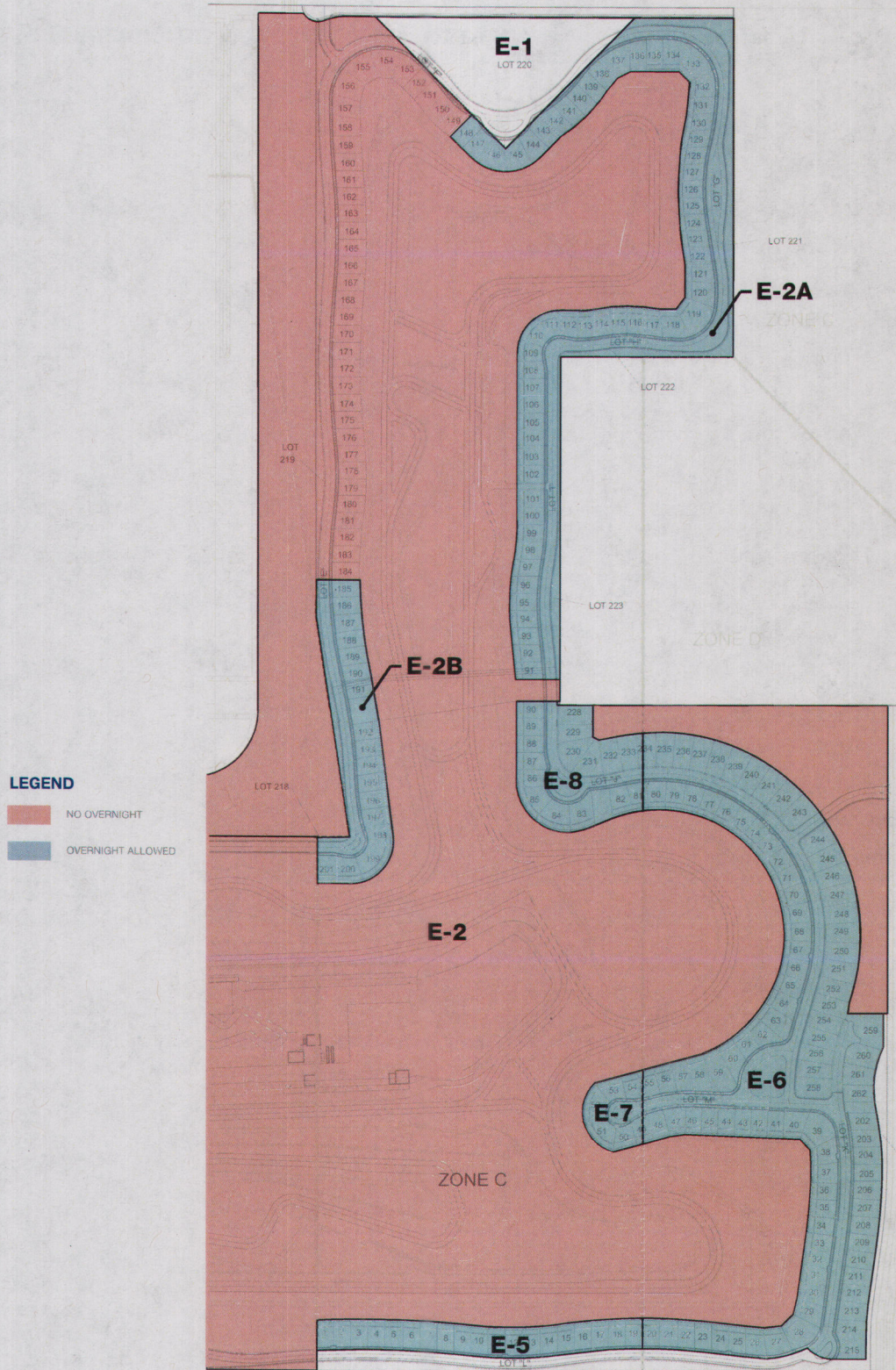
NEIGHBORHOOD E PLANNING STANDARDS

- (1) Portions of Neighborhood E are constrained by the OSZ and ETZ airport safety zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within Zones B1, C, and D of the CLUP as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan. As Neighborhood E allows for mixed-use residential uses in designated planning areas, **Figure 3.2-6A, Lots Allowing Overnight Use**, identifies which lots within those planning areas allow for such use. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive zone policy applies. For example, Lot 55 on **Figure 3.2-6A** lies within both Zone C and D. Because Zone D is the less restrictive zone, Zone D ALUC policies apply.
- (2) Tyler Street provides access to Planning Areas E-2, E-2A, E-2B, E-3, E-4, E-5, E-6, E-7, and E-8 through Planning Area A-6.
- (3) Avenue 60 provides access to Planning Area E-1 and E-2, E-2A, E-2B.
- (4) Polk Street provides access to Planning Area E-2, E-2A, E-2B, E-5, E-6, E-7, and E-8.
- (5) A Major Project Entry is located at Tyler Street and Polk Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (7) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**, with the exception of the ETZ in which no trees are to be planted.
- (8) A large-scale recreational use such as a motor sports race track is a permitted land use in seven of the eight planning areas as defined in Section 2 – Specific Plan Zoning Ordinance.
- (9) The north side of Avenue 62 will be developed as depicted in **Figure 3.1-10, Public Facilities** in accordance with **Figure 3.4-20, Avenue 62 Trail**.

- (10) The west side of Polk Street will be developed with a Class II Bike Path as described **Figure 3.1-10, Public Facilities.**



Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
E-1	Open Space (OS)		5.61	
E-2	Mix Use (MU)		127.34	
E-2A	Mix Use (MU)		16.48	
E-2B	Mix Use (MU)		4.92	
E-3	Open Space (OS)		69.33	
E-4	Commercial Retail (CR)		12.25	
E-5	Mix Use (MU)		5.31	
E-6	Mix Use (MU)		33.99	
E-7	Mix Use (MU)		1.32	
E-8	Mix Use (MU)		4.94	
TOTAL			281.49	



NEIGHBORHOOD E PLANNING AREAS**E-1: OPEN SPACE**

Planning Area E-1 provides for the development of 5.61 acres of open.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-1 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and the ETZ and OSZ airport safety zones. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area.
- (3) Planning Area E-1 is accessed from Avenue 60.
- (4) Large-scale recreational uses such as a golf course and a motor sports race track are permitted uses in this planning area.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

E-2: MIXED USE

Planning Area E-2 provides for the development of 127.34 acres of mixed-use nonresidential development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-2 is located within Zone C of the Jacqueline Cochran Regional Airport CLUP. This planning area will comply with all applicable restrictions and requirements of that zone.
- (2) Access to Planning Area E-2 is from Tyler and Polk Streets.
- (3) The edge treatment for the interface between the planned mixed use and adjacent open space is illustrated in **Figure 3.4-28, Airpark Drainage Buffer**. The edge treatment will be as described in **Figure 3.4-24, Golf Course/ Racetrack Edge Condition**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area. Signage shall be posted that overnight occupancy is prohibited in non-residential structures. No minimum setback or buffer is required for Planning Area E-2 from adjacent residential uses.
- (6) An observation tower built within Planning Area E-2 and built as part of a large scale recreational use shall not exceed 70 feet.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.

- (9) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Project Wide Development Standards.

E-2A: MIXED USE

Planning Area E-2A provides for the development of 16.48 acres of mixed-use development to include the development of recreational units which are non-residential member villas providing a private space for a Thermal Motorsports Park Club member to store personal belongings, vehicles, and equipment utilized for racing; utilized for vehicular repairs and other racing-related recreational activities; and are located adjacent to the racetrack with an open view deck facing the track to allow viewing of racing activities. These units are accessible 24 hours a day, seven days a week, however, they are restricted from permanent residency and have limitations imposed by the Thermal Motorsports Park Club for duration of stays. A maximum of 58 racetrack recreational units is planned.

Land Use and Development Standards

Please refer to Ordinance No. 348 and Specific Plan Amendment No. 4, Specific Plan Zoning Ordinance.

Planning Standards

- (1) Planning Area E-2A is located within Zone C of the Jacqueline Cochran Regional Airport CLUP. This planning area will comply with all applicable restrictions and requirements of that zone.
- (2) Access to Planning Area E-2A is from Tyler and Polk Streets.
- (3) The edge treatment for the interface between the planned mixed use and adjacent open space is illustrated in **Figure 3.4-28, Airpark Drainage Buffer**. The edge treatment will be as described in **Figure 3.4-24, Golf Course/ Racetrack Edge Condition**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area including racetrack recreational units defined as non-residential use identified as a member villa accessible 24 hours a day, seven days a week. The Racetrack Recreational Unit provides a private space for a Thermal Club members to store personal belongings, vehicles, and equipment utilized for racing. The unit can further be utilized for vehicular repairs, other racing-related recreational activities and is located directly adjacent to the racetrack with an open view deck facing the track allowing for viewing of racing activities. The unit would be restricted from permanent residency, and has limitations imposed by the club for duration of stays..
- (6) Standards for Racetrack Recreational Unit development:
 - a. A maximum of 58 racetrack recreational units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-2A.
 - b. Structures must provide vehicle storage area on the ground floor of structure and may include the following:
 - Garage Level (1st Floor):
 - Cars
 - Lifts
 - Work areas
 - Storage
 - Mechanical
 - Main Entry
 - Half baths

- Bath with locker rooms
- Washer/dryer area
- Elevator
- Stairs
- Media Room
- Kitchenette area

Second Level (and/or mezzanine):

- Elevator
- Stairs
- Storage
- Mechanical
- Bedrooms and closets
- Bath area
- Laundry
- Family Room/Game Room
- Kitchen
- Outside Patios
- Outside Planters

Third Level (Main Living Level):

- Bedrooms and Closets
- Bath
- Elevator
- Stairs
- Laundry
- Kitchen
- Dining
- Great Room
- Outside Patios
- Mechanical
- Storage
- Game Room
- BBQ area
- Outside pools and hot tub
- Planters

- c. Access to structures shall be from common access areas or corridors
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 2.0 – Zoning Ordinance for development of other allowable uses and associated developments standards.
- (10) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (11) Please refer to Section 3.1 – Project Wide Development Standards.

E-2B: MIXED USE

Planning Area E-2B provides for the development of 4.92 acres of mixed-use development to include the development of recreational units which are non-residential member villas providing

a private space for a Thermal Motorsports Park Club member to store personal belongings, vehicles, and equipment utilized for racing; utilized for vehicular repairs and other racing-related recreational activities; and are located adjacent to the racetrack with an open view deck facing the track to allow viewing of racing activities. These units are accessible 24 hours a day, seven days a week, however, they are restricted from permanent residency and have limitations imposed by the Thermal Motorsports Park Club for duration of stays. A maximum of 16 racetrack recreational units is planned.

Land Use and Development Standards

Please refer to Ordinance No. 348 and Specific Plan Amendment No. 4, Section 2 – Specific Plan Zoning Ordinance.

Planning Standards

- (1) Planning Area E-2B is located within Zone C of the Jacqueline Cochran Regional Airport CLUP. This planning area will comply with all applicable restrictions and requirements of that zone.
- (2) Access to Planning Area E-2B is from Tyler and Polk Streets.
- (3) The edge treatment for the interface between the planned mixed use and adjacent open space is illustrated in **Figure 3.4-28, Airpark Drainage Buffer**. The edge treatment will be as described in **Figure 3.4-24, Golf Course/ Racetrack Edge Condition**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area including racetrack recreational units defined as non-residential use identified as a member villa accessible 24 hours a day, seven days a week. The Racetrack Recreational Unit provides a private space for a Thermal Club members to store personal belongings, vehicles, and equipment utilized for racing. The unit can further be utilized for vehicular repairs, other racing-related recreational activities and is located directly adjacent to the racetrack with an open view deck facing the track allowing for viewing of racing activities. The unit would be restricted from permanent residency, and has limitations imposed by the club for duration of stays..
- (6) Standards for Racetrack Recreational Unit development:
 - a. A maximum of 16 racetrack recreational units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-2B.
 - b. Standard Setbacks
 - i. No minimum setback is required from any private street.
 - ii. Front Yard: 30-foot minimum
 - iii. Rear Yard: No minimum
 - iv. Side Yard: 5-feet.
 - c. Structures must provide vehicle storage area on the ground floor of structure and may include the following:
 - Garage Level (1st Floor):
 - Cars
 - Lifts
 - Work areas
 - Storage
 - Mechanical
 - Main Entry
 - Half baths
 - Bath with locker rooms
 - Washer/dryer area
 - Elevator

- Stairs
- Media Room
- Kitchenette area

Second Level (and/or mezzanine):

- Elevator
- Stairs
- Storage
- Mechanical
- Bedrooms and closets
- Bath area
- Laundry
- Family Room/Game Room
- Kitchen
- Outside Patios
- Outside Planters

Third Level (Main Living Level):

- Bedrooms and Closets
- Bath
- Elevator
- Stairs
- Laundry
- Kitchen
- Dining
- Great Room
- Outside Patios
- Mechanical
- Storage
- Game Room
- BBQ area
- Outside pools and hot tub
- Planters

- d. Access to structures shall be from common access areas or corridors
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 2.0 – Zoning Ordinance for development of other allowable uses and associated developments standards.
- (10) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (11) Please refer to Section 3.1 – Project Wide Development Standards.

E-3: OPEN SPACE

Planning Area E-3 provides for the development of 69.33 acres of open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards – General

- (1) Planning Area E-3 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and the ETZ and OSZ airport safety zones. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area.
- (3) Planning Area E-3 is accessed from Tyler Street.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail** with the exception of the ETZ where no trees shall be planted.
- (5) A Class I Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) The edge treatment for the interface between the open spaces uses and industrial uses in the adjacent planning area is illustrated in **Figure 3.4-28 Airpark Drainage Buffer**. If the race track is developed, the edge treatment will be as described in **Figure 3.4-24 Golf Course/Racetrack Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

E-4: MIXED USED

Planning Area E-4 provides for 12.25 acres of nonresidential mixed-use land development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-4 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-4 accessed from Tyler Street.
- (3) The edge treatment for the interface between the mixed use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-28 Airpark Drainage Buffer**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class I Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

E-5: MIXED USE

Planning Area E-5 provides for 5.31 acres of mixed-use land development with minimum lot sizes of 7,000 square feet as defined in Section 2 – Specific Plan Zoning.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-5 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-5 accessed from Polk and Tyler Streets.
- (3) The edge treatment for the interface between the mixed use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-28 Airpark Drainage Buffer**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class 1 Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Standards for Residential development:
 - a. A maximum of 19 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-5.
 - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
 - c. No buffer is required in Planning Area E-5 for residential uses contiguous to Planning Area E-2.
 - d. Residential structures must provide vehicle storage area on the ground floor of structure.
 - e. Access to residential structures shall be from common access areas or corridors.
 - f. Structures on lots with a net area of 7,540 square feet or less shall not exceed the following standards without further ALUC review:
 - i. 7,150 square feet; or
 - ii. two stories or 42 feet in height.
 - g. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
 - i. Floor Area Ratio – 0.95;
 - ii. Lot Coverage – 0.5; and
 - iii. 42 feet in height.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Project Wide Development Standards.

E-6: MIXED USE

Planning Area E-6 provides for 33.99 acres of residential and nonresidential mixed-use land development as defined in Section 2 – Specific Plan Zoning.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-6 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-6 accessed from Polk and Tyler Streets.
- (3) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) A Class II Bike Path is located in this planning area along west side of Polk Street. A Class I Bike Path is also located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (5) A Major Project Entry is located at the southeast corner of this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) A total of 8 acres of nonresidential development may occur in the northeastern corner of Planning Area E-6 in addition to residential development.
- (7) Standards for Residential development:
 - a. A maximum of 120 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-6.
 - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
 - c. No buffer is required in Planning Area E-6 for residential uses contiguous to Planning Area E-2.
 - d. Standard Setbacks
 - i. No minimum setback is required from any private street.
 - ii. Front Yard: 30-foot minimum
 - iii. Rear Yard: No minimum
 - iv. Side Yard: 5-feet.
 - e. Residential structures must provide vehicle storage area on the ground floor of structure.
 - f. Access to residential structures shall be from common access areas or corridors.
 - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the follow standards without further ALUC review:
 - i. 7,150 square feet; or
 - ii. two stories or 42 feet in height.
 - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
 - i. Floor Area Ratio – 0.95;
 - ii. Lot Coverage – 0.5; and
 - iii. 42 feet in height.
- (8) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (9) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (10) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (11) Please refer to Section 3.1 – Project Wide Development Standards.

E-7: MIXED USE

Planning Area E-7 provides for 1.32 acres of mixed-use land development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-7 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-7 accessed from Polk and Tyler Streets.
- (3) Standards for Residential development:
 - a. A maximum of 5 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-7.
 - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
 - c. No buffer is required in Planning Area E-7 for residential uses contiguous to Planning Area E-2.
 - d. Standard Setbacks
 - i. No minimum setback is required from any private street.
 - ii. Front Yard: 30-foot minimum
 - iii. Rear Yard: No minimum
 - iv. Side Yard: 5-feet.
 - e. Residential structures must provide vehicle storage area on the ground floor of structure.
 - f. Access to residential structures shall be from common access areas or corridors.
 - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the follow standards without further ALUC review:
 - i. 7,150 square feet; or
 - ii. two stories or 42 feet in height.
 - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
 - i. Floor Area Ratio – 0.95;
 - ii. Lot Coverage – 0.5; and
 - iii. 42 feet in height.
- (4) Please refer to Section 3.3 – Design and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

E-8: MIXED USE

Planning Area E-8 provides for 4.94 acres of mixed-use land development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

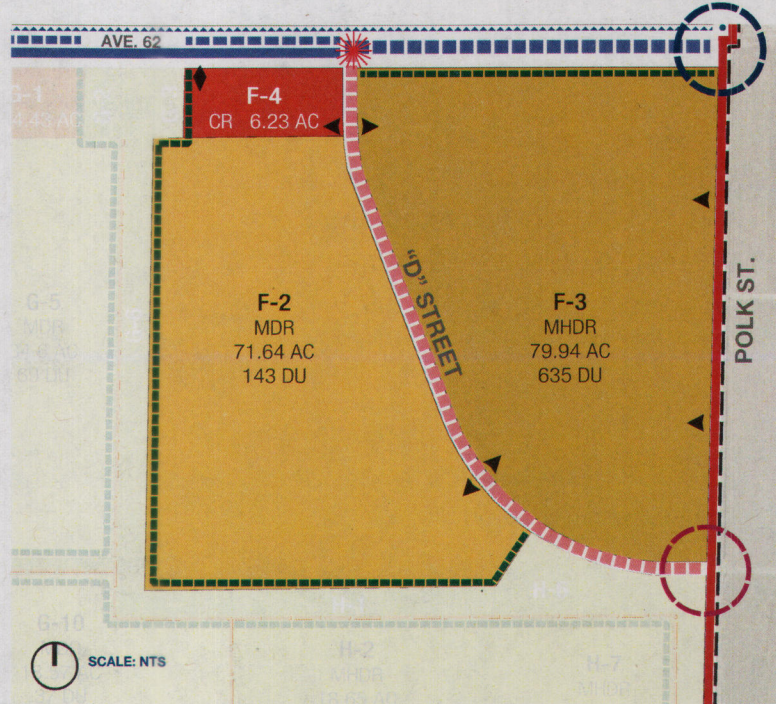
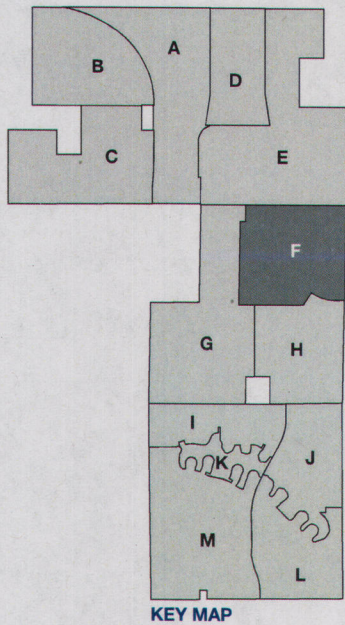
- (1) Planning Area E-8 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-8 accessed from Polk and Tyler Streets.
- (3) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Standards for Residential development:
 - a. A maximum of 15 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-8.
 - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
 - c. No buffer is required in Planning Area E-8 for residential uses contiguous to Planning Area E-2.
 - d. Standard Setbacks
 - i. No minimum setback is required from any private street.
 - ii. Front Yard: 30-foot minimum
 - iii. Rear Yard: No minimum
 - iv. Side Yard: 5-feet.
 - e. Residential structures must provide vehicle storage area on the ground floor of structure.
 - f. Access to residential structures shall be from common access areas or corridors
 - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the follow standards without further ALUC review:
 - i. 7,150 square feet; or
 - ii. two stories or 42 feet in height.
 - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
 - i. Floor Area Ratio – 0.95;
 - ii. Lot Coverage – 0.5; and
 - iii. 42 feet in height.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards

3.2.6 Neighborhood F

Neighborhood F totals 157.81 acres, and is located along Polk Street, just south of Avenue 62 as depicted in **Figure 3.2-7, Neighborhood F**. It consists of 71.64 acres of medium density residential, 79.94 acres of medium high density residential, and 6.23 acres of commercial retail land use designations. Primary access is provided by "D" Street, Avenue 62 and Polk Street. It is anticipated that the planned commercial land uses will provide employment to project area residents and will service an area larger than the site itself. A Major Entry is located at the intersection of Polk Street and Avenue 62.

NEIGHBORHOOD F PLANNING STANDARDS

- (1) Portions of the commercial land uses in Neighborhood F are constrained by the OSZ and ERC airport safety zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within zones B1, C and D of the CLUP as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Major project entry is located at the intersection of Avenue 62 and Polk Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (3) An intersection detail is planned for "D" Street and Avenue 62 to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) "D" Street will provide access to Planning Areas F-4, F-2 and F-3.
- (5) Avenue 62 provides access to Planning Area F-3 and F-4.
- (6) Polk Street provides access to Planning Area F-3.
- (7) Polk Street will be developed in accordance with **Figures 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (8) "D" Street will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (9) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**.



LEGEND

- PROJECT AREA BOUNDARY
- EXPRESSWAY STREETSCAPE, FORMAL CANOPY
- ARTERIAL STREETSCAPE 2, INFORMAL CANOPY
- COLLECTOR STREETSCAPE 2, INFORMAL CANOPY
- WINDROW PLANTING
- CLASS I BIKE PATH
- RIGHT IN/RIGHT OUT ACCESS
- FULL ACCESS
- INTERSECTION DETAIL
- MAJOR PROJECT ENTRY
- MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
F-2	Medium Density Residential (MDR)	143	71.64	2.0-5.0
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0
F-4	Commercial Retail (CR)		6.23	
TOTAL		778	157.81	

NEIGHBORHOOD F PLANNING AREAS**F-2: MEDIUM DENSITY RESIDENTIAL**

Planning Area F-2 provides for the development of 71.64 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 under cluster development. A target of 158 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area F-2 is located within Zones B1, C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-2 is accessed from "D" Street.
- (3) The edge treatment for the interface between the planning area and adjacent drainage channel is shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

F-3: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area F-3 provides for the development of 79.94 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 635 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area F-3 is located within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-3 is accessed from "D" Street, Avenue 62 and Polk Street.
- (3) A Major Project Entry is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-3, Major Entry**.
- (4) A Minor Entry is located in the southeast corner of the planning area to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (5) An intersection detail is located in the northwest corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards

F-4: COMMERCIAL RETAIL

Planning Area F-4 provides for the development of 6.23 acres of commercial land uses.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

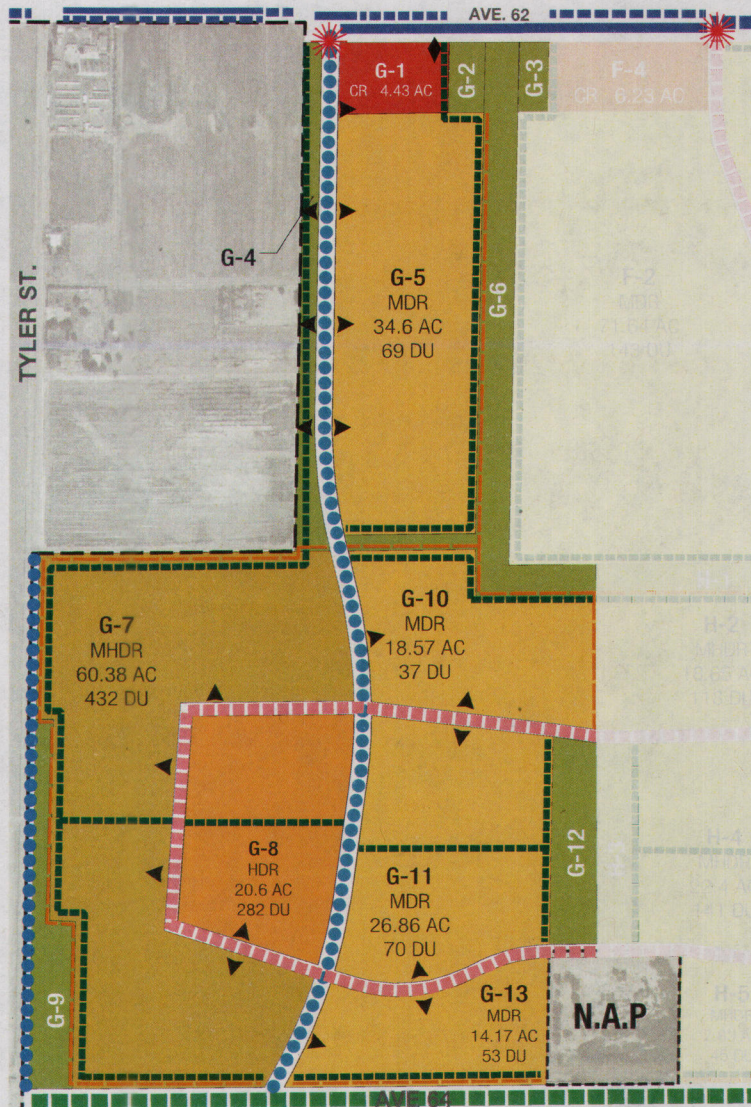
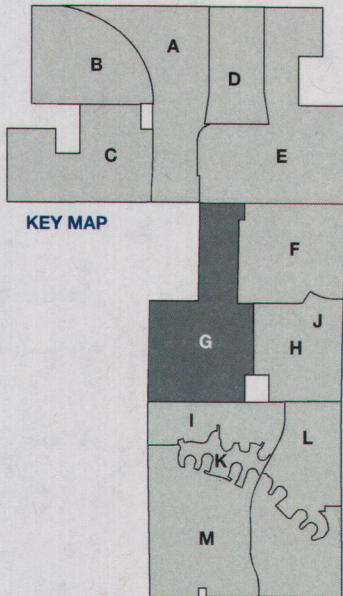
- (1) Planning Area F-4 is located within Zone B1 and C of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-4 is accessed from Avenue 62 and "D" Street.
- (3) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.7 Neighborhood G

Neighborhood G is a predominantly residential neighborhood located roughly in the center of the project site, just east of Tyler Street as depicted in **Figure 3.2-8, Neighborhood F**. Primary access to the neighborhood is provided by "C" Street and Avenue 62. This is a self-contained, inward-focused community that is physically separated from the airport and major circulation routes. A small neighborhood, commercial retail area (4.43 acres) at the northern end of the neighborhood is intended to serve residents of Neighborhood G as well as Neighborhoods C and H. Residential uses include: High Density Residential (20.60 acres), Medium High Density Residential (60.38 acres) and Medium Density Residential (94.20 acres). It is envisioned that this 213.01-acre neighborhood will be a retirement/second home community with a target of 943 dwelling units. Airport safety zones impacting the neighborhood dictate the location of medium density residential uses to the east of "C" Street and discourage the siting of public facilities within the neighborhood. Consequently, the neighborhood is served by public uses to the south in Neighborhood I, located south of Avenue 64. Open space totals 33.40 acres, functioning as part of the storm drainage system and enhanced by windrow plantings, provides a buffer from adjacent neighborhoods and off-site uses. A neighborhood park is located along the eastern edge of the neighborhood, serving Neighborhood H as well. A component of the project-wide trail system is integrated with a power line easement along the neighborhood's southern edge. This trail connects local parks to the north with public facilities to the south.

NEIGHBORHOOD G PLANNING STANDARDS

- (1) Portions of the residential and open space land uses in Neighborhood G are constrained by the OSZ, ERC and ETZ airport zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within Zones B1, C and D of the CLUP for the airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) "C" Street provides access to Planning Areas G-1, G-4, G-5, G-6, G-8, G-10, G-11 and G-13.
- (3) Avenue 62 provides access to Planning Area G-1, G-2, G-3 and G-6.
- (4) The interior east-west loop road provides access to Planning Areas G-7, G-8, G-9, G-10, G-11, G-12 and G-13.
- (5) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**, with the exception of the ETZ in which no trees shall be planted.
- (6) "C" Street will be developed in accordance with **Figure 3.4-13, Secondary Streetscape 1**.
- (7) The interior loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (8) Windrow plantings will be provided as a buffer between land uses and along drainage channels, and will be integrated into the trail system.
- (9) An intersection detail is planned for "C" Street and Avenue 62 to be developed in accordance with **Figure 3.4-5, Intersection Detail**.



LEGEND

- PROJECT AREA BOUNDARY
- SECONDARY STREETScape, 1 & 2, FORMAL COPY
- ■ ■ MAJOR STREETScape 1, PALM TREE
- ■ ■ EXPRESSWAY STREETScape, FORMAL CANOPY
- ■ ■ EXPRESSWAY STREETScape, PALM TREE
- ■ ■ COLLECTOR STREETScape 2, INFORMAL CANOPY
- WINDROW PLANTING
- - - PROJECT TRAIL
- ◆ RIGHT IN/RIGHT OUT ACCESS
- ▲ FULL ACCESS
- ☀ INTERSECTION DETAIL

SCALE: NTS

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
G-1	Commercial Retail (CR)		4.43	
G-2	Open Space (OS)		1.44	
G-3	Open Space (OS)		1.19	
G-4	Open Space (OS)		5.6	
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0
G-6	Open Space (OS)		13.65	
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0
G-9	Open Space (OS)		6.17	
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0
G-12	Open Space (OS)		5.35	
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0
TOTAL		943	213.01	

NEIGHBORHOOD G PLANNING AREAS**G-1: COMMERCIAL RETAIL**

Planning Area G-1 provides for the development of 4.43 acres of neighborhood commercial land uses.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-1 is located within Zone B1 of the CLUP for the Jacqueline Cochran Regional Airport and the OSZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-1 is accessed from Avenue 62 and "C" Street.
- (3) The edge treatment between the commercial and adjacent land uses is illustrated in **Figure 3.4-31, Commercial Buffer Streetscape**.
- (4) An intersection detail is located in the northwest corner of this planning area to be developed in accordance with **Figures 3.4-5, Intersection Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-2: OPEN SPACE

Planning Area G-2 provides for 1.44 acres of open space that will be developed as a local park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-2 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and ETZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-2 will be developed as a local park.
- (3) Planning Area G-2 is accessed from Avenue 62.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

G-3: OPEN SPACE

Planning Area G-3 provides of 1.19 acres of open space that will be developed as a local park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-3 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and ETZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-3 will be developed as a local park.
- (3) Planning Area G-3 is accessed from Avenue 62.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

G-4: OPEN SPACE

Planning Area G-4 provides for the development of 5.60 acres of open space that will be part of the drainage system for the site.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-4 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area G-4 is accessed from "C" Street.
- (3) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) This planning area will be developed with the cross-section shown in **Figure 3.4-22, Project Edge**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-5: MEDIUM DENSITY RESIDENTIAL

Planning Area G-5 provides for the development of 34.60 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 for cluster development as defined in Section 2 – Specific Plan Zoning Ordinance. A target of 69 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) The northern half of Planning Area G-5 is within Zone B1 of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. The southern half is located with Zone C of the CLUP. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-5 is accessed from "C" Street.
- (3) The edge treatment between the residential land use and the adjacent commercial uses to the north is illustrated in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) The edge treatment between the residential land use and the adjacent drainage channel is shown in **Figure 3.4-27, Channel Buffer Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-6: OPEN SPACE

Planning Area G-6 provides for the development of 13.65 acres of open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-6 is within the Jacqueline Cochran Regional Airport ETZ and ERC airport safety zones and Zone C of the CLUP. Development within these zones will comply with all applicable restrictions and requirements.
- (2) The northern most portion of the planning area (1.37 acres) will be developed as a local linear park.
- (3) The remainder of this planning area (12.28 acres) will be developed as a graded drainage channel, with a trail provided to connect with local parks to the north and to the south.
- (4) Planning Area G-6 is accessed from Avenue 62 and "C" Street.
- (5) This Planning Area will be developed with the channel cross-section described in **Figure 3.4-27, Channel Buffer Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

G-7: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area G-7 provides for the development of 60.38 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 432 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-7 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-7 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface between this planning area and the adjacent Evacuation Channel is shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway.**
- (4) The Project Edge Condition for this planning area adjacent to Tyler Street is shown in **Figure 3.4-22, Project Edge.**
- (5) The Project Edge Condition for this planning area and adjacent properties is shown in **Figure 3.4-29, out Parcel/Adjacent Property Buffer.**
- (6) Planning Area G-7 will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail.**
- (7) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway.**
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

G-8: HIGH DENSITY RESIDENTIAL

Planning Area G-8 provides for the development of 20.60 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional lots and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 282 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-8 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-8 is accessed from the interior east-west loop road.
- (3) Planning Area G-8 will be developed with a windrow/trail detail, as shown in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

G-9: OPEN SPACE

Planning Area G-9 provides of 6.17 acres of open space to be developed as part of the project drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area G-9 is accessed from the interior east-west loop road.
- (3) The project edge condition for this planning area is indicated in **Figure 3.4-22, Project Edge**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The edge treatment for the interface between this planning area and the adjacent Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

G-10: MEDIUM DENSITY RESIDENTIAL

Planning Area G-10 provides for the development of 18.57 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 under cluster development. A target of 37 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-10 is within the Jacqueline Cochran Regional Airport ERC and Zone C of the CLUP for the airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-10 is accessed from "C" Street and the interior east-west loop road.
- (3) The edge treatment for the interface between the residential uses and adjacent drainage facilities is illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (4) The trail along the eastern edge of the planning area will be developed as described in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-11: MEDIUM DENSITY RESIDENTIAL

Planning Area G-11 provides for the development of 26.86 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 70 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-11 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-11 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface with the adjacent park will be as described in **Figure 3.4-25, Park Buffers**.
- (4) Planning Area G-11 will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-12: OPEN SPACE

Planning Area G-12 provides for 5.35 acres of open space to be developed as a local park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-12 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport ERC and. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-12 will be developed as a local park.
- (3) Planning Area G-12 is accessed from the interior east-west loop road.
- (4) The park buffer between adjacent residential uses is illustrated in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-13: MEDIUM DENSITY RESIDENTIAL

Planning Area G-13 provides for the development of 14.17 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 53 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-13 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-13 is accessed from "C" Street and the interior east-west loop road.
- (3) The edge treatment for the interface with the adjacent Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

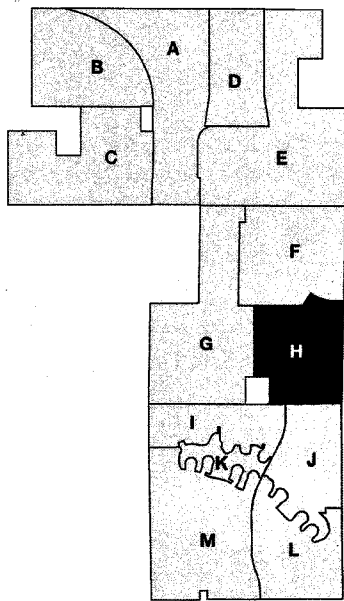
3.2.8 Neighborhood H

Neighborhood H, like Neighborhood G, is a residential neighborhood located in the central portion of the site as depicted in **Figure 3.2-9, Neighborhood H**. It is bounded by Polk Street on the east and the Avenue 64 Evacuation Channel to the south. The neighborhood is accessed from Polk Street and "C" Street. It is separated from the residential uses to the north and off-site uses to the east by a landscaped buffer. Residential uses include: High Density Residential (13.55 acres), Medium High Density Residential (69.87 acres) and Medium Density Residential (20.03 acres). A target of 710 dwelling units is planned in this 127.75-acre retirement/second home community. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

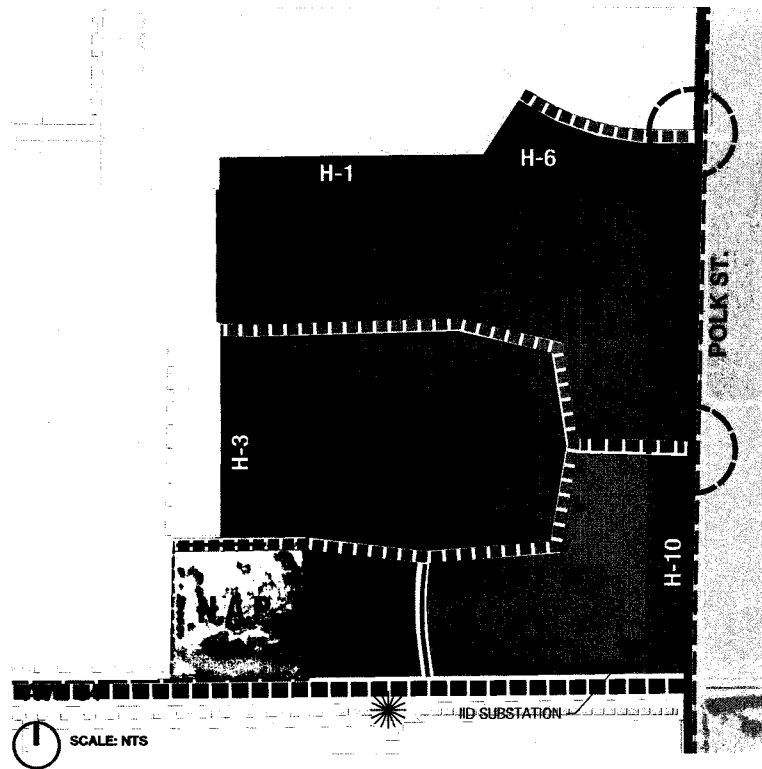
Open space totals 24.3 acres, functioning as part of the storm drainage system, and enhanced by windrow plantings, and provides a buffer from adjacent neighborhoods and off-site uses. A neighborhood park is located along the western edge of Neighborhood H, serving Neighborhood G as well. A component of the project-wide trail system is integrated with a power line easement along the neighborhood's southern edge. This trail connects local parks to the north with public facilities to the south.

NEIGHBORHOOD H PLANNING STANDARDS

- (1) The neighborhood is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) The interior loop road provides access to Planning Areas H-1, H-2, H-3, H-4, H-5, H-6, H-7, H-8, H-9 and H-10.
- (3) Polk Street provides access to Planning Areas H-6 and H-10.
- (4) The interior east-west loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (5) The interior north-south loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (6) The project entry from Polk Street will be developed in accordance with **Figure 3.4-16, Industrial Collector**.
- (7) A minor project entry is located at the intersection of Polk Street and the entry to the north-south loop road to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (8) Polk Street will be developed in accordance with **Figure 3.4-10, Arterial Streetscape 2 – Polk Street Streetscape 2B**.
- (9) Windrow plantings will be provided as a buffer between land uses and along drainage channels, and will be integrated into the trail system.



KEY MAP



LEGEND

- PROJECT AREA BOUNDARY
- ARTERIAL STREETScape 2, INFORMAL CANOPY
- |||| MAJOR STREETScape 1, PALM TREE
- ||||| COLLECTOR STREETScape 2, INFORMAL CANOPY
- ||||| COLLECTOR STREETScape 1, FORMAL CANOPY
- ||||| INDUSTRIAL COLLECTOR STREETScape, INFORMAL CANOPY
- WINDROW PLANTING
- PROJECT TRAIL
- ▲ FULL ACCESS
- ☼ INTERSECTION DETAIL
- SUBSTATION
- MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
H-1	Open Space (OS)		5.12	
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0
H-3	Open Space (OS)		3.79	
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0
H-6	Open Space (OS)		11.43	
H-7	Medium High Density Residential (MHDR)	161	20.99	5.0-8.0
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0
H-10	Open Space (OS)		3.95	
TOTAL		710	127.75	

NEIGHBORHOOD H PLANNING AREAS**H-1: OPEN SPACE**

Planning Area H-1 provides for the development of 5.12 acres of open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-1 is within Zone C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-1 is accessed from the interior east-west loop road, through Planning Areas H-2 and H-6.
- (3) This planning area will be developed with the channel cross-section illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

H-2: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area H-2 provides for the development of 18.65 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 118 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-2 is within Zones C and of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-2 is accessed from the interior east-west loop road.
- (3) The trail along the western edge of the planning area will be developed as described in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) The edge treatment for the interface between the planning area and adjacent drainage channel is illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

H-3: OPEN SPACE

Planning Area H-3 provides for 3.79 acres of open space to be developed as a local park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-3 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-3 will be developed as a local park.
- (3) Planning Area H-3 is accessed from the interior east-west loop road.
- (4) The edge treatment for this planning area will be as described in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

H-4: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area H-4 provides for the development of 22.40 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 141 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-4 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-4 is accessed from the interior east-west loop road.
- (3) The edge treatment for this interface between the planning area and the adjacent local park will be as illustrated in **Figure 3.4-25, Park Buffers**.
- (4) Planning Area H-4 will be developed with a buffer between the medium density residential uses in the planning area and the high density residential uses in the adjacent planning area as depicted in **Figure 3.4-32, Product Type Buffers**.
- (5) This planning area will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

H-5: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area H-5 provides for the development of 7.83 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 45 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-5 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-5 is accessed from the interior east-west and north-south loop roads.
- (3) The trail along the western edge of the planning area will be as shown in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) The edge treatment for the interface between the planning area residential use and the Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

H-6: OPEN SPACE

Planning Area H-6 provides for the development of 11.43 acres of open space which will be part of the project drainage network and trail system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-6 will be developed to incorporate an element of the project-wide trail system.
- (3) Planning Area H-6 is accessed from Polk Street and the interior east-west loop road.
- (4) This planning area will be developed with the cross-section shown in **Figure 3.4-10, Arterial Streetscape 2/Polk Street Streetscape 2B**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

H-7: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area H-7 provides for the development of 20.99 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 151 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-7 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface between the planning area and adjacent drainage channel is illustrated in **Figure 3.4-10, Arterial Streetscape 2/Polk Street Streetscape 2B**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

H-8: HIGH DENSITY RESIDENTIAL

Planning Area H-8 provides for the development of 13.55 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional lots and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 189 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-8 is accessed from the interior east-west loop road.
- (3) Planning Area H-8 will be developed with a buffer between the high density residential uses in the planning area, and the medium density residential uses in the adjacent planning area as depicted in **Figure 3.4-32, Product Type Buffer**.
- (4) This planning area will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.