

H-9: MEDIUM DENSITY RESIDENTIAL

Planning Area H-9 provides for the development of 20.03 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 66 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-9 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface with the planning area and the adjacent drainage channel is illustrated in **Figure 3.4-10, Arterial Streetscape 2/Polk Streetscape 2B**.
- (4) The edge treatment for the interface with the Evacuation Channel is shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

H-10: OPEN SPACE

Planning Area H-10 provides for the development of 3.96 acres of open space as part of the project drainage network and trail system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

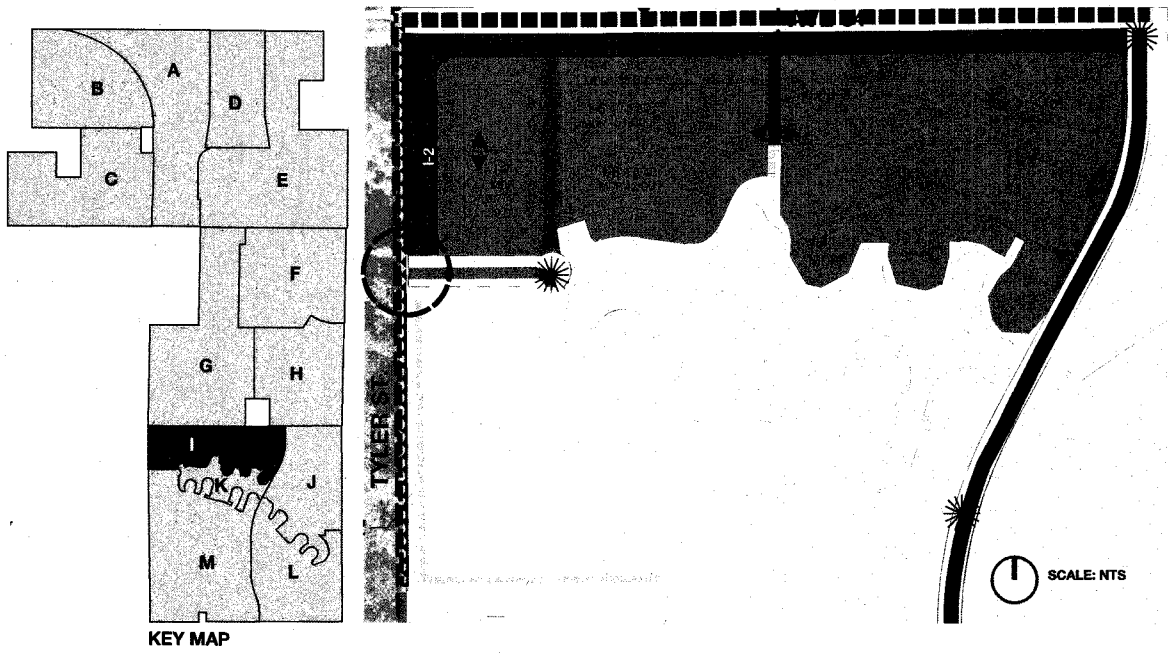
- (1) Planning Area H-10 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-10 is accessed from Polk Street and the interior east-west loop road.
- (3) The edge treatment between the planning area and adjacent residential uses is depicted in **Figure 3.4-10, Arterial Streetscape 2/Polk Streetscape 2B**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.10 Neighborhood I

Neighborhood I is a predominantly residential neighborhood located just south of the Avenue 64 Evacuation Channel, between Tyler and Polk Streets as depicted in **Figure 3.2-10, Neighborhood I**. This neighborhood contains eleven planning areas comprised of 89.9 acres and is the northernmost neighborhood outside of the airport Zone C. Proposed land uses include Medium Density Residential (75.9 acres) and Open Space to be developed as part of the project storm drainage system (14.0 acres). A target of 338 dwelling units is planned for Neighborhood I. The open space area serves as a land use buffer and part of the drainage network. The neighborhood is accessed from Tyler Street, "E" Street, "F" Street and Avenue 64. Within the Kohl Ranch Circulation Plan, "E" Street has been realigned to circulate north and south between Avenue 64 and Avenue 66, replacing its previously proposed curvilinear location south of the evacuation channel.

NEIGHBORHOOD I PLANNING STANDARDS

- (1) The neighborhood is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A Minor entry is located at the intersection of Tyler Street and "F" Street to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (3) Primary access to the neighborhood is provided from Tyler Street, "E" Street, "F" Street, Avenue 64 and the interior loop roads.
- (4) "E" Street provides access to Planning Area I-8, I-9, I-10 and I-11 through interior loop roads.
- (5) Avenue 64 provides access to Planning Areas I-1.
- (6) "F" Street provides access to Planning Areas I-2, I-3, I-4, I-5, I-6, I-7 and I-8 through interior loop roads.
- (7) Tyler Street provides access to Planning Area I-1 and I-2.
- (8) "E" Street and Avenue 64 will be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (9) Tyler Street will be developed in accordance with **Figure 3.4-14, Secondary Streetscape 2**.
- (10) The Evacuation Channel in Planning Area I-1 will be developed with the Canal Buffer depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (11) The intersection of Avenue 64 and "E" Street will be developed with the Intersection Detail as shown in **Figures 3.4-5, Intersection Detail**.
- (12) The east side of Tyler Street will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



- LEGEND**
- PROJECT AREA BOUNDARY
 - |||| MAJOR STREETSCAPE 1, PALM TREE
 - MAJOR STREETSCAPE 2, FORMAL CANOPY
 - MODIFIED SECONDARY STREETSCAPE, FORMAL CANOPY
 - COLLECTOR STREETSCAPE 1, FORMAL CANOPY
 - WINDROW PLANTING
 - CLASS I BIKE PATH
 - PROJECT TRAIL
 - ▲ FULL ACCESS
 - ☀ INTERSECTION DETAIL
 - MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
I-1	Open Space (OS)		9.6	
I-2	Open Space (OS)		3.9	
I-3	Open Space (OS)		0.5	
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0
I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0
TOTAL		338	89.90	

NEIGHBORHOOD I PLANNING AREAS**I-1: OPEN SPACE**

Planning Area I-1 provides for the development of 9.6 acres of open space that includes the Avenue 64 Evacuation Channel and land adjacent to the channel.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-1 is accessed from Avenue 64.
- (3) An Intersection Detail will be located at the northeast corner of the planning area as illustrated in **Figure 3.4-5, Intersection Detail**.
- (4) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Planning Area I-1 will be developed with a windrow/trail detail as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) A Class I Bike Path is located in this planning area on the east side of Tyler Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

I-2: OPEN SPACE

Planning Area I-2 provides for the development of 3.9 acres of open space to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from Tyler Street and "F" Street.
- (3) This planning area will be developed with the project edge treatment for the interface between the drainage facilities and project boundary, as shown in **Figure 3.4-22, Project Edge**.
- (4) A Minor Project Entry is located at the southwest corner of this planning area to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (5) A Class I Bike Path is located in this planning area on the east side of Tyler Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

I-3: OPEN SPACE

Planning Area I-3 provides for the development of 0.5 acres of open space that abuts the Avenue 64 Evacuation Channel to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-3 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-3 is accessed from "F" Street through interior loop roads.
- (3) This planning area will be developed with the drainage channel buffer for the interface with adjacent residential uses, as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) The drainage way in Planning Area I-3 will be designed to provide a pedestrian trail that connects with the project-wide trail system as depicted in **Figure 3.4-21 Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

I-4: MEDIUM DENSITY RESIDENTIAL

Planning Area I-4 provides for the development of 7.0 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 35 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-4 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from "F" Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

I-5: MEDIUM DENSITY RESIDENTIAL

Planning Area I-5 provides for 10.06 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development. A target of 51 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-5 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-5 is accessed from "F" Street through interior loop roads.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

I-6: MEDIUM DENSITY RESIDENTIAL

Planning Area I-6 provides for the development of 9.6 acres of medium density residential uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 42 dwelling units is planned within a density range 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-6 is accessed from "F" Street through the interior loop road.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

I-7: MEDIUM DENSITY RESIDENTIAL

Planning Area I-7 provides for the development of 6.7 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 29 dwelling units is planned within a density range 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-7 is accessed from "F" Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

I-8: MEDIUM DENSITY RESIDENTIAL

Planning Area I-8 provides for the development of 8.0 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 40 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-8 is accessed from "E" Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

I-9: MEDIUM DENSITY RESIDENTIAL

Planning Area I-9 provides for the development of 11.5 acres of medium density residential uses with minimum lot sizes of 5,000 square feet for conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 57 dwelling units is planned within a density range 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from "F" Street through interior loop roads.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

I-10: MEDIUM DENSITY RESIDENTIAL

Planning Area I-10 provides for the development of 12.4 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development. A target of 47 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-10 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-10 is accessed from "E" Street.
- (3) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

I-11: MEDIUM DENSITY RESIDENTIAL

Planning Area I-11 provides for the development of 10.1 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 37 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

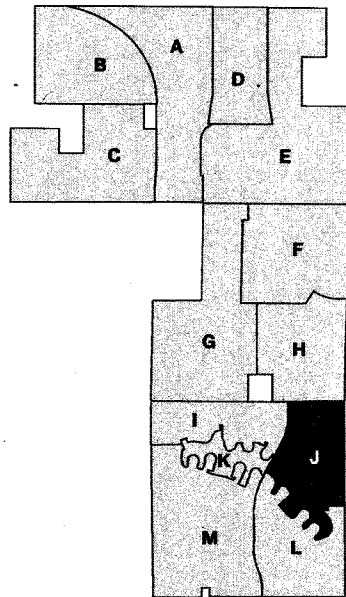
- (1) Planning Area I-11 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-11 is accessed from "E" Street.
- (3) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.11 Neighborhood J

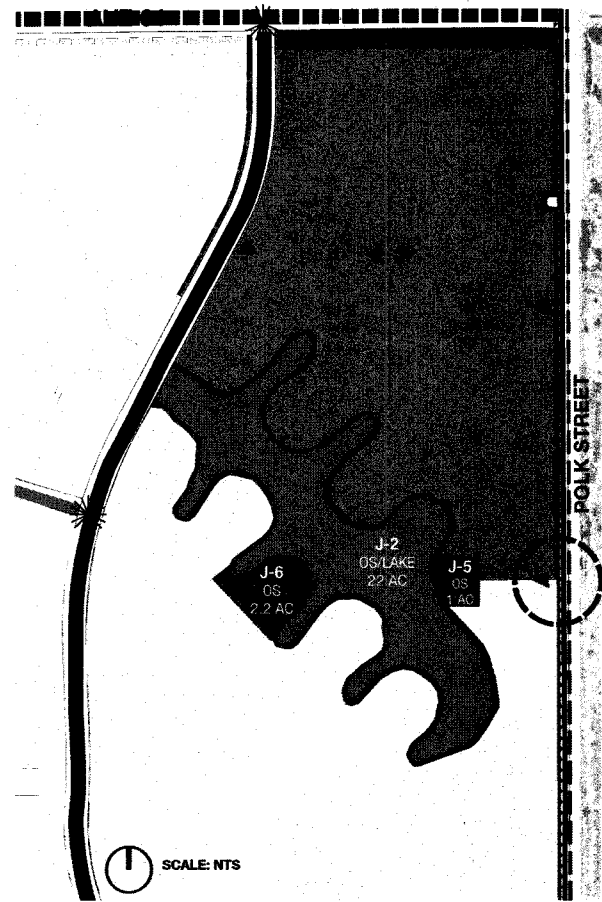
Neighborhood J is located in the southern section of the project site as depicted in **Figure 3.2-11, Neighborhood J**. The 119.49-acre neighborhood is located between "E" Street and Polk Street, just south of Avenue 64. It contains eight planning areas consisting of the following land uses: Medium Density Residential (80.88 acres) and Open Space (38.61 acres). A target of 319 units is planned for Neighborhood J. Access to the neighborhood is provided from "E" Street and Polk Street. The significant amount of open space within and adjacent to the neighborhood functions as part of the drainage system and provides a scenic amenity and buffer along Polk Street, as well as a local park. If developed with a golf course, a permitted use under provisions of Section 2 – Specific Plan Zoning, Neighborhood J would be equipped with a clubhouse which would provide meeting, dining and recreational opportunities to neighborhood residents.

NEIGHBORHOOD J PLANNING STANDARDS

- (1) A golf course use is permitted in all of the planning areas. A clubhouse is permitted in one of the planning areas. Additionally, recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted in all of the planning areas.
- (2) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (3) Primary access to Neighborhood J is from "E" Street, Avenue 64, Polk Street and the interior loop roads.
- (4) "E" Street provides access to Planning Areas J-1, J-6 and J-7.
- (5) Polk Street provides access to Planning Areas J-1 through J-8.
- (6) An Intersection Detail is located at the intersection of Avenue 64 and "E" Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (7) A Minor Entry is located at the intersection of Polk Street and the interior east-west loop road to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (8) "E" Street will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (9) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (10) Avenue 64 will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (11) The west side of Polk Street will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



KEY MAP



LEGEND

- PROJECT AREA BOUNDARY
- ▣ MAJOR STREETSCAPE 1, PALM TREE
- MAJOR STREETSCAPE 2, FORMAL CANOPY
- ▬ ARTERIAL STREETSCAPE 2, INFORMAL CANOPY
- ▬ COLLECTOR STREETSCAPE 1, FORMAL CANOPY
- ▬ WINDROW PLANTING
- ▬ CLASS I BIKE PATH
- PROJECT TRAIL
- ▲ FULL ACCESS
- ☼ INTERSECTION DETAIL
- MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
J-1	Medium Density Residential (MDR)	130	37.52	2.0-5.0
J-2	Open Space/Lake (OS-LAKE)	22	8.8	
J-3	Open Space/Lake (OS-LAKE)		8.8	
J-4	Medium Density Residential (MDR)	189	43.36	2.0-5.0
J-5	Open Space (OS)		1	
J-6	Open Space (OS)		2.2	
J-7	Open Space (OS)		3.3	
J-8	Open Space (OS)		1.3	
TOTAL		319	119.48	

NEIGHBORHOOD J PLANNING AREAS**J-1: MEDIUM DENSITY RESIDENTIAL**

Planning Area J-1 provides for 37.52 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 130 dwelling units is planned for this planning area within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-1 is accessed from "E" Street and Polk Street.
- (4) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway.**
- (5) Planning Area J-1 will be developed in accordance with **Figure 3.4-32, Product Type Buffer** between the medium density residential uses within the planning area and the high density residential uses in the adjacent planning area.
- (6) The edge treatment for the interface between the residential uses and adjacent drainage facilities is shown in **Figure 3.4-28, Airpark Drainage Buffer.**
- (7) A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities.**
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

J-2: OPEN SPACE

Planning Area J-2 provides for the development of 22.0 acres of open space to be developed as a lake and as part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-2 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) If the golf course or recreational lakes are not developed, this planning area will be developed with the edge treatment shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (4) Planning area J-2 is accessed from “E” Street.
- (5) The edge treatment for the interface between the drainage facilities and adjacent residential uses is shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

J-3: OPEN SPACE

Planning Area J-3 provides for the development of 8.8 acres of open space to be a landscaped perimeter to the lake, containing a walking trail for the use and enjoyment of project residents.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-3 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-3 is accessed from “E” Street and the interior loop roads. All planning areas abutting the lake will have access to the lake perimeter trail.
- (4) The edge treatment for the interface with the adjacent drainage facilities is shown in **Figure 3.4-23, Drainage Canal Edge Condition.**
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

J-4: MEDIUM DENSITY RESIDENTIAL

Planning Area J-4 provides for the development of 43.36 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 189 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-4 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course, including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-4 is accessed from Polk Street.
- (4) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-24, Golf Course (Racetrack) Edge Condition.**
- (5) A pedestrian trail is located along the eastern boundary of this planning area to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail.**
- (6) A minor project entry is located in the southeast corner of this planning area to be developed in accordance with **Figure 3.4-4, Minor Entry.**
- (7) If the golf course or recreational lakes are not developed, this planning area will be developed with the edge treatment shown in **Figure 3.4-23, Drainage Canal Edge Condition.**
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

J-5: OPEN SPACE

Planning Area J-5 provides for the development of 1.0 acres of open space to be developed as a local park or open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-5 is within Zone E of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-5 is accessed from Polk Street through interior loop roads.
- (4) The edge treatment for the interface between the park and adjacent residential uses is shown in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

J-6: OPEN SPACE

Planning Area J-6 provides for the development of 2.2 acres of open space to be developed as a park or open space that may include a recreation center.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-6 is accessed from "E" Street and Polk Street through interior loop roads.
- (4) The edge treatment for the interface between the park and adjacent residential land uses is illustrated in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

J-7: OPEN SPACE

Planning Area J-7 provides for the development of 3.3 acres of open space that includes the Avenue 64 Evacuation Channel and land adjacent to the channel.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning area J-7 is accessed from Avenue 64.
- (3) An Intersection Detail is located in the northwest corner of the planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) A pedestrian trail will be developed along the eastern boundary of the planning area to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (5) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

J-8: OPEN SPACE

Planning Area J-8 provides for the development of 1.31 acres of open space to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

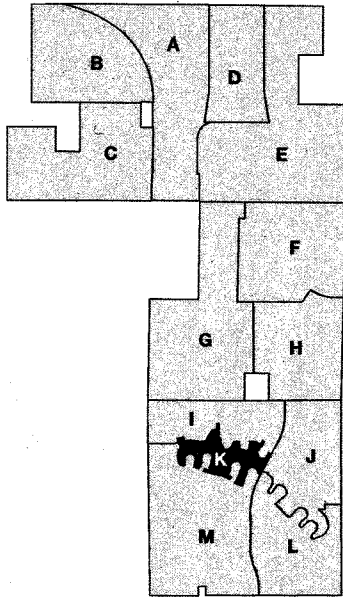
- (1) Planning Area J-8 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area J-8 is accessed from Polk Street.
- (3) The drainage way in Planning Area J-8 will be designed to provide a pedestrian trail that connects with the project-wide trail system as depicted in **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figures 3.1-10, Public Facilities**.
- (4) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land uses is depicted in **Figure, 3.4-23, Drainage Canal Edge Condition**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.12 Neighborhood K

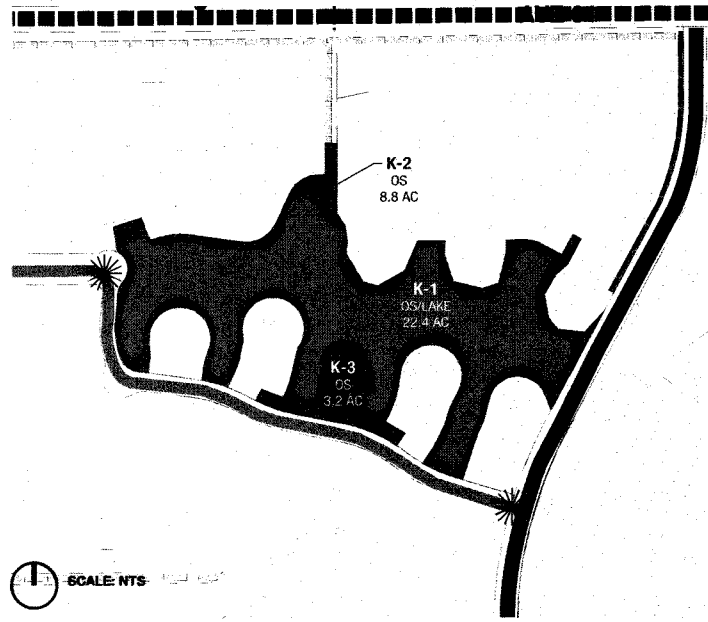
Neighborhood K is located in the southern portion of the project site as depicted in **Figure 3.2-12, Neighborhood K**. The neighborhood is 34.4 acres of open space with no dwelling units planned. The lake provides a scenic amenity, includes support of open space features such as a local park and recreation center, and functions as the dual water system (irrigation storage) and as part of the project drainage network. Primary access to the neighborhood is provided from "E" Street and "F" Street.

NEIGHBORHOOD K PLANNING STANDARDS

- (1) The neighborhood is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) is a permitted use in all of the planning areas.
- (3) Primary access to the neighborhood is provided by "E" Street and "F" Street as depicted in **Figure 3.3-1, Community Character**.
- (4) "E" Street provides access to Planning Area K-1 to K-3.
- (5) "F" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (6) "E" Street will be developed in accordance with **Figure 3.4-12, Major Streetscape 2**.









KEY MAP



SCALE: NTS

LEGEND

-  MAJOR STREETScape 1, PALM TREE
-  MAJOR STREETScape 2, FORMAL CANOPY
-  COLLECTOR STREETScape 1, FORMAL CANOPY
-  WINDROW PLANTING
-  PROJECT TRAIL
-  INTERSECTION DETAIL

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
K-1	Open Space/Lake (OS-LAKE)		22.4	
K-2	Open Space (OS)		8.8	
K-3	Open Space (OS)		3.2	
TOTAL		0	34.40	

NEIGHBORHOOD K PLANNING AREAS**K-1: Open Space**

Planning Area K-1 provides for the development of 22.4 acres of open space to be developed as a lake for the for the dual water system (irrigation storage), aesthetics and detention as part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area K-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-1 is accessed from "E" Street and "F" Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

K-2: Open Space

Planning Area K-2 provides for 8.8 acres of open space to be a landscaped perimeter to the lake, containing a walking trail for the use and enjoyment of project residents.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area K-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-2 is accessed from "E" Street and "F" Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

K-3: Open Space

Planning Area K-3 provides for the development of 3.2 acres of open space to be developed as a park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

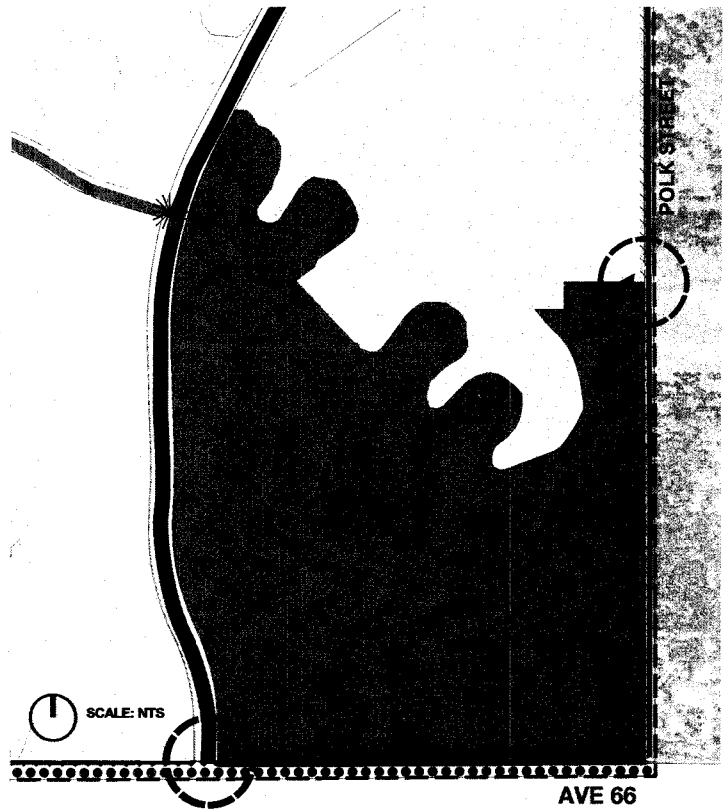
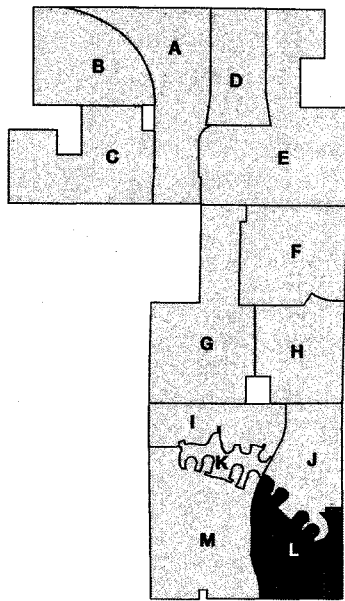
- (1) Planning Area K-3 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-3 is accessed from "F" Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.13 Neighborhood L

Neighborhood L is located in the southern portion of the project site as depicted in **Figure 3.2-13, Neighborhood L**. The 111.4-acre neighborhood is located in the southeast corner of the project site, and is bounded by and accessed from Polk Street on the east, Avenue 66 on the south and "E" Street on the west. Land uses consist of High Density Residential (106.2 acres) and open space (5.2 acres). A target of 1,158 dwelling units is planned for Neighborhood L. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Higher density residential uses are located at the project periphery, with lower density uses more interior to the site. If developed with a golf course, a permitted use under the provisions of Section 2 – Specific Plan Zoning, Neighborhood L would be equipped with a significant recreational and visual amenity.

NEIGHBORHOOD L PLANNING STANDARDS

- (1) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use and/or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) is a permitted use in all of the planning areas.
- (3) Primary access is taken from "E" Street, Avenue 66 and Polk Street.
- (4) "E" Street provides access to Planning Area L-1
- (5) Avenue 66 provides access to Planning Areas L-2.
- (6) Polk Street provides access to Planning Area L-3.
- (7) A Major Project Entry is located at the intersection of "E" Street and Avenue 66 to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (8) A Minor Entry is located at the intersection of Polk Street with the interior loop road to be developed in accordance with **Figure 3.4-4, Minor Project Entry**.
- (9) Avenue 66 will be developed in accordance with **Figure 3.4-7, Urban Arterial Streetscape**.
- (10) "E" Street will be developed in accordance with **Figure 3.4-12, Major Streetscape 2**.
- (11) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (12) The west side of Polk Street and the north side of Avenue 62 will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



LEGEND

- PROJECT AREA BOUNDARY
- MAJOR STREETSCAPE 2, FORMAL CANOPY
- COLLECTOR STREETSCAPE 1, FORMAL CANOPY
- ARTERIAL STREETSCAPE 2, INFORMAL CANOPY
- URBAN ARTERIAL STREETSCAPE, INFORMAL COPY
- WINDROW PLANTING
- CLASS I BIKE PATH
- ▲ FULL ACCESS
- ☀ INTERSECTION DETAIL
- MAJOR PROJECT ENTRY
- MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
L-1	High Density Residential (HDR)	1,158	106.2	8.0-14.0
L-2	Open Space (OS)		4.1	
L-3	Open Space (OS)		1.1	
TOTAL		1158	111.40	

NEIGHBORHOOD L PLANNING AREAS**L-1: HIGH DENSITY RESIDENTIAL**

Planning Area L-1 provides for the development of 106.2 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional development and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 1,158 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area L-1 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-1 is accessed from Avenue 66 and "E" Street.
- (4) The edge treatment for the interface between the residential land use and adjacent park use is shown in **Figure 3.4-25, Park Buffers**.
- (5) The edge treatment for the interface between the residential land uses and adjacent drainage facilities is shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

L-2: OPEN SPACE

Planning Area L-2 provides for the development of 4.1 acres of open space to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area L-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-2 is accessed from Avenue 66.
- (4) A major project entry is located in the southwest corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land use is depicted in **Figure 3.4-28, Airpark Drainage Buffer**.
- (6) The drainage way in Planning Area L-2 will be designed to provide a pedestrian trail that connects with the project-wide trail system in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the north side of Avenue 66 as illustrated in **Figure 3.1-10, Public Facilities**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

L-3: OPEN SPACE

Planning Area L-3 provides for the development of 1.1 acres of open space to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

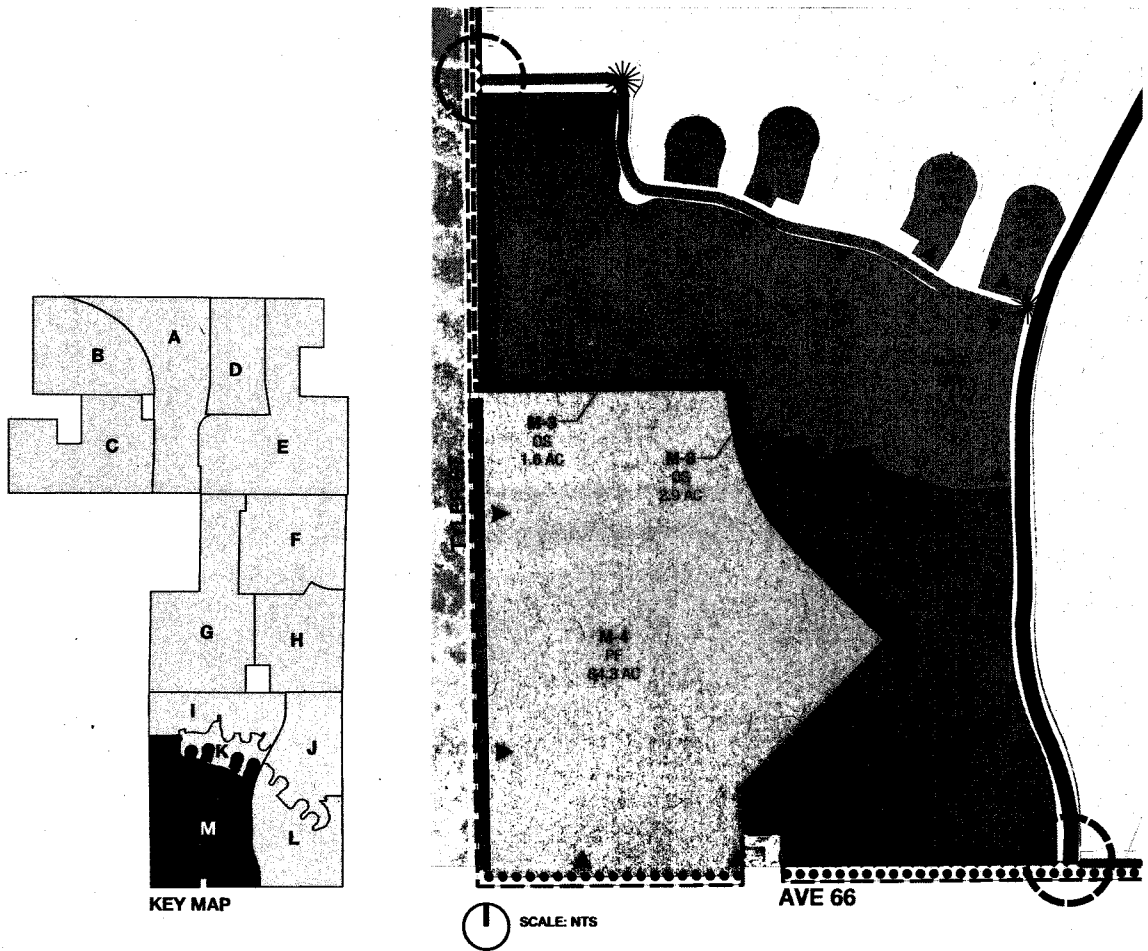
- (1) Planning Area L-3 is within Zone E of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-3 is accessed from Polk Street.
- (4) A major project entry is located in the southwest corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The drainage way in Planning Area L-3 will be designed to provide a pedestrian trail that connects with the project-wide trail system to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (6) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land uses is depicted in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.14 Neighborhood M

Neighborhood M is located in the southwest corner of the project site, north of Avenue 66 to "F" Street between Tyler Street and "E" Street as depicted in **Figure 3.2-14, Neighborhood M**. This 219.8-acre neighborhood contains Medium Density Residential (78.90 acres) and Medium High Density Residential (47.50 acres). A target of 555 dwelling units is planned for Neighborhood M. This neighborhood also includes 84.3 acres of Public Facilities/Schools. Open Space totals 9.1 acres and functions as part of the drainage network, provides a buffer along Tyler Street, and provides a buffer along the residential interface with the school site. Under a golf course scenario and recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) scenario, open space would provide active recreational benefits as well. Primary access to Neighborhood M is from Tyler Street, "E" Street, Avenue 66 and "F" Street.

NEIGHBORHOOD M PLANNING STANDARDS

- (1) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course and/or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in all of the planning areas.
- (3) Primary access is provided by Tyler Street, Avenue 66, "E" Street and "F" Street.
- (4) A Major Project Entry is located at the intersection of "E" Street and Avenue 66 to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) A Minor Project Entry is located at "F" Street and Tyler Street to be developed in accordance with **Figure 3.4-4, Minor Project Entry**.
- (6) "F" Street provides access to Planning Areas M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-6B and M-7A.
- (7) The intersection of "F" Street and "E" Street will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (8) "F" Street, just east of the Minor Project Entry on Tyler Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (9) "E" Street provides access to Planning Areas M-6B, M-7C, M-7D, M-7E and M-9.
- (10) "E" Street will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (11) "F" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (12) Avenue 66 will be developed in accordance with **Figure 3.4-7, Urban Arterial Streetscape**.
- (13) Tyler Street, south of "F" Street to the boundary of Planning Area M-2 will be developed in accordance with **Figure 3.4-15, Modified Secondary Streetscape 2**. Tyler Street, south of Planning Area M-2 to Avenue 66 will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (14) The north side of Avenue 62 will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



LEGEND

- PROJECT AREA BOUNDARY
- MAJOR STREETSCAPE 2, FORMAL CANOPY
- ▨ COLLECTOR STREETSCAPE 1, FORMAL CANOPY
- URBAN ARTERIAL STREETSCAPE, INFORMAL COPY
- MODIFIED SECONDARY STREETSCAPE, FORMAL CANOPY
- ▨ WINDROW PLANTING
- ▨ CLASS I BIKE PATH
- ▲ FULL ACCESS
- ☼ INTERSECTION DETAIL
- MAJOR PROJECT ENTRY
- MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0
M-2	Open Space (OS)		1.9	
M-3	Open Space (OS)		1.6	
M-4	Public Facility (PF)		84.3	
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0
M-7C	Medium High Density Residential (MDR)	32	5.9	5.0-8.0
M-7D	Medium High Density Residential (MDR)	82	15.1	5.0-8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
TOTAL		555	219.80	

NEIGHBORHOOD M PLANNING AREAS**M-1A: Medium Density Residential**

Planning Area M-1A provides for the development of 2.5 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 8 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-1A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-1A is accessed from “F” Street.
- (4) The edge treatment for the interface between the residential uses and the lake uses in the adjacent planning area is depicted in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

M-1B: Medium Density Residential

Planning Area M-1B provides for the development of 18.9 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development. A target of 68 dwelling units is planned within a density range 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-1B is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area M-1B is accessed from "F" Street.
- (3) The buffer between the Planning Area M-1B's residential uses and the adjacent lake facilities and will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

M-1C: Medium Density Residential

Planning Area M-1C provides for the development of 16.4 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 61 dwelling units is planned within a density range 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-1C is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area M-1C is accessed from "F" Street.
- (3) The buffer between the Planning Area M-1C's residential uses and the adjacent lake facilities and will be developed in accordance with **Figure 3.4-23, Drainage Canal Edge Condition**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-2: Open Space

Planning Area M-2 provides for the development of 1.9 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-2 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-2 is accessed from Tyler Street.
- (4) The northwest corner of this planning area will be developed in accordance with a **Figure 3.4-4, Minor Project Entry**.
- (5) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) The buffer between the Planning Area M-2's drainage facilities and the adjacent residential uses will be developed as shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

M-3: Open Space

Planning Area M-3 provides for the development of 1.60 acres of open space to contain part of the southern drainage channel.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-3 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-3 is accessed through the loop circulation system within Planning Area M-1B, from "F" Street.
- (4) This planning area will be developed with the edge conditions for the interface between drainage facilities and adjacent residential land uses, as shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (5) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

M-4: Public Facilities/Schools

Planning Area M-4 is an existing 84.30-acre public facilities/schools site owned by the Coachella Valley Unified School District housing the Las Palmitas Elementary School, Toro Canyon Middle School, and Desert Mirage High School.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-4 is accessed from Tyler Street and Avenue 66.
- (2) Planning Area M-4 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

M-5A: Medium Density Residential

Planning Area M-5A provides for the development of 4.1 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 14 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-5A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-5A is accessed from "F" Street.
- (3) This planning area will be developed in accordance with **Figure 3.4-25, Park Buffers** between planning areas residential and adjacent park uses.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-5B: Medium Density Residential

Planning Area M-5B provides for the development of 15.7 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 61 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-5B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-5B is accessed from "F" Street.
- (3) This planning area will be developed in accordance with **Figure 3.4-23, Drainage Canal Edge Condition** for the interface with adjacent drainage facilities.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-6A: Medium Density Residential

Planning Area M-6A provides for the development of 3.8 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 14 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-6A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-6A is accessed from "F" Street.
- (3) This planning area will be developed with in accordance with **Figure 3.4-25, Park Buffers** between residential land and adjacent park uses.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-6B: Medium Density Residential

Planning Area M-6B provides for the development of 13.4 acres of medium density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 57 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-6B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-6B is accessed from "E" Street and "F" Street.
- (3) The northeast corner of this planning area will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-7A: Medium Density Residential

Planning Area M-7A provides for the development of 4.1 acres of medium density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 20 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7A is accessed from "E" Street and "F" Street.
- (3) The southeast corner of this planning area will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-7B: Medium High Density Residential

Planning Area M-7B provides for the development of 11.0 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 56 dwelling units is planned within a density range of 5.0–8.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7B is accessed from "E" Street through interior loop roads.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-7C: Medium High Density Residential

Planning Area M-7C provides for the development of 5.9 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 32 dwelling units is planned within a density range of 5.0–8.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7C is within Zone E of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7C is accessed from "E" Street.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

M-7D: Medium High Density Residential

Planning Area M-7D provides for the development of 15.1 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 82 dwelling units is planned within a density range of 5.0–8.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7D is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7D is accessed from "E" Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-7E: Medium High Density Residential

Planning Area M-7E provides for the development of 15.5 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 82 dwelling units is planned within a density range of 5.0–8.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7E is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7E is accessed from "E" Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-8: Open Space

Planning Area M-8 provides for the development of 2.9 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-8 is accessed from the loop circulation road within Planning Area M-5B and from the loop circulation road within Planning Area M-7B via "E" Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-9: Open Space

Planning Area M-9 provides for the development of 2.7 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-9 is accessed from the intersection of Avenue 66 and "E" Street.
- (3) This planning area will be developed with the edge conditions for the interface with the drainage facilities and adjacent residential land use, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) A Major Project Entry is located in the southeast corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The drainage way in Planning Area M-9 will be designed to provide a pedestrian trail that connects with the project-wide trail system to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the north side of Avenue 66 as illustrated in **Figures 3.1-10, Public Facilities**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

3.3 DESIGN GUIDELINES

3.3.1 Purpose and Intent

The following Design Guidelines have been developed as a method of achieving a high-quality, cohesive design fabric for the Kohl Ranch Specific Plan area. The objectives of these guidelines are:

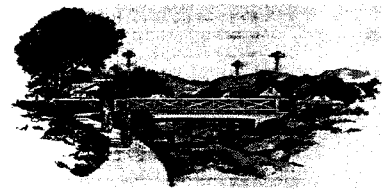
- to provide the County of Riverside with the necessary assurance that the Specific Plan area will develop in accordance with the quality and character proposed herein;
- to serve as design criteria for developers, builders, engineers, architects and landscape architects, and other professionals in preparing plans for construction; and
- to lend guidance to staff, the Joint Powers Authority and the Board of Supervisors in the review and evaluation of future development projects in the Specific Plan area.

Certain key design elements will contribute significantly to the visual order and consistency of the entire Specific Plan area. These common features—site planning, architecture, landscape architecture, signage, lighting and other details—are the subject of the Design Guidelines. The guidelines express the desired character of future development, which is represented conceptually in **Figure 3.3-1, Community Character**. Each guideline will be considered in terms of how it relates to a given development proposal, during the review process, and will be applied to the selected land use concept, as appropriate.

The Design Guidelines are intended to be flexible and are illustrative in nature. Therefore, over time they can respond to unanticipated conditions, such as changes in taste, community desires and the marketplace. Fundamental principles that are the foundation of the Design Guidelines include the coordinated use of land for passive and active recreational open space, to handle storm drainage and to comply with restrictions of the Jacqueline Cochran Regional Airport Safety Zones. In addition, use of landscaping materials and architectural treatments should be compatible with the desert landscape as well as the rich agricultural tradition of the Coachella Valley.



TRAIL ALONG EDGE



TRAIL



GOLF COURSE EDGE



PARK



COMMERCIAL



STREETSCAPE

Compliance with County Design Guidelines

The Design Guidelines and Development Standards for the Kohl Ranch Specific Plan are intended to establish community guidelines and standards for the project to ensure the creation of a quality and aesthetically pleasing environment. Section 3.4, Landscape Guidelines, contains the following subsections: Landscape Concept, Entries, Intersections, Streetscapes, Edges, Buffers, Landscape Palette, Paths and Trails, Walls and Fences, Lighting and Signage. Section C.4., Site Development Guidelines, contains the following subsections: Site Planning and Architectural Guidelines.

The Kohl Ranch Design Guidelines and Development Standards were developed prior to adoption of the Countywide Design Standards and Guidelines criteria (adopted January 13, 2004). Nonetheless, the adopted standards support the development of neighborhoods within the Specific Plan which will meet or exceed the intent and standards contained within the County-wide document. The Kohl Ranch Design Guidelines and Development Standards are consistent with the intent of the Design Strategies and Planning Objectives contained in the Countywide Design Standards and Guidelines (Sections I, Introduction, II, Residential, III, Residential Street Design, and IV, Landscape Design Standards). The Kohl Ranch Specific Plan complies with the standards, goals and objectives by, "*creating quality and visually distinctive development responsive to the natural and built environment of Riverside County*" (Countywide document, page 6).

Prior to approval of tentative tract maps, the Planning Commission and the Board of Supervisors may seek more site-specific information about whether or not the Countywide Design Standards and Guidelines will be met. Standards and features of design implementation illustrating specifically how these general guidelines will be implemented may help guide decision makers during public hearings. Therefore, a project proponent shall submit a design manual to the Planning Department for review and approval prior to public hearings on individual tentative tract maps. Each design manual shall be keyed to the Specific Plan planning area within the project site, and each shall identify how and where particular design features of note will be realized within the development.

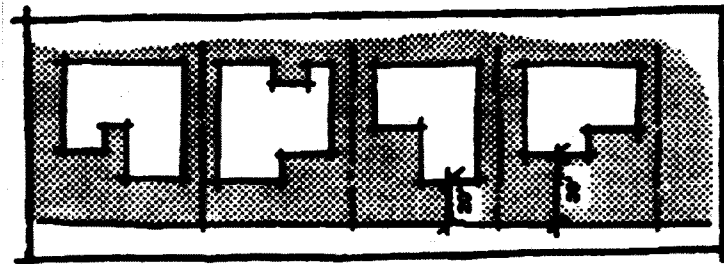
3.3.2 Site Development Guidelines

Site Planning

The intent of site planning is to integrate buildings and site improvements into a unified setting with minimal impact of the development on adjacent land uses. Site planning concentrates on the proper placement of buildings, roads and services.

The goal of residential site planning is to create a neighborhood fabric that offers a unique, safe and visually appealing environment to those in and around the development. The site plan should:

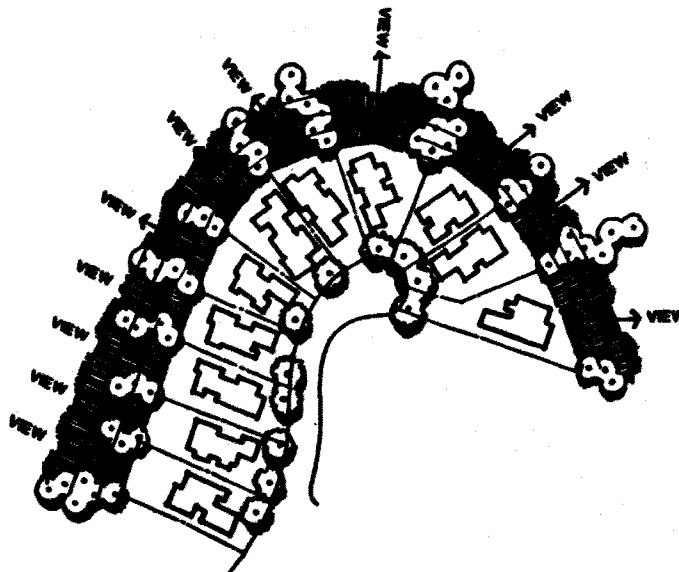
- Effectively accommodate the residential unit types proposed.
- Maximize unit exposure to parks, greenbelts and other amenities.
- Use of geometric "grid" layouts should be allowed.
- Cul-de-sac and curvilinear street layouts should be encouraged.
- Employ the latest techniques of energy-efficient/cost-effective subdivision design.
- Accommodate street drainage or underground drainage in accordance with generally accepted principals.
- Subdivision layouts should discourage through traffic while still permitting adequate emergency vehicle access.
- Variations of the building footprint with cut-outs and pop-outs can assure variety in a plan. Further variety and interest can be developed with plans by varying setback dimensions and positions of buildings.



Varying Setback Dimensions

Single-Family Detached Residential Site Planning

- To the extent possible, units should have views from the rear of the units toward special features, including golf course and water features. Views should be enhanced with landscaping which does not block the view of adjacent lots.
- Single-family detached residential uses shall be buffered from existing adjacent land uses, and high traffic arterials. Landscape design should be used to provide privacy between units.
- A variety of building pad configurations can create further interest.
- Provide for variety along the streetscape.
- Orient views of the units towards special features, distant mountains, or common area open space. Views should be enhanced with tree plantings framing the desired view, but shall not block the view of adjacent lots.



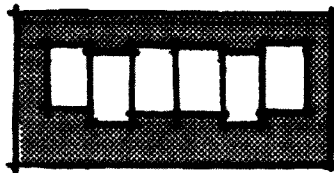
Views from Residential Units

Multi-Family Residential Site Planning

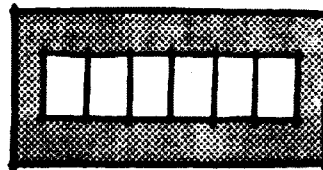


Multi-Family Housing Facades

- Multi-family housing facades shall be detailed to give a varied quality to the building exterior rather than one long, continuous building. Attached buildings shall not be more than six dwelling units per floor.

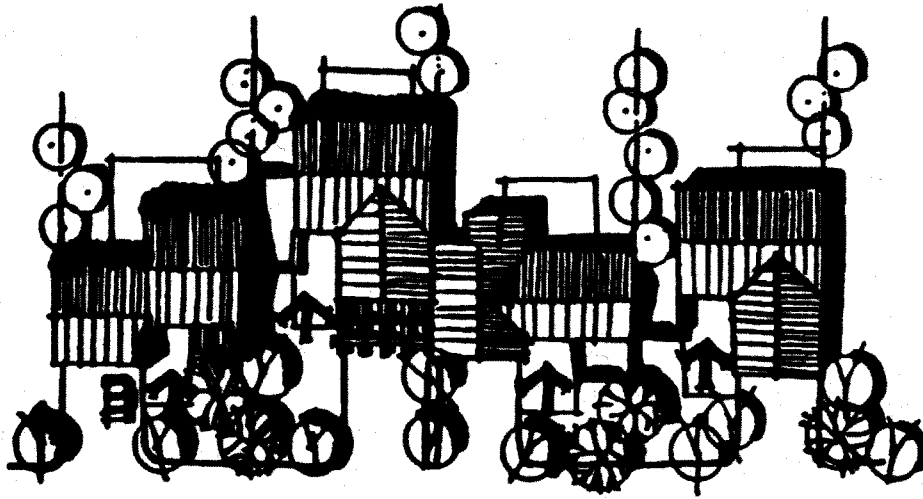


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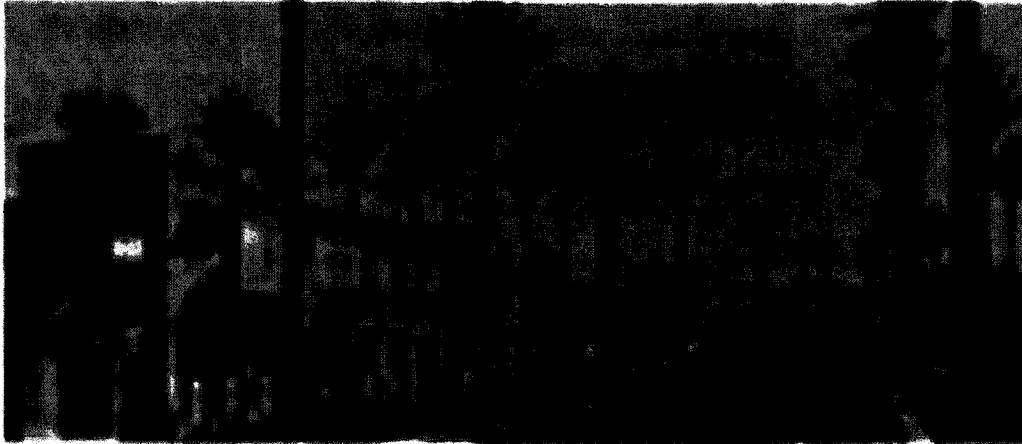
- In higher density areas, where lot sizes permit, individual units shall be oriented in a variety of ways to avoid the monotony of garage door corridors.
- Maximization of open space is an important element within a higher density project. Curving the streets and orienting road axis to open areas and views can attain the feeling of open space. Open parking areas can be treated as landscaped plazas and courts.
- In multi-family and attached housing, it is important to provide each unit with its own entry and identity. This can be accomplished by offsetting and staggering each separate unit and by combining one and two-story building forms to separate massing. This also will provide variety to the streetscape.



Streetscape Variety

Non-Residential Site Planning

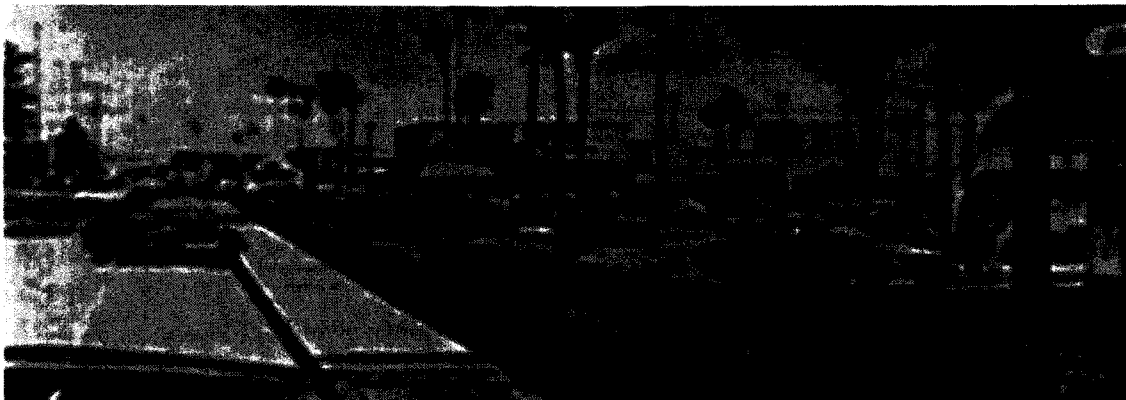
Non-residential areas will serve the project site as well as the regional community. Businesses which locate here will rely on a high degree of visibility.



Mixed Use/Air Park

Strong Linkages

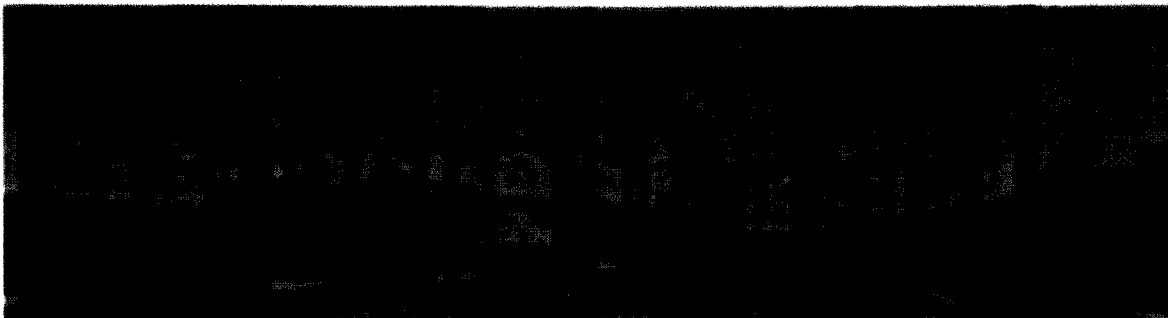
- Links within commercial developments shall be reinforced by building and landscape elements. In the case of landscape elements, these links may be made with both hardscape (such as paving), or softscape.
- Although the area is to be auto-oriented with regard to attracting those shoppers from the local community, the site planning shall emphasize pedestrian, and bicycle links to minimize auto trips from within the Kohl Ranch. Interaction of residential, park and commercial areas is encouraged.
- In some instances objectionable elements may require visual screens and sound barriers. Otherwise, these treatments should be minimized and used only when necessary.



Avenue 66 Commercial

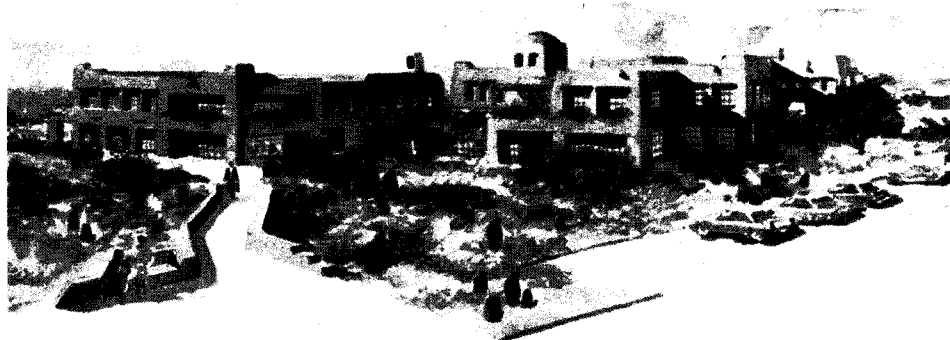
Orientation of Structures

- Buildings shall be arranged so that most store fronts are oriented toward and visible from the street and parking areas. The shape and configuration of the buildings will be determined by the site constraints. On corners or at major developments, satellite buildings shall be located closer to the street or at the corner, while the bulk of the building is placed far enough back from the street to allow circulation across the front of the parcels.
- Developments that are inward facing and that preclude through access are not encouraged, except when major attractions are destinations.
- Courtyards and outdoor seating areas are encouraged, and other ancillary structures, service areas, and trash enclosures shall be planned as part of the project, and placed or screened in a position of low prominence.
-



Commercial

- Landscaping shall be used to break up otherwise uninterrupted building mass, frame views, and connect with development on adjacent pads.
- Building placement along a street should be varied to avoid creating a harsh, monotonous blockface or streetscape. Buildings should be sited so their entrances are generally oriented toward the street and parking areas.



Office

Mixed Use Site Planning

Mixed Use planning areas are established to encourage an innovative array of uses to include a combination of commercial, business, industrial, recreational, and residential uses.

- Mixed-use developments contain different building types. A variety of building types are permitted; commercial, business, industrial, race related facilities, residential, and duplex units.
- Where residential uses are mixed with nonresidential uses, the residential use should be located either above the nonresidential component or adjacent to the nonresidential component, as allowed by Section 2 – Zoning Ordinance.
- Proper building massing may be achieved through the use of sufficient vertical, horizontal and roof articulation of a building. Combinations of one and two story elements on the same building are encouraged to facilitate articulation.
- Consistent with the architectural style of the development, street facing facades should incorporate articulation and mix of color and materials to create diversity in the streetscape.
- Buildings within a development shall use colors, materials and architectural details that are compatible among buildings within the same development.

Climatic Conditions

The effects of seasonal wind on development at the Kohl Ranch will, in some instances, be extensive. Wind may be intercepted, diverted or lessened. The following are suggested techniques of wind management:

Shelter Belts on a Community Scale

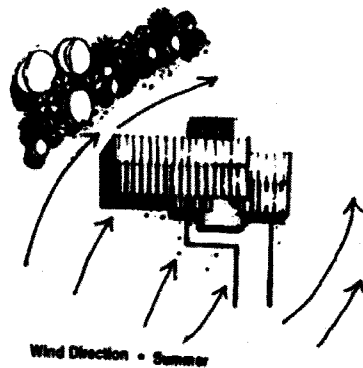
Shelter belts may be used in parallel rows to produce shelter on the leeward side. Belts with pinched profiles are less efficient in halting winds than belts with vertical edges.

Solid Barriers

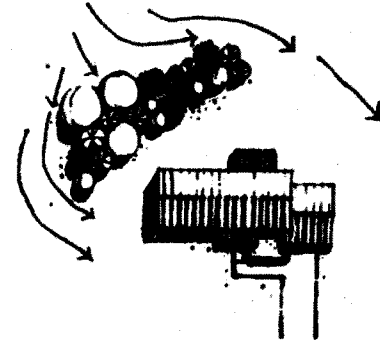
Fences and walls are effective for protection but are very hard edges and should be limited in use. Barriers penetrated by the wind create more effective and wider wind shadows.

Wind Breaks for Residential Use

Wind breaks placed on residential property can route winter wind around the home and allow cooling breezes during the summer to penetrate the rear yard.



Wind Direction - Summer



Wind Direction - Winter

Wind Breaks

3.3.3 Architectural Guidelines

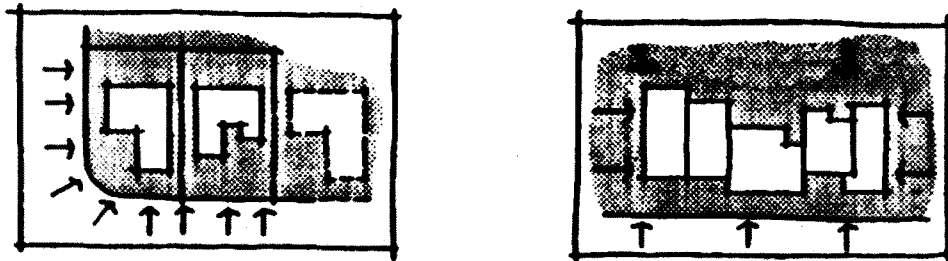
The architectural guidelines for the Kohl Ranch are intended to produce a feeling of authenticity without creating rigid, over-specific product/style requirements which often lead to architectural blandness and homogeneity. Rather, the Kohl Ranch should encourage a program of innovative design that reflects the historical desert landscape. The development programs should apply the guidelines in either literal or abstract forms.

These architectural guidelines are intended to establish design standards and form an encyclopedia of alternative design concepts.

Elevation Concepts

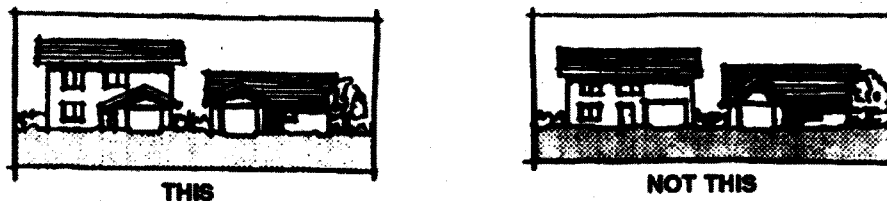
- These guidelines are primarily concerned with the physical appearance of the community environment as perceived by residents and visitors. Therefore, it is the intent of these guidelines to apply to the portion of a building that is visible to the public.
- Primary elevations are the surfaces of a building structure or yard that can be seen from public view (either vehicular or pedestrian). In a single-family project it usually means the front and/or exterior side yard views. In multi-family projects it may be all four elevations of a structure.

Primary Elevations



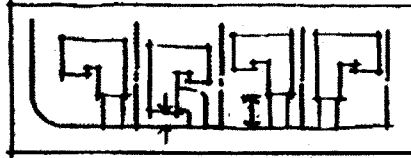
- Each building shall relate in terms of bulk and mass but should not be identical. A two-story building is more harmonious to a neighboring one-story building if it contains a one-story element.

Harmony of Elevations



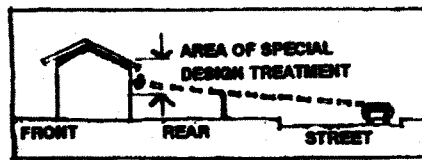
- Avoid stereotyped units which will produce monotonous elevations and street scene.
- A variety of elevations utilizing differing setbacks such as swing-in garages and reverse plans will ensure a varied street scene.

Variety of Elevations



- Where applicable, the rear side of the building located adjacent to streets and other areas of high visibility should have similar treatment as with the primary elevation criteria. If a fence is used as a screening device, then it too should be designed to be part of the architecture.

Special Treatment Area



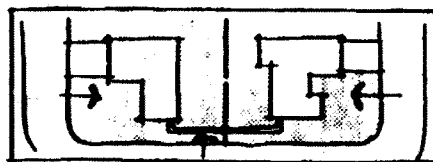
- Corner Elevations - Place the short side elevation on the corner to reduce the feeling of height and mass at the corner. Auto entry on a corner lot is also an important consideration. The high side can be placed on the corner as long as the side elevation is treated as a primary elevation.

Corner Elevations



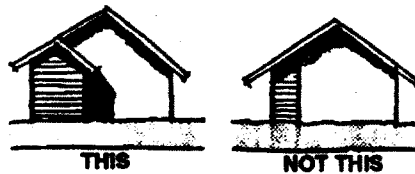
- Elevations and Side Streets - Fences or walls that connect two separate units should be of the same material and color and be compatible with the architecture of the buildings.

Elevations and Side Streets



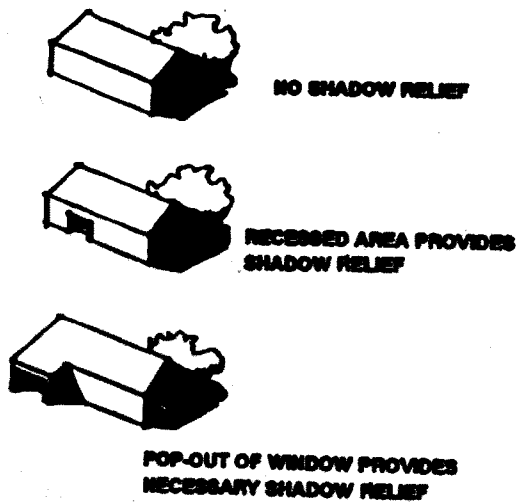
- Facades - If material changes do not occur in the same plane and if they intercept an architectural element, they are more effective.

Facades



- Allow for visual relief for interest and to break up long planar surfaces. Offsets, pop-outs, overhangs, and recesses all may be used to produce effective shadow interest areas. Larger buildings require more relief than do smaller buildings.

Shadow Interest Areas

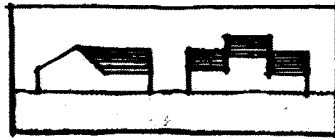


- It is desirable to accompany a plan offset with a change in the roof pitch orientation. This is important on the primary elevations.

Roof Pitch



THIS



NOT THIS

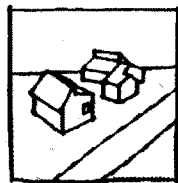
- The design theme should be continued throughout the project, including accessory items such as lighting fixtures and address plaques.

Design Theme

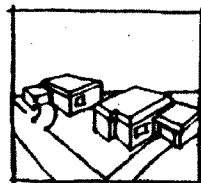


- Roof Design - Flat roof design for residential structures should be discouraged. If used, flat roofs should be a secondary design element only and should be contained within the scheme of an individual building design. Particular consideration as to color and material should be given to the design and treatment of roofs because of their visual impact.

Roof Design



THIS

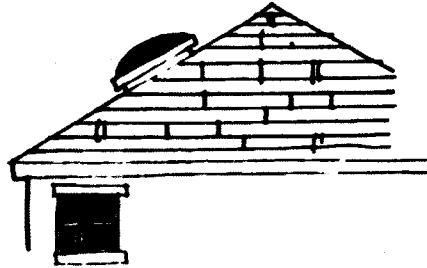


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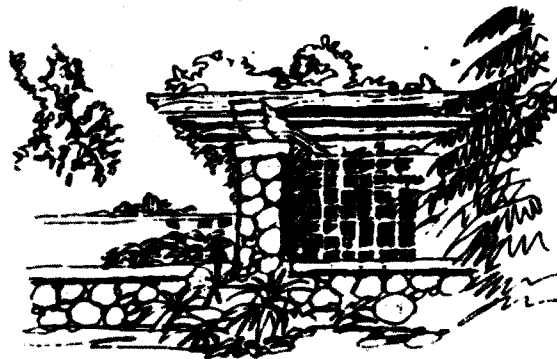
Details

There are many building elements that come into view. Usually they are secondary adjuncts to major element, such as a skylight on a roof. In some cases they become no more than a texture, such as a trellis. It is important in all cases, however, that these elements integrate with the whole. What may start out as a minor feature can become a distracting eyesore, if not thought through.

- Skylights should be integrated into the roof form.

Skylights

- Trellis/Pergolas can be very pleasing and functional structures, (i.e., carport, entry canopy). They provide partial shade, screens for privacy or an arbor for climbing plants. They offer the warmth and texture of wood at a relatively low cost. It is important to consider a trellis as a permanent structure and design it accordingly. Avoid corrugated fiberglass metal, or other temporary type material. Use adequately heavy members for the support structure working down to no less than 2" nominal for the smallest dimension of the lighter members. The trellis will look more substantial and will not bow from old age prematurely.

Trellis/Pergola

- Chimneys are usually very strong roof elements. Codes require that they extend higher than adjacent roof lines. Careful choice of proportion and material should give them a substantial and stable appearance.

- The use of prefab fireplaces and flues is economically sound. However, they need not have the stove-pipe appearance, depending of which spark arrester is used or required by fire code. Enclosing the flue in a masonry or wood chimney with substantial proportions will solve the problem.

Materials and Finish

In keeping with the goals of the Kohl Ranch to maintain and preserve the natural surroundings, designs in harmony with nature should be extended to material, finish and color selection.

- Natural materials by their nature inherently work well with the surroundings. Natural colors or transparent finishes on these materials not only enhance them, but actually improve with age and are low maintenance. Man-made materials of natural colors and textures are also suitable.
- Brick, natural tile, concrete, steel and glass, if used honestly and in a straight-forward manner, can be pleasant. Large expanses of a single material, especially if unbroken by detail or depth, can become overpowering to the rest of the building form and the surroundings. Conversely, over-detailing with use of too many different materials or textures can create confusion and distract from an otherwise good design.

3.3.4 Lighting

Lighting should be designed to ensure compatibility with the community architectural and street furniture themes, and the safety of the site users. An effort should be made to emphasize "human scale" in public areas adjacent to buildings and along walks. Streetlight standards, traffic signal poles, and pedestrian and building lighting play a vital role in defining a safe and secure appearance. This lighting concept has been devised to provide a hierarchy of lighting effects which will contribute to the overall cohesiveness of the community image.

General Guidelines

- Warm white lighting is encouraged. Bright colored or blinking lights are not encouraged except in theme restaurants and shops of commercial development areas.
- Building or roof outline tube lighting shall be subject to County of Riverside approval.

- Design and placement of site lighting should minimize glare affecting adjacent properties, buildings, and roadways.
- Careful consideration and coordination shall be given to avoid any potential conflicts with Jacqueline Cochran Regional Airport operations.
- Lighting shall be designed to minimize sky glow and effects on the Mt. Palomar Observatory and the nighttime desert sky.
- Fixtures and standards shall conform to state and local safety and illumination requirements. In particular, lighting shall conform to Riverside County Ordinance No. 655, which includes requirements related to the Mt. Palomar Observatory.
- Automatic timers on lighting should be designed to maximize personal safety during nighttime use while saving energy.

Roadway Lighting

Lighting fixtures and standards within the public right-of-way play a crucial role in displaying a level of quality for the development.

- Lighting shall be positioned to enhance the safety of vehicular and pedestrian flows at key points along the roadway. Light shall be concentrated at intersections and pedestrian crosswalks. Intersections may have increased wattage for definition and to alleviate automobile/pedestrian conflicts.
- The maximum height of roadway lighting should be twenty-five (25) feet with a minimum clearance of sixteen (16) feet above roadways.

Parking Area Lighting

Since landscaped islands within parking areas are adjacent to high use commercial, office, and recreation buildings, they typically may be hidden from view by landscaping and trees, therefore requiring a less overt lighting standard. Conversely, they may be highly refined and emphasized as a design element within commercial areas where visibility to buildings is crucial.

- Stone or concrete, limited to twenty-four (24) inches in height, may be used for light standard bases in parking area islands to protect lighting from automobile damage.

Pedestrian and Entry Area Lighting

Pedestrian and entry area lighting should be provided at plaza areas, improved trails, entryways, courtyards, parking lots and other public spaces requiring night illumination.

- For all uses permitted within the Kohl Ranch, where above ground illumination is desired, a lighting fixture and standard compatible with the community theme is recommended.
- In pedestrian areas the use of low level walkway lighting is preferred over lighting from above. Lights shall be positioned no higher than four feet above grade.
- Concrete or metal light bollards not exceeding four feet in height may be used to accentuate building entrance areas or to provide separation for pedestrians from vehicular traffic lanes.
- For pedestrian walkways, point-to-point lighting is acceptable with no specific illumination levels required. The main emphasis shall be to clearly identify the pedestrian walkway and direction of travel. These lamp elements may become an important design element on the landscape, or they may be sublime and unobtrusive.
- Fixtures which shield and limit spillover light into the night sky are recommended.

Architectural Lighting

- To highlight monument signs or architectural features such as walls, entry ways or lobbies, dramatic lighting should be used. Spillover lighting from lobby areas is acceptable although glare onto adjacent properties should not occur. Service area lighting shall be contained within service yard boundaries, with light sources concealed.
- Building illumination and architectural lighting shall be creative and must reinforce the design theme. Indirect wall lighting or "wall washing", overhead down lighting, or interior illumination which spills outside is encouraged. Wall "washing" effects shall be sparingly used to accentuate architecture at key visual areas and not an attempt to advertise buildings to roadways.
- Lighting shall not cast any glare onto adjacent lots and streets in such a manner as to decrease the safety of pedestrian and vehicular movement.
- Architectural lighting shall be used to articulate the particular building design. Rim lighting of eaves, bending, uplighting, wall washing, and other effects shall be used in compliance with the design theme for the planning area. Architectural lighting shall be integrated with building elements and concealed flush with grade wherever possible.
- Building or wall lighting shall be indirect. A limited number of lights may be used to create shadow, relief and outline effects.
- Lighting shall be used to enhance landscaping and reinforce architecture, with dramatic uplighting or wall shadow effects with plant materials encouraged.
- Parking lot light fixtures and bollards shall be consistent in styling with the design theme proposed for that subarea.
- Light standards shall not exceed 20 feet in height or the height of the building, whichever is less.
- All architectural lighting should be carefully integrated into design themes and should not be visually obtrusive and incongruent during the daytime.

Landscape Lighting

- Landscape lighting can be used to highlight landscape features such as specimen trees and pedestrian areas. As with architectural lighting, light sources should be designed as an integral design element, concealed flush with grade, or hidden by vegetation during the daytime.
- String lights, with small nonflashing bulbs, may be used to highlight trees and similar features within the golf course clubhouse, commercial areas in interior courts only, and other similar outdoor areas at night to create excitement and a festive ambiance.

Athletic Field, Court and Driving Range Lighting

For proper lighting of athletic fields, driving ranges and courts in recreation facilities of parks and schools, spillover lighting may occur. Lighting shall be designed to minimize the spillover effect. These areas shall be carefully site planned to impact as few neighbors as possible. Hours of operation shall be established which restrict intrusion of nighttime lighting, and posted at visible locations at these recreations areas.

3.3.5 Signage

General Guidelines

- All signs within the project should be designed to provide a consistent reinforcement of the Kohl Ranch character. Signs must communicate not only specific information but should, in themselves, add to the attractiveness of the area.
- Signs in the project area should advertise a place of business, a residential area or provide directions and information.
- Signs should not compete with each other or dominate the setting via inconsistent height, size, shape, number, color, lighting or movement.
- Signs shall conform to the guidelines set forth herein and with Riverside County Ordinances.
- No signs shall be placed upon or project into or above public property or the public right-of-way, except as noted herein.
- Signage typeface should be consistent with building architectural style.
- Public signs should be limited to those absolutely necessary for smooth traffic flow, safety, and public information.
- Clear sight triangles should be observed and enforced at all intersections, corners, parking areas, and drives at all times.
- All signage should be maintained in good repair, including the display surface, which shall be kept neatly painted or finished.
- The exposed backs of all signs visible to the public should be suitably finished and maintained.
- All signage should be designed free of bracing, angle-iron, guy wires, cables, or similar devices, except for commercial advertising signs as hereafter noted.
- Painted signs should use fade-resistant, durable, exterior finish paint. No luminous, fluorescent or iridescent paints or plastics are permitted. Permanent wooden signs or materials that may be degraded by the climate should be avoided unless adequately sheltered.
- An effort shall be made to achieve consistency between building style and sign design. In all cases, signage should be complimentary to the exterior treatment of the building or location involved.
- Color schemes for signage shall relate to other signs, graphics and color schemes in the vicinity in order to achieve an overall sense of identity.
- Signs may be lighted; however, no light that flashes or blinks or affects changes in hue or intensity of illumination is permitted. Illumination sources for any sign shall be hidden from view.

Community Monuments

The following sign standards will effectively regulate the placement, erection and maintenance of permanent signage within the Kohl Ranch. These standards are intended to provide equitable standards for the protection of property values, visual quality and public health, safety and general welfare. The hierarchy of signs for the Kohl Ranch is as follows:

Project Entries

- Monument signs shall identify the Kohl Ranch community at key project entry points. These monuments should be the most prominent in scale and should set the overall theme for the entire community.
- The wall or monument materials should establish the project theme which other developments should use as a general guide.

- Project entry signs should be designed with the angle, distance and length of view in mind. Signage viewed from vehicles should be large and/or conspicuous enough to be read quickly. Signs designed to be viewed from slower speeds or at shorter distance may have more attention given to details such as texture, and may have a reduced text size.
- Building materials should consist of native stone, river rock cobblestone, concrete, brick, stuccoed concrete block, wood rails and boards, metal lettering systems and logos, sheet metals and wires, and special architectural elements such as weather vanes or windmills.
- Maximum dimensions for message areas of signs should be limited to those established in the County of Riverside Ordinance No. 348.
- Lighting should be limited to those fixtures which are at grade or are concealed by landscape planting.
- Signage typography should be limited to bronze, copper, or any quality steel logos or channel letters.

Residential Neighborhood Entries

- Residential neighborhood entries should establish the theme of the individual development while continuing the community theme.
- Recommended building materials may consist of native stone, river rock cobblestone, concrete, brick, stuccoed concrete block, wood rails and boards, and special architectural theme elements.
- Residential entry monuments should be less grand in scale than the major intersection treatments. The monuments should be in proportion with the surrounding streetscape and should be highlighted by landscape plant materials and site furnishings. The monument may be part of a wall, or stand alone.
- Maximum dimensions for message sign areas shall be limited to dimensions established by Riverside County Ordinance No. 348.
- Graphic signage materials should be limited to bronze, copper or any other high quality steel logos or channel letters.
- Back-lit letters or a concealed lighting source, located flush with grade or within a covered entry structure may be used.

Commercial Entries

- Commercial entries should incorporate the commercial center building materials, while continuing the community theme. Entry monuments should be subdued and not over-powering. They should simply and discreetly identify the development rather than create a grand entry statement.
- The maximum dimensions of the sign base and message area shall adhere to Riverside County Ordinance No. 348.
- Signage typography should be limited to bronze, copper or quality finished steel logos or channel letters. Sign text should be limited to identification of the street address, project name, and major retailers.
- Back-lit letters or a concealed lighting source, located flush with grade may be used.
- Signs may be constructed on brick, native stone, wood or metal. Letters may be mounted upon the signage base.
- Monuments shall be integrated with berming and landscape materials with established streetscape design themes.

Retail Signs

- Only one monument sign shall be allowed per building. These signs shall be located at entry points from parking areas, adjacent to walkways, in planting beds or within building setback landscape areas adjacent to the street right-of-way.
- Signs constructed of brick and/or native stone base, a smooth finished concrete, carved wood or metal panel systems are recommended.
- Sign dimensions shall adhere to those established by County Ordinance No. 348. Letters should be consistent with the architectural material of the establishment.
- A street address is recommended to be incorporated into the sign banner. Lighting should be concealed by plants or flush with grade.

Directional Signs

Directional signage, used to identify and direct vehicular and pedestrian traffic to on-site destinations shall be provided along roadways and within all multi-parcel and multi-tenant developments. Directional signage should be of consistent design throughout each project. All such signage should be fabricated from the same materials, with a consistent color palette and common graphic theme. The use of materials compatible with the architectural design of each project and its corresponding site furnishings is encouraged. Directional signs should be of consistent dimensions, and located in a visually logical order.

These signs should provide on-site directional information and should not be used for advertising. They should include the following:

- Directory monuments
- Public service signs
- Traffic safety signs
- Residential street signs

Directory Monuments

- Directory monuments should be permitted as ground signs or on the faces of buildings or structures to illustrate the project layout and locate the establishments or residential buildings of the development.
- The sign area of such location markers should be designed for either pedestrian or automobile users.
- The location and number of these markers should be determined at the individual project design level.
- All dimensions, material types and signage area requirements shall conform to County Ordinance No. 348.

Public Service Signs

Service signs should only provide general public information to direct the way to public facilities such as information centers, rest rooms, telephones, emergency stations, etc. Service signs should be permitted as wall or ground signs and should be limited in number and location as outlined below.

- Signs are permitted on the faces of buildings or structures provided that such signs should be placed at eye level above the immediately adjacent ground.
- Signs setting forth the location of, or directions to, parking or buildings located on the premises, or regulating the flow of on-premise traffic, should be permitted as part of the separate free-standing ground structure or kiosk. Such signs may include pictorial and decorative designs.

- The signs should include a minimum of words and numbers necessary to accurately communicate the required information.
- The signs shall be compatible with the adjacent architectural theme.

Traffic Safety

Traffic safety signs shall be subject to the standards of the County and State agencies.

Residential Street Signs

All street signs shall be consistent with the overall Kohl Ranch theme.

Temporary Signs

Signage that identifies uses or activities temporary in nature, such as that associated with real estate sales and leasing or the development and construction of buildings, shall be allowed on a temporary basis if consistent with the design character of the surrounding area. No illumination of temporary signs should be permitted. Temporary signage should be located on or in close proximity to the uses identified. The multitude of signs associated with development, design, construction and leasing should be combined into one sign for each project and the sign should be located on site.

Project Marketing

- These signs should provide community recognition for the Kohl Ranch during the construction and sales period.
- Wood with a painted sign surface is the recommended construction material. Signs may be double-sided. Signs may be painted onto the temporary construction safety walls erected to hide construction from view.

Non-Residential Construction and Leasing

- These signs are intended to inform the viewer of new buildings and leasing opportunities, opening dates, and builder names and telephone numbers of individual parcels within the Kohl Ranch project.
- Wood construction with a painted sign surface is recommended for this application.
- One sign per individual parcel or project should be allowed and should be located on-site.
- Sign face may include any sketch or architectural rendering of the proposed use.
- Sign removal should occur after lease out.

Residential Sales and Leasing

- Temporary ground signs for real estate sales, leasing, construction or model homes are permitted. One project identification monument or ground sign should be permitted for each major entrance to a development. Sign copy is limited to project name and address in addition to the word "sales" or "leasing."

Prohibited Signs

- Obsolete or Abandoned Signs. Any sign located on vacant or unoccupied property that was erected for a business which no longer exists, or any sign which pertains to a time, event or purpose which no longer exists, shall be removed within 30 days after the use has been abandoned.
- Signs constituting a potential traffic hazard or which simulates or imitates in size, color, lettering or design any traffic sign or signal.

- Temporary signs on public property (street, median island, parkway, sidewalks, traffic control sign posts, utility poles, park land, trees, etc.)
- Animated or moving signs: signs consisting of any moving, swinging, rotating, flashing, blinking or otherwise animated components.
- Off-premise signs: any sign, other than a simple directional sign, installed for the purpose of advertising a project, development, event, person or subject not related to the premises upon which sign is located.
- Vehicle signs: signs on or affixed to trucks, vans, automobiles, trailers or other vehicles which identify, or provide direction to a use or activity not related to its lawful making of deliveries or sales of merchandise or rendering of service from such vehicles.
- Portable signs: a freestanding sign not permanently affixed, anchored, or secured to the ground or structure it occupies, including tailored signs, except as approved by the County of Riverside.
- Roof signs: any sign erected, constructed and maintained upon or over the roof of any building, unless it is a projecting canopy sign or is an explicit part of the building architecture.
- Other prohibited signs: advertising signs and billboards, inflatable signs or balloons, inflatable animals, magnetic signs, plastic flags unless otherwise specified in these guidelines or approved by the County of Riverside.

3.3.6 Walls and Fences

Walls and fences are of particular functional importance for the Kohl Ranch as depicted in **Figure 3.3-2, Wall and Fence Plan**. Other allowed uses in open space that are in the vicinity of the airport safety zones will also be appropriately fenced or walled. If the golf course or motor sports race track are implemented, an individual fencing plan for those uses will be prepared.

Walls and fences establish enclosure, confer physical and visual privacy and also provide vertical texture. Walls and fences are especially important in creating a theme for the residential portion of the community. They should, however, be used sparingly and with great consideration so as not to detract from the open space or to simply demark property lines. The material, style and height of walls and fences shall provide an element of continuity throughout the project to insure visual consistency. The walls and fences should be designed with the intent of furthering the architectural character of the site.

Walls may be solid, perforated, or hidden from view when security is required, but view retention is desired. As fences are subject to severe exposure they must be well-made. All community and perimeter project walls and fencing are to be provided by the project builder at the time of development. Wherever practical, plant material should be used as a barrier rather than walls and fences.

General Guidelines

The horizontal mass of continuous walls should be softened by landscaping as specified in the landscape guidelines. No wall or fence shall exceed 6 feet in height unless it is used for noise attenuation where a combination of wall and berming is not sufficient. If retaining walls are necessary they should be terraced and should not exceed this 6-foot height limit.

Long stretches of unrelieved walls and fences should be broken up with varied setbacks or recesses for plantings. When a change in pad elevation occurs, the wall or fence should be stepped in equal vertical intervals. No step should exceed 12 inches in height.

Community Walls and Fences

Community walls and fences will provide community identity, security, privacy and sound buffering for residential units adjacent to project streets or incompatible land uses as depicted in **Figure 3.3-3, Wall and Fence Types**.

- Solid theme walls may be required along major thoroughfares where sound attenuation and privacy are required and when residential areas are adjacent to schools, commercial areas, or parks.
- Solid walls along residential development edges visible from surrounding arterial roadways shall be composed of solid masonry covered with smooth stucco. The wall will stand six (6) feet in height. Walls should be planted with vines or screened with other plant materials to reduce their visual impact. This will contribute to the shade and residential environment of the development the wall is intended to screen.
- Wall materials shall consist of simple masonry construction finished with colored smooth stucco, consistent with the desert color palette.

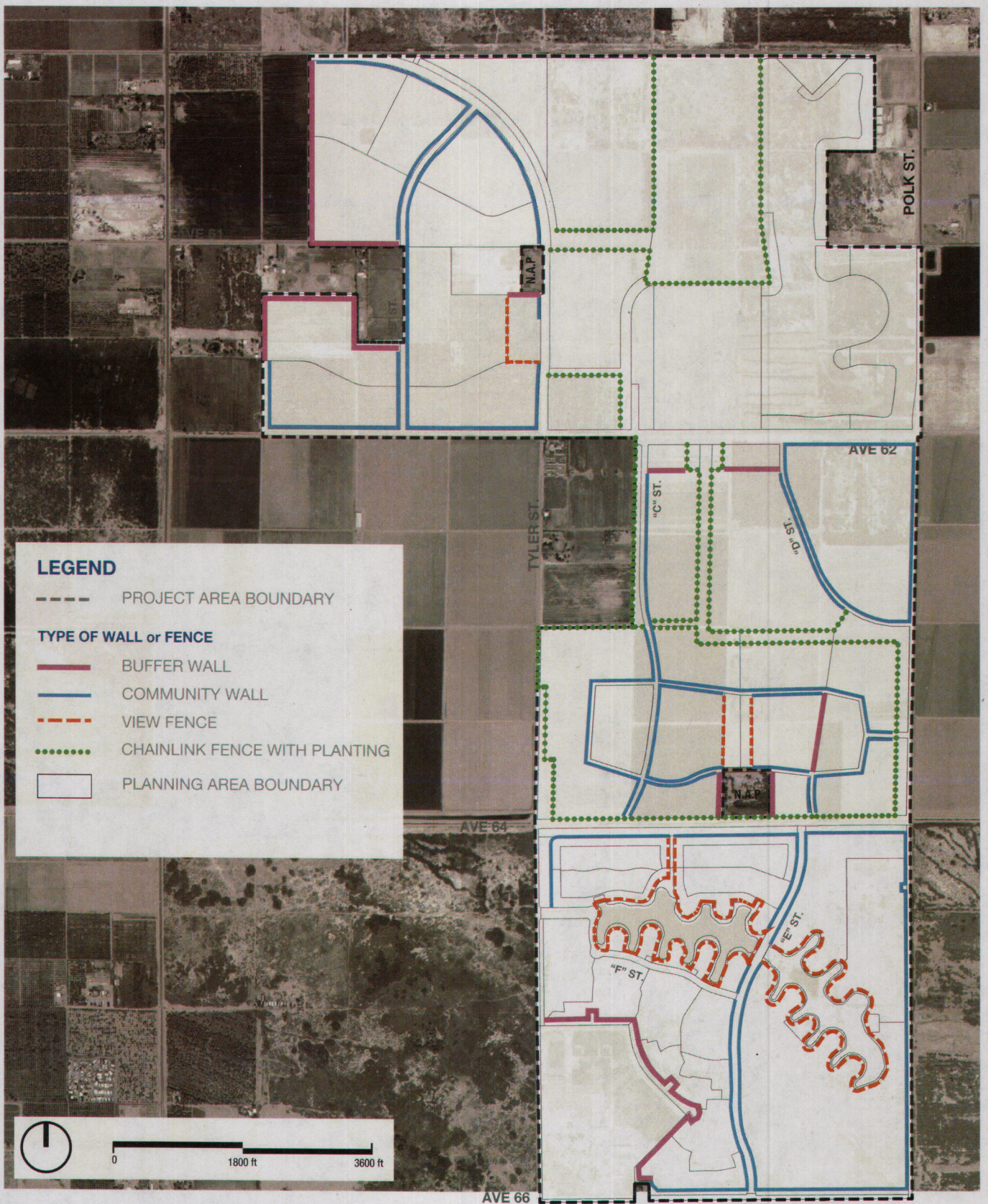
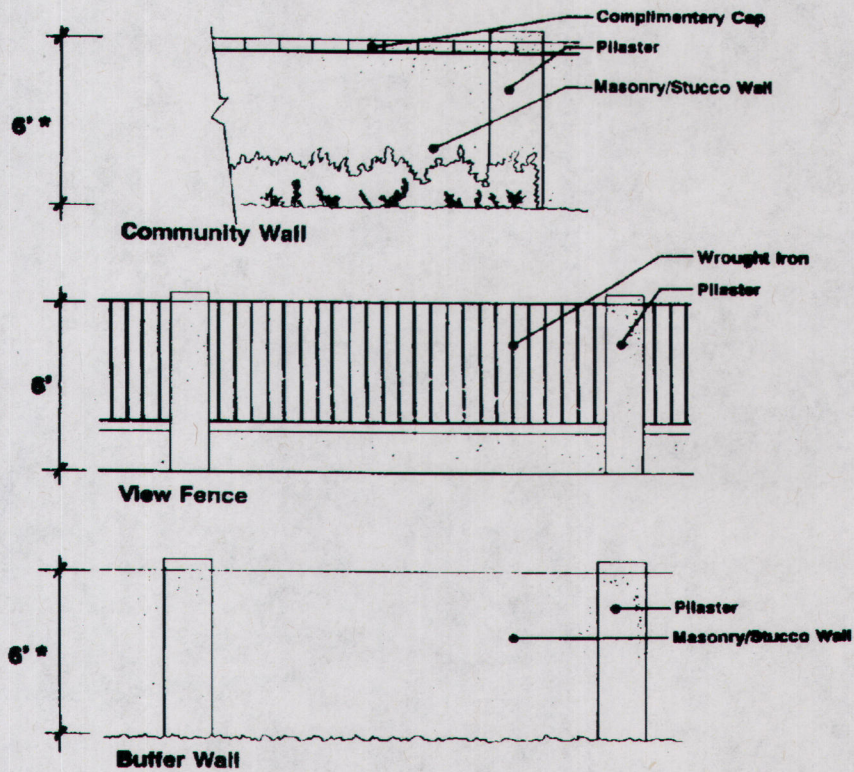


Figure 3.3-3, Wall and Fence Type



*Unless otherwise determined by a noise study for an adjacent land use.

Perimeter Walls and Fences

Fencing should be allowed within the project area and along the project boundaries. Landscape hedges and other screening measures should be used to reduce the visual incongruity of such fences.

View Fences

These fences should be used to increase the sense of openness in rear yards, along open space, golf courses, parks and schools.

- If golf courses are built, fences adjacent to golf courses visible from surrounding arterials should be view fences. Recommended material includes wrought iron, wood pickets, or welded wire on wood posts. Spires, spikes or other detailing consistent with the community theme may be used to impede trespassing over metal rails.

Privacy Walls and Fences

The privacy walls and fences are utilized alongside and rear lot lines where residential units need more privacy. Recommended material includes wood panels, masonry, stucco and stone.

Theme Walls and Fences

In certain instances such as project entries and at restaurants, thematic fences or walls may be appropriate. An example would be a western steak house with a low profile split rail fence at the entry. Special conditions that warrant thematic uses shall be approved by the County of Riverside.

3.4 LANDSCAPE DESIGN GUIDELINES

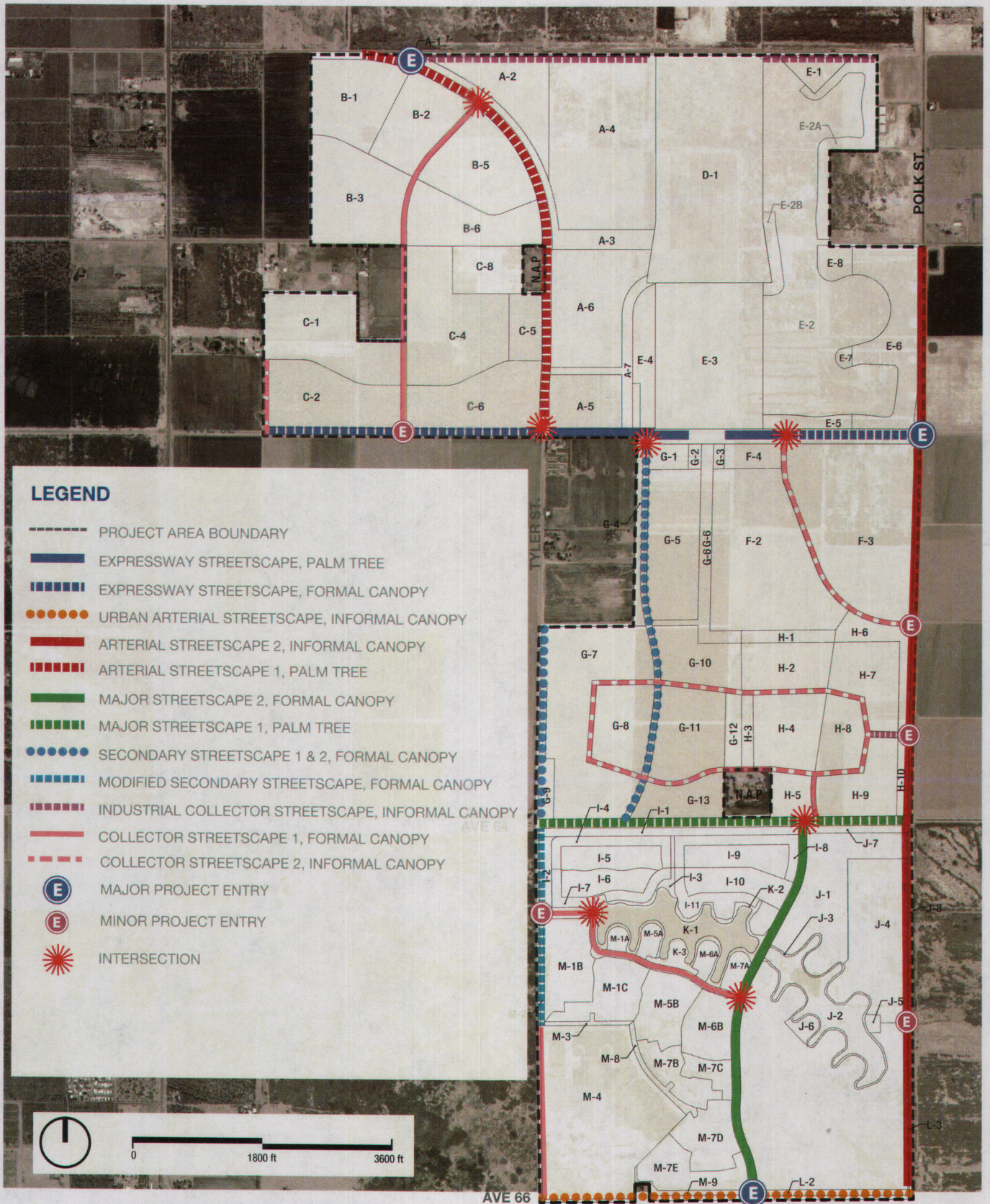
3.4.1 Landscape Concept

The landscape guidelines for the Kohl Ranch shall contribute greatly to the establishment of a community theme for the project. The landscape concept shall transcend all individual parcel boundaries and unify the community "edges," those areas along roadways, parks, commercial shopping areas, undisturbed areas and drainage ways and the adjacent residential development areas.

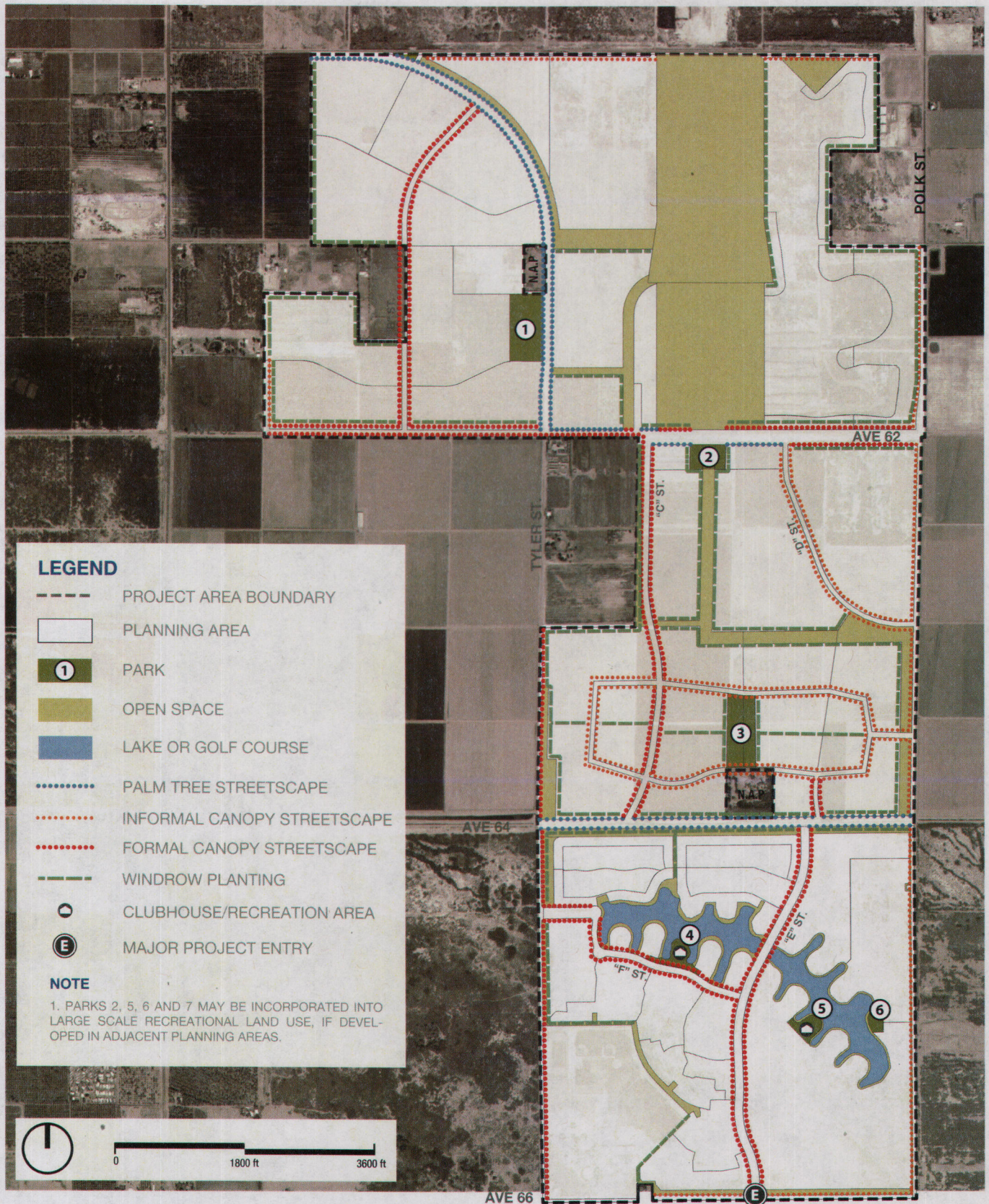
The landscape concept provides criteria for the treatment of all areas within roads and easements, medians, development edges, and project entries. The concept has been conceived to organize and present a memorable image for the Kohl Ranch project. Its purpose is to establish a unified landscape framework that provides continuity throughout the project area, and encourages the desert community theme. To accomplish this purpose, these guidelines should be consistently applied to define major project roadways (expressways, arterials, secondary and major highways, collectors, industrial collectors, and loop roads) roadway medians, internal circulation routes, major entries, and intersections as reflected in **Figure 3.4-1, Landscape Key Map**. Landscape materials within these areas should direct and guide the resident or visitor to the development, screen sensitive views, and frame or create focal points and views as the motorist, pedestrian, or bicycle moves through and around the project. A high degree of landscape quality is to be maintained throughout the Kohl Ranch Specific Plan area. Key elements of the concept are presented in **Figure 3.4-2, Community Structure Plan**.

The following are key objectives that the landscape concept should foster:

- The Kohl Ranch project should create a landscape theme that is compatible with the native desert community and architectural and site planning themes.
- The landscape is intended to work in association with the architectural guidelines and the grading plan to create screens and buffers where needed and views where the opportunity exists.
- The landscape should serve to augment sound attenuation efforts made with the landform or hardscape elements.
- Plant material selection for streetscapes, entry areas and development areas should be limited to those plants listed in the suggested plant palette.
- Landscape plant materials should be "long lived" varieties. "Short lived" materials such as flowering annuals and perennials may be utilized to accent or augment the "longer lived" base elements which form the landscape framework.
- Landscape elements within the front building setback visible from the public rights-of-way should blend with street edge landscaping.



THE KOHL RANCH
COACHELLA VALLEY, CALIFORNIA



- All common landscaped areas shall be designed with the objective of reducing long-term water use to a minimum in accordance with the Coachella Valley Water District Ordinance 1302.1 Landscape and Irrigation System Design Criteria and the Kohl Ranch Plant Palette. Native and drought tolerant plant materials and water-efficient irrigation practices shall be used. Within the golf course(s), if developed, water-efficient landscaping shall be encouraged in the "rough" and peripheral areas.
- Recycling of irrigation water is encouraged.

It is the intent of the landscape guidelines to foster innovative design and discourage the bland and mediocre. The design of the Kohl Ranch draws upon the character of the natural setting and applies this theme to concepts which are both cost effective and maintainable. Furthermore, the landscape guidelines foster a spirit of stewardship for the project open spaces and create a symbiotic existence with the developed areas, ultimately creating a unique recreational amenity for the community.

The essence of the landscape guidelines consists of the elements described below.

Community Identity

These guidelines establish a benchmark for high quality community landscape architecture by establishing guidelines for essential landscape elements that carry forth the landscape concept rather than relying upon a static and all inclusive plant palette. This flexible concept will allow for personal expression for individual builders and their designers within the community while maintaining a community structure and theme.

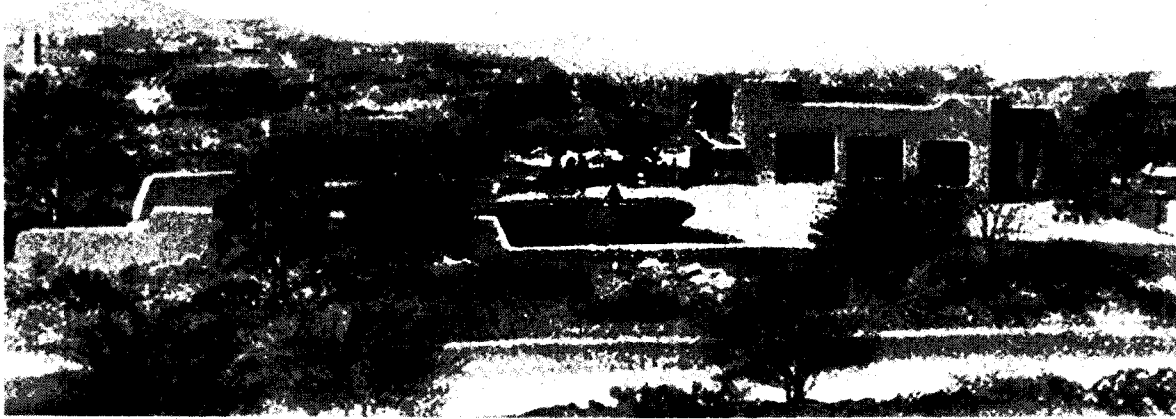
Visual Screens

The guidelines form a program which mitigates the effects of the built environment upon the undisturbed open space. The landscape guidelines also provide concepts for buffering buildings from adjacent land uses, both on and off site.

Conservation of Resources

The landscape plan and suggested plant materials have been selected with careful regard for the Coachella Valley's water supply and CVWD requirements and therefore, the resulting long-term viability of the landscape. The landscape will be efficient in terms of water consumption as well as maintenance requirements. Resources shall be concentrated in those areas of most intense human use, such as parks and other high-use areas. All common landscaped areas shall be designed with the objective of reducing long-term water use to a minimum. The use of native and drought tolerant plant materials and the use of water-efficient irrigation practices and the development of programs which "wean" plant materials from irrigation after their establishment are encouraged, while the use of high water-consuming plant materials shall be limited to selected areas. The landscape concept should encourage minimizing long-term maintenance for the majority of landscaped areas. This will be achieved by limiting areas of turf, clipped hedges, and exotic ornamentals to special places where they can be emphasized. Drainage channels through the site will be developed with native desert landscaping.

Alternative sources of irrigation water shall be considered, including reclaimed water, Colorado River water and surface runoff. The proximity of the site to Water Reclamation Plant No. 4 would allow for the use of reclaimed water for irrigation once tertiary water becomes available.



Neighborhood Park

The landscape guidelines encourage the use of the landscape as more than a visual aesthetic by creating environments that use plant materials as architectural elements which define space, establish proportion, and influence climate. The guidelines will encourage spaces which will, through the thoughtful arrangement of plant materials, encourage a range of emotions from intimate enclosure to vast openness. The landscape should affect the tactile senses by creating microclimates where people can escape the heat of summer or be warmed by the sun in the winter. The landscape should provide pleasing odors of flowers, rustling sounds of leaves in the wind, in addition to visual screens and focal points.

Application of Concepts

The guidelines provide a detailed, user-friendly landscape plan and plant matrix which outlines specific treatments and suggested plant materials to carry out the community theme. This plan and matrix should be easily understood by individual developers, builders, and designers.



The Community Structure Plan (Figure 3.4-2) shows how the various landscape conditions interact with one another to create the overall community landscape theme. In general, the plan is concerned with those areas within road rights-of-way, drainage areas, parks, and visually sensitive development areas such as entries and focal points.



3.4.2 Landscape Plant Palette





The following plant palette has been derived to enhance the Kohl Ranch desert landscape theme. The plant material has been selected to accent the desert environment while providing important functional characteristics. This list is intended to be a menu from which landscape architects can mix and match plant materials for various situations. The following general criteria have been established to determine plant selections:


- Desert Compatible Plants – Plants shall be tolerant of the harsh desert environment.
- Low/Moderate Water Consumption – Plants should not be high water consumers. Vast expanses of lawn areas are discouraged. Golf courses shall be primary water consumers and shall be irrigated with canal water to the extent possible.
- Broadleaf Deciduous Trees – These plants should be used where the need for summer shade and winter sunshine is important.
- Groundcover – Use of drought tolerant groundcovers and decomposed granite should be encouraged to reduce blowing dust.
- Maintenance of Established Themes – The Kohl Ranch streetscape and entry themes should be maintained (i.e., Palm trees evenly spaced with citrus understory and desert groundcover throughout).

Table 3.4-A, Kohl Ranch Plant Palette




Plant Form.	Botanical Name	Common Name	Location
PALMS			
	<ul style="list-style-type: none"> • <i>Arecastrum romanzoffianum</i> • <i>Brahea armata</i> • <i>Chamaerops humilis</i> • <i>Phoenix canariensis</i> • <i>Phoenix dactylifera</i> • <i>Washingtonia filifera</i> • <i>Washingtonia robusta</i> 	<p>Queen palm</p> <p>Mexican blue palm</p> <p>Mediterranean fan palm</p> <p>Canary Island date palm</p> <p>Date palm</p> <p>California fan palm</p> <p>Mexican fan palm</p>	<p>Palm Tree Streetscape for Major and Minor entries, Intersection Details, Expressway Streetscape, Urban Arterial Streetscape, Arterial Streetscape 1 and Major Streetscape 1.</p>
LARGE SHADE TREES			
	<ul style="list-style-type: none"> • <i>Brachychiton populneus</i> • <i>Cassia leptophylla</i> • <i>Dalbegia sissoo</i> • <i>Fraxinus velutina</i> • <i>Koelreuteria bipinata</i> 	<p>Bottle tree</p> <p>Golden Medallion</p> <p>Rosewood</p> <p>Arizona ash</p> <p>Chinese Flame Tree</p>	<p>Formal Canopy Streetscape for Expressway Streetscape, Major Streetscape 2, Secondary Streetscape 1, Secondary Streetscape 2, and Collector Streetscape 1.</p>

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> • <i>Olea europaea</i> • <i>Pinus Eldarica</i> • <i>Platanus wrightii</i> • <i>Quercus virginiana</i> • <i>Tipuana tipu</i> • <i>Ulmus parvifolia</i> 	<p>'Swan Hill' olive Afgan Pine Arizona sycamore Southern live oak Tipu Tree Evergreen Elm</p>	
	<ul style="list-style-type: none"> • <i>Acacia salicina</i> • <i>Acacia farnesiana</i> • <i>Acacia smallii</i> • <i>Acacia stenophylla</i> • <i>Cercidium floridum</i> • <i>Cercidium h. 'Desert Museum'</i> • <i>Chitalpa Tashkentensis</i> • <i>Geijera parvifolia</i> • <i>Parkinsonia aculeata</i> • <i>Prosopis alba</i> • <i>Prosopis chilensis</i> • <i>Prosopis hybrid 'Phoenix'</i> • <i>Rhus lancea</i> 	<p>Willow acacia Sweet acacia Sweet acacia Shoestring acacia Blue palo verde Hybrid palo verde Pink Dawn Australian willow Mexican palo verde Argentine mesquite Chilean mesquite Hybrid mesquite African sumac</p>	<p>Formal Canopy Streetscape for Expressway Streetscape, Major Streetscape 2, Secondary Streetscape 1, Secondary Streetscape 2, and Collector Streetscape 1.</p>
STREET TREES (NON-DESERT)			
	<ul style="list-style-type: none"> • <i>Brachychiton populneus</i> • <i>Eucalyptus species</i> • <i>Fraxinus velutina</i> • <i>Platanus wrightii</i> 	<p>Bottle tree Various Arizona ash Arizona sycamore</p>	<p>Informal Canopy Streetscape for Urban Arterial Streetscape, Arterial Streetscape 2, Polk Street Streetscape 2a and 2b, Industrial Collector Streetscape, Collector Streetscape 2 and Tyler Street Project Edge.</p>
STREET TREES (DESERT THEME)			
	<ul style="list-style-type: none"> • <i>Cercidium floridum</i> • <i>Cercidium h. 'Desert Museum'</i> 	<p>Blue palo verde Hybrid palo verde</p>	<p>Informal Canopy Streetscape for Urban Arterial Streetscape, Arterial Streetscape 2, Polk Street Streetscape 2a and 2b, Industrial</p>

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> • <i>Cercidium praecox</i> • <i>Chilopsis linearis</i> • <i>Prosopis alba</i> • <i>Prosopis chilensis</i> 	<p>Sonoran palo verde Desert willow Argentine mesquite Chilean mesquite</p>	<p>Collector Streetscape, Collector Streetscape 2 and Tyler Street Project Edge.</p>
ACCENT TREES			
	<ul style="list-style-type: none"> • <i>Albizia julibrissin</i> • <i>Bauhinia variegata</i> • <i>Jacaranda mimosifolia</i> • <i>Lagerstroemia indica</i> • <i>Pistacia 'Red push'</i> • <i>Thevetia peruviana</i> • <i>Citrus sp</i> 	<p>Silk tree Purple orchid tree Jacaranda Crape myrtle Red push pistache Yellow oleander Orange</p>	<p>Palm Tree Streetscape for Major and Minor Entries, Intersection Details, Expressway Streetscape, Urban Arterial Streetscape, Arterial Streetscape 1, and Major Streetscape.</p>
WINDROW			
	<ul style="list-style-type: none"> • <i>Eucalyptus species</i> • <i>Oleander</i> 	<p>Various Various</p>	<p>Edge and buffer conditions.</p>
LARGE SHRUBS			
	<ul style="list-style-type: none"> • <i>Baccharis sarothoides</i> • <i>Caesalpinea pulcherrima</i> • <i>Cassia nemophylla</i> • <i>Cistus purpureus</i> • <i>Cistus ladaniferus</i> 	<p>Desert broom Red bird of paradise Senna Orchid rock rose Crimson spot rock rose Hopseed bush</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> • <i>Dodonaea viscosa</i> • <i>Fouquieria splendens</i> • <i>Leucophyllum frutescens</i> • <i>Liqustrum japonicum</i> • <i>Nerium oleander</i> • <i>Photinia fraseri</i> • <i>Pittosporum tobira</i> • <i>Pittosporum tobira</i> `variegata` • <i>Xylosma congestum</i> 	<p>Ocotillo</p> <p>Texas Ranger</p> <p>Japanese privet</p> <p>Petite Pink & `Petite Salmon`</p> <p>Fraser photinia</p> <p>Mock orange</p> <p>Yellow oleander</p> <p>Shiny xylosma</p>	
MEDIUM/SMALL SHRUBS			
	<ul style="list-style-type: none"> • <i>Agave shawii</i> • <i>Aloe species</i> • <i>Bougainvillea 'Oh La La'</i> • <i>Carissa g.</i> • <i>Encelia farinosa</i> • <i>Euonymus japonica</i> • <i>Euryops virides</i> • <i>Ferocactus species</i> • <i>Hemerocallis</i> • <i>Hesperaloe parviflora</i> • <i>Lantana camara</i> • <i>Larrea tridentate</i> • <i>Leucophyllum f.</i> `Green Cloud` • <i>Muhlenbergia capillaries</i> • <i>Muhlenbergia rigens</i> • <i>Opuntia violacea</i> • <i>Santolina virens</i> • <i>Simmondsia chenensis</i> 	<p>Shaw's century plant</p> <p>Aloe</p> <p>Bougainvillea</p> <p>Green Carpet</p> <p>Encelia</p> <p>Euonymus</p> <p>Green euryops</p> <p>Barrel cactus</p> <p>Daylily</p> <p>Red yucca</p> <p>Radiation lantana</p> <p>Creosote bush</p> <p>Texas Sage</p> <p>Regal Mist</p> <p>Deer Grass</p> <p>`San Rita` prickly pear</p> <p>Santolina</p> <p>Jojoba</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>
GROUNDCOVER			



Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> • <i>Acacia redonlens</i> 'prostrata' • <i>Baccharis pilularis</i> • <i>Dalea capitata</i> • <i>Lantana montevidensis</i> • <i>Lantana</i> 'New Gold' • <i>Lantana sellowiana</i> • <i>Lonicera japonica</i> • <i>Santolina chamaecyparissus</i> • <i>Trachelospermum jasminoides</i> • <i>Verbena gooddingii</i> 	<p>Trailing acacia 'Centennial' Sierra Gold Trailing lantana New Gold lantana Trailing lantana Honeysuckle Lavender cotton Star jasmine Sandpaper Verdena</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>
VINES			
	<ul style="list-style-type: none"> • <i>Bougainvillea spectabilis</i> • <i>Ficus pumila</i> • <i>Lonicera japonica</i> • <i>Rose banksiae</i> 	<p>Various Creeping fig Hall's honeysuckle Lady Bank's rose</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>
TURF			
	<ul style="list-style-type: none"> • <i>Hybrid bermuda</i> 	<p>Tifgreen or 'Santa Ana'</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>

3.4.3 Entries

Entries or focal points are those key areas along the roadway where change in travel direction may occur. Often at these points a different land use may occur and the landscape should announce this change. Working in concert with signage, lighting and hardscape elements, the landscape plant materials will create "outdoor rooms" with an individual identity and level of importance that creates a unique mood as the motorist, pedestrian or cyclist passes through each space. Often a stoppage in movement occurs at these points; therefore, design elements that create a more lush effect of color, texture, and form should be concentrated at these locations.

Major Project Entries

The landscapes at the project entries along with the signage, lighting and hardscape elements will form a gateway into the Kohl Ranch and set the stage for the community identity. The landscape should draw views into the site and toward important development features. Tasteful plantings should indicate a transition from the surrounding land uses to the more formal roadways. Water resources should be concentrated toward irrigation of trees and shrubs. Ground covers may be of a more xeric nature.



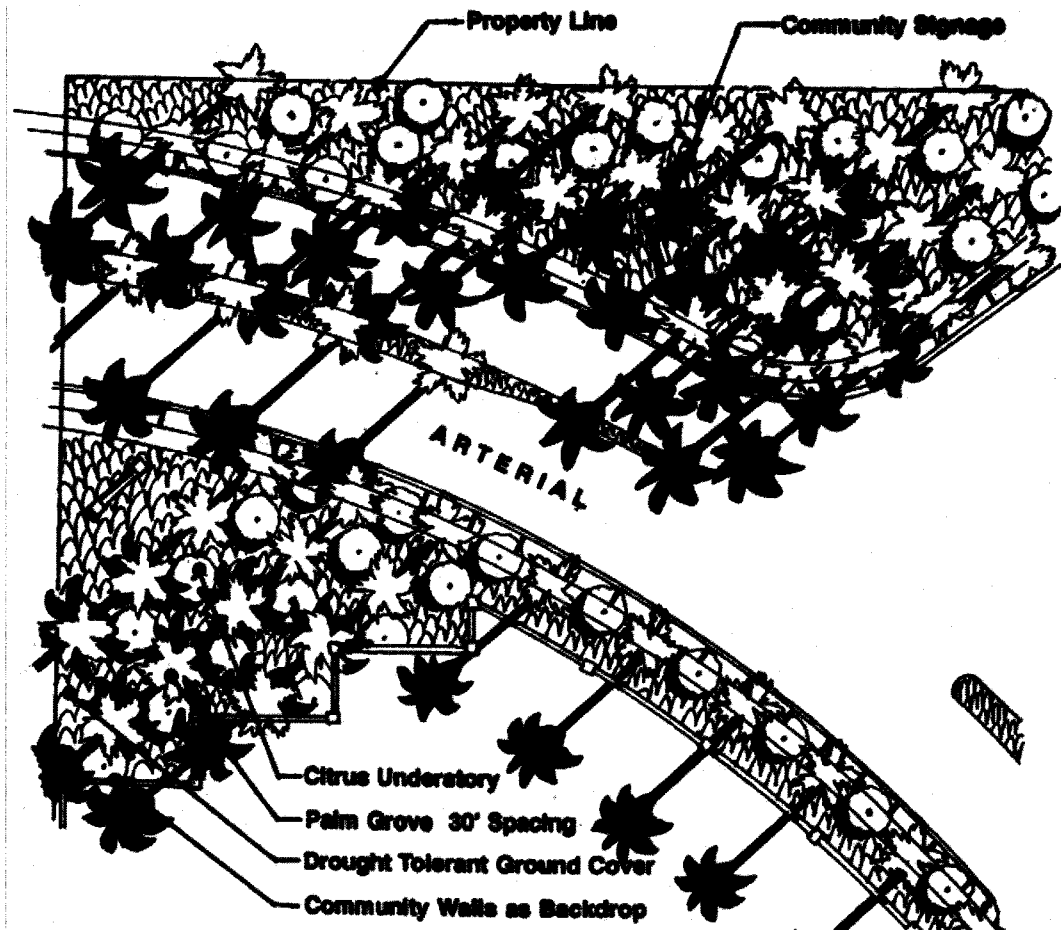
Major Entry

Major project entries are located at the following intersections:

- Avenue 60, north of "B" Street;
- Avenue 62 and Polk Street; and
- "E" Street and Avenue 66.

Major project entries will be landscaped with palm trees, with a maximum spacing of 30 feet on-center, with a citrus understory as designated in **Figure 3.4-3, Major Project Entry**. These plantings will be underlain by a drought tolerant groundcover. Community walls will create a backdrop for the entry landscape features.

Figure 3.4-3, Major Project Entry



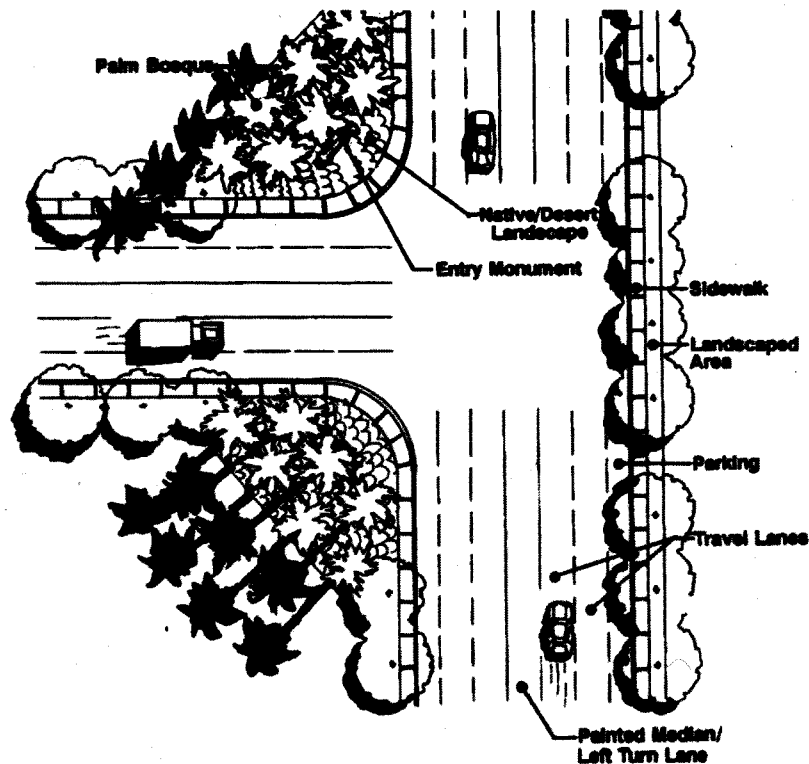
Minor Entries

The minor entries should convey the same objectives as the major entries but on a less grand and more intimate scale. **Figure 3.4-4, Minor Entry**, depicts a typical minor entry at the following locations:

- "B" Street and Avenue 62;
- "F" Street and Tyler Street;
- "D" Street and Polk Street;
- the interior loop road and Polk Street; and
- Polk Street and boundary of Planning Areas J-4 and L-3.

The plantings which create the walls of the outdoor "room" should be pulled in and a greater sense of enclosure should be created. Elements of color, form, and texture should be highly developed for these intensely visible areas. Palm tree groves should be supplemented with native/desert landscape elements.

Figure 3.4-4, Minor Entry

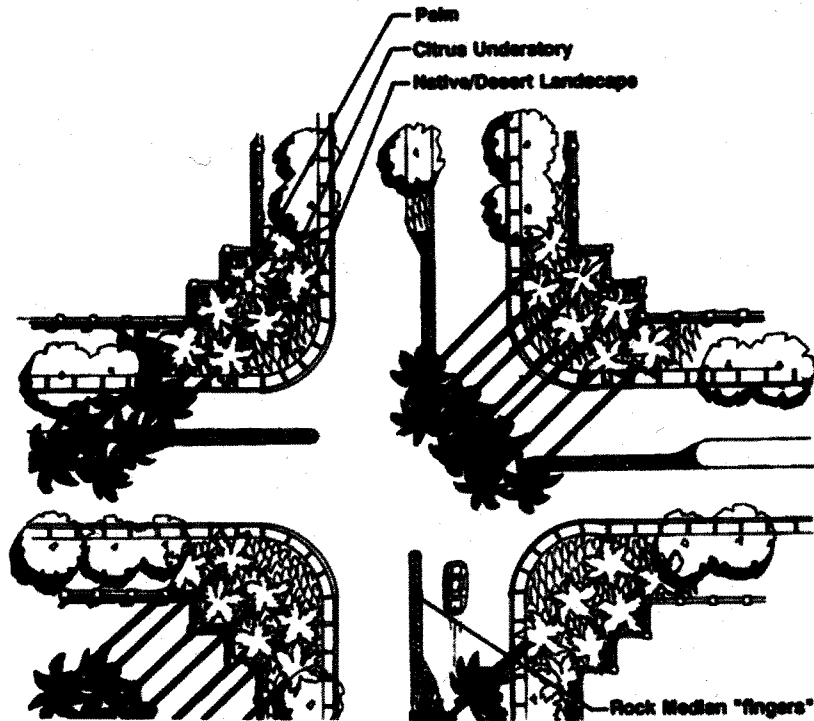


3.4.4 Intersections

Within the project site, certain key intersections will have special landscape treatments to denote the entry into a project neighborhood. This intersection treatment is illustrated in **Figure 3.4-5, Intersection Detail**. The landscape concept will be adapted to three-way intersections. Three-way intersections include the intersection of Avenue 62 and "C" Street, the intersection of "E" Street and Avenue 64, the intersection of "B" Street and Tyler Street, the intersection of "B" Street and Avenue 62, the intersection of "D" Street and Avenue 62, the intersection of "C" Street and Avenue 62, and the intersection of "E" Street and "F" Street. Intersections with special intersection treatments also include "F" Street and it turns south adjacent to Planning Area M-1B.

Major intersection landscapes should act as terminus points along the project roadways. They should appear as large enclosed outdoor rooms where the traveler can easily decide their direction of travel, react, then continue moving through the site. The edges of the room should be vertical to provide enclosure, with plant material located so that signage is emphasized. Color, form and texture should further define the importance of these decision points through an increased emphasis on water resources and plant materials.

Figure 3.4-5, Intersection Detail



Note: Landscape concept will be adapted to 3-way intersections.

3.4.5 Streetscapes

In order to achieve a cohesive overall circulation system for the Kohl Ranch and provide a strong community structure and identity, a consistent level of landscape design quality for public and private rights-of-way will be maintained. A consistent streetscape treatment will be implemented throughout the project as well as for the roadways surrounding the site. The general streetscape concept combines the use of palm trees with citrus understory, massed in selected locations, supported by turf and plantings of lush desert plants and annual color. The streetscapes are broken down into five major categories with special features and plant forms as indicated, and are applied to the hierarchy of the project roadways:

- Palm Tree Streetscape
- Formal Canopy Streetscape
- Informal Canopy Streetscape
- Windrow Planting
- Expressway Planting

Plant materials within these areas are intended to create a mood of movement and procession. Plant materials should direct and guide the traveler, screening sensitive views, or framing and creating focal points or "rooms" as the motorist, cyclist or pedestrian moves through the streetscape. Water resources should be minimized through the use of drip irrigation systems for trees and shrubs, and groundcovers which are attractive and thrive with limited irrigation or seasonal rains.

The five types of streetscape planting schemes listed above are applied to the hierarchy of project roadways which are broken down into seven roadway categories with special features and suggested plant forms as follows.



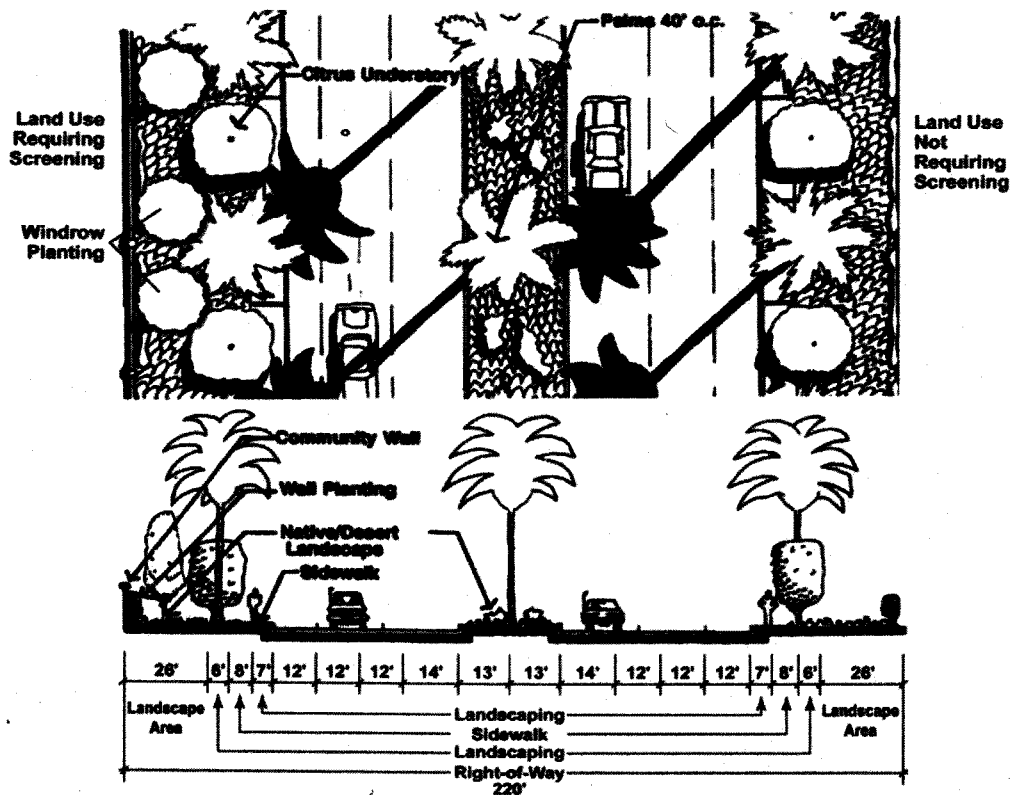
Expressway Streetscapes

Expressway Streetscape (220' ROW)

This streetscape applies to Avenue 62, the single project expressway, to serve as a formal progression through the site in the "grand allee" tradition of an estate roadway. The Palm Tree Streetscape is used as the basis for this streetscape, providing continuity across the project site. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial or residential, while streetscapes in front of commercial uses and/or parks would not require screening. **Figure 3.4-6, Expressway Streetscape**, shows the concept for either situation.

- Where the expressway passes through the airport Emergency Touchdown Zone (ETZ), no trees should be planted.

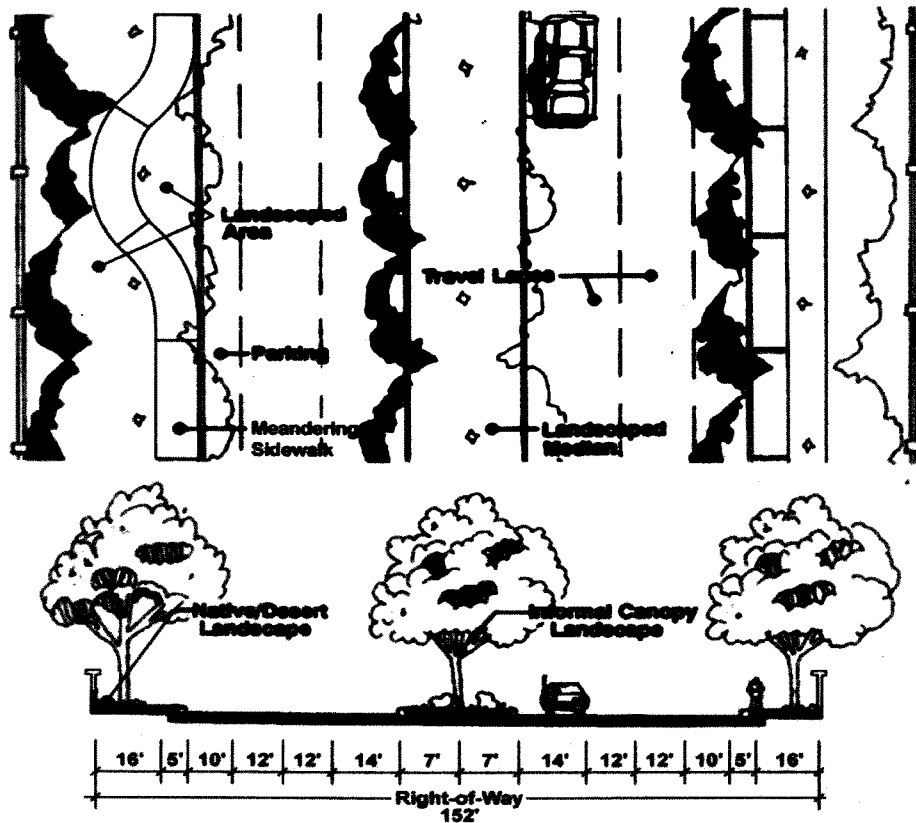
Figure 3.4-6, Expressway Streetscape



Urban Arterial (152' ROW)

This streetscape is intended for Avenue 66 only, and is interspersed with the Informal Canopy Streetscape, which should include trees that create a canopy by touching at their edges providing an overhead plane to impart a feeling of enclosure and to provide dappled shade as reflected in **Figure 3.4-7, Urban Arterial Streetscape**.

Figure 3.4-7, Urban Arterial Streetscape



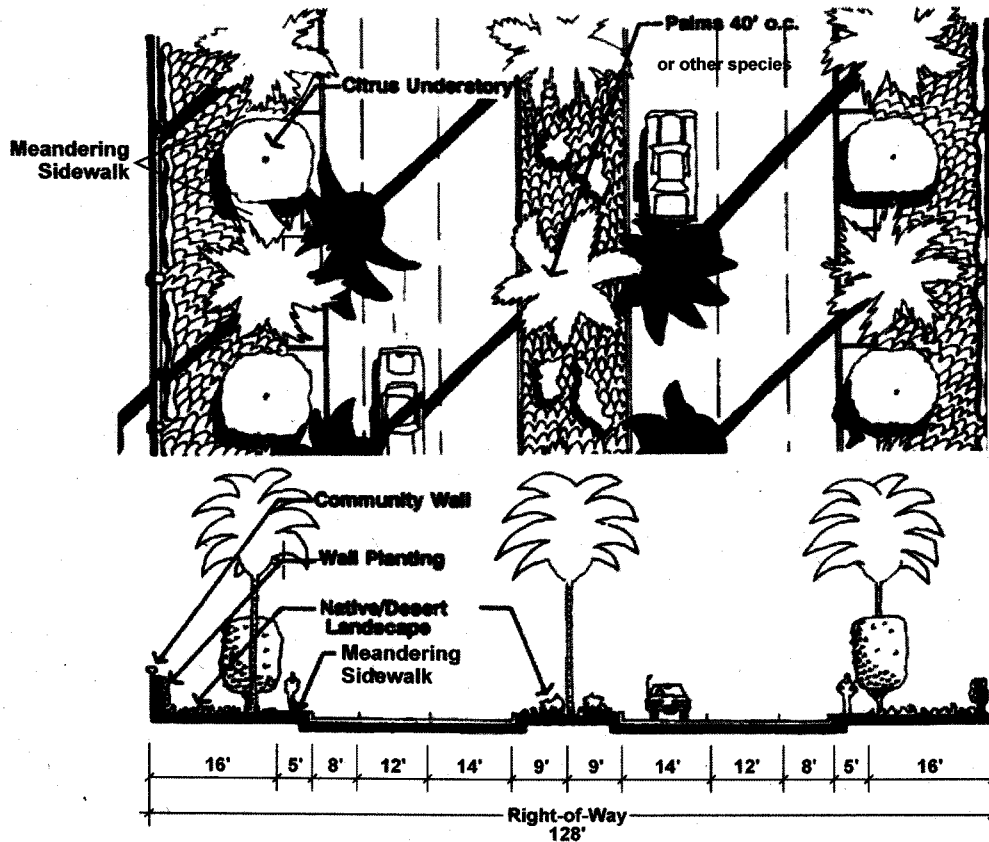
Arterial Highway Streetscape (128' ROW)

This streetscape is intended for Tyler Street north of Avenue 62, and Polk Street. Two different streetscapes are intended for these two arterials since Tyler Street is a major roadway within the project and Polk Street is along the edge of the project.

Arterial Streetscape 1 –Tyler Street, north of Avenue 62

- This roadway will include automobiles, bicycle and pedestrian traffic; therefore, foreground treatment and trees should be designed with regard to automobile speeds and/or the pedestrian experience depending on whether off-pavement trails are provided. The Palm Tree Streetscape is used as the basis for Arterial Streetscape 1. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial or residential, while streetscapes in front of commercial or airport mixed uses would not require screening. **Figure 3.4-8, Arterial Streetscape 1**, shows the concept for either situation. Although palms may be used, other columnar tree types may also be used to better address pedestrian needs
- Arterial Streetscape 1 should include more formal elements near the entry focal points including a citrus understory to further emphasize the project entry.

Figure 3.4-8, Arterial Streetscape 1



Arterial Streetscape 2 – Polk Street

The Informal Canopy Streetscape is used as the basis for Arterial Streetscape 2. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial, while streetscapes adjacent to the drainage canal also varies from a typical condition. **Figure 3.4-9, Arterial Streetscape 2 - Polk Street Streetscape 2A**, shows the typical situation. Polk Street, north of Avenue 62, would include Windrow Plantings, if screening is needed.

Figure 3.4-9, Arterial Streetscape 2, Polk Street Streetscape 2A

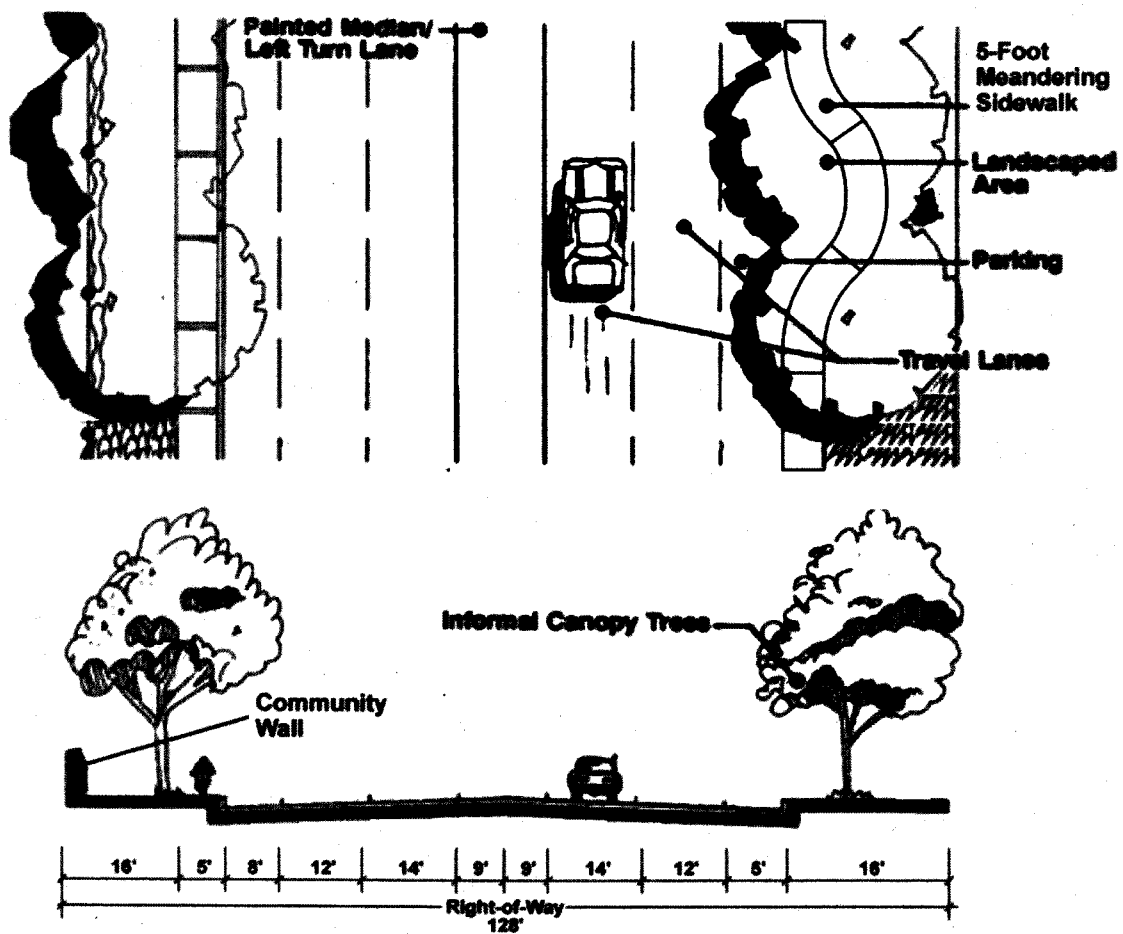
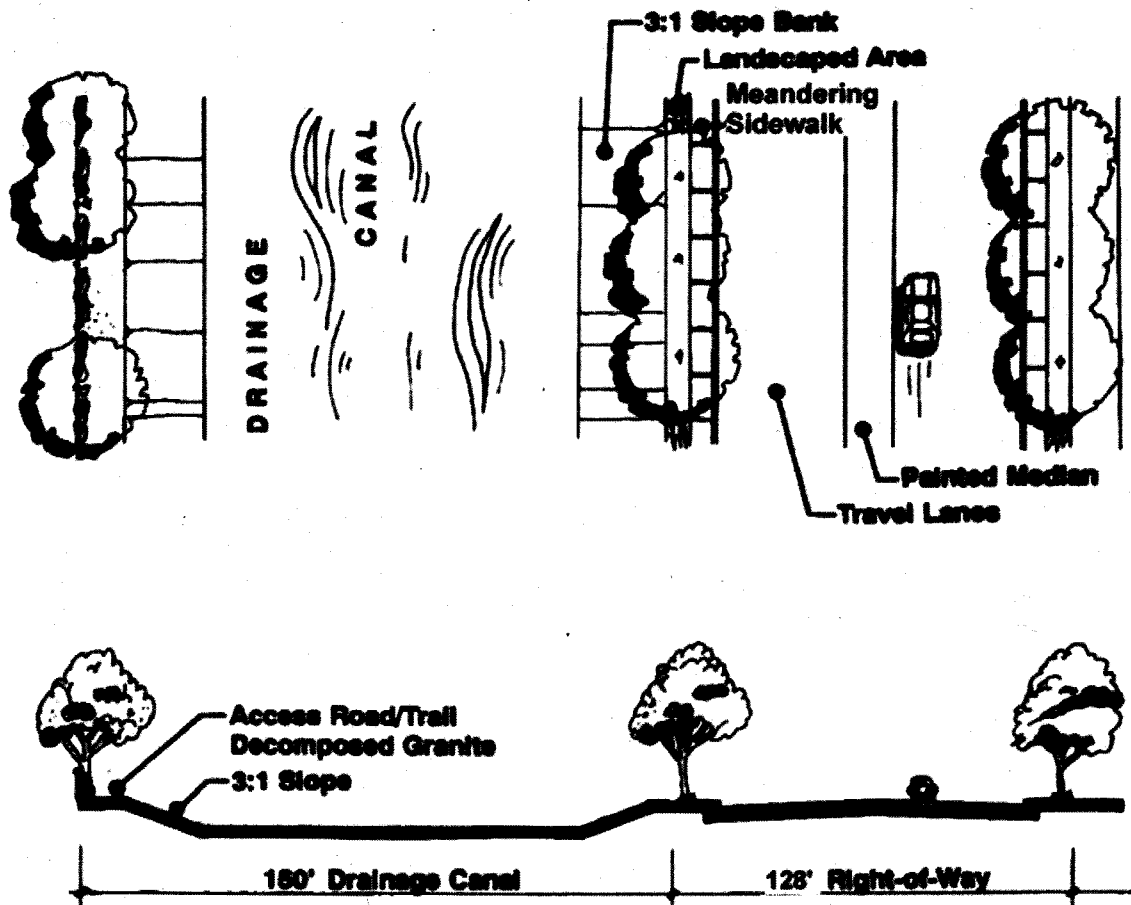


Figure 3.4-10, Arterial Streetscape 2, Polk Streetscape 2B depicts the project edge where the drainage facilities about Polk Street.

Figure 3.4-10, Arterial Streetscape 2, Polk Street Streetscape 2B



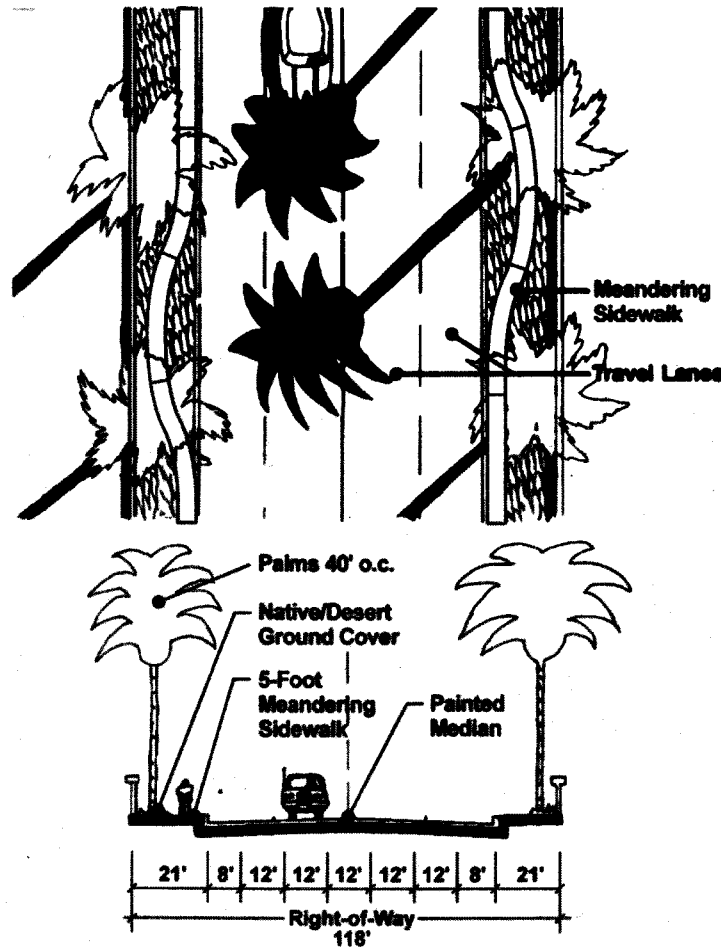
Major Highway Streetscape (118' ROW)

This streetscape is intended for Avenue 64 and "E" Street. Two different streetscapes are intended for these two majors since Avenue 64 is bordered by the utility corridor and evacuation channel while "E" Street meanders through residential areas.

Major Streetscape 1 – Avenue 64

The Palm Tree Streetscape is used as the basis for **Figure 3.4-11, Major Streetscape 1**. This streetscape is intended for Avenue 64 as it traverses the project site. Palm trees provide a vertical element that denote the important roads through the site, and should be spaced at 40 feet on center with a native/desert ground cover. Avenue 64 is flanked by the utility corridor and the evacuation canal so screening of these areas is appropriate by adding Windrow Planting at the back of the ROW to provide a pleasant driving experience through the project area.

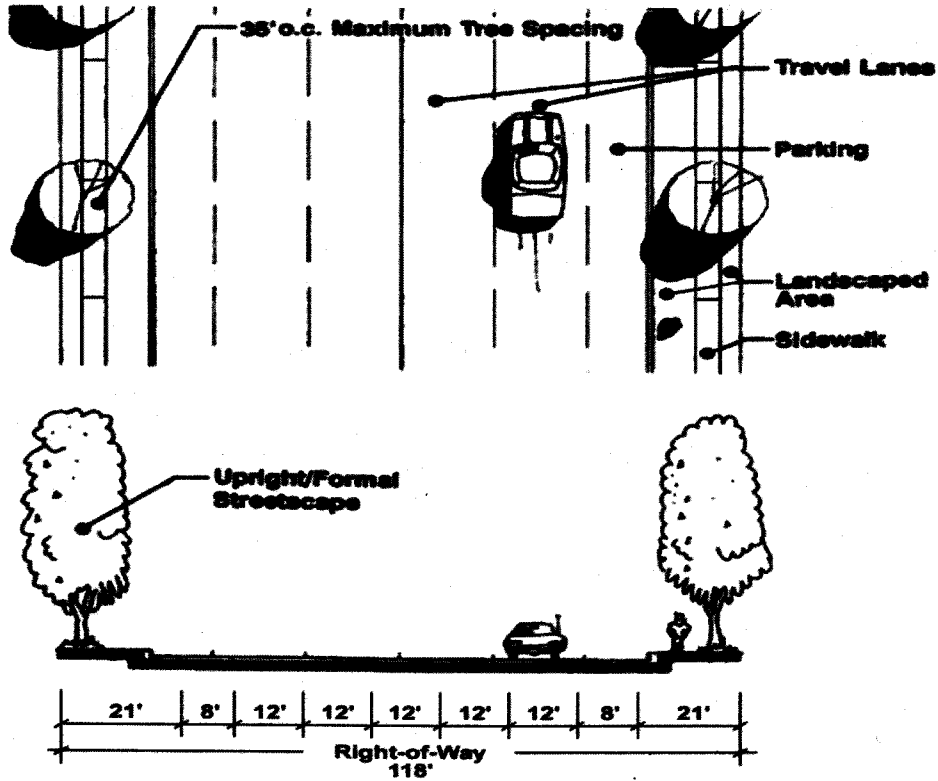
Figure 3.4-11, Major Streetscape 1



Major Streetscape 2 – "E" Street

Figure 3.4-12, Major Streetscape 2, illustrates the Formal Canopy Streetscape, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area. This streetscape should be used for "E" Street.

Figure 3.4-12, Major Streetscape 2



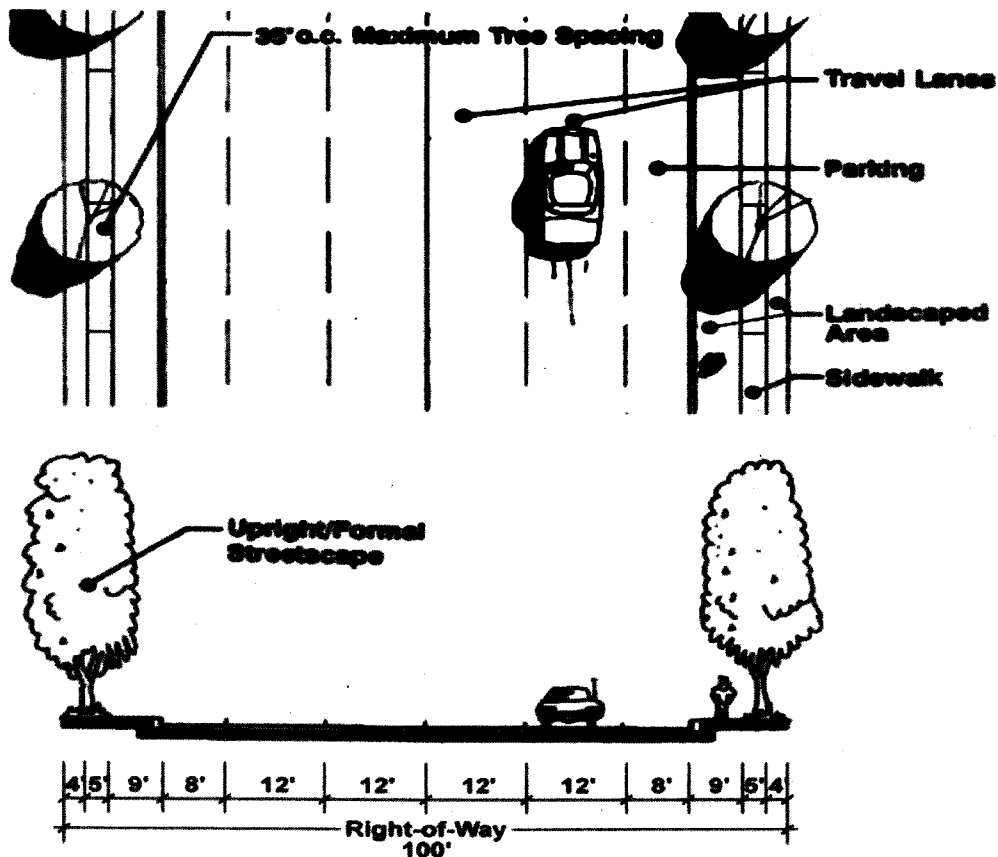
Secondary Streetscape (100' ROW)

This roadway treatment is intended be the next progression from an automobile dominated streetscape to a more bicycle and pedestrian-oriented streetscape. Design speeds along this roadway will be slower, views from passing motorists will be longer, and occasional bicyclists and pedestrians will be present, therefore plant material color and texture will be of importance along this roadway. Just as along the expressway and arterial streetscape, the plant material will open and close to provide or control the view along the roadway. Three streetscapes are proposed for the Secondary Highways in the Kohl Ranch Specific Plan area.

Secondary Streetscape 1

Figure 3.4-13, Secondary Streetscape 1, illustrates the Formal Canopy Streetscape, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area. This streetscape should be used for "C" Street and Tyler Street, north of Avenue 64.

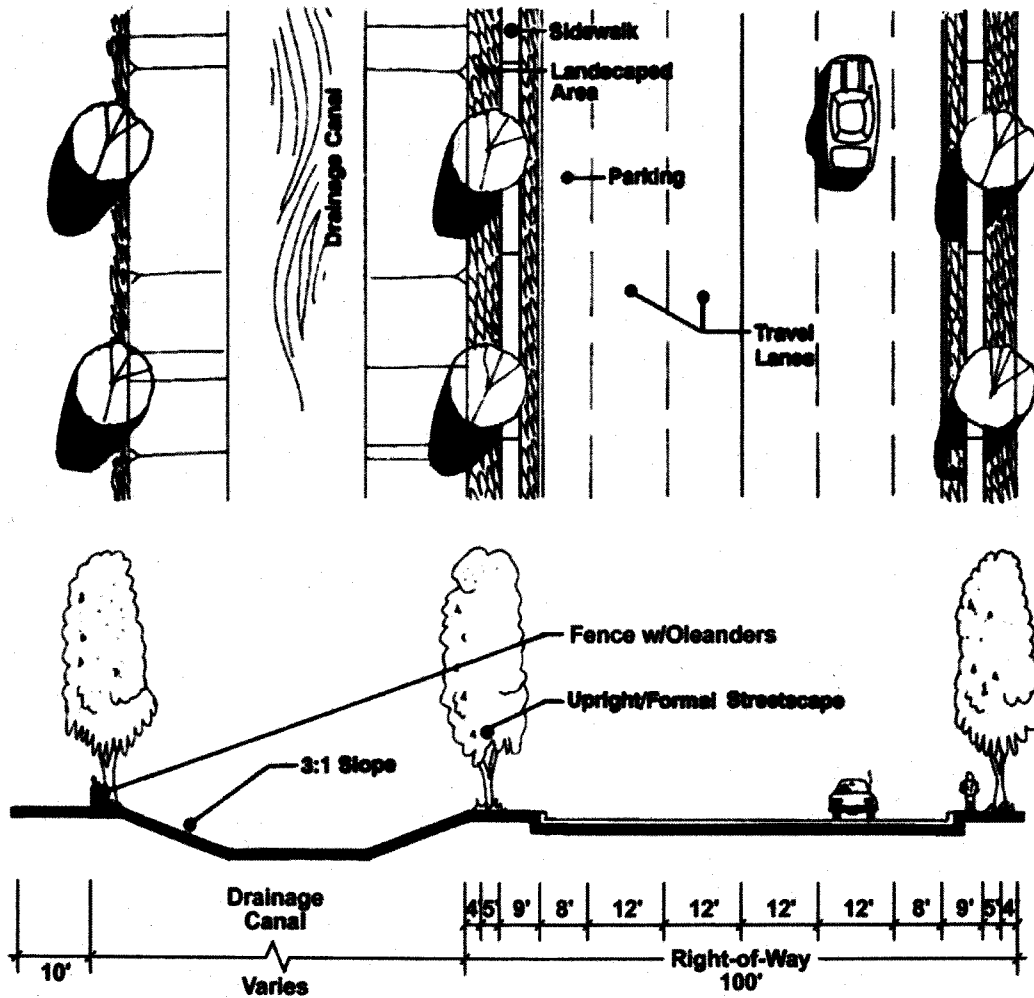
Figure 3.4-13, Typical Secondary Streetscape 1



Secondary Streetscape 2

Figure 3.4-14, Secondary Streetscape 2 illustrates the Formal Canopy Streetscape alongside the drainage channel, which could occur along Tyler Street, north of Avenue 64. Upright trees are used at the edge between the roadway and the 3:1 side slope of the drainage facilities.

Figure 3.4-14, Secondary Streetscape 2

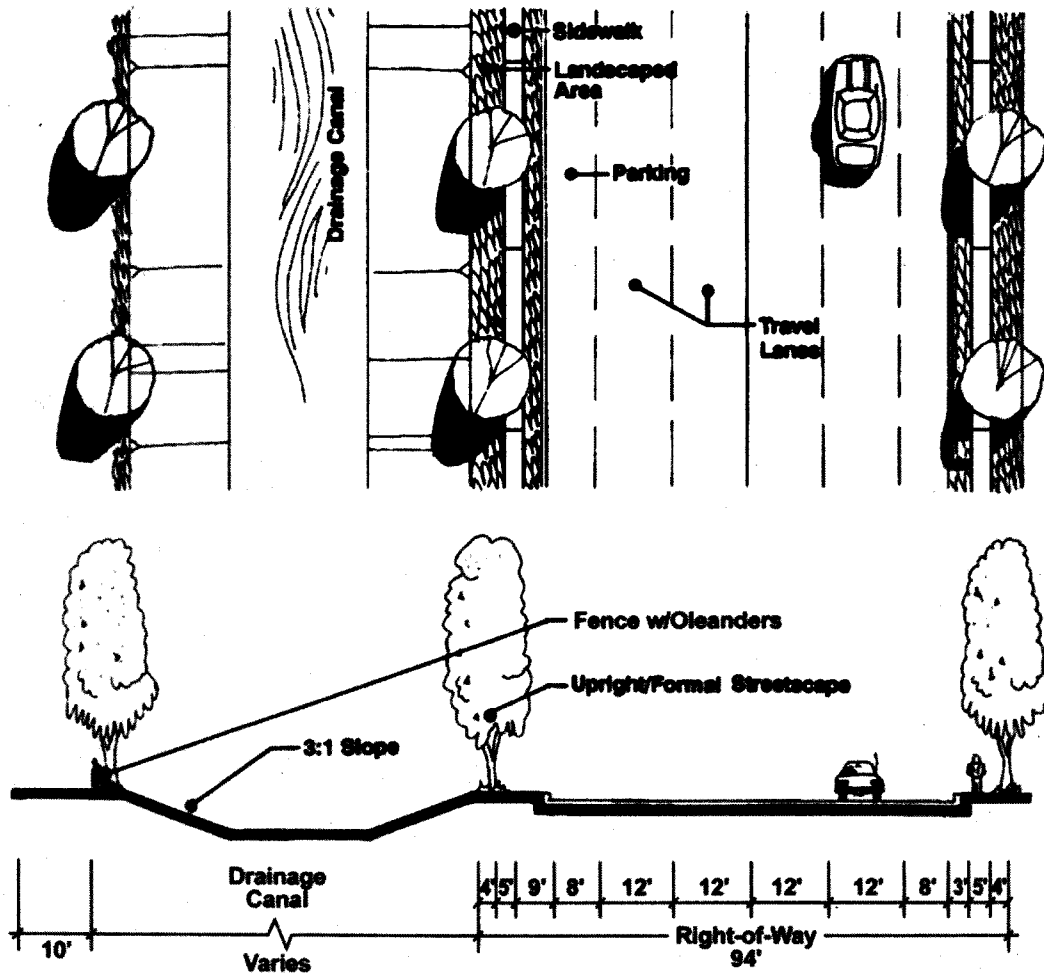


Note: Typical channel section - widths may vary.

Modified Secondary Streetscape

Tyler Street is also a secondary highway with a modified cross section south of Avenue 64. **Figure 3.4-15, Modified Secondary Streetscape 2** illustrates the Formal Canopy Streetscape alongside the drainage channel, which could occur along Tyler Street. Upright trees are used at the edge between the roadway and the 3:1 side slope of the drainage facilities.

Figure 3.4-15, Modified Secondary Streetscape 2

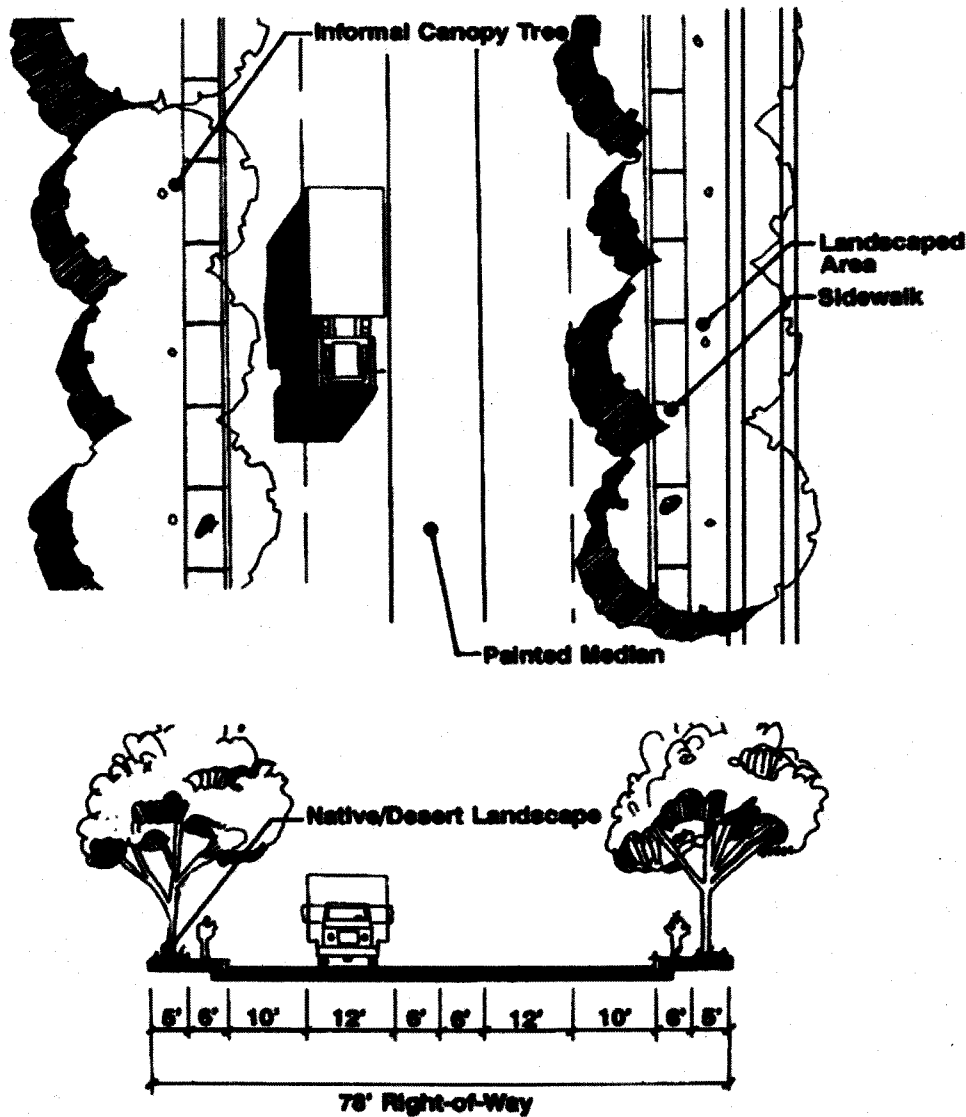


Note: Typical channel section - widths may vary.

Industrial Collector Streetscape (78' ROW)

The streetscape proposed for the Industrial Collector roadway classification is shown on **Figure 3.4-16, Industrial Collector Streetscape**, which illustrates Avenue 60 adjacent to heavy industrial and airport mixed use areas. Informal canopy trees should be used to frame the roadway, augmented by native/desert ground cover. A painted median is proposed.

Figure 3.4-16, Industrial Collector Streetscape



Collector Streetscape (74' ROW)

Collector roadways provide access to residential areas beyond the arterial roadways. Pedestrian and bicycle uses will be of a major concern in the development of the landscape plan for these intimate streetscapes. Front yard landscapes may be increased along this low speed roadway to create a more detailed streetscape of interesting forms, colors, textures, odors and sounds. Residential units front this roadway and access to driveways and compatibility of right-of-way landscapes to residential front yard landscapes must be carefully integrated. Two streetscapes are proposed for the Collector Streets within the Kohl Ranch Specific Plan area.

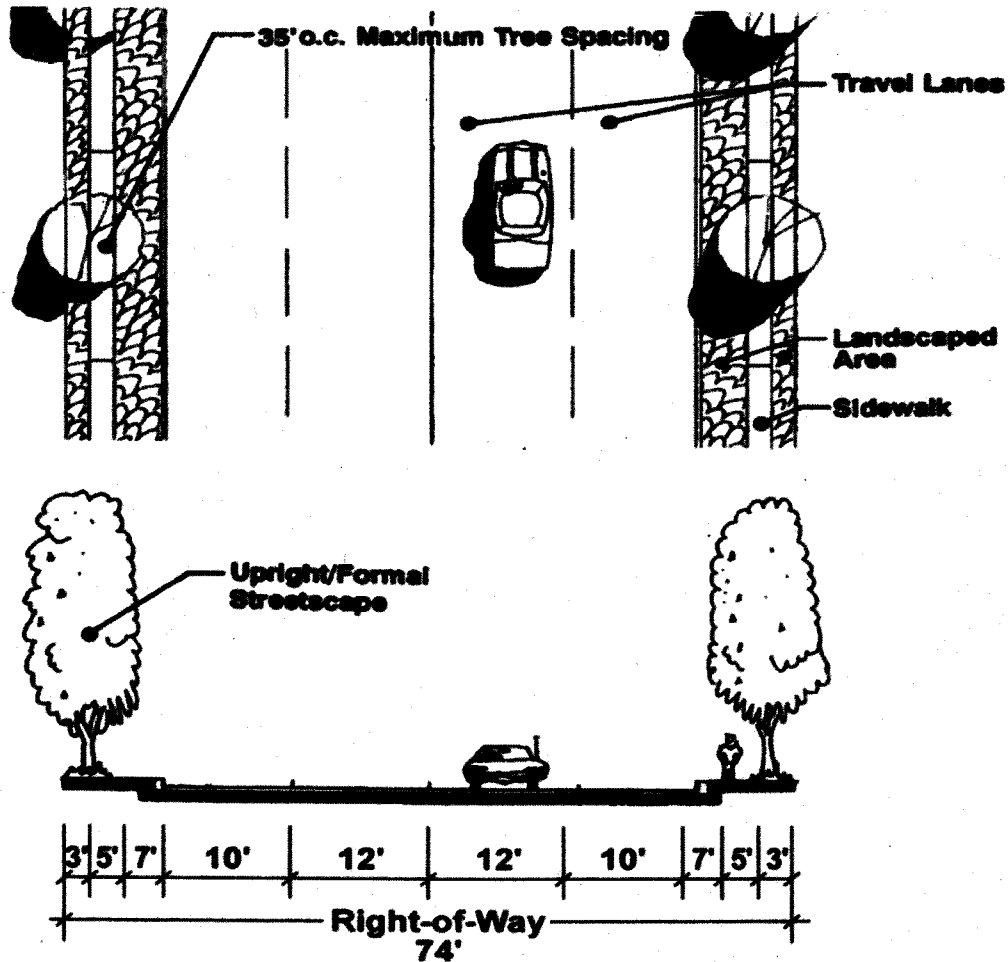


Residential Collector

Collector Streetscape 1

Figure 3.4-17, Collector Streetscape 1 illustrates the landscaping for "B" Street and Tyler Street north of Avenue 66, adjacent to Planning Area M-4. The Formal Canopy Streetscape is incorporated in Collector Streetscape 1, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area.

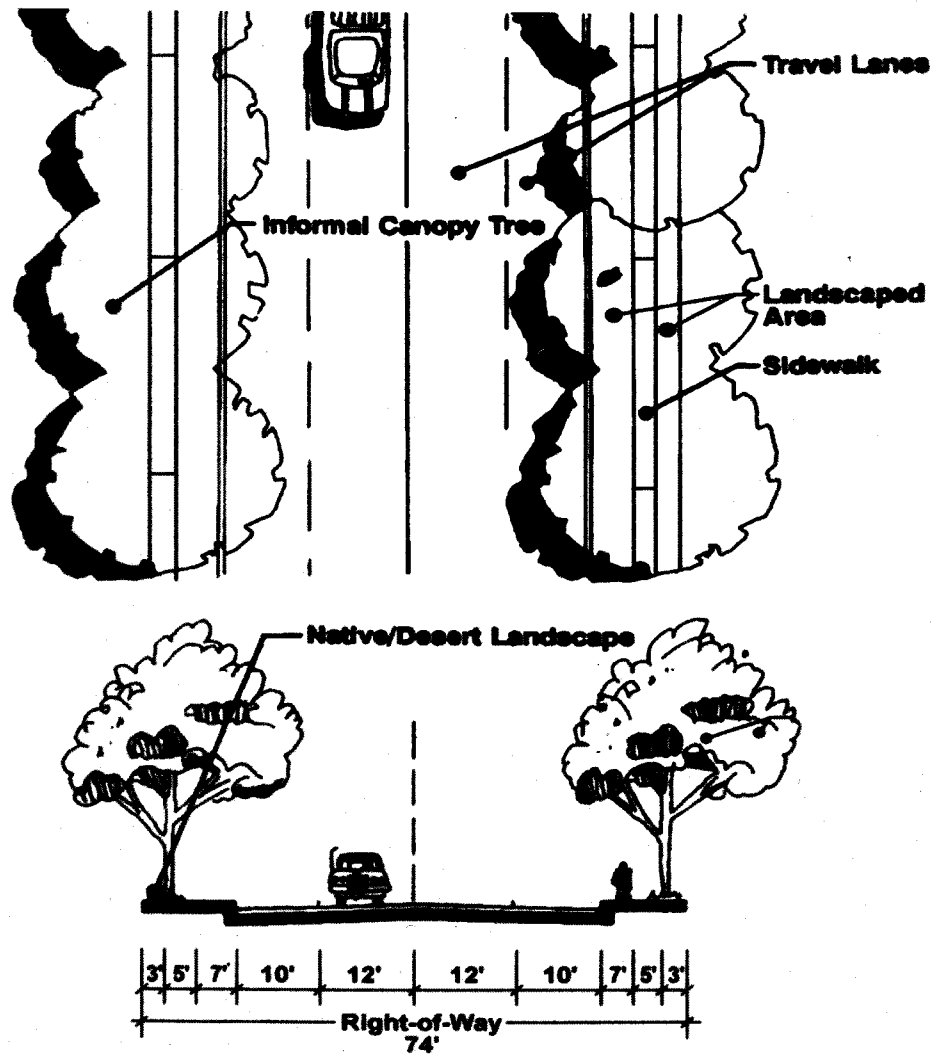
Figure 3.4-17, Collector Streetscape 1



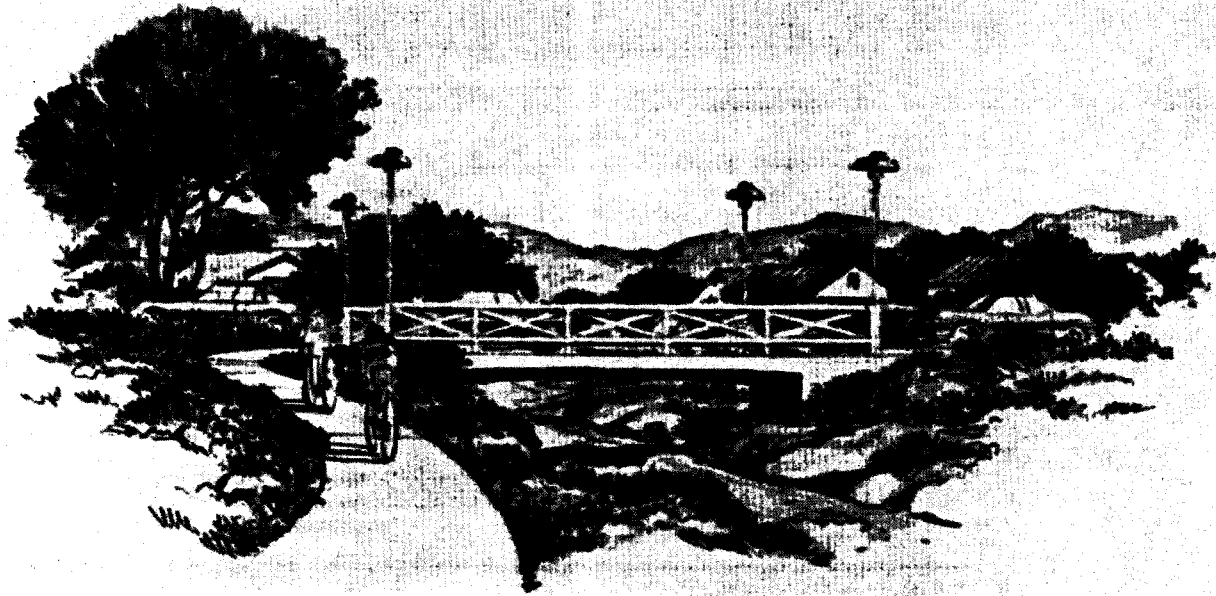
Collector Streetscape 2

The second collector streetscape is utilized on the more interior loop roads within the residential neighborhoods. It should be developed with the Informal Canopy Streetscape shown in **Figure 3.14-18, Collector Streetscape 2**.

Figure 3.14-18, Collector Streetscape 2



3.4.6 Paths and Trails



Paths and Trails

Project Trails

The Kohl Ranch shall have improved trails which will access the extensive open space and recreation areas of the site, and connect with the county trail system. The trails should be incorporated into the project drainage network, and should connect residential areas with schools, commercial areas, public facilities and parks. Improved trails dimensions and surface material should be governed by the County of Riverside standards for construction. Special landscape statements should be created for locations where trail segments intersect, and especially where they cross roadways. Boulders can be used to call attention to trail crossings and intersections, and can function as seats. A large shade tree also should be provided.

Bicycle Paths

Bicycle trails may be incorporated as a portion of the project roadways. Mountain bicycles should not be allowed off-road except on improved trails.

Tyler Street Trail

Tyler Street will contain both a Class II Bike Path and Regional Trail connection between Avenue 60 and Avenue 62 on the east side of Tyler Street. Regional trails are the main primary long distance trails within the County and are typically designed to provide linkages between communities, regional parks, and open space areas. Regional trails are designed to serve users needing soft trail surfaces, including equestrians, pedestrians, joggers, and mountain bikers and should consist of decomposed granite.

Class II Bike Paths provide a striped lane for one-way bike travel on a street or highway.

The Class II Bike Path along Tyler Street shall be a striped lane located within the right-of-way on pavement as illustrated in **Figure 3.4-19, Tyler Street Trail** or as agreed to by the Riverside County Regional Parks and Recreation District.

Figure 3.4-19, Tyler Street Trail

