

- C 2.4** The direct project related traffic impacts of new development proposals shall be mitigated via conditions of approval requiring the construction of any improvements identified as necessary to meet level of service targets.
- C 2.5** The cumulative and indirect traffic impacts of development may be mitigated through the payment of various impact mitigation fees such as County of Riverside Development Impact Fees, Road and Bridge Benefit District Fees, and Transportation Uniform Mitigation Fees to the extent that these programs provide funding for the improvement of facilities impacted by development.
- C 2.8** Riverside County shall coordinate with Caltrans, RCTC and adjacent local jurisdictions in conformance with the Riverside County Congestion Management Program to determine the appropriate LOS threshold for determining significance when reviewing development proposals that directly impact nearby State Highway facilities or city streets.
- C 3.1** Design, construct, and maintain Riverside County roadways as specified in the Riverside County Road Improvement Standards and Specifications. The standards shown in Figure C-4 may be modified by Specific Plans, Community Guidelines, or as approved by the Director of Transportation if alternative roadway standards are desirable to improve sustainability for the area.
- C 3.2** Maintain the existing transportation network, while providing for future expansion and improvement based on travel demand, and the development of alternative travel modes.
- C 3.6** Require private developers to be primarily responsible for the improvement of streets and highways that serve as access to developing commercial, industrial, and residential areas. These may include road construction or widening, installation of turning lanes and traffic signals, and the improvement of any drainage facility or other auxiliary facility necessary for the safe and efficient movement of traffic or the protection of road facilities.
- C 3.9** Design off-street loading facilities for all new commercial and industrial developments so that they do not face surrounding roadways or residential neighborhoods. Truck backing and maneuvering to access loading areas shall not be permitted on the public road system, except when specifically permitted by the Transportation Department.
- C 3.11** Generally locate commercial and industrial land uses so that they take driveway access from General Plan roadways with a classification of Secondary Highway or greater, consistent with design criteria limiting the number of such commercial access points and encouraging shared access. Exceptions to the requirement for access to a Secondary Highway or greater would be considered for isolated convenience commercial uses, such as standalone convenience stores or gas stations at an isolated off ramp in a remote area. Industrial park type developments may be provided individual parcel access via an internal network of Industrial Collector streets.

- C 3.13** Design street intersections, where appropriate, to assure the safe, efficient passage of through traffic and the negotiation of turning movements.
- C 3.18** Align right-of-way dedications with existing dedications along adjacent parcels and maintain widths consistent with the ultimate design standard of the road, including required turning lanes.
- C 3.20** Determine location of General Plan road rights of way and levels of road improvements needed based primarily upon land uses and travel demand.
- C 3.24** Provide a street network with quick and efficient routes for emergency vehicles, meeting necessary street widths, turn-around radius, secondary access, and other factors as determined by the Transportation Department in consultation with the Fire Department and other emergency service providers.
- C 3.25** Restrict on-street parking to reduce traffic congestion and improve safety in appropriate locations such as General Plan roadways.
- C 6.3** Limit access points and intersections of streets and highways based upon the road's General Plan classification and function. Require that access points be located so that they comply with Riverside County's minimum intersection spacing standards. Under special circumstances the Transportation Department may consider exceptions to this requirement

#### *Elsinore Area Plan*

- ELAP 9.1** Design and develop the vehicular roadway system per Figure 7, Circulation, and in accordance with the functional classifications and standards specified in the Circulation Element.
- ELAP 9.2** Maintain Riverside County's roadway Level of Service standards as described in the Circulation Element.

### **5.7.3 Comments Received in Response to NOP**

No comment letters were received related to transportation/traffic in response to the Notice of Preparation (NOP). However, Caltrans did provide a comment letter to the applicant as part of the applicant's Encroachment Permit Application process with comments related to the Traffic Impact Analysis (TIA). The applicant, in coordination with the County, incorporated some changes to the TIA in response to Caltrans comments and provided a written response back to Caltrans.

### **5.7.4 Project Design Considerations**

Design considerations refer to ways in which the proposed Project will limit or mitigate for potential impacts through the design of the project. The proposed Project will include the following improvements:

#### Phase I

- Temescal Canyon Road will be constructed to its ultimate half section as an Arterial from Indian Truck Trail to the northerly project boundary.
- Construction of a raised median along Temescal Canyon Road to prohibit left turns from Driveway 1.
- Provide stop sign control at the project driveways until signal warrants are met.
- The developer shall construct a 450 foot (minimum) northbound left turn lane at Driveway 2.
- The developer shall construct a 300 foot (minimum) northbound left turn lane at Driveway 3.
- The developer shall construct a 100 foot (minimum) northbound left turn lane at Driveway 4.

#### Phase II

- Install a stop sign control and add one eastbound right turn lane at the Temescal Canyon Road / Driveway 1 intersection in Phase II (funded by the Project Developer).
- Add one northbound left turn lane, add one eastbound left turn lane, and add one eastbound right turn lane at the Temescal Canyon Road / Driveway 2 intersection in Phase II (funded by the Project Developer).
- Add one northbound left turn lane, add one eastbound left turn lane, and add one eastbound right turn lane at the Temescal Canyon Road / Driveway 3 intersection in Phase II (funded by the Project Developer).
- Add one northbound left turn lane, and provide eastbound thru lane improvements at the Temescal Canyon Road / Driveway 4 intersection in Phase II (funded by the Project Developer).

### **5.7.5 Thresholds of Significance**

The Riverside County Planning Department has not established local CEQA significance thresholds as described in Section 15064.7 of the State CEQA Guidelines. The Riverside County Planning Department generally utilizes the CEQA significance thresholds in Appendix G ("Environmental Checklist") of the State CEQA Guidelines. The Environmental Checklist prepared by the County for the Project (see Appendix A of this document) indicates that impacts related the Toscana Village at Temescal Valley Project may be considered potentially significant if the proposed project would:

- conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit; and/or

- conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways.

### 5.7.6 Environmental Impacts before Mitigation

*Due to the repetitive nature of the information and analysis presented herein, the first two thresholds are have been combined as presented below, and are analyzed simultaneously.*

**Threshold:** Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit; or

Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways.

#### **Intersections**

Intersections will continue to operate at acceptable LOS in the E+P, E+A+P (Phase I) and E+A+P (Phase II) conditions as reflected in **Table 5.0-Y, Intersection Analysis**, below.

#### **Freeway Segments**

As reflected in **Table 5.0-Z, Freeway Segment Analysis**, below, the following freeway segment is anticipated to operate at an unacceptable LOS during the PM peak hour in the E+P, E+A+P (Phase I) and E+A+P (Phase II) conditions as reflected below. All other study area freeway segments are anticipated to operate an acceptable LOS D or better during peak hours.

- I-15 southbound segment north of Indian Truck Trail

#### **Freeway Ramps**

As reflected in **Table 5.0-AA: Freeway Ramp Analysis**, below, the following freeway ramp continues to operate at an unacceptable LOS in both the E+P and E+A+P (Phase I) and E+A+P (Phase II) conditions. All other study area ramp locations are operating an acceptable LOS D or better during peak hours.

- I-15 southbound off-ramp at Indian Truck Trail

Table 5.0-Y: Intersection Analysis

No	Intersection	Traffic Control	Target LOS	No Project Existing						Plus Project / Without Mitigation													
				AM			PM			Existing + Project (Phase I & II)				Existing + Ambient + Project (Phase I)				Existing + Ambient + Project (Phase II)					
				Delay (sec)	LOS	Density	Delay (sec)	LOS	Density	Change	LOS	Change	LOS	Change	LOS	Change	LOS	Change	LOS	Change	LOS	Change	
1	Temescal Cyn Rd/Indian Truck Trl	TS	D	24.1	C	19.8	B	26.7	C	27.0	C	26.5	2.4	C	24.1	4.3	C	26.9	2.8	C	27.3	7.5	C
2	I-15 NB Ramps/Indian Truck Trl	TS	D	28.7	C	27.7	C	28.4	C	26.7	(1.0)	27.6	(1.1)	C	26.6	(1.1)	C	28.5	(0.2)	C	26.8	(0.9)	C
3	I-15 SB Ramps/Indian Truck Trl	TS	D	18.7	B	21.0	C	21.9	C	24.9	3.9	-20.1	1.4	C	22.6	1.6	C	21.8	3.1	C	25.0	4.0	C
4	Campbell Ranch Rd/Indian Truck Trl	TS	D	17.6	B	18.7	B	18.5	B	20.2	1.5	18.2	0.6	B	19.4	0.7	B	18.6	1.0	B	20.3	1.6	C
5	Campbell Ranch Rd/Temescal Cyn Rd	TS	D	24.0	C	18.7	B	23.2	(0.8)	16.4	(2.3)	23.8	(0.2)	C	17.6	(1.1)	B	23.8	(0.2)	C	16.6	(2.1)	B
6	Campbell Ranch Rd/Mayhew Cyn Rd	CSS	D	9.8	A	9.8	A	10.2	B	10.3	0.5	10.1	0.3	B	10.1	0.3	B	10.3	0.5	B	10.4	0.6	B
7	Temescal Cyn Rd/Horsechief Rd	CSS	D	15.3	C	10.3	B	17.2	C	13.3	3.0	16.7	1.4	C	11.5	1.2	C	16.0	2.7	C	13.5	3.2	B
8	Temescal Cyn Rd / Dwy 1	CSS	D	Does Not Exist		Does Not Exist		10.7	B	19.2	18.2	9.6	9.6	A	11.2	11.2	B	10.7	10.7	B	19.4	19.4	C
9	Temescal Cyn Rd / Dwy 2	CSS	D	Does Not Exist		Does Not Exist		13.8	B	31.8	31.8	10.8	10.8	B	12.1	12.1	B	13.8	13.8	B	32.1	32.1	D
10	Temescal Cyn Rd / Dwy 3	CSS	D	Does Not Exist		Does Not Exist		12.4	B	26.2	26.2	9.7	9.7	A	10.4	10.4	B	12.4	12.4	B	26.5	26.5	D
11	Temescal Cyn Rd / Dwy 4	CSS	D	Does Not Exist		Does Not Exist		10.8	B	17.3	17.3	9.7	9.7	A	10.4	10.4	B	10.9	10.9	B	17.5	17.5	C

Source: TSI, Tables 3-1, 5-1, 5-4, 5-10  
TS = Traffic Signal; CSS = Cross Street Stop; **CSS** = Cross Street Stop (Project Design Feature)

**BOLD** = Exceeds Target LOS  
**Shaded** = Significant Impact

Table 5.0-Z: Freeway Segments Analysis

Freeway	Segment	Lanes	Target LOS	No Project Existing						Plus Project / Without Mitigation																
				Existing			Existing + Project (Phase I & II)			Existing + Ambient + Project (Phase I)				Existing + Ambient + Project (Phase II)												
				Volumes	Density	LOS	Volumes	Density	LOS	Volumes	Density	LOS	Volumes	Density	LOS	Volumes	Density	LOS								
I-15 Southbound	North of Indian Truck Trail	3	D	2,868	5,427	16.2	33.9	B	D	3,076	5,568	16.8	35.5	B	3,084	5,599	16.8	35.8	B	3,195	5,785	17.4	38.0	B		
	South of Indian Truck Trail	3	D	2,630	4,942	16.5	29.3	B	D	2,878	5,012	15.5	29.9	B	D	2,934	5,112	16.0	30.8	B	D	3,018	5,298	16.5	32.6	B
I-15 Northbound	North of Indian Truck Trail	3	D	3,262	3,121	17.8	17.0	B	B	3,372	3,262	18.4	17.8	C	B	3,376	3,241	18.5	17.7	C	B	3,502	3,387	19.2	18.5	C
	South of Indian Truck Trail	3	D	3,722	3,722	20.5	18.4	C	C	3,770	3,442	20.8	18.8	C	C	3,834	3,505	21.2	19.2	C	C	3,946	3,666	21.9	20.1	C


Source: TSI, Tables 3-3, 5-3, 5-6, 5-12  
**BOLD** = Exceeds Target LOS  
**Shaded** = Significant Impact



Table 5.0-AA: Freeway Ramp Analysis

Freeway	Ramp Location	Lanes <sup>1</sup>	Target LOS	No Project						Plus Project / Without Mitigation																	
				Existing			Existing + Project (Phase I & II)			Existing + Ambient + Project (Phase I)			Existing + Ambient + Project (Phase II)														
				Volumes	Density	LOS	Volumes	Density	LOS	Volumes	Density	LOS	Volumes	Density	LOS												
AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM														
I-15	SB Off-Ramp at Indian Truck Trl	3	D	317	706	22.7	35.7	C	E	427	847	23.6	36.6	C	E	382	783	23.5	36.6	C	E	440	876	24.3	37.5	C	E
Southbound	SB On-Ramp at Indian Truck Trl	3	D	181	221	18.7	29.8	B	D	256	380	19.3	31.1	B	D	232	296	19.3	31.0	B	D	263	389	19.9	32.2	B	D
I-15	NB Off-Ramp at Indian Truck Trl	3	D	162	132	24.2	23.3	C	C	272	273	25.0	24.3	C	C	224	197	24.9	24.1	C	C	278	278	25.7	25.0	C	C
Northbound	NB On-Ramp at Indian Truck Trl	3	D	622	383	24.8	22.2	C	C	697	542	25.2	23.3	C	C	682	481	25.3	23.0	C	C	722	557	26.0	24.1	C	C

Source: TSI, Tables 3-2, 5-2, 5-5, 5-11

- Notes  
 1. Existing ramp locations consist of 1 lane (on/off ramps)  
**BOLD** = Exceeds Target LOS  
 Significant Impact



### **Level of Impact Following Implementation of Mitigation Measures**

Traffic mitigation is typically in the form of improvements to the roadway or intersection that are engineered to enable more cars and trucks to pass through an intersection or along a roadway. For example a traffic signal organizes the traffic at the intersection and allows each leg of the intersection a certain amount of time to complete their movement. While this may result in a little more delay for some legs of the intersection, the signal light ensures that all turning movements get a chance to occur which reduces the amount of backup associated with vehicles waiting to make a turn. Widening of a roadway increases capacity by adding physical area to the street, changes in timing of a signal light can improve the efficiency of traffic flow, adding a median to prevent turning movements or even striping of a roadway can help with traffic flow and efficiency. With implementation of mitigation measures **Trans 1** thru **Trans 6**, impacts will be less than significant.

The freeway ramp analysis shows the ramps identified in **Table 5.0-AB: Project Freeway Improvements** operating at unacceptable levels of service in the existing condition. The Project will contribute additional traffic to the freeway contributing to this deficiency.

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Table 5.0-AB: Project Freeway Improvements

Freeway	Location	Type	Jurisdiction	Target LOS	LOS without Improvements			LOS w/ Improvement		
					Ramp Lanes	Freeway Lanes	AM PM	Ramp Lanes	Freeway Lanes	AM PM
I-15 Southbound	SB Off-ramp at Indian Truck Trail	Ramp	Caltrans	D	1	C	E	2	B	D
	North of Indian Truck Trail	Segment	Caltrans	D	3	B	E	4	B	C
I-15 Southbound	SB Off-Ramp at Indian Truck Trail	Ramp	Caltrans	D	1	C	E	2	B	D
	North of Indian Truck Trail	Segment	Caltrans	D	3	B	E	4	B	C

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As reflected in **Table 5.0-AB** above, ramp and lane improvements will be necessary to reduce impacts to ramp and freeway segments. However, as identified in the cumulative discussion in Section 6.0 and below, all state freeways are under the authority of Caltrans. Thus, where traffic increases would affect freeways, there is no mechanism for development project proponents to pay fees or make fair share contributions toward improving mainline freeway segments or freeway ramps. Even if there were such a mechanism, there is no way to ensure that such payments would be directed to a specific freeway improvement project. Consequently, there are no feasible mitigation measures for impacts to freeways; impacts would be significant and unavoidable. (GP FEIR, p. 5-13).

Further, the County of Riverside does not have jurisdiction over the freeway improvements. Hence, the timing of improvements needed to improve levels of service on a regional basis would be determined by Caltrans based upon need and the availability of funding. The priority and timing of these improvements cannot be determined at this time, nor are they under the sole control of the Project proponent or the County of Riverside to implement. Thus, it is possible that the required improvements will not be constructed in time to mitigate any impacts the project may contribute upon freeways to below the level of significance. Therefore, because of the uncertainty in timing of such improvements, lack of feasible mitigation measures and funding mechanisms in which to contribute fair share fees for impacts to state freeways, any project impacts to freeways are **significant and unavoidable** and a **statement of overriding consideration** would be required to be adopted by the County of Riverside prior to project approval.

### **Cumulative Condition**

Traffic from other projects within the study area is expected to have an impact on LOS. These projects were included in the TIA pursuant to discussions with County of Riverside staff. To ascertain Project impacts in combination with other approved and pending (cumulative) Projects in the vicinity, an analysis of the existing condition, plus traffic from ambient growth, plus cumulative traffic conditions, plus Phase II project conditions (E+A+C+P) is discussed in Section 6.1.7 to address the Projects potential cumulative traffic contribution.

### **General Plan Buildout**

Section 6.1.7 also describes the general plan buildout condition. This scenario, describes the general plan buildout year traffic volumes (2035) which were derived from the regional travel demand model currently being used for long range planning in Riverside County to analyze potential Project impacts at the buildout year (B+P).

### **5.7.7 Proposed Mitigation Measures**

An EIR is required to describe feasible mitigation measures which could minimize significant adverse impacts pursuant to State *CEQA Guidelines* section 15126.4. Mitigation measures were evaluated for their ability to eliminate the potential significant adverse impacts upon traffic or to reduce impacts to below the level of significance.

### **Construction Mitigation**

**MM Trans 1:** A Construction Traffic Control Plan shall be prepared for each phase of development (Phase I and Phase II) by the implementing developer and submitted to the County of Riverside Transportation Department for approval prior to the issuance of building permits for the Project. The Construction Traffic Control Plan shall include the estimated day(s), time(s) and duration of any lane closures that are anticipated to be required due to Project construction.

The Construction Traffic Control Plan shall include measures such as, but not limited to, signage, flagmen, cones, advance community notice, or other acceptable measures to the satisfaction of the County of Riverside Transportation Department. The purpose of the measures shall be to safely guide motorists, cyclists, and pedestrians, minimize traffic impacts and ensure the safe and even flow of traffic consistent with County level of service standards and safety requirements.

The implementing developer or its general contractor shall be required to notify the County of Riverside Transportation Department at least five (5) business days in advance of any planned lane closure that will be caused by Project construction. The County shall evaluate any other known lane closures, construction activities or special events which may conflict with the Project's scheduled lane closure or create additional impacts to traffic flow and, if deemed necessary by the County of Riverside Transportation Department, the Project's lane closure may be postponed or rescheduled.

**MM Trans 2:** Sight distance at the Project driveways shall be reviewed with respect to the County's sight distance standards at the time of preparation of final grading, landscape, and street improvement plans.

**MM Trans 3:** Signing/stripping shall be implemented in conjunction with detailed construction plans for the Project site.

### **Operational Mitigation**

#### **Phase II**

**MM Trans 4:** Prior to issuance of occupancy permit for Phase II Buildings, the implementing developer shall install a traffic signal at the intersection of Temescal Canyon Road and Project Driveway No. 2.

**MM Trans 5:** Prior to issuance of occupancy permit for Phase II Buildings, the implementing developer shall install a traffic signal at the intersection of Temescal Canyon Road and Project Driveway No. 3.

**MM Trans 6:** Prior to issuance of occupancy permit for Phase II Buildings, the implementing developer shall install a cross street stop at the intersection of Temescal Canyon Road and Project Driveway No. 4.

**MM Trans 7:** Prior to issuance of occupancy permit for Phase II Buildings, the implementing developer shall install the following improvements at the intersection of Temescal Canyon Road and Indian Truck Trail:

- A second southbound through lane; and
- One southbound right turn overlap phase.

### 5.7.8 Summary of Project-Specific Environmental Effects after Mitigation Measures are Implemented

Impacts related to new or altered maintenance of roads, conflicts with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, and impacts associated with the provision of new or physically altered bike trails, were all found to be less than significant and no mitigation measures are required. Mitigation measures **MM Trans 1** and **MM Trans 3** will ensure potential impacts related to circulation during the Project's construction are less than significant.

In addition to mitigation measure **MM Trans 1** through **MM Trans 3**, mitigation measures **MM Trans 4** through **MM Trans 7** are applicable to development of Phase II. Because there are no feasible mitigation measures or available funding mechanisms with which to contribute fair share fees for potential project-specific impacts related to freeways, impacts are **significant and unavoidable** and a **statement of overriding consideration is required**.

Cumulative traffic impacts are discussed in Section 6.1.7 of this EIR.

### 5.7.9 References

The following references were used in the preparation of this section of the DEIR:

- |                |  |
|----------------|--|
| COR ORD<br>695 | County of Riverside, Development Impact Fee-Ordinance 659, <i>An Ordinance of the County of Riverside, Amending Ordinance 659 Establishing A Development Impact Fee Program</i> . (Available at <a href="http://www.rivcocob.org/ords/600/659.pdf">http://www.rivcocob.org/ords/600/659.pdf</a> , accessed July 27, 2017.)   |
| EAP            | Riverside County, <i>General Plan General Plan Amendment No.960, Elsinore Area Plan</i> , dated April 2017. (Available at <a href="http://planning.rctlma.org/Portals/0/genplan/general%20Plan%202017/areaplans/EAP_041117.pdf?ver=2017-10-06-094258-763">http://planning.rctlma.org/Portals/0/genplan/general Plan 2017/areaplans/EAP_041117.pdf?ver=2017-10-06-094258-763</a> , accessed March 12, 2018.)  |
| GP EIR         | County of Riverside, <i>General Plan Final Environmental Impact Report No. 521</i> , certified December 8, 2015. (Available at <a href="http://planning.rctlma.org/ZoningInformation/GeneralPlan/GeneralPlanAmendmentNo960EIRNo521CAPFebruary2015/DraftEnvironmentalImpactReportNo521.aspx">http://planning.rctlma.org/ZoningInformation/GeneralPlan/GeneralPlanAmendmentNo960EIRNo521CAPFebruary2015/DraftEnvironmentalImpactReportNo521.aspx</a> , accessed August 2, 2017.) |
| RCTC           | Riverside County Transportation Commission, <i>2011 Riverside County Congestion Management Program</i> , December 14, 2011. (Available at <a href="http://www.rctcdev.info/uploads/media_items/congestionmanagementprogram.original.pdf">http://www.rctcdev.info/uploads/media_items/congestionmanagementprogram.original.pdf</a> , accessed July 18, 2017.)   |

RTP	Regional Transportation Plan, <i>Regional Transportation Plan 2016-2040 Sustainable Communities Strategy</i> . (Available at <a href="http://scagrtpscs.net/Documents/2016/final/f2016RTPSCS.pdf">http://scagrtpscs.net/Documents/2016/final/f2016RTPSCS.pdf</a> , accessed July 27, 2017.)
SCAG	Southern California Association of Governments (Available at <a href="http://www.scag.ca.gov/about/Pages/Home.aspx">http://www.scag.ca.gov/about/Pages/Home.aspx</a> , accessed July 27, 2017.)
TSI	Trames Solutions, Inc., <i>Temescal Valley Gateway (CUP 3712) Traffic Impact Analysis</i> , dated March 6, 2018. (Appendix G).
TUMF	Transportation Uniform Mitigation Fee Program, <i>Authorizing Participation in the Western Riverside County Transportation Uniform Mitigation Fee Program</i> . (Available at <a href="http://www.rivcocob.org/ords/800/824.pdf">http://www.rivcocob.org/ords/800/824.pdf</a> , accessed July 27, 2017.)

## 5.8 Tribal Cultural Resources

The focus of this section is to analyze potential impacts related to Tribal Cultural Resources. The analysis in this section is based on the consultation between Riverside County and the Pechanga Band of Luiseño Indians, the Soboba Band of Luiseño Indians, and the Phase I Cultural Resources Assessment.

### 5.8.1 Setting

#### Consultation with the Soboba Band of Luiseno Indians and the Pechanga Band of Luiseño Indians

In consultation with the Pechanga Band of Luiseño Indians (Pechanga Tribe), the Pechanga Tribe asserts that the proposed Project area is in an area associated with the 'Atdaxum Luiseño as evidenced by the existence of 'Atdaxum place names, a Traditional Cultural Property (TCP), several large village complexes, toota yixelval (rock art, pictographs, petroglyphs), an extensive artifact record, and known human remains in the vicinity of the Project. This culturally sensitive area is affiliated with the Pechanga Temecula Band of Luiseño Indians because of the tribe's cultural ties to this area.

During consultation with the Pechanga Tribe, no specific place name was identified for the current project area. Although there are several large villages complexes in the area, none are present within the current project area. Although there is rock art in the vicinity, there is none present within the project area. The project area is within the limits of a larger TCP. Pechanga provided no information that documents the site contains Tribal Cultural Resources nor that any would be impacted by the project.

During consultation with the Soboba Tribe information was provided by the Tribe that there is a village associated with the project area. The tribe also expressed concern for subsurface cultural resources to be present but provided no information that documents the site contains Tribal Cultural Resources nor that any would be impacted by the project.

## **5.8.2 Related Regulations**

### **5.8.2.1 State Regulations**

#### **Senate Bill 18**

California Senate Bill (SB) 18, effective September 2004, requires local government to notify and consult with California Native American tribes when the local government is considering adoption or amendment of a general or specific plan. Prior to adoption of a specific plan, a local government must refer the proposed action to those tribes that are on the Native American Heritage Commission contact list and have traditional lands located within the city or county's jurisdiction. Pursuant to Government Code §65352.3, prior to adoption or any amendment to a General Plan, proposed on or after March 1, 2005, the city or county shall conduct consultations with California Native American tribes for the purpose of preserving or mitigation impacts to Cultural Places. The tribe(s) has 90 days from when the tribe is contacted by the city or county in which to request a consultation.

#### **Assembly Bill 52**

Assembly Bill 52, effective July 2015, Section 1 of the bill states the legislature's intent as follows: In recognition of California Native American tribal sovereignty and the unique relationship of California local governments and public agencies with California Native American tribal governments, and respecting the interests and roles of project proponents, it is the intent of the Legislature, in enacting this act, to accomplish all of the following:

1. Recognize that California Native American prehistoric, historic, archaeological, cultural, and sacred places are essential elements in tribal cultural traditions, heritages, and identities.
2. Establish a new category of resources in the California Environmental Quality Act called "tribal cultural resources" that considers the tribal cultural values in addition to the scientific and archaeological values when determining impacts and mitigation.
3. Establish examples of mitigation measures for tribal cultural resources that uphold the existing mitigation preference for historical and archaeological resources of preservation in place, if feasible.
4. Recognize that California Native American tribes may have expertise regarding their tribal history and practices, which concern the tribal cultural resources with which they are traditionally and culturally affiliated. Because the California Environmental Quality Act calls for a sufficient degree of analysis, tribal knowledge about the land and tribal cultural resources at issue should be included in environmental assessments for projects that may have a significant impact on those resources.
5. In recognition of their governmental status, establish a meaningful consultation process between California Native American tribal governments and lead agencies, respecting the interests and roles of all California Native American tribes and project proponents, and the level of required confidentiality concerning tribal cultural

resources, at the earliest possible point in the California Environmental Quality Act environmental review process, so that tribal cultural resources can be Discussion Draft Technical Advisory: AB 52 and Tribal Cultural Resources in CEQA.

### **California Health and Safety Code §7050.5, §7051, and §7054**

California Health and Safety Code §7050.5, §7051, and §7054 collectively address the illegality of interference with human burial remains as well as the disposition of Native American burials in archaeological sites. The law protects such remains from disturbance, vandalism, or inadvertent destruction, and establishes procedures to be implemented if Native American skeletal remains are discovered during construction of a project, including the treatment of remains prior to, during, and after evaluation, and reburial procedures.

#### **5.8.2.2 Local Regulations**

##### **Riverside County General Plan Multipurpose and Open Space Element**

The following policies of the Multipurpose and Open Space Element are intended to ensure that cultural resources are appropriately considered:

- OS 19.1 Cultural resources (both prehistoric and historic) are a valued part of the history of the County of Riverside.
- OS 19.2 The County of Riverside Shall establish a Cultural Resources Program in consultation with Tribes and the professional cultural resources consulting community that, at a minimum would address each of the following: application of the Cultural Resources Program to projects subject to environmental review; government-to-government consultation; application processing requirements; information database(s); confidentiality of site locations; content and review of technical studies; professional consultant qualifications and requirements; site monitoring; examples of preservation and mitigation techniques and methods; curation and the descendant community consultation requirements of local, state and federal law.
- OS 19.3 Review proposed development for the possibility of cultural resources and for compliance with the cultural resources program.
- OS 19.4 To the extent feasible, designate as open space and allocated resources and/or tax credits to prioritize the protection of cultural resources preserved in place or left in an undisturbed state.
- OS 19.5 Exercise sensitivity and respect for human remains from both prehistoric and historic time periods and comply with all applicable laws concerning such remains.



### 5.8.3 Comments Received in Response to NOP

Two comment letters were received in response to the NOP and related to cultural resources (Appendix A). One letter was from the Native American Heritage Commission (NAHC) which included a recommendation for the County as lead agency to consult with all California Native American tribes that are traditionally and culturally affiliated with the geographic area of the Project as early as possible in order to avoid inadvertent discoveries of Native American human remains and best protect tribal cultural resources.

One comment letter was from the Pechanga Tribe, which indicated that they do not concur with the County's AB52 consultation conclusion letter for GPA 01146 and CUP 03712 dated June 5, 2017 and requested to reopen AB52 consultation.

### 5.8.4 Project Design Considerations

There are no proposed Project Design features to reduce potential impacts to tribal cultural resources as none were identified within the Project's development footprint (including onsite and offsite grading and infrastructure improvements).

### 5.8.5 Thresholds of Significance

The Riverside County Planning Department has not established local CEQA significance thresholds as described in Section 15064.7 of the State CEQA Guidelines. The Riverside County Planning Department generally utilizes the CEQA significance thresholds in Appendix G ("Environmental Checklist") of the State CEQA Guidelines. The Environmental Checklist prepared by the County for the Project (see Appendix A of this document) indicates that impacts related the Toscana Village at Temescal Valley Project may be considered potentially significant if the proposed project would:

- Would the Project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native tribe, and that is:
  - Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code Section 5020.1(k)?
  - A resource determined by the lead agency, in its discretion and supported by substantial evidence to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1? In applying the criteria set forth in the subdivision (c) of the Public Resources Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe?

### 5.8.6 Environmental Impacts before Mitigation

**Threshold:** Cause a substantial adverse change in the significance of a tribal cultural resource as defined in Public Resources Code 217074.

No Tribal Cultural Resources are present and none were identified by the tribes .

The County provided notice to the Pechanga Tribe, the Agua Caliente Band of Cahuilla Indians, and the Soboba Band of Luiseño Indians pursuant to AB52 in July 2015. In response the Pechanga Tribe (August 15, 2015) and Soboba Band of Luiseño Indians (on August 13, 2015) requested to consult with the County. Subsequent to this initiation of consultation, the project was placed on hold until May 2016. When the Project was active again consultation was re-initiated. A meeting was held with the Soboba Band of Luiseño Indians on February 16, 2017 in which they requested a tribal monitor.

The Pechanga Tribe and the County had several discussions regarding the proposed Project in both May 2016 and March 2017. The County sent their AB52 consultation conclusion letter for GPA01146 and CUP 03712 dated June 5, 2017 to the Pechanga Tribe, with which the Pechanga Tribe did not concur.

The County received a letter from the Pechanga Tribe dated August 2, 2017 where the Pechanga Tribe requests the reopening of AB52 consultation with the County to discuss how the DEIR will address tribal cultural resources and further requests to be involved in reviewing drafts of the language for the tribal cultural resource section of the DEIR prior to its release for public review. The Pechanga Tribe asserts that the proposed Project area is in an area associated with the 'Atdaxum Luiseño as evidenced by the existence of 'Atdaxum place names, , several large village complexes, toota yixelval (rock art, pictographs, petroglyphs), an extensive artifact record, and known human remains in the vicinity of the Project. The project area also falls within the limits of a large Traditional Cultural Property (TCP). This culturally sensitive area is affiliated with the Pechanga Temecula Band of Luiseño Indians because of the tribe's cultural ties to this area. In the letter from the Pechanga Tribe, the Pechanga Tribe lists relevant regulations for the County to ensure compliance with and then discusses potential impacts to tribal cultural resources and recommends mitigation measures should the project uncover potential tribal cultural resources.

The County had an additional discussion with the Pechanga Tribe on April 18, 2018 and provided revised and final conditions of approval to the Pechanga Tribe. On April 18, 2018 The Pechanga Tribe responded with an email indicating with the inclusion of the conditions provided they consider AB 52 consultation complete.

The County is requiring the following Conditions of Approval (COA) be implemented by the applicant for the Project:

- PLANNING-CUL. 1: If human remains are found on this site, the developer/permit holder or any successor in interest shall comply with State Health and Safety Code Section 7050.5.
- PLANNING-CUL. 2: The developer/permit holder or any successor in interest shall comply with the following for the life of the CUP 03712 permit.

- a. If during ground disturbance activities, unanticipated cultural resources\* are discovered, the following procedures shall be followed:
  - b. All ground disturbance activities within 100 feet of the discovered cultural resource shall be halted and the applicant shall call the County Archaeologist immediately upon discovery of the cultural resource. A meeting shall be convened between the developer, the project archaeologist\*\*, the Native American tribal representative (or other appropriate ethnic/cultural group representative), and the County Archaeologist to discuss the significance of the find. At the meeting with the aforementioned parties, a decision is to be made, with the concurrence of the County Archaeologist, as to the appropriate treatment (documentation, recovery, avoidance, etc) for the cultural resource. Further ground disturbance shall not resume within the area of the discovery until the appropriate treatment has been accomplished. \*A cultural resource site is defined, for this condition, as being a feature and/or three or more artifacts in close association with each other, but may include fewer artifacts if the area of the find is determined to be of significance due to sacred or cultural importance. \*\* If not already employed by the project developer, a County approved archaeologist shall be employed by the project developer to assess the value/importance of the cultural resource, attend the meeting described above, and continue monitoring of all future site grading activities as necessary.
- 60. PLANNING-CUL.1: Prior to issuance of grading permits, the developer/permit applicant shall enter into an agreement with the consulting tribe(s) for a Native American Monitor. The Native American Monitor(s) shall be on-site during all initial ground disturbing activities and excavation of each portion of the project site including clearing, grubbing, tree removals, grading and trenching. In conjunction with the Archaeological Monitor(s), the Native American Monitor(s) shall have the authority to temporarily divert, redirect or halt the ground disturbance activities to allow identification, evaluation, and potential recovery of cultural resources. The developer/permit applicant shall submit a fully executed copy of the agreement to the County Archeologist to ensure compliance with this condition of approval. Upon verification, the Archaeologist shall clear this condition. This agreement shall not modify any condition of approval or mitigation measure.
  - 60. PLANNING-CUL. 2: Prior to issuance of grading permits: The applicant/developer shall provide evidence to the County of Riverside Planning Department that a County certified professional archaeologist (Project Archaeologist) has been contracted to implement a Cultural Resource Monitoring Program. A Cultural Resource Monitoring Plan shall be developed that addresses the details of all activities and provides procedures that must be followed in order to reduce the impacts to cultural and historic resources to a level that is less than significant as well as address potential impacts to undiscovered buried archaeological resources associated with this project. A fully executed copy of the contract and a wet-signed copy of the Monitoring Plan shall be provided to the County Archaeologist to ensure compliance with this condition of approval. Working directly under the Project Archaeologist, an adequate number of qualified Archaeological Monitors shall be present to ensure that all earth moving activities are observed and shall be on-site during all grading activities for areas to be monitored including off-site improvements. Inspections will vary based on the rate of excavation, the materials excavated, and the presence and abundance of artifacts and features. The frequency and location of inspections will be determined by the Project Archaeologist.

- 70. PLANNING-CUL. 1: Prior to Grading Permit Final Inspection, the landowner(s) shall relinquish ownership of all cultural resources that are unearthed on the Project property during any ground-disturbing activities, including previous investigations and /or Phase III data recovery. Historic Resources – all historic archaeological materials recovered during the archaeological investigations (this includes collections made during an earlier project, such as testing of archaeological sites that took place years ago), shall be curated at the Western Science Center, a Riverside County curation facility that meets State Resources Department Office of Historic Preservation Guidelines for the Curation of Archaeological Resources ensuring access and use pursuant to the Guidelines. Prehistoric Resources – One of the following treatments shall be applied.
  - a. Reburial of the resources on the Project property. The measures of reburial shall include, at least, the following: measures to protect the reburial area from any future impacts. Reburial shall not occur until all required cataloging, analysis and studies have been completed on the cultural resources, with an exception that sacred items, burial goods and Native American human remains are excluded. Any reburial processes shall be culturally appropriate. Listing of contents and location of the reburial shall be included in the confidential Phase IV Report. The Phase IV Report shall be filed with the County under a confidential cover and not subject to a Public Records request.
  - b. If reburial is not agreed upon by the Consulting Tribes then the resources shall be curated at a culturally appropriate manner at the Western Science Center, a Riverside County curation facility that meets State Resources Department Office of Historic Preservation Guidelines for the Curation of Archaeological Resources ensuring access and use pursuant to the Guidelines. The collection and associated records shall be transferred, including title, and are to be accompanied by payment of the fees necessary for permanent curation. Evidence of curation in the form of a letter from the curation facility stating that subject archaeological materials have been received and that all fees have been paid, shall be provided by the landowner to the County. There shall be no destructive or invasive testing on sacred items, burial good and Native American human remains.
- 70 PLANNING-CUL. 2: Prior to Grading Final Inspection, a Phase IV Cultural Resources Monitoring Report shall be submitted that complies with the Riverside County Planning Department's requirements for such reports for all ground disturbing activities associated with this grading permit. The report shall follow the County of Riverside Planning Department Cultural Resources (Archaeological) Investigations Standard Scopes of Work posted on the TLMA website. The report shall include results of any feature relocation or residue analysis require as well as evidence of the required cultural sensitivity training for the construction staff held during the required pre-grade meeting and evidence that any artifacts have been treated in accordance to the procedures stipulated in the Cultural Resources Management Plan.

Potential adverse impacts to Tribal Cultural Resources are less than significant.

**5.8.7 Proposed Mitigation Measures**

No Tribal Cultural Resources were identified so there are no potentially significant impacts to mitigate. Potential adverse impacts to Tribal Cultural Resources are less than significant and no mitigation measures are required.

**5.8.8 Summary of Project-Specific Environmental Effects after Mitigation Measures are Implemented**

No Tribal Cultural Resources were identified so there are no potentially significant impacts to mitigate. Potential adverse impacts to Tribal Cultural Resources are less than significant and no mitigation measures are required.

**5.8.9 References**

The following references were used in the preparation of this section of the DEIR:

MOSE	Riverside County, General Plan 2016, Multipurpose Open Space Element, (Available at <a href="http://planning.rctlma.org/Portals/0/genplan/general_plan_2016/elements/Ch05_MOSE_120815.pdf?ver=2016-04-01-100801-367">http://planning.rctlma.org/Portals/0/genplan/general_plan_2016/elements/Ch05_MOSE_120815.pdf?ver=2016-04-01-100801-367</a> , accessed August 23, 2017)
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## 6.0 OTHER CEQA TOPICS

The *CEQA Guidelines* stipulate several general content requirements for EIRs. Those applicable to this project include: cumulative impacts (Section 15130), unavoidable adverse impacts (Section 15126(b)), irreversible changes (Section 15126 (c)), growth inducing impacts (Section 15126(f)), and energy conservation. The following addresses each of these general requirements.

### 6.1 Cumulative Impact Analysis

CEQA requires that an EIR contain an assessment of the cumulative impacts that may be associated with the proposed Project. As noted in CEQA Guidelines Section 15130(a), "an EIR shall discuss cumulative impacts of a project when the project's incremental effect is cumulatively considerable." "A cumulative impact consists of an impact which is created because of the combination of the project evaluated in the EIR together with other projects creating related impacts."

The CEQA Guidelines (Section 15130(b)) describes two acceptable methods for identifying a study area for purposes of conducting a cumulative impact analysis. These two approaches include: "1) a list of past, present, and probable future projects producing related or cumulative impacts, including if necessary those projects outside the control of the agency, or 2) a summary of projections contained in an adopted general plan or related planning document, or in a prior environmental document which has been adopted or certified, which described or evaluated regional or area wide conditions contributing to the cumulative impact."

The summary of projections approach is used in this EIR, except for the evaluation of cumulative traffic and vehicular-related air quality, greenhouse gas, and noise impacts. The cumulative traffic and vehicular-related air quality, greenhouse gas, and noise impacts is based on the past, present and reasonably foreseeable projects, or list of projects approach. The list of past, present, and reasonably foreseeable projects includes the nearby developments for opening year traffic conditions provided by the County of Riverside Department of Transportation staff and reflected in **Table 6.0-A: Cumulative Project Developments** below.

**Table 6.0-A: Cumulative Project Developments**

TAZ ID	Project Name	Land Use	Quantity	Peak Hour						Daily	
				AM		PM		TOTAL			
				IN	OUT	IN	OUT	IN	OUT	TOTAL	TOTAL
1	TR 36317	Single Fam. Detached	94 DU	18	53	71	59	35	94	895	
	TR 30760	Single Fam. Detached	285 DU	54	160	214	180	105	285	2,713	
	TR 31818	Single Fam. Detached	311 DU	59	174	233	196	115	311	2,961	
	TAZ 2 Subtotal			113	334	447	376	220	596	5,961	
3	TR 32984	Single Fam. Detached	108 DU	21	60	81	68	40	108	1,028	
	TR 37002	Single Fam. Detached	131 DU	25	73	98	83	48	131	1,247	
	SP 333	Single Fam. Detached	141 DU	27	79	106	89	52	141	1,342	
	TAZ 3 Subtotal			73	212	285	240	140	380	3,617	
4	TR 36316	Single Fam. Detached	87 DU	17	49	66	55	32	87	828	
5	TR 36593	Single Fam. Detached	610 DU	116	342	458	384	226	610	5,807	
6	SP 387	Single Fam. Detached	410 DU	78	230	308	258	152	410	3,903	
<b>TOTAL CUMULATIVE TRIPS</b>				<b>415</b>	<b>1,220</b>	<b>1,635</b>	<b>1,372</b>	<b>805</b>	<b>2,177</b>	<b>20,724</b>	

Source: Traffic Impact Analysis (TIA), Table 4-4



### 6.1.1 Air Quality

The cumulative area for air quality impacts is the South Coast Air Basin (Basin). As discussed in Section 5.1, the portion of the Basin within which the Project is located is designated as a non-attainment area for particulate matter less than 10 microns in diameter (PM-10) under State standards, and for ozone and PM less than 2.5 microns in diameter (PM-2.5) under both State and federal standards (see **Table 5.0-A**). Ozone is not directly emitted into the atmosphere; rather, it forms via a reaction of volatile organic compounds (VOC) and oxides of nitrogen (NO<sub>x</sub>) in the atmosphere. Therefore, it is also important to consider these emissions and their potential to contribute to ozone pollution in the region.

Based on the Air Quality analysis prepared for the proposed Project (Appendix B), none of the SCAQMD mass daily significance thresholds are exceeded during Project construction (see **Table 5.0-D** above). As shown in **Table 5.0-E** and **Table 5.0-F**, the mass daily significance threshold for NO<sub>x</sub> would be exceeded during Project operation. Implementation of Existing Regulation (ER) **ER AQ 1** and Mitigation measures **MM AQ 2** through **MM AQ 5** will be implemented to reduce operational NO<sub>x</sub> emissions; however, they do not have quantitative reductions associated with them available in CalEEMod. Therefore, operational emissions of NO<sub>x</sub> will exceed the SCAQMD threshold, even after implementation of mitigation measures.

SCAQMD considers the thresholds for project-specific impacts and cumulative impacts to be the same. Therefore, projects that exceed project-specific significance thresholds are considered by SCAQMD to be cumulatively considerable. Based on SCAQMD's regulatory jurisdiction over regional air quality, it is reasonable to rely on its thresholds to determine whether there is a cumulative air quality impact.

As such, cumulative impacts regarding operational NO<sub>x</sub> emissions will be **significant and unavoidable** and the County will be required to adopt a Statement of Overriding Considerations to move forward with the Project.

### 6.1.2 Biological Resources

This cumulative impact analysis considers development of the Project in conjunction with other development projects in the geographic area covered by the Western Riverside County MSHCP (MSHCP) based on a summary of projections approach resulting from full General Plan buildout in Riverside County and other jurisdictions in the region within the boundaries of the MSHCP.

The primary effects of the proposed Project, when considered with the buildout of long range plans in the geographic area covered by the MSHCP, would be the cumulative loss of habitat for sensitive species. Although the Project site is disturbed and does not support habitat for sensitive species, it still provides open space for foraging, refuse, and potentially nesting habitat for birds, including raptors and passerines.

Anticipated cumulative impacts to biological resources are addressed within the MSHCP cumulative study area. The MSHCP addresses 146 Covered Species that depend on a broad range of habitats and geographic areas within Western Riverside County, and includes threatened and endangered species and regionally- or locally- sensitive species that have specific habitat requirements and conservation and management needs. The MSHCP addresses biological impacts for take of Covered Species within the MSHCP Area. Impacts to Covered



Species and establishment and implementation of a regional conservation strategy and other measures included in the MSHCP address federal, state, and local mitigation requirements for these species and their habitats. Specifically, Section 4.4 of the MSHCP states that:

*The MSHCP was specifically designed to cover a large geographical area so that it would protect numerous endangered species and habitats throughout the region. It is the projected cumulative effect of future development that has required the preparation and implementation of the MSHCP to protect multiple habitats and multiple endangered species.*

The MSHCP goes on to state that:

*The LDMF [Local Development Mitigation Fee] is to be charged throughout the Plan Area to all future development within the western part of the County and the Cities in order to provide a coordinated conservation area and implementation program that will facilitate the preservation of biological diversity, as well as maintain the region's quality of life.*

The reason for the imposition of the mitigation fee over the entire region is that the loss of habitat for endangered species is a regional issue resulting from the cumulative effect of continuing development throughout all the jurisdictions in Western Riverside County. Finally, Section 5.1 of the MSHCP states that:

*It is anticipated that new development in the Plan Area will fund not only the mitigation of the impacts associated with its proportionate share of regional development, but also the impacts associated with the future development of more than 332,000 residential units and commercial and industrial development projected to be built in the Plan Area over the next 25 years.*

As discussed in Section 5.2.6, Biological Resources, the proposed Project site is not required for MSHCP conservation/ reserve assembly, but it is located adjacent to Temescal Canyon Wash that is part of the proposed reserve assembly. Temescal Wash is part of a Criteria Cell group with the goal focusing on riparian scrub, woodland, forest, and Riversidean alluvial fan sage scrub habitat associated with Temescal Wash that provides habitat and movement for listed species. Analysis contained in Section 5.2.6 concludes that the Project would be fully consistent with the MSHCP, although a mitigation measures is required to reduce potential impacts to active burrowing owl nests, if they occur onsite at the time of construction start. The Project will construct an onsite enhanced riverine/riparian/wetland feature to mitigate for impacts to existing riverine/riparian/wetland habitat, as outlined in the required DBESP for these impacts. With implementation of the required mitigation, the Project would be fully consistent with the MSHCP. Other cumulative development projects throughout the MSHCP Area would similarly be required to demonstrate compliance with the MSHCP. Because the Project and other cumulative developments would be required to comply with the MSHCP, cumulatively-considerable impacts would be less than significant. Cumulative impacts to biological resources except for non-MSHCP covered species would be less than significant on a cumulative basis provided that the terms of the MSHCP are fully implemented. The species identified as a potential to occur on site including Munz's onion, many-stemmed dudleya, Hammitt's clay-cress, round-leaved filaree, burrowing owl, least Bell's vireo, and California gnatcatcher. There are no sensitive species that are expected to occur on site that are not Covered Species under the MSHCP.

The Project site lacks migratory wildlife corridors and wildlife nursery sites, and does not occur within MSHCP cores or linkages. Accordingly, the Project would not result in a cumulatively-considerable impact on the movement of any species or on established native or resident or migratory wildlife corridors. Additionally, the Project would not have a cumulatively considerable impact on the use of native wildlife nursery sites. No impact would occur.

The Project will result in impacts to 0.01 acre of wetland waters of the US and 0.12-acre non-wetland waters of the U.S., for a total of 0.13 acre of impacts under USACE jurisdiction. The Project will impact a total of 0.12 un-vegetated streambed, 0.38 streambed associated riparian vegetation, and 0.01-acre wetland, for a total of 0.51 acre of impacts under CDFW jurisdiction.

As outlined in the DBESP, the loss of 0.12 acres of riverine habitat, 0.38 acres of riparian habitat and 0.01 acres of wetland habitat will be offset/mitigated for at a ratio of 2:1 for riverine habitat, 3:1 for riparian habitat and 4:1 for wetland habitat by the enhancement/revegetation of an onsite riverine/riparian/wetland feature that will be permanently preserved and maintained as an onsite conservation area. As more habitat is being created and conserved on-site than is being lost, the Project would not have a cumulatively-considerable impact.

The proposed Project would not conflict with any local policies or ordinances protecting biological resources; accordingly, a cumulatively-considerable impact due to a conflict with such local policies or ordinances would not occur.

### 6.1.3 Greenhouse Gas Emissions

Individual projects would incrementally contribute toward the potential for global climate change on a cumulative basis in concert with all other past, present, and probable future projects. While individual projects are unlikely to measurably affect global climate change, each of these projects incrementally contributes toward the potential for global climate change on a cumulative basis, in concert with all other past, present, and probable future projects.

However, the County has adopted a Climate Action Plan (CAP) to set a goal to reduce emissions back to 1990 levels by the year 2020. This target was calculated as a 15 percent decrease from 2008 levels, as recommended in the AB 32 Scoping Plan. As described in Section 5.4 - Greenhouse Gas Emissions, projects have the option of demonstrating compliance with the CAP by obtaining 100 points on the County's screening table or by demonstrating a 25 percent reduction in GHG emissions compared to the adjusted business as usual scenario. The Project earned 125 points on the County's screening table by agreeing to implement specific measures to reduce GHG emissions, which exceeds the County's 100-point threshold for CAP compliance (see Appendix B).

Consistent with CEQA Guidelines, projects that garner a total of 100 points or greater on the County's CAP Screening Tables "would be determined to have a less than significant individual and cumulative impact for GHG emissions" (CAP 2015). Accordingly, the Project will have a **less than significant individual and cumulative impact** with implementation of the Project's CAP Screening Table, **ER AQ 1**, and **MM AQ 2** through **MM AQ 5**.

### 6.1.4 Hydrology and Water Quality

For the purposes of evaluation, the Project's cumulatively considerable impacts to hydrology and water quality, the cumulative study area encompasses the Santa Ana River watershed,

which would receive runoff from the Project site. The analysis in this section assumes buildout of the cities and unincorporated lands in accordance with the applicable General Plan land use designations.

The proposed Project BMPs that would ensure the Project's post-development drainage conditions closely resemble the existing conditions and are consistent with Riverside County requirements. The BMPs would ensure that substantial erosion and siltation do not occur onsite or offsite, and that the Project's drainage would not exceed the capacity of existing drainage systems.

Other projects within the cumulative study area similarly would be required to demonstrate through site specific hydrology and hydraulics analyses that drainage would not substantially increase because of development. As with the proposed Project, these cumulative developments would be required to comply with Riverside County requirements for storm drainage systems and post-development runoff. Therefore, since other projects within the cumulative study area would be required to comply with similar measures as the proposed Project, cumulatively significant impacts would not be expected.

During construction activities, the proposed Project and other projects within the cumulative study area would be required to obtain NPDES permit that would regulate construction activities to ensure that polluted runoff does not occur. Since construction activities would be fully regulated pursuant to the NPDES, cumulatively considerable impacts from construction activities would be prevented.

Under long-term operating conditions, the Project and other cumulative development projects would be required to comply with the NPDES permit requirements for post-construction activities, which require the preparation of site-specific WQMPs that ensure the ongoing protection of the watershed basin by requiring structural and programmatic controls. As each development proceeds, site-specific BMPs must be developed to further reduce the potential for polluted runoff. Therefore, since long-term operational development for all projects within the cumulative study area would be regulated through the NPDES permit, long-term cumulatively significant impacts to water quality would also be prevented.

BMPs proposed as part of the Project's WQMP would occur onsite within areas proposed for disturbance by the Project. There are no direct or cumulative impacts that would result from construction of these BMPs, except as noted otherwise in the direct and cumulative analysis provided throughout this EIR. Cumulative significant impacts associated with the Project's BMPs are not anticipated.

The Project has been designed to ensure that post-development flows do not substantially exceed those that occur under existing conditions. The Project would not result in a substantial change in the amount of stormwater runoff that would contribute to flooding onsite or offsite or change the amount of surface water in any water body. Accordingly, cumulative significant impacts would not occur.

#### **6.1.5 Land Use and Planning**

The proposed Project includes a GPA for land use designation change from LI to CR, a CZ from M-SC to C-P-S, and development of restaurant, gas station with convenient store, retail, office, supermarket, bank, and pharmacy uses. The CR land use designation proposed by the Project

would not differ substantially from the range of uses that are allowed under existing land use designations and zoning. Thus, the Project's conversion of areas planned for LI to CR is not regarded as a substantial alteration of the planned land use for the Project site. **The proposed GPA and associated commercial/retail/office development would complement the surrounding and planned residential land uses.** When considered in the context of other past, present, and reasonably foreseeable developments within nearby portions of the Temescal Valley, the Project's proposed change to the site's land use designation also would not result in any cumulatively considerable impacts related to land use and planning. The proposed Project will not conflict with the Riverside County General Plan, the Elsinore Area Plan or SCAG's RTP/SCS. Accordingly, a cumulatively significant impact would not occur.

#### 6.1.6 Noise

The geographic scope for noise impacts associated with on-site construction and operations is the immediate vicinity of the Project site because noise is a localized phenomenon, and drastically reduces in magnitude as the distance from the noise sources increases. Consequently, only those cumulative development projects within the immediate vicinity of the proposed Project will be likely to contribute to cumulative noise impacts resulting from Project construction or operation. Only two of the cumulative development projects identified in **Table 6.0-A: Cumulative Developments** are within one-half (0.5) mile of the Project site: TR 36316 and TR 36317.

Potential impacts from Project-related construction will be less than significant through compliance with Ordinance No. 847 and implementation of mitigation measures **MM Noise 3 through MM Noise 5**. Potential cumulative impacts from construction noise could result if construction of the proposed Project and one or more of the two cumulative development projects within 0.5 miles of the Project site occurred simultaneously. However, because TR 36316 and TR 36317 are located south of I-15, any construction-generated noise from the proposed Project to these locations would be masked by the traffic noise from I-15. Therefore, the Project's contribution to construction noise is not cumulatively considerable and **cumulative impacts from construction noise are less than significant.**

The geographic scope for noise impacts associated with Project operations are sensitive receptors in proximity to the Project site because noise is a localized phenomenon, and drastically reduces in magnitude as the distance from the noise sources increases. Further, because the dominant noise source in the Project area is traffic-noise from I-15, sensitive receptors south of I-15 would not likely perceive operational noise from the Project site, so the concern would be limited to sensitive receptors north of I-15. There are no existing sensitive receptors north of I-15 within 0.5 miles of the Project site. Cumulative development projects TR 36593 and SP 387 are north of I-15; however, they are approximately one mile away from the Project site. Because noise is such a localized phenomenon, **cumulative impacts with regard to operational noise are not significant.**

The geographic scope for noise impacts associated with Project-generated vehicular noise is the roadways that will be used by Project-generated traffic in combination with traffic from the cumulative development projects. As shown in **Table 5.0-P: Existing and Existing Plus Project Traffic Noise Levels**, the Project's contribution to future noise levels on area roadways is less than 2 dBA CNEL for all roadway segments except for Temescal Canyon Road from Campbell Ranch Road to I-15 Freeway (3.01 dBA CNEL increase), Temescal Canyon Road north of Indian Truck Trail (8.47 dBA CNEL increase), and Indian Truck Trail from Temescal Canyon Road to I-

215 Freeway NB ramps (3.68 dBA CNEL increase). Although these noise levels will be perceptible, because there are no sensitive receptors in proximity to these roadway segments, the Project's contribution to cumulative traffic noise is not cumulatively considerable and **cumulative impacts with regard to traffic noise are not significant.**

**6.1.7 Transportation/Traffic**

Utilizing the list method, the cumulative impact area for transportation and traffic includes existing, proposed, planned and approved projects in the County of Riverside. As development in the County region would change the intensity of land uses in the region and increase housing, employment, shopping and recreational opportunities, regional traffic volume conditions anticipated for the buildout of County of Riverside are also included.

Pursuant to State *CEQA Guidelines* Section 15130, an EIR shall discuss cumulative impacts of a project when the project's incremental effect is cumulatively considerable. To ascertain Project impacts to Study Area intersections, freeway segments, and freeway ramps in combination with other approved and pending projects in the vicinity, an analysis of the existing traffic, plus traffic from ambient growth, plus traffic from cumulative development projects, plus Project traffic (E+A+C+P) was performed for both Phase I and Phase II Project conditions.

**Cumulative Trip Generation**

The future, cumulative model includes "reasonably foreseeable" projects in the study area, and generally corresponds to a year 2035 horizon, and includes all projected projects from the General Plan and RTP/SCS and includes approved and pending development projects in the study area, including within the cities of Hemet, Moreno Valley, Perris, Riverside, and San Jacinto. That is to say, any approved or pending project that was not already included in the 2012-2035 RTP/SCS land use data set were added to the model to ensure all reasonably foreseeable development projects were included in the assessment. The list of approved and pending projects obtained for this assessment is included in Appendix D of the Traffic Impact Analysis (TIA).

Cumulative trip generation rates were utilized to estimate cumulative traffic totals as reflected in **Table 6.0-B: Cumulative Trip Generation Rates**. **Table 6.0-C: Cumulative Trip Generation Summary**, provides cumulative development land uses and anticipated number of trips.

**Table 6.0-B: Cumulative Trip Generation Rates**

Land Use	ITE Code	Quantity	Peak Hour Trip Rates						Daily
			AM			PM			
			IN	OUT	Total	IN	OUT	Total	
Single Fam. Detached	210	1,733 DU	0.19	0.56	0.75	0.63	0.37	1.00	9.52

Source: TIA, Table 4-3

**Table 6.0-C: Cumulative Trip Generation Summary**

TAZ ID	Project Name	Land Use	Quantity	Peak Hour						Daily
				AM			PM			
				IN	OUT	Total	IN	OUT	Total	
1	TR 36317	Single Fam. Detached	94 DU	18	53	71	59	35	94	895
2	TR 30760	Single Fam. Detached	285 DU	54	160	214	180	105	285	2,713
	TR 31818	Single Fam. Detached	311 DU	59	174	233	196	115	311	2,961
	TAZ 2 Subtotal			113	334	447	376	220	596	5,674
3	TR 32984	Single Fam. Detached	108 DU	21	60	81	68	40	108	1,028
	TR 37002	Single Fam. Detached	131 DU	25	73	98	83	48	131	1,247
	SP 333	Single Fam. Detached	141 DU	27	79	106	89	52	141	1,342
	TAZ 3 Subtotal			73	212	285	240	140	380	3,617
4	TR 36316	Single Fam. Detached	87 DU	17	49	66	55	32	87	828
5	TR 36593	Single Fam. Detached	610 DU	116	342	458	384	226	610	5,807
6	SP 387	Single Fam. Detached	410 DU	78	230	308	258	152	410	3,903
<b>Total Cumulative Trips</b>				<b>415</b>	<b>1,220</b>	<b>1,635</b>	<b>1,372</b>	<b>805</b>	<b>2,177</b>	<b>20,724</b>

Source: TIA, Table 4-4

As reflected above, cumulative developments are projected to generate a total of approximately 20,724 trip-ends per day with 1,635 vehicles per hour during the AM peak hour and 2,177 vehicles per hour during the PM peak hour.

Peak hour intersection analysis has been performed at the study area intersections for projected future conditions. Improvements are recommended to satisfy the level of service requirements of the County of Riverside and if the cumulative traffic exceeds the target LOS and impacts cannot be mitigated through existing infrastructure funding mechanisms.

### Intersections

**Table 6.0-D: Cumulative: Intersection Analysis**, represents the cumulative traffic conditions with inclusion of Project (Phase II) traffic without improvements. Under this scenario, the following intersections exceed the acceptable LOS:

- Temescal Canyon Road/Indian Truck Trail
- Temescal Canyon Road/Driveway 2
- Temescal Canyon Road/Driveway 3

### Freeway Segments

**Table 6.0-E:**, represents the cumulative traffic conditions with inclusion of Project traffic without improvements. Under this scenario, the following freeway segments exceed the acceptable LOS:

- I-15 Southbound, north of Indian Truck Trail

### Freeway Ramps

**Table 6.0-F:**, represents the cumulative traffic conditions with inclusion of Project traffic without improvements. Under this scenario, the following freeway ramps exceed the acceptable LOS:

- I-15 southbound off-ramp at Indian Truck Trail

**Table 6.0-D: Cumulative: Intersection Analysis**

No	Intersection	Traffic Control	Target LOS	No Project						Plus Project / Without Mitigation			
				Existing Plus Ambient Plus Cumulative Condition						Existing Plus Ambient Plus Cumulative Condition			
				AM			PM			AM		PM	
				Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS
1	Temescal Cyn Rd/Indian Truck Trl	TS	D	26.9	C	27.3	C	33.8	C	62.4	E		
2	I-15 NB Ramps/Indian Truck Trl	TS	D	28.5	C	26.8	C	30.5	C	31.4	C		
3	I-15 SB Ramps/Indian Truck Trl	TS	D	21.8	C	25.0	C	25.5	C	29.5	C		
4	Campbell Ranch Rd/Indian Truck Trl	TS	D	18.6	B	20.3	C	21.1	C	36.6	D		
5	Campbell Ranch Rd/Temescal Cyn Rd	TS	D	23.8	C	16.6	B	24.2	C	16.8	B		
6	Campbell Ranch Rd/Mayhew Cyn Rd	CSS	D	10.3	B	10.4	B	10.7	B	11.0	B		
7	Temescal Cyn Rd/Horsethief Rd	CSS	D	18.0	C	13.5	B	26.7	D	21.0	C		
8	Temescal Cyn Rd / Dwy 1	<b>CSS</b>	D	10.7	B	19.4	C	12.1	B	24.9	C		
9	Temescal Cyn Rd / Dwy 2	<b>TS</b>	D	13.8	B	32.1	D	17.4	C	62.7	<b>F</b>		
10	Temescal Cyn Rd / Dwy 3	<b>CSS</b>	D	12.4	B	26.5	D	15.5	C	62.1	<b>F</b>		
11	Temescal Cyn Rd / Dwy 4	<b>CSS</b>	D	10.9	B	17.5	C	12.8	B	29.5	D		

Source: TIA, Tables 5-10, 5-13

TS = Traffic Signal; CSS = Cross Street Stop; **CSS** = Cross Street Stop (Project Design Feature)

**BOLD** = Exceeds Target LOS



**Table 6.0-E: General Plan Buildout: Freeway Segments Analysis**

Freeway	Segment	Lanes	Target LOS	No Project						Plus Project / Without Mitigation					
				Volumes		Density		LOS		Volumes		Density		LOS	
				AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
I-15 Southbound	North of Indian Truck Trail	3	D	3,195	5,785	17.4	38.0	B	E	3,316	6,185	18.1	43.5	C	E
	South of Indian Truck Trail	3	D	3,018	5,298	16.5	32.6	B	D	3,317	5,496	18.1	34.7	C	D
	South of Indian Truck Trail	3	D	3,502	3,387	19.2	18.5	C	C	3,604	3,724	19.8	20.5	C	C
Northbound	North of Indian Truck Trail	3	D	3,946	3,666	21.9	20.1	C	C	4,301	3,901	24.3	21.6	C	C

Source: TIA, Tables 5-12, 5-15  
**BOLD** = Exceeds Target LOS

**Table 6.0-F: General Plan Buildout: Freeway Ramp Analysis**

Freeway	Ramp Location	Lanes	Ramps	Target LOS	No Project						Plus Project / Without Mitigation					
					Volumes		Density		LOS		Volumes		Density		LOS	
					AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
I-15 Southbound	SB Off-Ramp at Indian Truck Trl	3	1	D	440	876	24.3	37.5	C	E	561	1,276	25.2	40.2	C	E
	SB On-Ramp at Indian Truck Trl	3	1	D	263	389	19.9	32.2	B	D	562	587	22.3	33.8	C	D
	NB Off-Ramp at Indian Truck Trl	3	1	D	278	278	25.7	25.0	C	C	380	614	26.4	27.5	C	C
Northbound	NB On-Ramp at Indian Truck Trl	3	1	D	722	557	26.0	24.1	C	C	1,077	791	28.9	26.0	D	C

Source: TIA, Tables 5-11, 5-14  
**BOLD** = Exceeds Target LOS



Intersections that will operate at an unacceptable LOS with implementation of the proposed Project in the cumulative condition will require payment of fees that will contribute to these needed improvements. Payment of fees for these improvements are required as mitigation for the Project.

### **General Plan Buildout**

The General Plan buildout traffic conditions analyses can be used to determine whether improvements funded through regional transportation mitigation fee programs, such as the Transportation Uniform Mitigation Fee (TUMF), City Development Impact Fee (DIF) programs, or other approved funding mechanism can accommodate the long-range cumulative traffic at the target level of service identified by the County of Riverside General Plan. If the funded improvements can provide the target LOS, the project's payment into TUMF, DIF or other fair share contribution programs will be considered as cumulative mitigation through the conditions of approval. Other improvements needed beyond the funded improvements (such as localized improvements to non-TUMF or DIF facilities) are identified as such.

### **Intersections**

**Table 6.0-G: General Plan Buildout Intersections Analysis**, represents the cumulative traffic conditions with and without inclusion of Project traffic without improvements. Under this scenario, the following intersections exceed the acceptable LOS.

- Temescal Canyon Road/Indian Truck Trail
- I-15 NB Ramps/Indian Truck Trail
- Campbell Ranch Road/Indian Truck Trail
- Campbell Ranch Road/Temescal Canyon Road
- Temescal Canyon Road/Horsethief Road

### **Freeway Segments**

**Table 6.0-H: General Plan Buildout Freeway Segment Analysis**, represents the cumulative traffic conditions with and without inclusion of Project traffic without improvements. Under this scenario, the following segments of I-15 exceed the acceptable LOS.

- I-15 Southbound, north of Indian Truck Trail
- I-15 Southbound, south of Indian Truck Trail
- I-15 Northbound, north of Indian Truck Trail

### **Freeway Ramps**

**Table 6.0-I: General Plan Buildout Freeway Ramp Analysis**, represents the cumulative traffic conditions with and without inclusion of Project traffic without improvements. Under this scenario, the following freeway ramps exceed the acceptable LOS.

- I-15 southbound off-ramp at Indian Truck Trail
- I-15 northbound on-ramp at Indian Truck Trail

**Table 6.0-G: General Plan Buildout: Intersection Analysis**

No	Intersection	Traffic Control	Target LOS	No Project						Plus Project / Without Mitigation					
				AM		PM		AM		PM		AM		PM	
				Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS	Delay (sec)	LOS
1	Temescal Cyn Rd/Indian Truck Trl	TS	D	>100	F	39.3	D	>100	F	50.2	D	>100	F	50.2	D
2	I-15 NB Ramps/Indian Truck Trl	TS	D	77.3	E	19.8	B	84.4	F	35.3	D	84.4	F	35.3	D
3	I-15 SB Ramps/Indian Truck Trl	TS	D	32.0	C	27.1	C	38.7	D	41.9	D	38.7	D	41.9	D
4	Campbell Ranch Rd/Indian Truck Trl	TS	D	87.1	F	33.4	C	97.6	F	46.6	D	97.6	F	46.6	D
5	Campbell Ranch Rd/Temescal Cyn Rd	TS	D	57.6	E	63.9	E	55.7	E	59.4	E	55.7	E	59.4	E
6	Campbell Ranch Rd/Mayhew Cyn Rd	CSS	D	14.6	B	20	C	15.6	C	23.3	C	15.6	C	23.3	C
7	Temescal Cyn Rd/Horsethief Rd	CSS	D	>100	F	>100	F	>100	F	>100	F	>100	F	>100	F
8	Temescal Cyn Rd / Dwy 1	<b>CSS</b>	D					14.6	B	>100	F	14.6	B	>100	F
9	Temescal Cyn Rd / Dwy 2	<b>TS</b>	D					81.7	F	21.9	C	81.7	F	21.9	C
10	Temescal Cyn Rd / Dwy 3	<b>CSS</b>	D					93.4	F	17.9	C	93.4	F	17.9	C
11	Temescal Cyn Rd / Dwy 4	<b>CSS</b>	D					35.2	E	13.7	B	35.2	E	13.7	B

Source: TIA, Tables 5-16, 5-19  
 TS = Traffic Signal; CSS = Cross Street Stop; **CSS** = Cross Street Stop (Project Design Feature)  
**BOLD** = Exceeds Target LOS

**Table 6.0-H: General Plan Buildout: Freeway Segments Analysis**

Freeway	Segment	Lanes	Target LOS	No Project						Plus Project / Without Mitigation					
				Volumes		LOS		Density		Volumes		Density		LOS	
				AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
I-15 Southbound	North of Indian Truck Trail	3	D	3,455	6,354	18.9	46.3	C	F	3,565	6,495	19.6	48.8	C	F
	South of Indian Truck Trail	3	D	3,720	5,545	20.5	35.2	C	E	3,795	5,704	20.9	37.0	C	E
I-15 Northbound	South of Indian Truck Trail	3	D	4,878	4,388	28.7	24.9	D	C	4,988	4,529	29.7	25.9	D	C
	North of Indian Truck Trail	3	D	6,277	4,461	45.0	25.4	E	C	6,352	4,620	46.2	26.6	F	D

Source: TIA, Tables 5-18, 5-21  
**BOLD** = Exceeds Target LOS



**Table 6.0-I: General Plan Buildout: Freeway Ramp Analysis**

Freeway	Ramp Location	Lanes	Ramps	Target LOS	No Project						Plus Project / Without Mitigation					
					Volumes		Density		LOS		Volumes		Density		LOS	
					AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
I-15 Southbound	SB Off-Ramp at Indian Truck Trl	3	1	D	754	1,300	26.4	41.9	C	E	864	1,441	27.2	43.3	C	E
	SB On-Ramp at Indian Truck Trl	3	1	D	1,019	491	25.7	33.8	C	D	1,094	650	26.3	35.1	C	E
I-15 Northbound	NB Off-Ramp at Indian Truck Trl	3	1	D	527	675	33.0	31.0	D	D	637	816	33.7	31.9	D	D
	NB On-Ramp at Indian Truck Trl	3	1	D	1,926	748	41.7	28.8	E	D	2,001	907	42.3	30.1	E	D
Source: TIA, Tables 5-17, 5-20																
<b>BOLD = Exceeds Target LOS</b>																



The County of Riverside General Plan FEIR determined that build-out of the General Plan would add substantial traffic volumes on local roadways resulting in significant traffic impacts to County and adjoining jurisdictions. Implementing Project developers will pay into the following “fair share” mitigation programs which would be collected and utilized by the various programs to construct the improvements necessary to maintain target LOS:

- Transportation Uniform Mitigation Fee (TUMF)
- Development Impact Fees (DIF)

As discussed above, implementation of the proposed Project without improvements will contribute to unacceptable LOS in the cumulative and 2035 buildout conditions at study area intersections, freeway segments, and freeway ramps. As identified below in **Table 6.0-J: Intersections: Cumulative and Buildout Project Conditions with Improvements**, **Table 6.0-K: Freeway Segments: Cumulative and Buildout Project Conditions with Improvements**, **Table 6.0-L: Freeway Ramps: Cumulative Project Conditions with Improvements**, with contribution of fair share fees and implementation of these improvements, LOS for intersections, freeway segments, and freeway ramps will lessen impacts in the cumulative and buildout scenarios.

**Table 6.0-J: Intersections: Cumulative and Buildout Project Conditions with Improvements**

No.	Intersection	Target LOS	Cumulative				Buildout			
			LOS W/O Mitigation		LOS/With Mitigation		LOS W/O Mitigation		LOS/With Mitigation	
			AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak
1	Temescal Cyn Rd/Indian Truck Trl	D	C	E	C	D	F	D	C	C
2	I-15 NB Ramps/Indian Truck Trl	D	C	C	C	C	F	D	D	C
3	I-15 SB Ramps/Indian Truck Trl	D	C	C	C	C	D	D	D	D
4	Campbell Ranch Rd/Indian Truck Trl	D	C	D	C	D	F	D	C	C
5	Campbell Ranch Rd/Temescal Cyn Rd	D	C	B	C	B	E	E	C	B
6	Campbell Ranch Rd/Mayhew Cyn Rd	D	B	B	B	B	C	C	C	C
7	Temescal Cyn Rd/Horsethief Rd	D	D	C	D	C	F	F	B	D
8	Temescal Cyn Rd / Dwy 1	D	B	C	B	C	B	F	B	F
9	Temescal Cyn Rd / Dwy 2	D	C	F	C	C	F	C	B	C
10	Temescal Cyn Rd / Dwy 3	D	C	F	C	C	F	C	B	C
11	Temescal Cyn Rd / Dwy 4	D	B	D	B	D	E	B	A	B
Source: TIA, Tables 5-13, 5-19										
<b>BOLD = Exceeds LOS; 1 = Lane Improvement</b>										

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**Table 6.0-K: Freeway Segments: Cumulative and Buildout Project Conditions with Improvements**

Freeway	Segment	Target LOS	Cumulative						Buildout					
			LOS Without Mitigation			LOS With Mitigation			LOS Without Mitigation		LOS With Mitigation			
			Lanes	AM Peak	PM Peak	Lanes	AM Peak	PM Peak	Lanes	AM Peak	PM Peak	Lanes	AM Peak	PM Peak
I-15	North of Indian Truck Trl	D	3	C	E	4	B	D	3	C	F	4	B	D
Southbound	South of Indian Truck Trl	D	3	C	D	3	C	D	3	C	E	4	B	C
I-15	North of Indian Truck Trl	D	3	C	C	3	C	C	3	D	C	3	D	C
Northbound	South of Indian Truck Trl	D	3	C	C	3	C	C	3	F	D	4	D	C

Source: TIA, Tables 5-15, 5-20  
**BOLD** = Exceeds LOS; **1** = Lane Improvement

**Table 6.0-L: Freeway Ramps: Cumulative Project Conditions with Improvements**

Freeway	Ramp Location	Target LOS	Cumulative						Buildout									
			Without Mitigation			With Mitigation			Without Mitigation			With Mitigation						
			Number of Lanes		LOS	Number of Lanes		LOS	Number of Lanes		LOS	Number of Lanes		LOS				
Lane	Ramp	AM Peak	PM Peak	Lane	Ramp	AM Peak	PM Peak	Lane	Ramp	AM Peak	PM Peak	Lane	Ramp	AM Peak	PM Peak			
I-15	SB Off-Ramp at Indian Truck Trl	D	3	1	C	E	3	1	A	C	3	1	C	E	4	2	B	C
Southbound	SB On-Ramp at Indian Truck Trl	D	3	1	C	D	3	1	C	D	3	1	C	E	4	1	C	C
I-15	NB Off-Ramp at Indian Truck Trl	D	3	1	C	C	3	1	C	C	3	1	D	D	3	1	D	D
Northbound	NB On-Ramp at Indian Truck Trl	D	3	1	D	C	3	1	D	C	3	1	E	D	4	2	D	B

Source: TIA, Tables 5-14, 5-20  
**BOLD** = Exceeds LOS; **1** = Lane Improvement



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In the buildout condition, despite implementation of mitigation measures, including the payment of TUMF/DIF fees, the intersection of Temescal Cyn Rd / Dwy 1 will exceed acceptable LOS. No other intersections, freeway segments, or freeway ramps will exceed acceptable LOS in the cumulative or buildout conditions with implementation of project mitigation measures identified in Section 5.7 and payment of TUMF/DIF fees for the project.

The Project will be required to pay for and construct general plan level road improvements on roads adjacent to the development sites and will pay "fair share" fees for construction of area wide improvements necessary to address the cumulative impact of area-wide development. While the proposed Project includes mitigation measures for area-wide improvements, improvements in regional transportation facilities associated with cumulative and buildout traffic conditions are intended to be addressed through implementation of regional programs such as the SCAG Regional Transportation plan.

Additionally, all state freeways are under the authority of Caltrans. Thus, where traffic increases would affect freeways, there is no mechanism for development project proponents to pay fees or make fair share contributions toward improving mainline freeway lanes. Also, even if there were such a mechanism, there is no way to ensure that such payments would be directed to a specific freeway improvement project. Consequently, there are no feasible mitigation measures for impacts to freeways; impacts would be significant and unavoidable. (GP FEIR, p. 5-13).

The County of Riverside does not have jurisdiction over all the freeway improvements that require mitigation. These improvements are under the control of Caltrans. While the Project will be conditioned to make the recommended improvements or contributions to fair share mechanisms to reduce project specific and cumulative impacts, there is no guarantee that Caltrans will allow the improvements to freeways to occur. Furthermore, the precise timing of future development that will occur to reach general plan buildout cannot be determined presently because of the complex nature of land development.

Hence, the timing of improvements needed to improve levels of service on a regional basis would be determined by the County of Riverside and Caltrans based upon need and the availability of funding. The priority and timing of these improvements cannot be determined now, nor are they under the sole control of the Project proponent or the County of Riverside to implement. Thus, it is possible that the required improvements will not be constructed in time to mitigate the project's direct and cumulative impacts upon freeways to below the level of significance. Therefore, because of the uncertainty of when mitigation would be implemented in relationship to project development, cumulative impacts are **significant and unavoidable** and a **statement of overriding consideration** would be required to be adopted by the County of Riverside prior to project approval.

### 6.1.8 Tribal Cultural Resources

The analysis in this section assumes buildout in accordance with the applicable General Plan land use designations. Requirements in addressing tribal cultural resources are part of CEQA's AB52 regulation that, among other elements, requires lead agencies to coordinate with designated and interested tribes that have a potential tribal cultural resource within a project's site or around the area of a project's site. Thus, impacts to tribal cultural resources are typically considered in a project-by-project basis where project characteristics, such as ground disturbance, are analyzed and considered by tribes for the potential to uncover tribal cultural

resources. Likewise, other projects within the cumulative study area similarly would be required to demonstrate through site specific cultural and tribal resources assessment that tribal cultural resources would not be significantly impacted because of development. As with the proposed Project, these cumulative developments would be required to comply with CEQA and Riverside County requirements for potential tribal cultural resources impacts. Therefore, when considered in the context of other past, present, and reasonably foreseeable developments within nearby portions of the Temescal Valley, the proposed Project's construction and operations and maintenance activities would not result in any cumulatively significant impacts related to tribal cultural resources.

## 6.2 Significant and Unavoidable Adverse Impacts

The CEQA Guidelines required that an EIR disclose the significant environment effects of a project which cannot be avoided if the proposed project is implemented (CEQA Guidelines Section 15126(b)). As described in detail in Section 5.0 of this EIR, the proposed Project is anticipated to result in the following impacts related to air quality and traffic that cannot be reduced to below a level of significance after the implementation of relevant standard conditions of approval, compliance with applicable laws and regulations, and application of feasible mitigation measures. The significant effects of the proposed Project that cannot be feasibly mitigated are as follows:

- **Air Quality Threshold: Conflict with or obstruct implementation of the applicable air quality plan.** Because the proposed Project is not compliant with local and use plans and population projections and because the proposed Project will exceed the SCAQMD regional significance threshold for NO<sub>x</sub>, the proposed Project would conflict with or obstruct implementation of the AQMP and impacts will be significant and unavoidable.
- **Air Quality Threshold: Violate any air quality standard or contribute substantially to an existing or projected air quality violation.** The long-term operation emissions will only exceed the threshold for NO<sub>x</sub>, even with the incorporation of ER AQ 1 and mitigation measures MM AQ 2 through MM AQ 5. Therefore, long-term regional air quality impacts are significant and unavoidable after implementation of mitigation measures.
- **Air Quality Threshold: Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is nonattainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors).** The Project would have a cumulatively considerable increase in emissions due to operational NO<sub>x</sub>, even with implementation of mitigation measures identified above, the impact is significant and unavoidable after implementation of mitigation
- **Traffic Threshold: Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit; or Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for**

**designated roads or highways.** Ramp and lane improvements will be necessary to reduce impacts to ramp and freeway segments. However, all state freeways are under the authority of Caltrans. Thus, where traffic increases would affect freeways, there is no mechanism for development project proponents to pay fees or make fair share contributions toward improving mainline freeway lanes. Also, even if there were such a mechanism, there is no way to ensure that such payments would be directed to a specific freeway improvement project. Consequently, there are no feasible mitigation measures for impacts to freeways; impacts would be significant and unavoidable.

The timing of improvements needed to improve levels of service on a regional basis would be determined by the County of Riverside and Caltrans based upon need and the availability of funding. The priority and timing of these improvements cannot be determined now, nor are they under the sole control of the Project proponent or the County of Riverside to implement. Thus, it is possible that the required improvements will not be constructed in time to mitigate the project's direct and cumulative impacts upon freeways to below the level of significance. Therefore, because of the uncertainty of when mitigation would be implemented in relationship to project development, cumulative impacts are significant and unavoidable.

### 6.3 Growth Inducing Impacts

According to State *CEQA Guidelines* Section 15126.2(d), a project may foster economic or population growth, or additional housing, either indirectly or directly, in a geographical area if it meets any one of the following criteria:

- a project would remove obstacles to population growth;
- increases in the population may tax existing community service facilities, causing significant environmental effects; or
- a project would encourage and facilitate other activities that could significantly affect the environment.

Under CEQA, growth inducement is not considered necessarily detrimental, beneficial, or of significance to the environment. Typically, growth inducing potential of a project would be considered significant if it fosters growth or a concentration of population in excess of what is assumed in pertinent master plans, land use plans, or in projections made by regional planning agencies such as SCAG. Significant growth impacts also could occur if a project provides infrastructure or service capacity to accommodate growth beyond the levels currently allowed under local and regional plans and policies. In general, growth induced by a project is considered a significant impact if it directly or indirectly affects the ability of agencies to provide needed public services, or if it can be demonstrated that the potential growth significantly affects the environment in some other way.

The Project does not include the development of housing and therefore, would not directly induce population growth. Although the Project will include infrastructure improvements, including roadway improvements and extension and connections of water, non-potable water, sewer and storm drains, backbone infrastructure already occurs in the Project area and these improvements from the Project would not remove obstacles to population growth.

Because all users of the Project's buildings are not yet known, the number of jobs that the Project would generate cannot be precisely determined; therefore, for the purposes of analysis, employment estimates were calculated using data and average employment density factors (square feet/employee factor) utilized in the County of Riverside General Plan (GP Appendix E-1). The General Plan estimates that Commercial Retail (CR) would employ one (1) worker for every 500 square feet of building area and that Commercial Office (CO) would employ one (1) worker for every 300 square feet of building space. Based on this employment generation rate, the Project is expected to create approximately 470 new jobs. The Project would also create short-term construction related jobs.

The jobs-housing ratio of the updated General Plan at buildout (2060) was calculated at 1.08 and at 0.86 for year 2035. In addition, the as growth in Riverside county proceeds from 2035 to 2060, the balance in the jobs-to-housing ratio for unincorporated Riverside County improves from 0.86 in 2035 to 1.08 in 2060. This ratio measures the extent to which job opportunities in a given geographic area match the available housing in the same area. This ratio highlights the basic planning premise that a "balanced" community provides a reasonable amount of both housing and jobs in close proximity, so that residents can both live and work in their local community, avoiding long commutes that trigger higher traffic, increase air pollution and noise, and generally decrease the overall standard of living. Accordingly, a commonly accepted "optimal balance" for a jobs-to-housing ratio is in the range of 1.3 or 1.4 jobs-to-housing is in the range of 1.3 or 1.4 jobs-to-housing units. That is, the number of employed persons (jobs) divided by the number of housing units available. A range in the 1.3 to 1.4 range would mean that there is a net surplus of jobs available, which would draw job-seekers into the community, rather than necessitate workers' leaving outward for employment. (GP EIR, Section 5.0)

Riverside County overall is housing rich/jobs poor, with an even greater housing rich/jobs poor condition in the unincorporated areas. It is expected that the majority of the new long-term jobs as well as the short-term construction jobs would be filled by the existing labor force available in the County of Riverside, as the County has a shortage of jobs. Thus, the Project would not indirectly induce substantial population growth in an area.

Potential growth inducing impacts from the Project are **less than significant**.

#### **6.4 Significant Irreversible Environmental Changes**

The CEQA Guidelines (Section 15126.2) require EIRs to address any significant irreversible environmental changes that would be involved in the proposed action should it be implemented. An environmental change would fall into this category if: a) the project would involve a large commitment of non-renewable resources; b) the primary and secondary impacts of the project would generally commit future generations to similar uses; c) the project involves uses in which irreversible damage could result from any potential environmental accidents; or d) the proposed consumption of resources is not justified (e.g., the project results in wasteful use of energy).

Determining whether the proposed Project may result in significant irreversible environmental changes requires a determination of whether key non-renewable resources would be degraded or destroyed in such a way that there would be little possibility of restoring them. Natural resources in the form of construction materials and energy resources would be used in the construction of the proposed Project, but development of the Project site as proposed would have no measurable adverse effect on the availability of such resources, including resources that

may be non-renewable (e.g., fossil fuels). Construction and operation of the proposed Project would not involve the use of large sums or sources of non-renewable energy.

As outlined in Section 6.5.1, Energy Requirements below, collectively, compliance with regulatory programs and implementation of these mitigation measures would ensure that the Project would not result in the inefficient, unnecessary, or wasteful consumption of energy.

The Environmental Assessment Form: Initial Study (included in Appendix A), provides an analysis of the proposed Project's potential to transport or handle hazardous materials which, if released into the environment, could result in irreversible damage to the environment. As concluded in the analysis, compliance with federal, state, and local regulations related to hazardous materials would be required of all contractors working on the Project's construction and all the tenants that occupy the Project's buildings. As such, construction of long-term operation of the proposed Project would not have the potential to cause significant irreversible damage to the environment, including damage that may result from upset or accident conditions.

## 6.5 Energy Conservation

Appendix F of the State *CEQA Guidelines* requires an EIR to include a discussion of the potential energy impacts of projects, with particular emphasis on avoiding or reducing inefficient, wasteful, and unnecessary consumption of energy.

### 6.5.1 Energy Requirements

#### Construction

This discussion utilizes the assumptions from the *Air Quality Analysis for the Toscana Village at Temescal Valley Project* (the AQ Study) evaluated in Section 5.1 Air Quality and Section 5.3 Greenhouse Gas Emissions, respectively (the AQ Study is contained in Appendix B). Because the California Emissions Estimator Model (CalEEMod) program used to evaluate air quality/greenhouse gas emissions does not display the amount and fuel type for construction-related sources, additional calculations were conducted and are summarized below. These calculations are contained in Appendix B of this Draft EIR. Construction of the proposed Project would require the use of construction equipment for grading, building construction, paving, architectural coating, and hauling activities; additionally, construction workers and vendors traveling to and from the site of the proposed Project has the potential to generate emissions. Construction equipment generally requires diesel as the fuel source.

Fuel consumption from heavy-duty construction equipment was calculated based on the equipment mix and usage factors provided in the CalEEMod construction output files as part of the AQ Study included in Appendix B of this Draft EIR. The total horsepower was then multiplied by fuel usage estimates per horsepower-hour included in Table A9-3-E of the SCAQMD's CEQA Air Quality Handbook (SCAQMD (a)). Fuel consumption from construction worker and vendor/delivery trucks was calculated using the trip rates and distances provided in the CalEEMod construction output files. Total vehicle miles traveled (VMT) was then calculated for each type of construction-related trip and divided by the corresponding county-specific miles per gallon factor using California Air Resources Board's (CARB's) EMFAC 2014 model (CARB 2015). EMFAC provides the total annual VMT and fuel consumed for each vehicle type. Consistent with CalEEMod, construction worker trips were assumed to include 50 percent light duty gasoline auto and 50 percent light duty gasoline trucks. Construction vendor trucks were

assumed to be 50 percent medium-duty and 50 percent heavy-duty diesel trucks. Construction hauling trips were assumed to be 100 percent heavy-duty diesel trucks. Please refer to Appendix B of the Draft EIR for detailed calculations.

As shown below in **Table 6.0-M: Construction Energy Use**, a total of 143,372 gallons of diesel fuel and 93,226 gallons of gasoline are estimated to be consumed during Project construction.

**Table 6.0-M: Construction Energy Use**

Fuel	Fuel Consumption
<b>Diesel</b>	
On-Road Construction Trips <sup>a</sup>	55,798 Gallons
Off-Road Construction Equipment <sup>b</sup>	87,574 Gallons
<b>Diesel Total</b>	<b>143,372 Gallons</b>
<b>Gasoline</b>	
On-Road Construction Trips <sup>a</sup>	93,226 Gallons
Off-Road Construction Equipment <sup>b</sup>	-- Gallons
<b>Gasoline Total</b>	<b>93,226 Gallons</b>

Notes:

Source: Table 1 – Total Construction-Related Fuel Consumption, Appendix B of Draft EIR.

<sup>a</sup> On-road mobile source fuel use based on vehicle miles traveled (VMT) from CalEEMod for all years of construction and fleet average fuel consumption in gallons per mile from EMFAC2014 for 2018 in the SCAQMD to ensure a conservative estimate. See Table 2 – On Road Construction Trip Estimates, Appendix B of DEIR for calculation details.

<sup>b</sup> Off-road mobile source fuel usage based on a fuel usage rate of 0.05 gallons of diesel per horsepower (HP)-hour, based on SCAQMD CEQA Air Quality Handbook, Table A9-3E.

<sup>c</sup> All emissions from off-road construction equipment were assumed to be diesel.

Fuel energy consumed during construction would be temporary in nature and would not represent a significant demand on energy resources. Construction equipment is also required to comply with regulations limiting idling to five minutes or less (CCR Title 13 §2449(d)(3)). Furthermore, there are no unusual characteristics of the proposed Project that would necessitate the use of construction equipment that would be less energy-efficient than at comparable construction sites in other parts of the State. Further, construction would be required to comply with all applicable local, state, and federal fuel efficiency standards.

For comparison, the State of California consumed 15.39 billion gallons of gasoline and 2.90 billion gallons of diesel fuel in fiscal year 2015-2016 (CA Board of Equalization 2017 (a), (b)). The fuel usage during Project construction would account for approximately 0.0006 percent of the existing gasoline related energy consumption and 0.005 percent of the existing diesel fuel related energy consumption in the State of California.

Therefore, it is expected that construction-related fuel consumption associated with the Project would not be any more inefficient, wasteful, or unnecessary than at other construction sites in the region.

## Operations

The Project will promote building energy efficiency through compliance with energy efficiency standards (Title 24 and Cal Green). The Project also reduces vehicle fuel usage due to compliance with regulatory programs and Project design features that reduce VMT, such as installation of sidewalks to connect to neighborhoods in the Project vicinity. Specifically, the Project will comply with the following regulations, among others, to reduce fuel usage:

- AB 1493 ("the Pavley Standard") requires reduction in GHG emissions from non-commercial passenger vehicles and light-duty trucks of model year 2009 and thereafter (CARB 2017).
- Executive Order S-01-07 went into effect in 2010 and requires a reduction in the carbon intensity of transportation fuels used in California by at least 10 percent by 2020 (CARB 2009). It imposes fuel requirements on fuel that will be sold in California that will decrease GHG emissions by reducing the full fuel-cycle and the carbon intensity of the transportation fuel pool in California.
- The Advanced Clean Cars program, introduced in 2012, combines the control of smog, soot causing pollutants and greenhouse gas emissions into a single coordinated package of requirements for model years 2017 through 2025 (CARB 2012).

For operational activities, annual electricity and natural gas consumption were calculated using demand factors provided in the CalEEMod output as part of the air quality and GHG analysis evaluated in Section 5.1, Air Quality, and Section 5.3, Greenhouse Gas Emissions, of this DEIR and included in Appendix B. The Project's electrical consumption was estimated to be approximately 4,585,909 kWh (approximately 4.59 million kWh) of electricity per year and the natural gas consumption was estimated to be approximately 8,669,777 kilo-British thermal units (kBtUs) or approximately 86,697.77 therms (Appendix B, Table 3 – Annual Energy Consumption from Operation). The electricity use associated with the Project water consumption was also estimated to be approximately 357,611 kWh per year (Appendix B, Table 3 – Annual Energy Consumption from Operation).

Southern California Edison (SCE), the electricity provider for the Project site, produced approximately 87.7 billion kWh of electricity in 2014 and SoCal Gas (SCG), the natural gas provider for the site, produced approximately 5.0 million therms in 2014. At full build-out, the Project's electricity demand would be a negligible amount of the existing electricity demand in SCE's service area and the natural gas demand would be a negligible percent of the existing natural gas use in SCG's service area. As such, there will be adequate capacity to serve the proposed Project.

Energy impacts associated with transportation during operation were also assessed using the traffic data contained in the air quality and GHG analysis. Based on the annual VMT, gasoline and diesel consumption rates were calculated using the South Coast Air Quality Management District-specific miles per gallon in EMFAC2014. As shown below in **Table 6-L – Annual Fuel Consumption**, a total of 357,089 gallons of diesel fuel, and 952,452 gallons of gasoline is estimated to be consumed each year.



**Table 6.0-N: Annual Fuel Consumption<sup>a</sup>**

Fuel Type <sup>b, c</sup>	Fuel Consumption (gallons/year)
Gasoline	952,452
Diesel	357,089

Notes:

<sup>a</sup> Source: Table 3 - Annual Energy Consumption from Operation, Appendix B of DEIR.

<sup>b</sup> Mobile source fuel use based on annual vehicle miles traveled (VMT) from CalEEMod output (DEIR Appendix B) for operational year 2019 and fleet-average fuel consumption in gallons per mile from EMFAC2014 web based data in the South Coast Air Quality Management District.

<sup>c</sup> Operational VMT for the Project was calculated at 26,070,182 miles per year based on the CalEEMod output (Appendix B).

Regulations previously identified related to energy conservation and fuel efficiency include, but are not limited to, Title 24 requirements for windows, roof systems, and electrical systems, and Pavley standards and Advanced Clean Cars Program. Implementation of the Riverside County Climate Action Plan (CAP) Screening Table measures (Appendix D) identified in Section 5.3, and mitigation measures **MM AQ 1** through **MM AQ 4** and **MM AQ 5** identified in Section 5.1, Air Quality, also serve to reduce energy and fuel consumption.

Specifically, mitigation measures **MM AQ 1** and **MM AQ 3** will reduce mobile emissions by encouraging use of car-share, carpool, and ultra-low or zero emission vehicles. **MM AQ 2** will also reduce mobile emissions by requiring signal synchronization of the signal at the intersection of Temescal Canyon Road and Indian Truck Trail as well as proposed signals at Project driveways 2, 3, and 4. Mitigation measure **MM AQ 5** has the potential to further reduce fuel use by encouraging telecommuting and alternative work schedules for future office tenants.

Collectively, compliance with regulatory programs and implementation of these mitigation measures would ensure that the Project would not result in the inefficient, unnecessary, or wasteful consumption of energy.

### 6.5.2 Energy Standards

Implementation of the proposed Project would comply with applicable County, state, and federal energy conservation measures. Many of the regulations regarding energy efficiency are focused on increasing building efficiency and renewable energy generation, promoting sustainability through energy conservation measures, as well as reducing water consumption and vehicle miles traveled. As described in Section 5.1 and Section 5.3 of the DEIR, the proposed Project will meet and/or exceed these regulatory requirements. Additionally, the Project will be consistent with applicable goals and policies of the County's General Plan and the County CAP.

There are no unusual characteristics of the Project that would necessitate the use of equipment during construction or operation that would be less energy-efficient than at comparable facilities in other parts of the State. Through implementation of energy conservation measures and sustainable practices, the Project will not use large amounts of energy in a manner that is wasteful or otherwise inconsistent with adopted plans or policies.

## 6.6 References

The following references were used in the preparation of this section of the DEIR:

- CA Board of Equalization 2017 (a) California Board of Equalization, *Net Taxable Gasoline Gallons 10 Year Report*, 2017. (Available at [https://www.boe.ca.gov/sptaxprog/reports/mvf\\_10\\_year\\_report.pdf](https://www.boe.ca.gov/sptaxprog/reports/mvf_10_year_report.pdf), accessed July 18, 2017.)
- CA Board of Equalization 2017 (b) California Board of Equalization, *Taxable Diesel Gallons of Diesel 10 Year Report*, 2017. (Available at [http://www.boe.ca.gov/sptaxprog/reports/Diesel\\_10\\_Year\\_Report.pdf](http://www.boe.ca.gov/sptaxprog/reports/Diesel_10_Year_Report.pdf), accessed July 18, 2017.)
- CARB 2009 California Air Resources Board, *California Air Resources Board, Low Carbon Fuel Standard Basics*. (Available at <https://www.arb.ca.gov/fuels/lcfs/background/basics-notes.pdf>, accessed July 19, 2017.)
- CARB 2012 California Air Resources Board, *LEV III and ZEV Regulation Amendments for Federal Compliance Option*, December 31, 2012. (Available at <http://www.arb.ca.gov/regact/2012/leviiidtc12/leviiidtc12.htm>, accessed July 19, 2017.)
- CARB 2015 California Air Resources Board, *EMFAC Web Database*, December 2015. (Available at <https://www.arb.ca.gov/emfac/>, accessed July 18, 2017.)
- CARB 2017 California Air Resources Board, *Clean Car Standards – Pavely, Assembly Bill 1493*, January 2017. (Available at <https://www.arb.ca.gov/cc/ccms/ccms.htm>, accessed July 19, 2017.)
- GP EIR County of Riverside, *General Plan Final Environmental Impact Report No. 521*, certified December 8, 2015. (Available at <http://planning.rctlma.org/ZoningInformation/GeneralPlan/GeneralPlanAmendmentNo960EIRNo521CAPFebruary2015/DraftEnvironmentallImpactReportNo521.aspx>, accessed August 2, 2017.)
- GP Appendix E-1 County of Riverside, *General Plan Appendix E-1: Socioeconomic Build-Out Assumptions and Methodology*, December 8, 2015. (Available at [http://planning.rctlma.org/Portals/0/genplan/general\\_plan\\_2016/appendices/Appendix%20E-1\\_120815.pdf?ver=2016-04-01-142000-897](http://planning.rctlma.org/Portals/0/genplan/general_plan_2016/appendices/Appendix%20E-1_120815.pdf?ver=2016-04-01-142000-897), accessed on August 31, 2017.)
- SCAQMD (a) South Coast Air Quality Management District, *CEQA Air Quality Handbook, 1993*. (Available at SCAQMD.)

## 7.0 Alternatives to the Proposed Project

The following discussion considers alternatives to implementation of the Project. The discussion examines the potential environmental impacts resulting from each alternative. Through comparisons of these alternatives to the Project, the relative advantage(s) of each can be weighed and analyzed.

The *CEQA Guidelines*, Section 15126.6, identify the parameters within which consideration and discussion of alternatives to the proposed Project should occur. As stated in this section of the guidelines, an EIR shall describe a range of reasonable alternatives to the Project, or to the location of the Project, which would feasibly attain most of the basic objectives of the Project but would avoid or substantially lessen any of the significant effects of the project, and evaluate the comparative merits of the alternatives.

As outlined in *Section 4.0, Environmental Effects Found not to be Significant*, the following environmental topics were found not to be significant during the preparation of the NOP and therefore are not discussed in the EIR:

- Aesthetics
- Agriculture and Forestry Resources
- Cultural Resources
- Geology and Soils
- Hazards and Hazardous Materials
- Mineral Resources
- Population and Housing
- Public Services
- Recreation
- Utility and Service Systems.

These environmental topics were scoped out of the EIR because as a part of the NOP they were determined to be less than significant. For the purposes of the alternatives analysis since none of these topics were determined to be significant, they are not included in the detailed analysis of the alternatives to compare to the Proposed Project.

### 7.1 Project Objectives

As stated in Section 3.0 of this EIR, the Project objectives include:

- Provide a mix of commercial uses that utilizes the site's job-creation potential and that will be conveniently located in proximity to Temescal Valley residents, thereby helping to diversify employment opportunities in the vicinity, provides the chance to live and work within their community, and helps improve the relationship and ratio between jobs and housing.
- Develop the site with a variety of uses that serve the local community and will be located close to residential areas to help reduce automobile trips and travel distances, which can help improve air quality and encourage non-automobile trips.
- Provide a highly convenient location for shopping and dining for the local developing community of Terramor, that caters to the 55+ crowd.
- To create a visually attractive development through consistent application of architectural elements and landscape standards that will create a community atmosphere, reinforce the community character of Temescal Valley utilizing specific quality materials and Tuscan theme styles.
- To establish a well-balanced and carefully planned collection of specialized and general retail uses that can take full advantage of the site's visibility and accessibility from I-15 Freeway/ transit corridor.
- To provide daytime and nighttime shopping and dining opportunities in a safe and secure environment.
- To augment the County's economic base by providing a variety of sales tax-generating uses which minimize leakage of sales from Temescal Valley.
- To accommodate the development of a balance of land uses that maintains and enhances the county's fiscal viability, economic diversity, and environmental integrity.

## 7.2 Significant Unavoidable Impacts

The CEQA Guidelines required that an EIR disclose the significant environment effects of a project which cannot be avoided if the proposed project is implemented (CEQA Guidelines Section 15126(b)). As described in detail in Section 5.0 of this EIR, the proposed Project is anticipated to result in the following impacts related to air quality and traffic that cannot be reduced to below a level of significance after the implementation of relevant standard conditions of approval, compliance with applicable laws and regulations, and application of feasible mitigation measures. The significant effects of the proposed Project that cannot be feasibly mitigated are as follows:

- **Air Quality Threshold: Conflict with or obstruct implementation of the applicable air quality plan.** Because the proposed Project is not compliant with local and use plans and population projections and because the proposed Project will exceed the SCAQMD regional significance threshold for NO<sub>x</sub>, the proposed Project would conflict with or obstruct implementation of the AQMP and impacts will be significant and unavoidable.

- **Air Quality Threshold: Violate any air quality standard or contribute substantially to an existing or projected air quality violation.** The long-term operation emissions will only exceed the threshold for NO<sub>x</sub>, even with the incorporation of mitigation measures MM AQ 1 through MM AQ 5. Therefore, long-term regional air quality impacts are significant and unavoidable after implementation of mitigation measures.
- **Air Quality Threshold: Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is nonattainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors).** The Project would have a cumulatively considerable increase in emissions due to operational NO<sub>x</sub>, even with implementation of mitigation measures identified above, the impact is significant and unavoidable after implementation of mitigation
- **Traffic Threshold: Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit; or Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways.** Ramp and lane improvements will be necessary to reduce impacts to ramp and freeway segments. However, all state freeways are under the authority of Caltrans. Thus, where traffic increases would affect freeways, there is no mechanism for development project proponents to pay fees or make fair share contributions toward improving mainline freeway lanes. Also, even if there were such a mechanism, there is no way to ensure that such payments would be directed to a specific freeway improvement project. Consequently, there are no feasible mitigation measures for impacts to freeways; impacts would be significant and unavoidable.

The timing of improvements needed to improve levels of service on a regional basis would be determined by the County of Riverside and Caltrans based upon need and the availability of funding. The priority and timing of these improvements cannot be determined now, nor are they under the sole control of the Project proponent or the County of Riverside to implement. Thus, it is possible that the required improvements will not be constructed in time to mitigate the project's direct and cumulative impacts upon freeways to below the level of significance. Therefore, because of the uncertainty of when mitigation would be implemented in relationship to project development, cumulative impacts are significant and unavoidable.

### 7.3 Rationale for Alternative Selection

#### Alternatives Under Consideration

CEQA Guidelines (Section 15162.6(e)) requires that an EIR include an alternative that describes what would reasonably be expected to occur on the property in the foreseeable future if the Project were not approved, based on current plans and consistent with available infrastructure and community services. For development projects that include a revision to an existing land

use plan, the “no project” alternative is considered to be the continuation of the existing land use plan into the future. For projects other than a land use plan, the “no project” alternative is considered to be a circumstance under which the project does not proceed (CEQA Guidelines Section 15126(e)(3)(A-B)). For the alternatives analysis in this EIR, the potential scenario where the Project does not proceed is considered to be the “No Development Alternative,” Alternative 1 while the potential scenario where the existing land use plan is continued into the future is considered to be the “No Project Alternative/ Existing General Plan Designation Alternative,” Alternative 2. The applicant intends to develop the site in two Phases, Phase I is roughly the southern half and Phase II is roughly the northern half. The applicant considered developing Phase I as commercial and Phase II as industrial and office. This alternative is represented in a May 2014 version of the site plan. This alternative represents a mixed use alternative, Alternative 3.

### **Alternatives Considered and Rejected**

Alternative Locations - Development of the Project in an alternative location would have similar impacts as would occur with implementation of the Project at its proposed location, with the potential for greater impacts. All undeveloped land within the surrounding area that is similar in size to the Project site that would be available for Community Development – Light Industrial land use is located farther from I-15 than the Project site. Therefore, operational impacts associated with traffic and vehicular noise and air emissions would be greater as the vehicles traveling to and from the proposed Project would need to travel farther distances on local roads to reach the state highway system.

There is undeveloped property directly adjacent to the Project site, to the northwest and southeast, also between I-15 freeway and Temescal Canyon Rd that is of similar size and is designated as Light Industrial. Development of the proposed Project on an adjacent property would have the same impacts including significant and unavoidable impacts related to air quality and traffic. Therefore, the development of the Project on a different but nearby site would not avoid or lessen the significant impacts from the proposed Project. Because an alternative location would not reduce significant impacts from the Proposed Project it is rejected and not considered further.

Reduced Project - As outlined above, the proposed Project will have significant impacts on air quality from operational NO<sub>x</sub> emissions and on freeway ramp and segment traffic. In order to reduce impacts related to traffic and air quality a reduction in the proposed Project would be required that would in turn reduce the associated vehicle trips. A reduction of approximately 80-85% of operational NO<sub>x</sub> emissions would be required to reduce emissions to below the SCAQMD threshold. To accomplish this an approximate 80-85% reduction in the Project's size based on square footage would be required. An alternative such as this would also be expected to reduce traffic related impacts on the freeway as well. While impacts to air quality and traffic would be reduced to less than significant, this alternative would grossly underutilize the site and would not meet any of the Project objectives. Section 15126.6(f)(1) of the State CEQA Guidelines states that economic viability may be taken in to account when addressing the feasibility of alternatives. This alternative is infeasible because constructing the 15-20% of the total project would still require the extension of water, sewer, and non-potable pipelines as well as storm drain and roadway improvements. The infrastructure costs carried by the significantly reduced Project would make the cost of renting or selling the 15-20% project uncompetitive due to cost. Therefore, a Reduced Project Alternative that would reduce air quality impacts, and presumably

traffic impacts as well, to less than significant levels is economically infeasible and is rejected and not considered further.

The following scenarios are identified by the County of Riverside as potential alternatives to implementation of the proposed Project.

## **7.4 Description of Alternatives Considered**

### **7.4.1 Alternative 1 – No Project/ No Development**

The No Project/No Development Alternative considers no development/disturbance on the Project site beyond which occurs under the existing conditions. As such, the entire 27-acre site would remain vacant and undeveloped. The site would continue to be regularly maintained for weed abatement and wildlife suppression purposes. Under this alternative, no improvements would be made to the Project site and none of the Project roadway, utility, and storm drain system improvements would be completed. This alternative was selected by the Lead Agency to compare the environmental effects of the proposed Project with an alternative that would leave the property in its existing condition.

#### **7.4.1.1 Evaluation of Alternative 1**

##### **Air Quality**

Under the No Project/No Development Alternative, no development would occur on the Project site; therefore there would be no potential sources of short-term (construction) or long-term (operational) air pollutant emissions. All of the Project's short-term and long-term air quality impacts would be avoided under this alternative.

##### **Biological Resources**

The No Project/ No Development Alternative would leave the property in its existing condition; however, routine weed abatement (discing) would be anticipated to continue. Although disturbance of the property would occur under this alternative due to mandatory maintenance obligations for weed abatement, impacts would be less than the proposed Project which includes permanent disturbance of the entire site. This alternative would avoid any new or additional impacts to biological resources.

##### **Greenhouse Gas Emissions**

Under the No Development Alternative, no new development would occur on the site and therefore, there would be no potential sources of near-term or long-term greenhouse gas (GHG) emissions. No impacts would occur under this alternative.

##### **Hydrology and Water Quality**

The No Project/No Development Alternative would result in no grading or development of the property; therefore, no impacts to hydrology or water quality would occur. Erosion and sedimentation may be greater under this alternative, because the site would not receive benefit from the storm water drainage and water quality features and system.

## Land Use and Planning

The No Project/No Development Alternative would leave the property in its existing condition as vacant, disturbed, mostly undeveloped land and the property would not be developed in accordance with the Riverside County General Plan and Elsinore Area Plan's vision for developing a majority of the site with employment-generating Light Industrial uses adjacent to the state highway system.

## Noise

The No Project/No Development Alternative would not result in new sources of stationary noise or new traffic trips. This alternative would not contribute to an incremental increase in area-wide noise levels. Selection of this alternative would avoid all Project related noise impacts.

## Transportation/Traffic

Under the No Project/ No Development Alternative no new would occur that would generate traffic. Significant and unavoidable cumulatively considerable traffic impacts on the state highway system would be avoided with this alternative. However, because there would be no new development on the Project site under this alternative, no monetary contributions would be made by the Project applicant to the Riverside County DIF program or the Transportation Uniform Mitigation (TUMF) program to assist in the funding of needed local and regional circulation network improvements.

## Tribal Cultural Resources

The No Project/No Development Alternative would leave the property in its existing condition as vacant, disturbed, and mostly undeveloped land. Potential impacts to any unknown Tribal Cultural Resources would be avoided with this alternative.

### 7.4.2 Alternative 2 – No Project/ Existing General Plan Designation Alternative

The No Project/ Existing General Plan Alternative considers development of the Project site with land use that is in conformance with the Project site's existing Riverside County General Plan and Elsinore Area Plan land use designation and applicable policies. As shown in **Figure 3.0-5, General Plan Land Use**, the entire site is designated as Community Development - Light Industrial (CD-LI). The CD-LI land use designation allows for industrial and related uses including warehousing/distribution, assembly and light manufacturing, repair facilities, and supporting retail uses with a building intensity range from a minimum 0.25 floor-area-ratio (FAR) to a maximum building intensity of 0.60 FAR. This alternative was selected to compare the environmental effects of the proposed Project with a land use scenario comprised of a land use that is planned for the property by the Riverside County General Plan.

#### 7.4.2.1 Evaluation of Alternative 2

### Air Quality

Under the No Project/Existing General Plan Designation Alternative it is anticipated that the Project site would be developed with general warehousing buildings and surface parking lots.



Because this alternative is consistent with local land use plans and population projects it would not conflict with or obstruct implementation of the AQMP. As compared to the proposed Project that would result in significant impacts as a result of being in conflict with or obstructing implementation of the AQMP, this alternative would not have significant impacts. However, it would be expected to result in operational NOx emissions that would exceed the Regional Significant Threshold and thus would conflict with the AQMP.

It is anticipated that development under this alternative would have similar impacts to air quality related to construction emissions (short-term regional significance thresholds, short-term localized significance threshold) and CO hot spots, as the proposed Project, all of which are less than significant. Commercial uses tend to generate more passenger car trips and less truck trips than warehousing. However, the truck trips associated with warehouse uses tend to be longer trips. Because there are so many variables in estimating emissions, it is difficult to directly compare the level of emissions from different land use types. As with the proposed Project, this alternative is also expected to result in long-term operational emissions that would exceed the threshold for NOx, even with incorporation of mitigation measures, that would be significant and unavoidable and cumulatively considerable.

Under this alternative, the warehousing use would have a higher number of truck trips that would result in increased diesel particulate matter (DPM) emissions, than the proposed Project. However, as the closest sensitive receptors (residential/houses) to the Project site are located approximately 170 meters and across I-15 and significant impacts from DPM emissions would not be expected.

### **Biological Resources**

Because the No Project/Existing General Plan Designation Alternative would disturb the same amount of land as the proposed Project, all of the potential impacts to sensitive species and riparian/riverine habitat types would remain the same.

### **Greenhouse Gas Emissions**

Development consistent with the existing CD-LI land use designation would result in different GHG emissions than the proposed Project. The No Project/Existing General Plan Designation Alternative is expected to result in less than significant impacts related to GHG emissions, as with the proposed Project, because this alternative would also be required to comply with the Riverside County Climate Action Plan. With compliance with the Climate Action Plan, this Alternative's GHG emissions would be less than significant.

### **Hydrology and Water Quality**

The No Project/Existing General Plan Designation Alternative would be expected to have the same or a very similar drainage system design as with the proposed Project. Neither the proposed Project nor the No Project/Existing General Plan Designation Alternative would result in substantial alterations to the drainage pattern of the site or would result in substantial erosion effects (with mandatory compliance with a Storm Water Pollution Prevention Plan (SWPPP)). Accordingly, implementation of either the proposed Project or this alternative would result in less-than-significant impacts to existing drainage patterns. Similar to the proposed Project, this alternative would require compliance with a site-specific Water Quality Management Plan

(WQMP) and to implement project- and site-specific Best Management Practices (BMPs). Therefore, implementation of this alternative would result in less-than-significant water quality impacts with compliance to a SWPPP and site-specific WQMP and its associated BMPs and result in similar impacts as the proposed Project.

### **Land Use and Planning**

The No Project/Existing General Plan Designation Alternative would not require a General Plan Amendment or Change of Zone. There would be no change to the planned use of the site as outlined in the Riverside County General Plan and Elsinore Area Plan. This alternative would not have significant impacts associated with land use and planning.

### **Noise**

The No Project/Existing General Plan Designation Alternative would have similar impacts from highway noise from traffic on I-15 and for short term construction equipment noise and vibration as the proposed Project. These impacts can be reduced to less than significant levels with implementation of mitigation measures. Under this alternative, the warehousing use would have a higher number of truck trips which would replace other vehicle trips associated with commercial/retail use. As with the proposed Project, although traffic will increase on roadways used to access the Project site, because there are currently no sensitive land uses located adjacent to these roadways impacts are less than significant.

### **Transportation/Traffic**

Under the No Project/Existing General Plan Designation Alternative, the warehousing use would generate a higher number of truck trips which would replace other vehicle trips associated with commercial/retail use. As with the proposed Project, improvements to the local roadway network including turn lanes, traffic signals, etc. would be required, along with the Project's fair share contribution toward roadway improvements, to reduce impacts to intersections and roadway segments to less than significant levels.

As with the proposed Project, the same freeway segment and ramp (I-15 southbound segment north of Indian Truck Trail and southbound off-ramp at Indian Truck Trail) would be expected to operate at an unacceptable LOS with E+A+P (P= buildout of a warehousing development for this alternative). This alternative would also be expected to result in cumulatively significant and unavoidable impacts to I-15 as there is no mechanism for development projects to pay fees or make fair share contributions toward improving mainline freeway lanes.

### **Tribal Cultural Resources**

Because the No Project/Existing General Plan Designation Alternative would disturb the same amount of land as the proposed Project, all of the potential impacts to tribal cultural resources that may occur at the site would remain the same.

#### **7.4.3 Alternative 3 – Mixed Use: Light Industrial and Commercial Retail**

The Mixed Use: Light Industrial and Commercial Alternative considers development of the Project site with land use that is in conformance with the Project site's existing Riverside County

General Plan and Elsinore Area Plan land use designation and applicable policies for roughly half of the site in Phase II and roughly half of the site, Phase I would not be in conformance with the General Plan land use designation. Phase I would be developed with fast food and sit down restaurants, a gas station & convenient store, and retail with a combined total of 28,300 square feet of building area. Phase II would be developed with light industrial and office buildings for a total of 131,700 square feet of building area.

#### **7.4.3.1 Evaluation of Alternative 3**

##### **Air Quality**

Phase I development of this alternative would require a General Plan Amendment from CD:LI to CD:CR and as with the proposed Project and would be inconsistent with local land use plans and population projections. However, Phase II development would be consistent with local land use plans and population projects. As with the proposed Project that would result in significant impacts as a result of being in conflict with or obstructing implementation of the AQMP, this alternative would also be in conflict for Phase I and would have significant impacts.

It is anticipated that development under this alternative would have similar impacts to air quality related to construction emissions (short-term regional significance thresholds, short-term localized significance threshold) and CO hot spots, as the proposed Project, all of which are less than significant. As with the proposed Project, this alternative is also expected to result in long-term operational emissions that would exceed the threshold for NOx, even with incorporation of mitigation measures, that would be significant and unavoidable and cumulatively considerable.

Under this alternative, the warehousing use for Phase II would have a higher number of truck trips that would result in increased diesel particulate matter (DPM) emissions, as compared to a commercial/retail use with the proposed Project. However, as the closest sensitive receptors (residential/houses) to the Project site are located approximately 170 meters and across I-15 and significant impacts from DPM emissions would not be expected.

##### **Biological Resources**

Because the Mixed Use: Light Industrial and Commercial Retail Alternative would disturb the same amount of land as the proposed Project, all of the potential impacts to sensitive species and riparian/riverine habitat types would remain the same.

##### **Greenhouse Gas Emissions**

The Mixed Use: Light Industrial and Commercial Retail Alternative is expected to result in similar impacts related to GHG emissions, as with the proposed Project, this alternative would also be required to comply with the Riverside County Climate Action Plan. With compliance with the Climate Action Plan, this Alternative's GHG emissions would be less than significant.

##### **Hydrology and Water Quality**

The Mixed Use: Light Industrial and Commercial Retail Alternative would be expected to have the same or a very similar drainage system design as with the proposed Project. Neither the proposed Project nor the Mixed Use: Light Industrial and Commercial Retail Alternative would

result in substantial alterations to the drainage pattern of the site or would result in substantial erosion effects (with mandatory compliance with a Storm Water Pollution Prevention Plan (SWPPP). Accordingly, implementation of either the proposed Project or this alternative would result in less-than-significant impacts to existing drainage patterns. Like the proposed Project, this alternative would require compliance with a site-specific Water Quality Management Plan (WQMP) and to implement project- and site-specific Best Management Practices (BMPs). Therefore, implementation of this alternative would result in less-than-significant water quality impacts with compliance to a SWPPP and site-specific WQMP and its associated BMPs and result in similar impacts as the proposed Project.

### **Land Use and Planning**

Phase I of this alternative would require a General Plan Amendment and Change of Zone to change the planned use of the site in the Riverside County General Plan and Elsinore Area Plan. The CR land use designation proposed under this alternative for Phase I would not differ substantially from the range of uses that are allowed under existing land use designations and zoning. Thus, this alternative's conversion of areas planned for LI to CR in Phase I is not regarded as a substantial alteration of the planned land use for Phase I of the site and potential impacts would be less than significant.

Phase II would not require a General Plan Amendment or Change of Zone and there would be no change to the planned use of the site for Phase II. Thus, this alternative would not have significant impacts associated with land use and planning.

### **Noise**

The No Project/Existing General Plan Designation Alternative would have similar impacts from highway noise from traffic on I-15 and for short term construction equipment noise and vibration as the proposed Project. These impacts can be reduced to less than significant levels with implementation of mitigation measures. Under this alternative, the warehousing use for Phase II would have a higher number of truck trips which would replace other vehicle trips associated with commercial/retail use. As with the proposed Project, although traffic will increase on roadways used to access the Project site, because there are currently no sensitive land uses located adjacent to these roadways impacts are less than significant.

### **Transportation/Traffic**

Under the Mixed Use: Light Industrial and Commercial Retail Alternative, the warehousing use for Phase II would generate a higher number of truck trips which would replace other vehicle trips associated with commercial/retail use. As with the proposed Project, improvements to the local roadway network including turn lanes, traffic signals, etc. would be required, along with the Project's fair share contribution toward roadway improvements, to reduce impacts to intersections and roadway segments to less than significant levels.

As with the proposed Project, the same freeway segment and ramp (I-15 southbound segment north of Indian Truck Trail and southbound off-ramp at Indian Truck Trail) would be expected to operate at an unacceptable LOS with E+A+P (P= buildout of a warehousing development for this alternative). This alternative would also be expected to result in cumulatively significant and

unavoidable impacts to I-15 as there is no mechanism for development projects to pay fees or make fair share contributions toward improving mainline freeway lanes.

**Tribal Cultural Resources**

Because the Mixed Use: Light Industrial and Commercial Retail Alternative would disturb the same amount of land as the proposed Project, all the potential impacts to tribal cultural resources that may occur at the site would remain the same.

### 7.5 Comparison of Alternatives

**Table 7.0-A: Comparison of Alternatives Matrix**, below, compares the potential environmental impacts of each alternative and ranks each alternative as **less, same, or greater** in comparison to the significance determinations that the proposed Project would have with respect to each issue area.

**Table 7.0-A: Comparison of Alternatives Matrix**

Environmental Issue	Proposed Project	Alternative 1 No Project/No Development	Alternative 2 No Project/ Existing General Plan Designation	Alternative 3 Mixed Use: Light Industrial and Commercial Alternative
Air Quality	The Project would conflict with an air quality plan; would violate air quality standards or contribute substantially to an existing or projected air quality violation; would result in a cumulatively considerable net increase of any criteria pollutant for which the Project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors); would not expose sensitive receptors to substantial pollutant concentrations; or create objectionable odors	Less – Impacts on air quality from construction and operation would be avoided. Impacts would be less than significant.	Less – This alternative would not conflict with an air quality plan. <b>Same</b> - all other air quality impacts from emissions would be similar to the proposed Project. Significant impacts after mitigation.	<b>Same</b> – this alternative would conflict with an air quality plan and this Alternative exceeds the same thresholds as the Project. Significant impacts after mitigation.

Environmental Issue	Proposed Project	Alternative 1 No Project/No Development	Alternative 2 No Project/ Existing General Plan Designation	Alternative 3 Mixed Use: Light Industrial and Commercial Alternative
	affecting substantial number of people. Significant impacts after mitigation.			
Biological Resources	Less than significant impacts to habitat. The Project would comply with MSCHP requirements and would have less than significant impacts, with mitigation.	<b>Less</b> – No loss of land to development and all open space is retained thus, no loss of foraging habitat. No impacts would occur.	<b>Same</b> – No change in conservation from the proposed Project, and less than significant impacts, with mitigation.	<b>Same</b> – No change in conservation from the proposed Project, and less than significant impacts, with mitigation.
Greenhouse Gas (GHG) Emissions	The Project would have a less than significant individual and cumulative impact for GHG emissions with implementation of mitigation measures and compliance with the Riverside County Climate Action Plan. Impacts would be less than significant, with mitigation.	<b>Less</b> – GHG emissions from new construction on the site and operations would be avoided. Impacts would be less than significant.	<b>Same</b> – This Alternative would have a less than significant individual and cumulative impact for GHG emissions with implementation of mitigation measures and compliance with the Riverside County Climate Action Plan. Impacts would be less than significant, with mitigation.	<b>Same</b> – This Alternative would have a less than significant individual and cumulative impact for GHG emissions with implementation of mitigation measures and compliance with the Riverside County Climate Action Plan. Impacts would be less than significant, with mitigation.
Hydrology and Water Quality	The Project would have less than significant impacts related to	<b>Less</b> – No impacts associated with drainage, hydrology, and water	<b>Same</b> – This Alternative would have less than significant impacts related	<b>Same</b> – This Alternative would have less than significant impacts related

Environmental Issue	Proposed Project	Alternative 1 No Project/No Development	Alternative 2 No Project/ Existing General Plan Designation	Alternative 3 Mixed Use: Light Industrial and Commercial Alternative
<p>Land Use and Planning</p>	<p>drainage, hydrology, and water quality with compliance with existing regulations and mitigation measures are not required.</p> <p>The Project would amend the General Plan and Elsinore Area Plan to change the designation of the entire Project site from CD:LI to CD: CR but this change is not a substantial alteration of planned use and impacts are less than significant and mitigation measures are not required.</p> <p>The Project would not affect land use within a city sphere of influence or adjacent city or county; would be compatible with existing surrounding zoning; would be compatible with existing and planned surrounding land uses, land use designations and policies, SCAG's RTP/SCS goals.</p> <p>Impacts would be less than significant.</p>	<p>quality as no construction or operational activities would occur.</p> <p>No impacts would occur.</p> <p><b>Greater</b> – The site would remain vacant and underutilized and thus, not meet the goals and policies of the Riverside County General Plan and Elsinore Area Plan's vision for development of employment-generating Light Industrial uses adjacent to the state highway system.</p> <p>Impacts would be less than significant.</p>	<p>to drainage, hydrology, and water quality with compliance with existing regulations and mitigation measures are not required.</p> <p><b>Less</b> – This Alternative would be consistent with General Plan and Elsinore Area Plan's land use designations, proposed zoning and surrounding land use designations and zoning.</p> <p>Impacts would be less than significant.</p>	<p>to drainage, hydrology, and water quality with compliance with existing regulations and mitigation measures are not required.</p> <p><b>Same</b> – This Alternative would be partially consistent with the General Plan and Elsinore Area Plan's land use designations, proposed zoning and surrounding land use designations and zoning.</p> <p>Impacts would be less than significant.</p>





Environmental Issue	Proposed Project	Alternative 1 No Project/No Development	Alternative 2 No Project/ Existing General Plan Designation	Alternative 3 Mixed Use: Light Industrial and Commercial Alternative
Noise	<p>The Project would not expose people working in the project area to excessive highway noise; result in substantial permanent increase in ambient noise levels; result in substantial temporary or periodic increase in ambient noise levels; expose persons to or generate noise levels in excess of standards; expose persons to or generate excessive groundborne vibration or noise levels.</p> <p>Less than significant impacts, with mitigation.</p>	<p><b>Less</b> – This Alternative would not involve construction of the Project site and would not increase traffic on area roadways that would increase ambient noise. No impacts would occur.</p>	<p><b>Same</b> – This Alternative would still generate noise similar to the Project and can be mitigated with the same mitigation measures. Impacts would be less than significant, with mitigation.</p>	<p><b>Same</b> – This Alternative would still generate noise similar to the Project and can be mitigated with the same mitigation measures. Impacts would be less than significant, with mitigation.</p>
Transportation / Traffic	<p>Impacts related to new or altered maintenance of roads, conflicts with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, and impacts associated with the provision of new or physically altered bike trails, from the Project are less than significant and</p>	<p><b>Less</b> – No generation of new trips. No impacts would occur.</p>	<p><b>Same</b> – This Alternative would generate similar number of trips compared to the proposed Project. Impacts to levels of service for local roadways and intersections can be mitigated through implementation of improvements similar to the proposed Project.</p>	<p><b>Same</b> – This Alternative would generate similar number of trips compared to the proposed Project. Impacts to levels of service for local roadways and intersections can be mitigated through implementation of improvements similar to the proposed Project.</p>

Environmental Issue	Proposed Project	Alternative 1 No Project/No Development	Alternative 2 No Project/ Existing General Plan Designation	Alternative 3 Mixed Use: Light Industrial and Commercial Alternative
	no mitigation measures are required. Impacts to intersections and roadway segments are less than significant with mitigation. Project impacts to freeways are significant and unavoidable.		Project impacts to freeways are significant and unavoidable.	Project impacts to freeways are significant and unavoidable.
Tribal Cultural Resources	Less than significant impacts. No known tribal cultural resources occur onsite.	<b>Less</b> – No construction and grading activities would not occur and thus there is less potential to impact any unknown tribal cultural resources that could occur on site. No impacts would occur.	<b>Same</b> – No change in construction and grading footprint.	<b>Same</b> – No change in construction and grading footprint.
Environmentally Superior to Proposed Project?	<b>Not applicable</b>	<b>Yes</b>	<b>Yes</b>	<b>No</b>
Meets Project Objectives?	<b>Yes</b> (Meets 8 of the 8 Project Objectives)	<b>No</b> – this alternative does not meet any of the 8 Project Objectives.	<b>No</b> – this alternative meets 3 of the 8 Project Objectives.	<b>Yes</b> - this alternative meets 4 of the 8 Project Objectives.

## **7.6 Environmentally Superior Alternative**

### **Alternative 1 – No Project/ No Development**

Because this alternative would avoid most of the project's impacts, it warrants consideration as the "environmentally superior alternative." This alternative would have greater impacts than the proposed Project related to Land Use and Planning as the site would remain vacant and underutilized and thus, not meet the goals and policies of the Riverside County General Plan and Elsinore Area Plan's vision for development of employment-generating Light Industrial uses adjacent to the state highway system. However, all other types of impacts would be avoided or lessened with this alternative as compared to the proposed Project. This alternative does not meet any of the 8 project objectives.

### **Alternative 2 – No Project/Existing General Plan Land Use Designation**

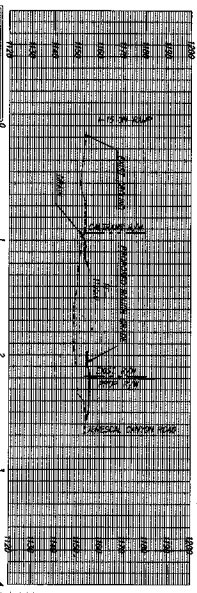
Because this alternative would also avoid more impacts than the proposed Project, it also warrants consideration as the "environmentally superior alternative." This alternative would have less impacts than the proposed Project related to Air Quality, as it would not conflict with the AQMP, and would have less impacts related to Land Use and Planning as it would be consistent with the Riverside County General Plan and Elsinore Area Plan's vision for development of employment-generating Light Industrial uses adjacent to the state highway system. However, all other types of impacts would be avoided or lessened with this alternative as compared to the proposed Project. This alternative does not meet any of the 8 project objectives.

### **Alternative 3 – Mixed Use: Light Industrial and Commercial Retail**

This alternative would not avoid most of the project's impacts. This alternative would have similar impacts to the proposed Project related to Air Quality, as it would also conflict with the AQMP. All other types of impacts would be similar to the proposed Project. This alternative partially meets 4 of the 8 project objectives.

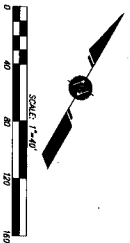
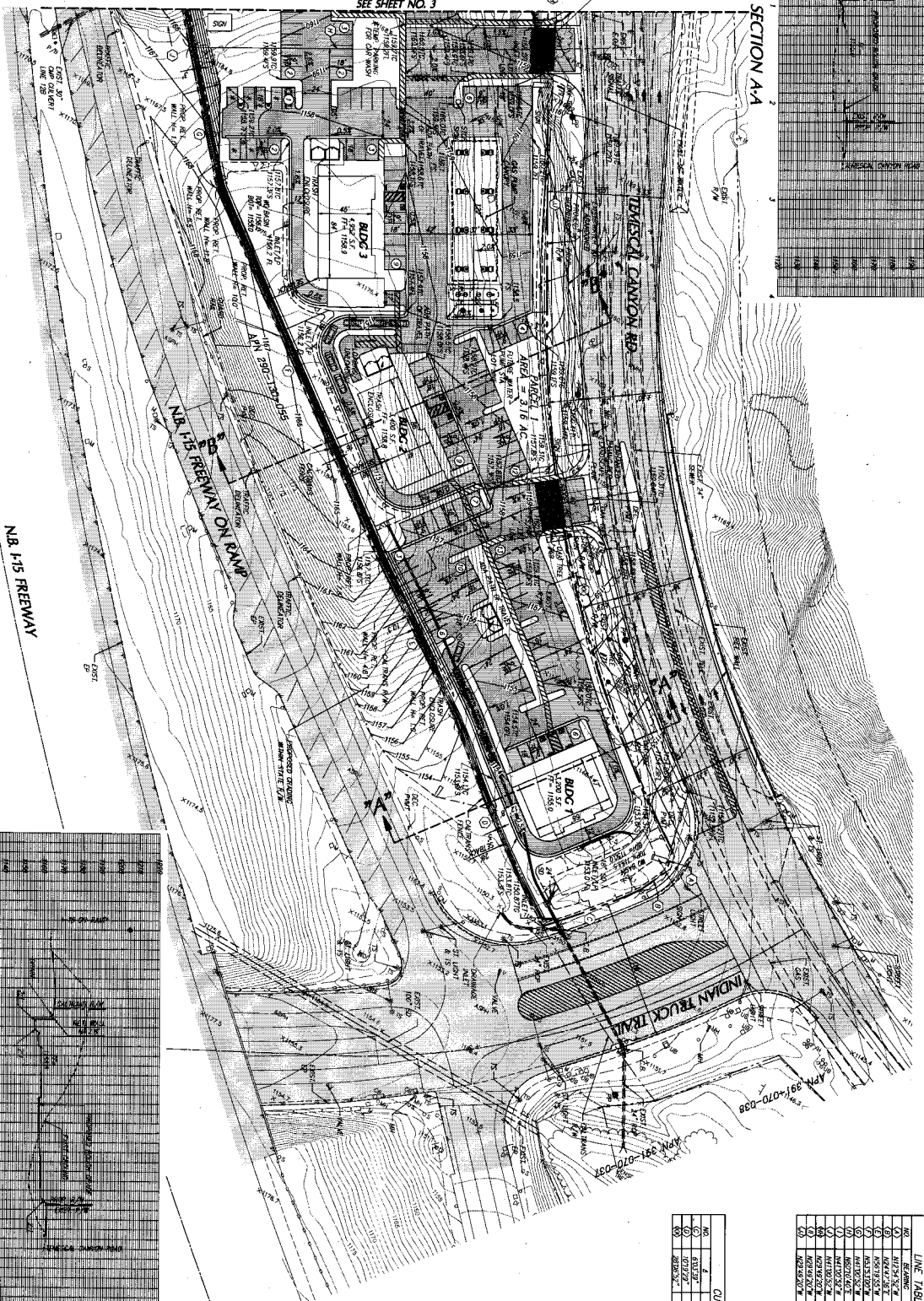


**PROBE SCALE**  
 HORIZ. 1" = 50'  
 VERT. 1" = 25'



SECTION A-A

SEE SHEET NO. 3



NO.	DATE	REVISIONS



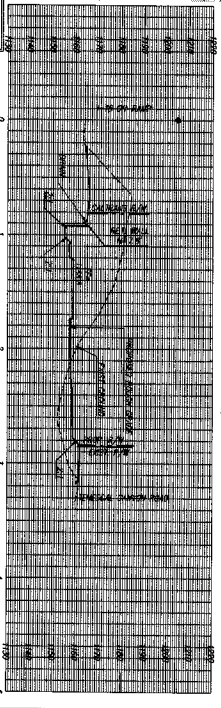
**KA**  
 ENGINEERING  
 LAND PLANNING  
 SURVEYING  
 501 N. SPENGLER STREET  
 OAKLAND, CALIFORNIA 94608  
 TEL (510) 222-4800  
 FAX (510) 222-4800

SCALE: AS SHOWN

COUNTY OF RIVERSIDE  
 TOROYIA VILLAGE AT RIVERSIDE VALLEY  
 RD0154V-2008 - SHEET N  
 PLAN SHEET

SHEET NO. 2 OF 6 SHEETS

**PROBE SCALE**  
 HORIZ. 1" = 50'  
 VERT. 1" = 25'



SECTION B-B

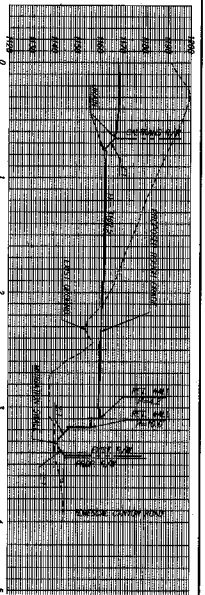
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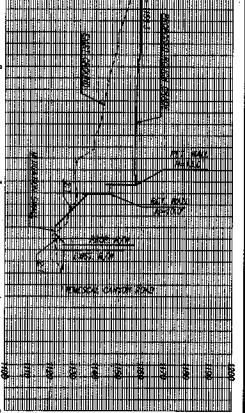
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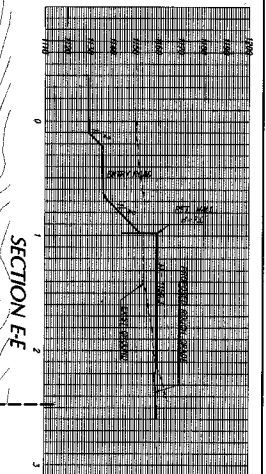
**PROBE SCALE**  
 HORIZ. 1" = 50'  
 VERT. 1" = 25'



SECTION C-C

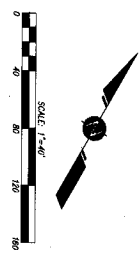


SECTION D-D

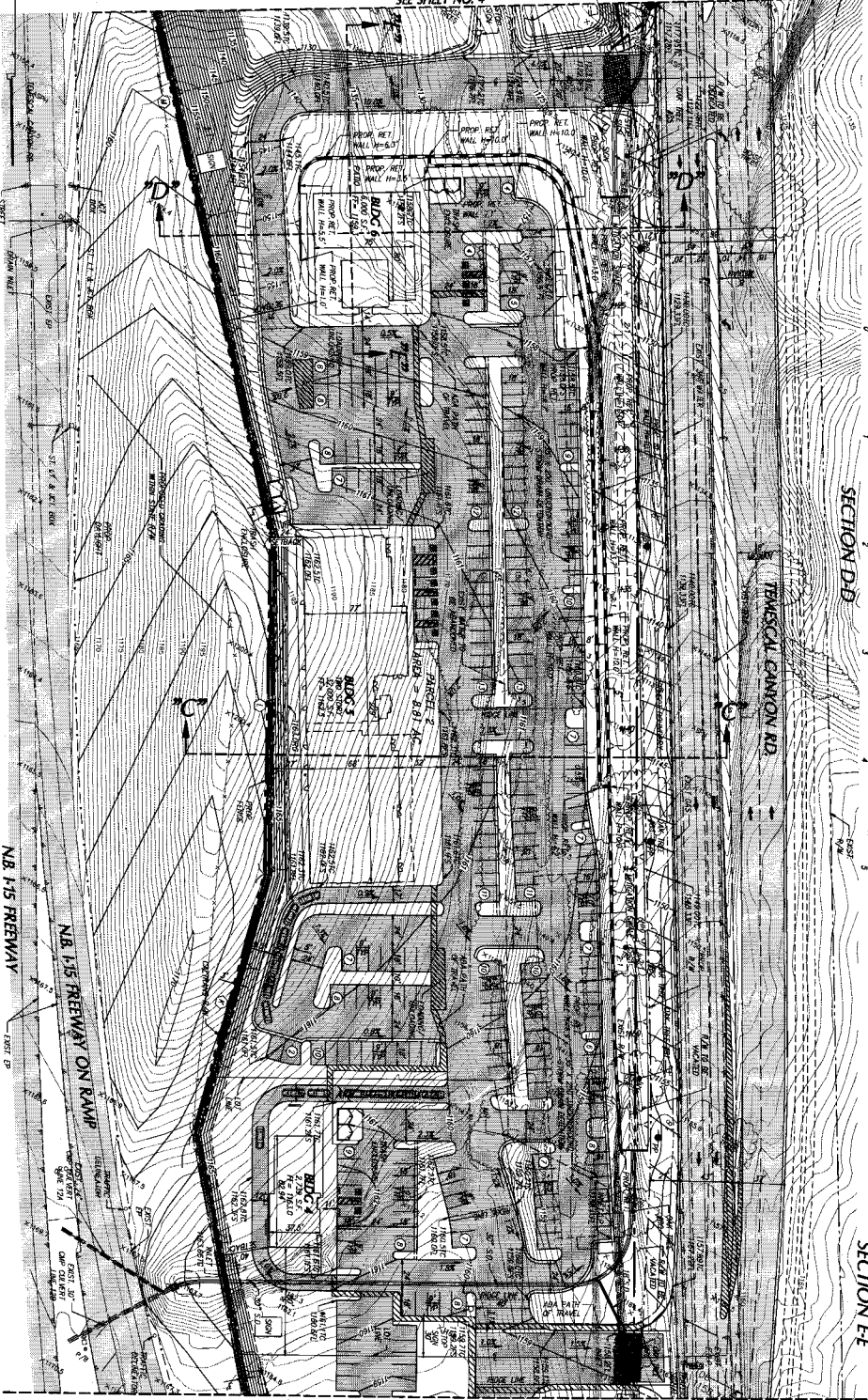


SECTION E-E

PROPOSED PAVING PER LANDSCAPE PLAN



SEE SHEET NO. 4



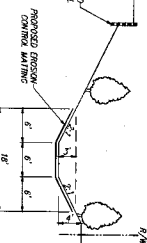
SEE SHEET NO. 2

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4	4.52	4.58	4.64	4.70	4.76	7.50	+	1
5	4.58	4.64	4.70	4.76	4.82	7.50	+	1



MITIGATION SWALE DETAIL

REVISIONS

NO.	BY	DATE	REVISIONS

**KAV** ENGINEERING  
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121 N. SERRANO STREET  
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PROJ. NO.: \_\_\_\_\_  
 SHEET NO. **3** of **6** SITS  
**COUNTY OF RIVERSIDE**  
**TRUCKEE VALLEY AT TRINITY VALLEY**  
**PHASE II DEVELOPMENT**  
**PLAN SHEET**

DATE: 05/24/2011 10:40 AM

NO.	DATE	DESCRIPTION
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3	3/14/57	REVISED
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5	5/14/57	REVISED
6	6/14/57	REVISED
7	7/14/57	REVISED
8	8/14/57	REVISED
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11	11/14/57	REVISED
12	12/14/57	REVISED

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DATE	BY	DATE	REVISIONS

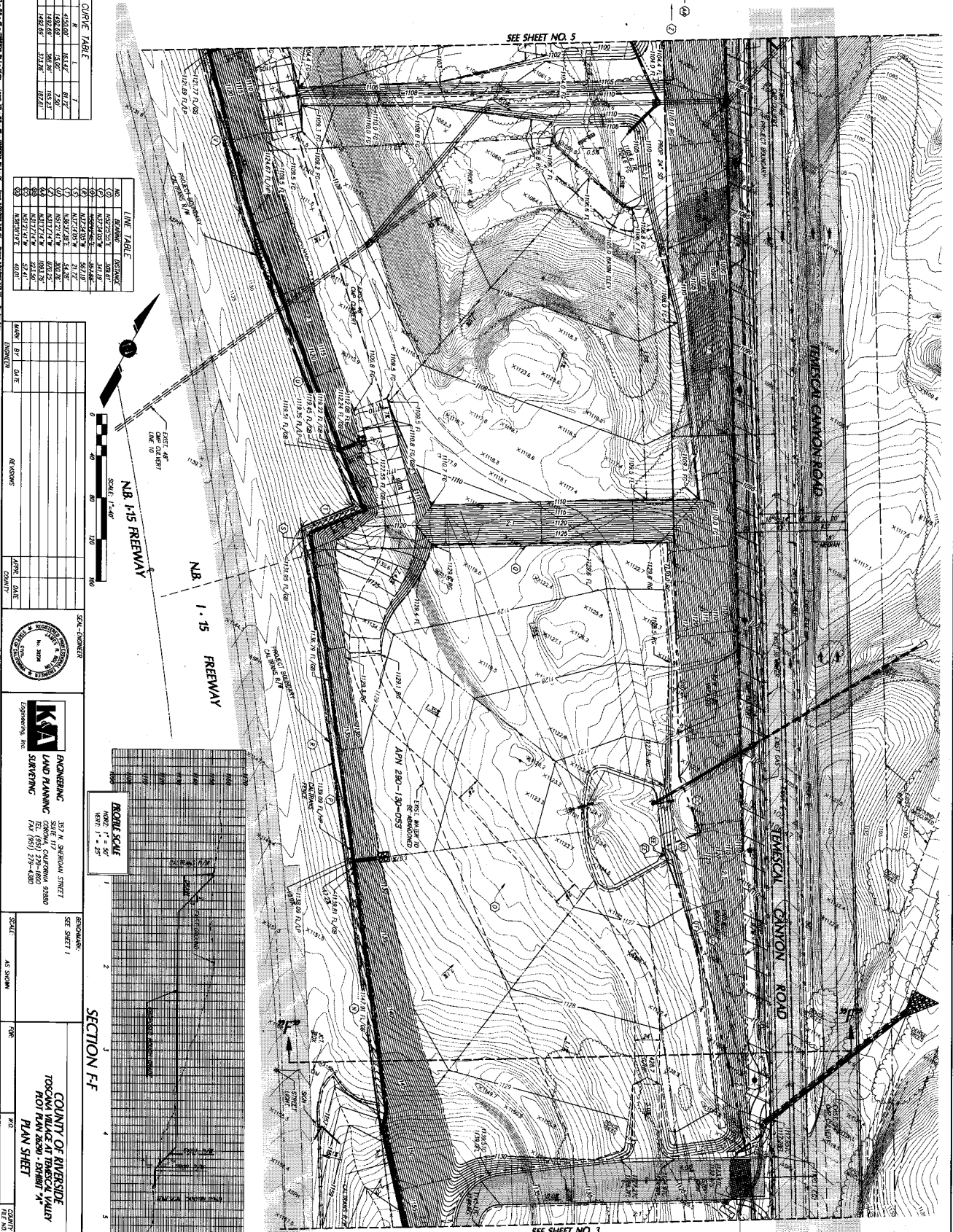
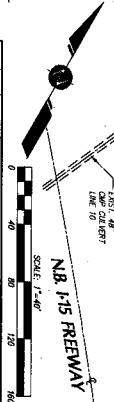
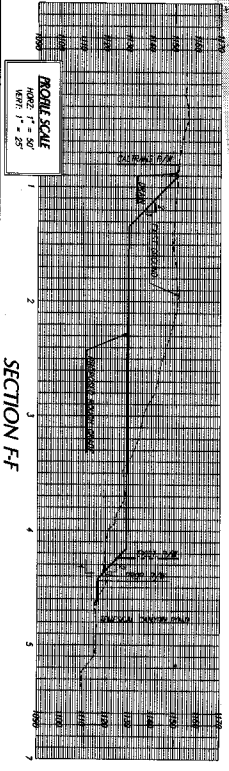


**K.A. ENGINEERING, INC.**  
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 12345 SERRANO STREET  
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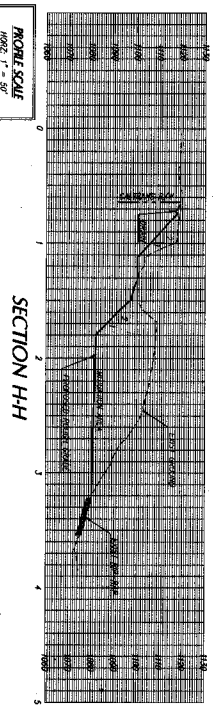
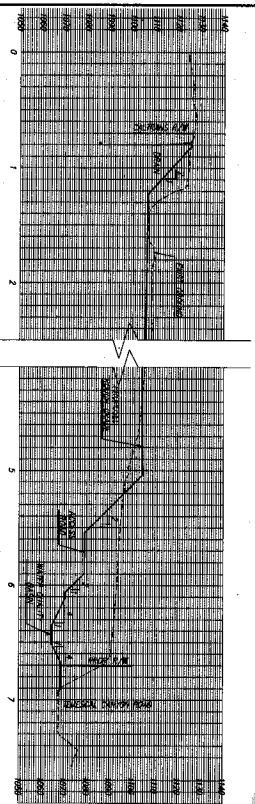
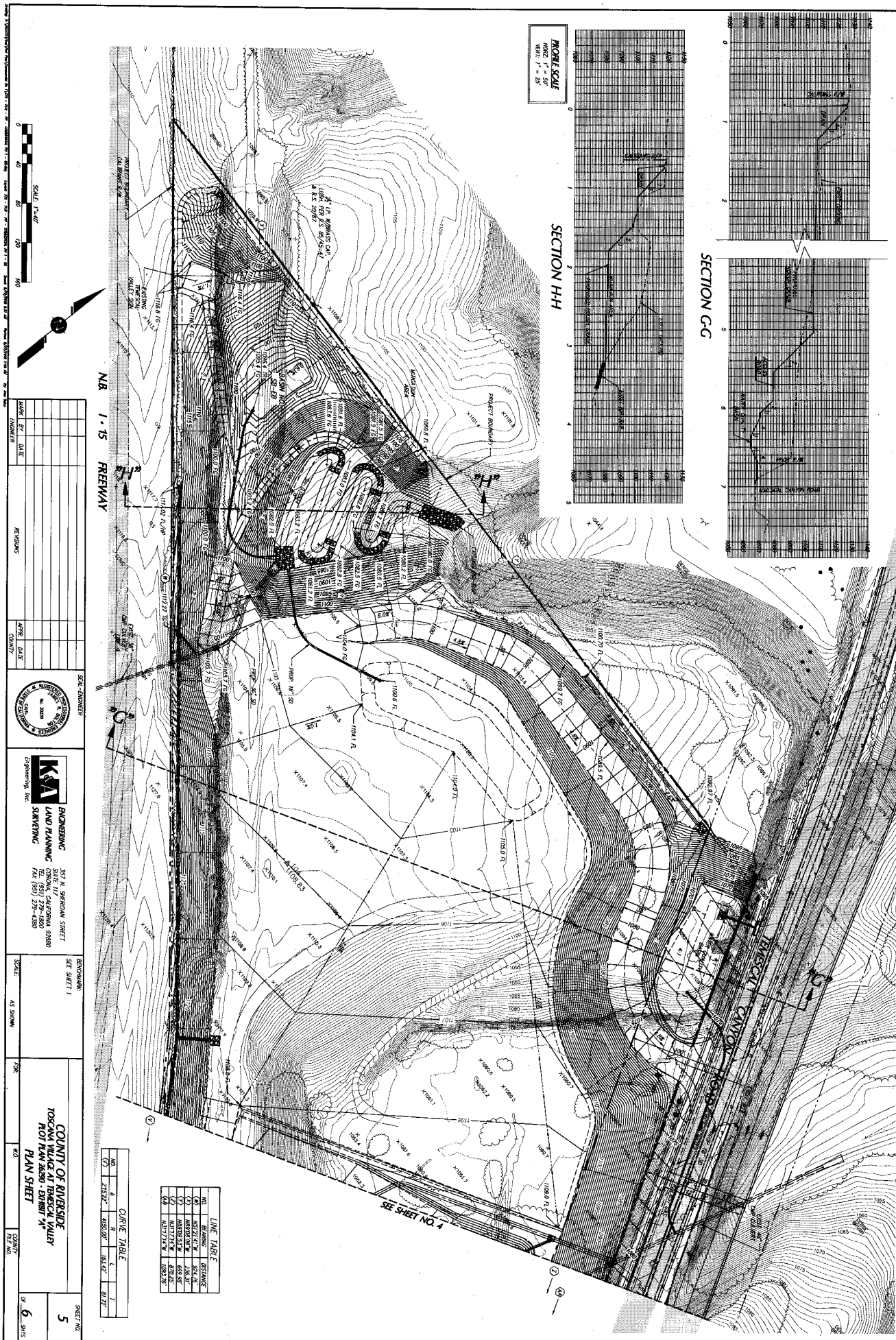
BOUNDARY: SEE SHEET 1  
 COUNTY OF RIVERSIDE  
 TOROYA VALLEY AT TEMESCAL VALLEY  
 HOV PLAN AREA - SHEET 4  
 PLAN SHEET  
 SHEET NO. 4 OF 6 SHEETS

**PROBABLE SCALE**  
 HORIZ. 1" = 50'  
 VERT. 1" = 25'

**SECTION F-F**



DESIGNED BY: [Name]  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]  
 DATE: [Date]



**NOTE SCALE**  
 HORIZ. 1" = 60'  
 VERT. 1" = 20'

<p>SCALE 1"=60'</p>		<p>DATE</p>		<p>REVISIONS</p>		<p>DATE</p>		<p>DATE</p>	
<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>	
<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>	
<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>	
<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>		<p>NO. BY DATE</p>	

STATE ENGINEER

**KAV**  
 ENGINEERING, INC.  
 ENGINEERING, INC.

**ENGINEERING**  
 LAND PLANNING  
 SURVEYING

323 N. BROADWAY STREET  
 SUITE 111  
 OAKLAND, CALIFORNIA 94608  
 TEL (510) 238-4280  
 FAX (510) 238-4280

PROJECT NO. 12345

SHEET NO. 5

COUNTY OF RIVERSIDE  
 TORJANA VILLAGE AT MASCOCA VALLEY  
 PHASE 1A - SUBDIVISION PLAN SHEET

DATE: 12/15/2023

SCALE: AS SHOWN

DATE: 12/15/2023

DATE: 12/15/2023

DATE: 12/15/2023

DATE: 12/15/2023

**LINE TABLE**

NO.	BEARING	DISTANCE
1	N 0° 00' 00" E	100.00
2	S 0° 00' 00" E	100.00
3	S 0° 00' 00" W	100.00
4	N 0° 00' 00" W	100.00
5	N 0° 00' 00" E	100.00

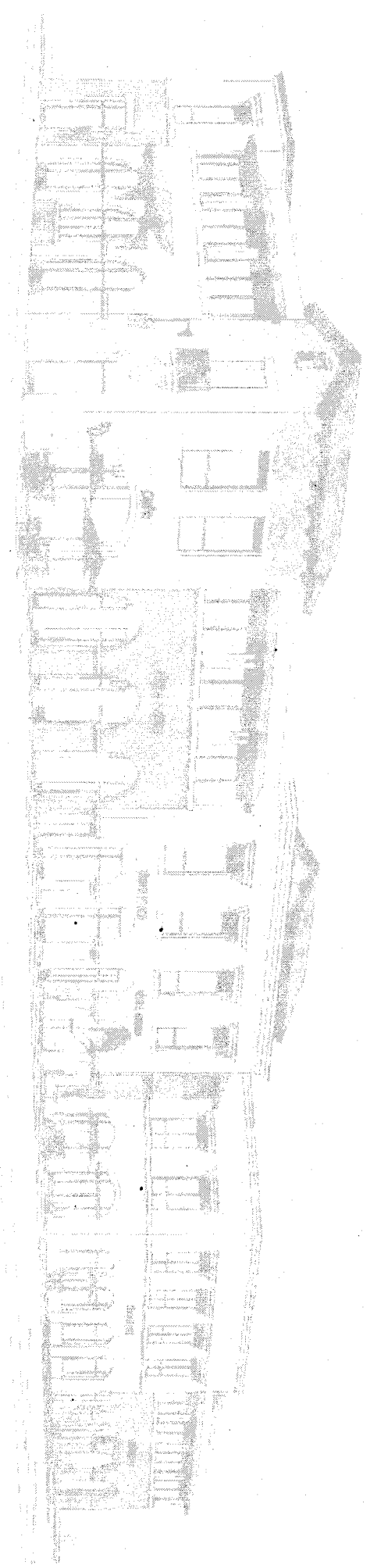
**CHURCH TABLE**

NO.	A	B	C	D	E
1	0	273.22	438.00	583.22	711.44

SEE SHEET NO. 4







### SHEET INDEX

S1	Conceptual Site Plan	A7	Building 4 Elevations
A1	Building 1 Elevations	A8	Building 4 Model Views
A2	Building 1 Model Views	A9	Building 5 Elevations
A3	Building 2 Elevations	A10	Building 5 Model Views
A4	Building 2 Model Views	A11	Building 6 Elevations
A5	Building 3 Elevations	A12	Building 6 Model Views
A6	Building 3 Model Views	A13	Color Board & Materials Schedule
CA2-2	ARCO Canopy Elevations		

SPEEDWAY DEVELOPMENT LLP.  
TOSCANA VILLAGE AT TEMESCAL VALLEY

RIVERSIDE COUNTY, CA

September 13, 2018 16047.06  
DANIELIAN ASSOCIATES  
ARCHITECTURE + PLANNING



**SPEEDWAY DEVELOPMENT LLP.**  
**TOSCANA VILLAGE AT TEMESCAL VALLEY**  
 RIVERSIDE COUNTY, CA

**CONCEPTUAL SITE PLAN**

AUGUST 23, 2018  
 DANIELIAN ASSOCIATES  
 ARCHITECTURE + PLANNING

S1

**PHASE 2**

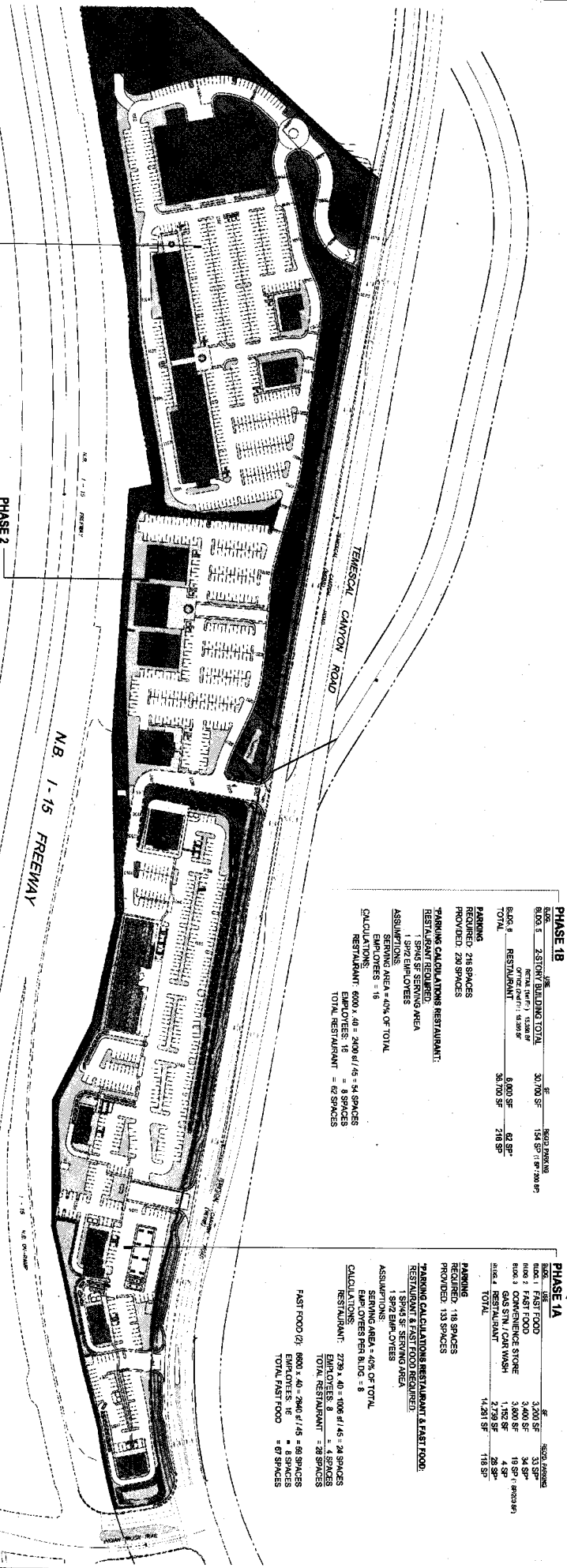
BLDG.	USE	SF	PROV. SPACES
BLDG. 11	PHARMACY	14,000 SF	70 SP (1 service SP)
BLDG. 12	RESTAURANT	3,200 SF	16 SP
BLDG. 13	BANK	3,200 SF	14 SP
BLDG. 14	SUPERMARKET	43,000 SF	215 SP (1 service SP)
<b>TOTAL</b>		<b>63,400 SF</b>	<b>441 SP</b>

**PARKING**  
 REQUIRED: 441 SPACES  
 PROVIDED: 444 SPACES

**PHASE 2**

BLDG.	USE	SF	PROV. SPACES
BLDG. 7	RESTAURANT	6,000 SF	62 SP
BLDG. 8	OFFICE	14,000 SF	70 SP (1 service SP)
BLDG. 9	OFFICE	14,000 SF	70 SP (1 service SP)
<b>TOTAL</b>		<b>34,000 SF</b>	<b>202 SP</b>

**PARKING**  
 REQUIRED: 272 SPACES  
 PROVIDED: 278 SPACES



**PHASE 1B**

BLDG.	USE	SF	PROV. SPACES
BLDG. 5	2-STORY BUILDING TOTAL	30,700 SF	154 SP (1 service SP)
BLDG. 6	OFFICE BUILDING	14,000 SF	70 SP (1 service SP)
BLDG. 8	RESTAURANT	6,000 SF	62 SP
<b>TOTAL</b>		<b>50,700 SF</b>	<b>286 SP</b>

**PARKING**  
 REQUIRED: 286 SPACES  
 PROVIDED: 289 SPACES

**PARKING CALCULATIONS RESTAURANT:**  
 RESTAURANT REQUIRED: 1 SP/45 SF SERVING AREA  
 1 SP/45 EMPLOYEES

**ASSUMPTIONS:**  
 SERVING AREA = 40% OF TOTAL  
 CALCULATIONS:  
 RESTAURANT: 6000 x .40 = 2400 sf / 45 = 54 SPACES  
 EMPLOYEES: 18 = 8 SPACES  
 TOTAL RESTAURANT = 62 SPACES

**PHASE 1A**

BLDG.	USE	SF	PROV. SPACES
BLDG. 1	FAST FOOD	3,200 SF	33 SP
BLDG. 2	FAST FOOD	3,400 SF	34 SP
BLDG. 3	COFFEE STORE	1,200 SF	4 SP
BLDG. 4	Q&S STM / CAR WASH	1,200 SF	4 SP
BLDG. 1	RESTAURANT	2,778 SF	28 SP
<b>TOTAL</b>		<b>14,278 SF</b>	<b>118 SP</b>

**PARKING**  
 REQUIRED: 118 SPACES  
 PROVIDED: 133 SPACES

**PARKING CALCULATIONS RESTAURANT & FAST FOOD:**  
 RESTAURANT & FAST FOOD REQUIRED: 1 SP/45 SERVING AREA  
 1 SP/45 EMPLOYEES

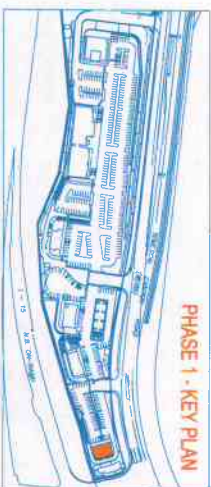
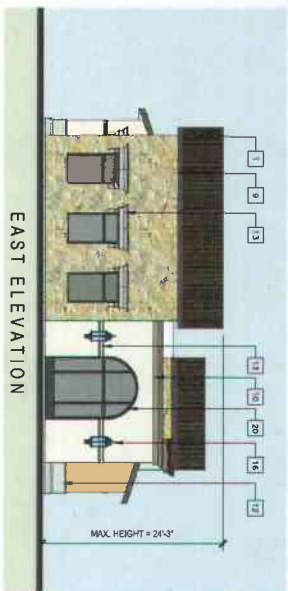
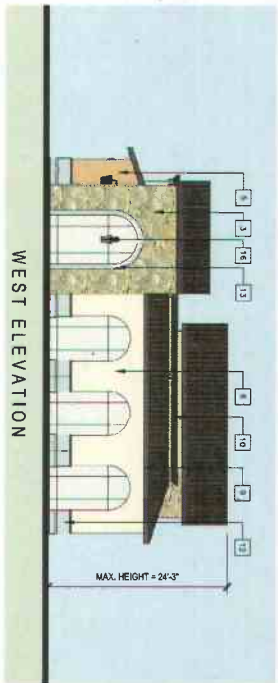
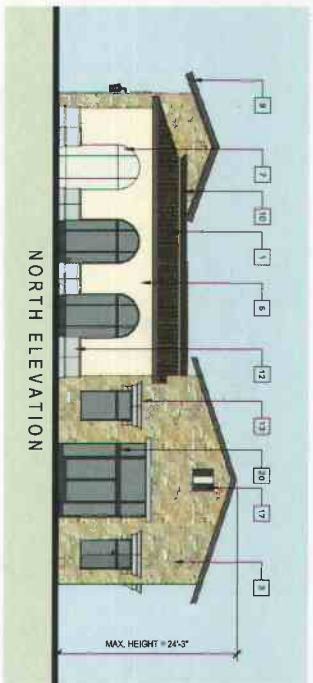
**ASSUMPTIONS:**  
 SERVING AREA = 40% OF TOTAL  
 CALCULATIONS:  
 FAST FOOD: 2778 x .40 = 1111 sf / 45 = 24 SPACES  
 EMPLOYEES: 8 = 4 SPACES  
 TOTAL RESTAURANT = 28 SPACES

**FAST FOOD (2):**  
 6000 x .40 = 2400 sf / 45 = 54 SPACES  
 EMPLOYEES: 18 = 8 SPACES  
 TOTAL FAST FOOD = 62 SPACES

**BUILDING 1 - ELEVATIONS**

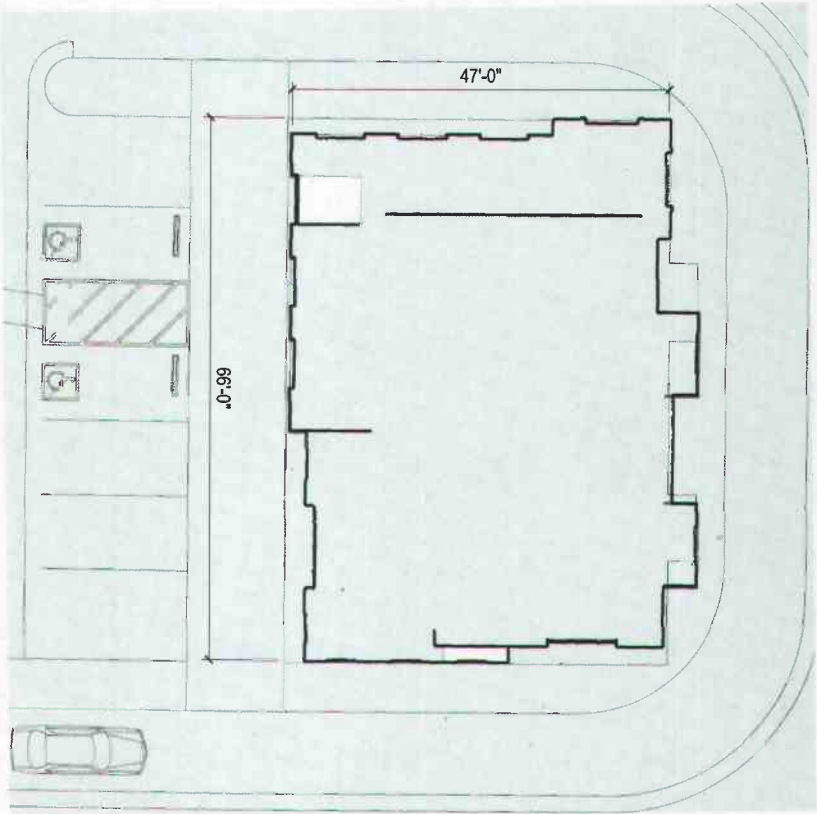
September 13, 2018  
 DANIELLIAN ASSOCIATES  
 ARCHITECTURE + PLANNING  
 A1

(N.T.S.)  
 16047\_06

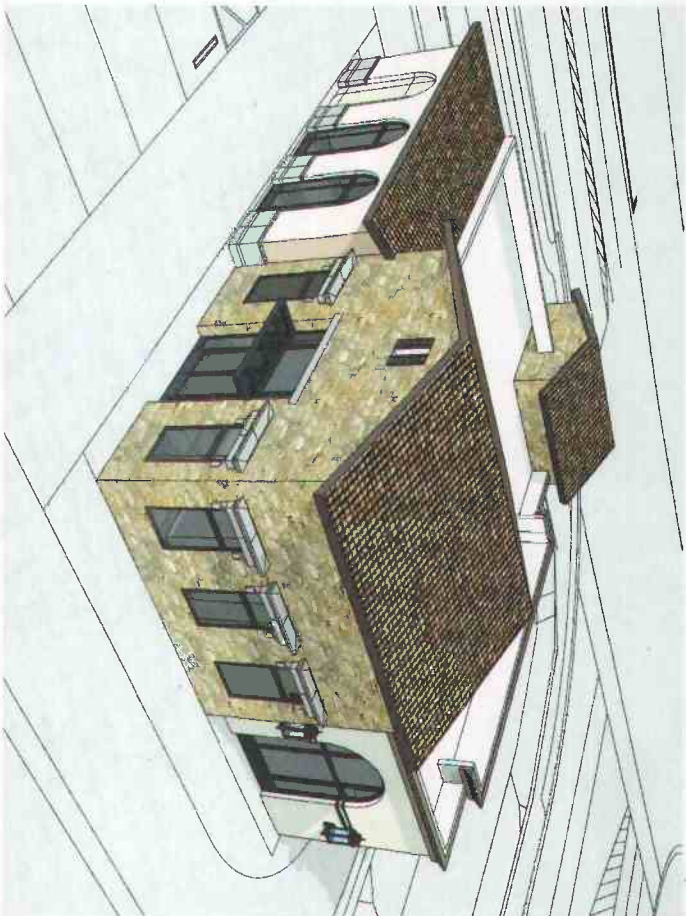


**MATERIALS LEGEND**

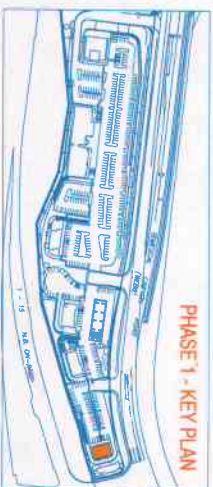
- 1 CLAY TILE ROOF
- 2 RAFTER TAILS
- 3 STONE KENNER
- 4 PAINT 1 - SW 2803 (STUCCO)
- 5 PAINT 2 - SW 6116 (STUCCO)
- 6 PAINT 3 - SW 9111 (STUCCO)
- 7 PAINT 4 - SW 6385 (STUCCO)
- 8 PAINT 5 - SW 7821 (STUCCO)
- 9 PAINT 6 - SW 6076 (STUCCO TRIM 1)
- 10 PAINT 7 - SW 7261 (STUCCO TRIM 2)
- 11 PAINT 8 - SW 8006 (WROUGHT IRON)
- 12 PRE-CAST TOWER/CHIMNEY PANEL
- 13 PRE-CAST TRIM
- 14 PRE-CAST COLUMNS
- 15 METAL & GLASS CANOPY
- 16 DECORATIVE LIGHTING
- 17 DECORATIVE SHUTTER
- 18 DECORATIVE WROUGHT IRON
- 19 WOOD TRELLIS
- 20 ANODIZED ALUMINUM STOREFRONT
- 21 PAINTED METAL CANOPY
- 22 FEATURE WINDOW 2PP OUT NW
- 23 ALUMINUM BREAK METAL PANELS
- 24 ILLUMINATED LIGHT BOX
- 25 INTERNALLY ILLUMINATED WALL SIGN DISPLAY POSTER PANEL
- 26 PAINT 9 - SW 1827 RED SAND
- 27 PAINT 10 - RAL 7022 UMBER GREY



PLAN VIEW



PERSPECTIVE VIEW



**BUILDING 1 - MODEL VIEWS**  
(N.T.S.)

September 13, 2018 16047.06

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ARCHITECTURE + PLANNING A2



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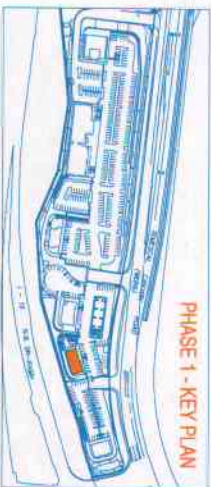
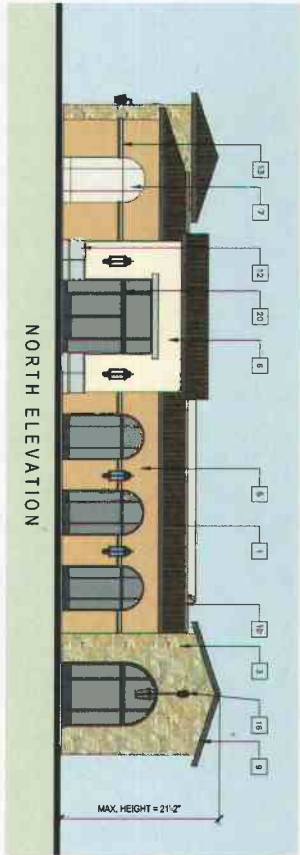
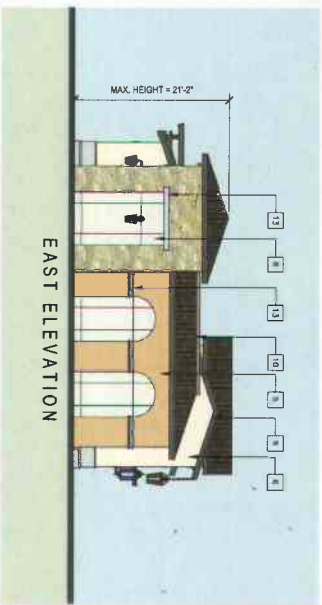
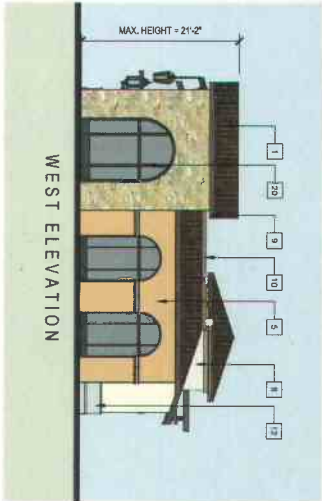
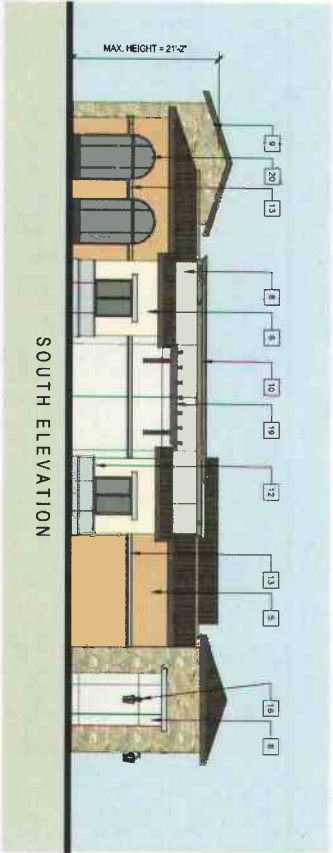
RIVERSIDE COUNTY, CA

**BUILDING 2 - ELEVATIONS**

(N.T.S.)

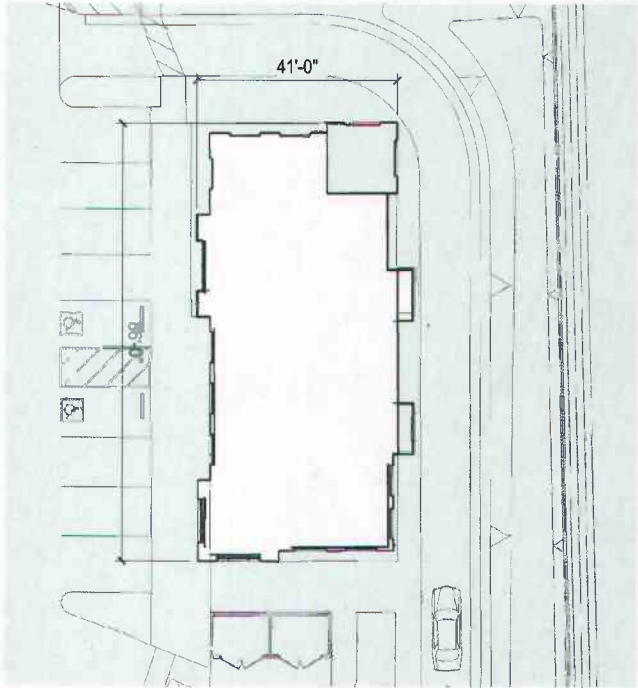
September 13, 2018 16047.06

DANIELLIAN ASSOCIATES  
 ARCHITECTURE + PLANNING



PHASE 1 - KEY PLAN

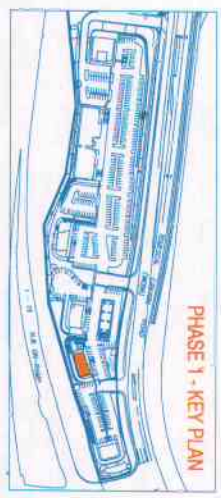
MATERIALS LEGEND	
1	CLAY TILE ROOF
2	Rafter Tails
3	STONE VENEER
4	PAINT 1 - SW 2833 (STUCCO)
5	PAINT 2 - SW 6116 (STUCCO)
6	PAINT 3 - SW 9101 (STUCCO)
7	PAINT 4 - SW 6285 (STUCCO)
8	PAINT 5 - SW 7621 (STUCCO)
9	PAINT 6 - SW 6078 (STUCCO - TRIM 1)
10	PAINT 7 - SW 7358 (STUCCO - TRIM 2)
11	PAINT 8 - SW 6068 (WROUGHT IRON)
12	PRE-CAST WAINSCOT PANEL
13	PRE-CAST TRIM
14	PRE-CAST COLUMNS
15	METAL & GLASS CANOPY
16	DECORATIVE LIGHTING
17	DECORATIVE SHUTTER
18	DECORATIVE WROUGHT IRON
19	WOOD TRELLIS
20	ANODIZED ALUMINUM STOREFRONT
21	PAINTED METAL CANOPY
22	FEATURE WINDOW POP-OUT WITH ALUMINUM BREAK METAL PANELS
23	ILLUMINATED LIGHT BOX
24	INTERNALLY ILLUMINATED WALL SIGN DISPLAY POSTER PANEL
25	PAINT 9 - SW 6241 RED BAY
26	PAINT 10 - PAL 7022 UMBRA GREY



PLAN VIEW



PERSPECTIVE VIEW



**BUILDING 2 - MODEL VIEWS**  
(N.T.S.)

September 13, 2018 16047.06  
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 ARCHITECTURE + PLANNING



A4

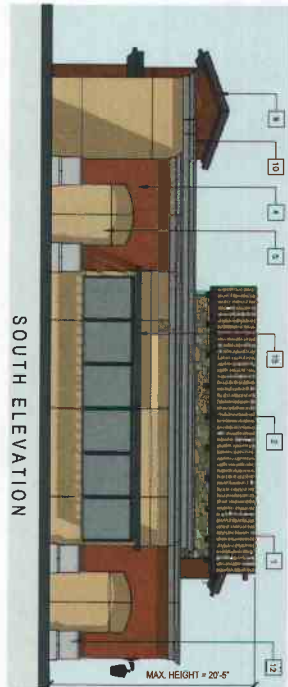
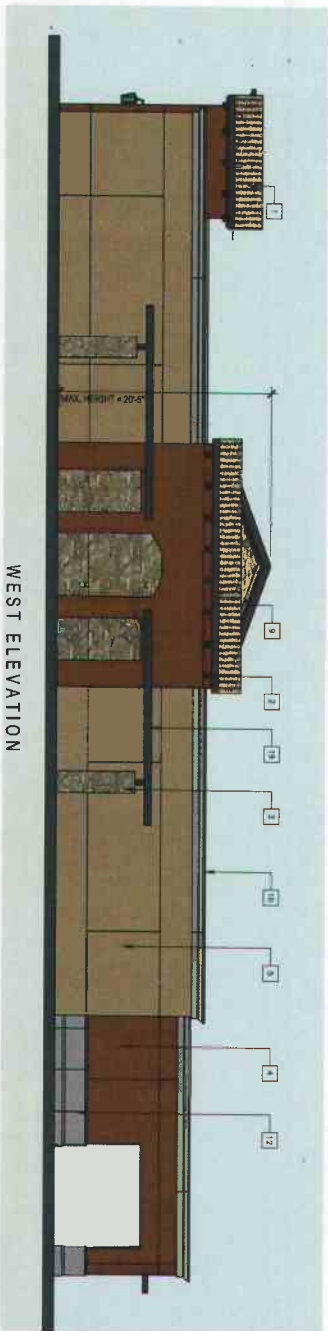
SPEEDWAY DEVELOPMENT LLP.  
**TOSCANA VILLAGE AT TEMESCAL VALLEY**

RIVERSIDE COUNTY, CA

SPEEDWAY DEVELOPMENT LLP.  
**TOSCANA VILLAGE AT TEMESCAL VALLEY**  
 RIVERSIDE COUNTY, CA

**BUILDING 3 - ELEVATIONS**  
 (N.T.S.)

September 13, 2018  
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 ARCHITECTURE + PLANNING  
 A5

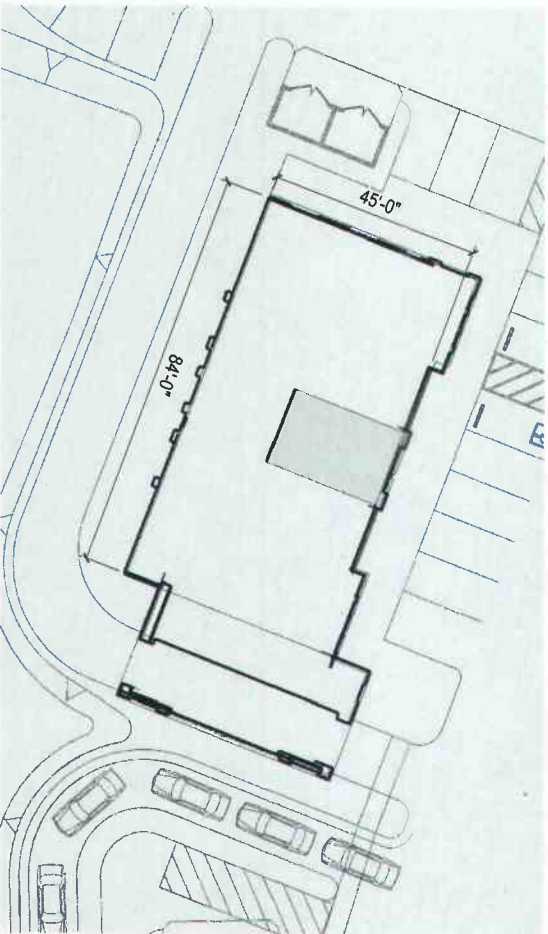


MATERIALS LEGEND	
1	CLAY TILE ROOF
2	WARTER WALLS
3	STONE VENEER
4	PAINT 1 - SW 2803 (STUCCO)
5	PAINT 2 - SW 615 (STUCCO)
6	PAINT 3 - SW 910 (STUCCO)
7	PAINT 4 - SW 688 (STUCCO)
8	PAINT 5 - SW 703 (STUCCO)
9	PAINT 6 - SW 615 (STUCCO) (TRAIL 1)
10	PAINT 7 - SW 706 (STUCCO) (TRAIL 2)
11	PAINT 8 - SW 606 (WOOD/ST. TRAIL)
12	PRE-CAST CONCRETE PANEL
13	PRE-CAST TRIM
14	PRE-CAST COLUMNS
15	METAL & GLASS CANOPY
16	DECORATIVE CORNING
17	DECORATIVE SHUTTER
18	DECORATIVE WOOD/ST. TRAIL
19	WOOD TRELLIS
20	ANODIZED ALUMINUM STOREFRONT
21	PAINTED METAL CANOPY
22	FINISHES: MIMCON PER FOR TRAIL
23	ALUMINUM SCREEN WITH AL. FINISHES
24	LUMINATED SIGN BOX
25	INTERMEDIATE LUMINATED WALL SIGN
26	INTERMEDIATE LUMINATED SIGN
27	PAINT 9 - SW 623 (RED BAY)
28	PAINT 10 - RAL 7022 (DARK GREY)

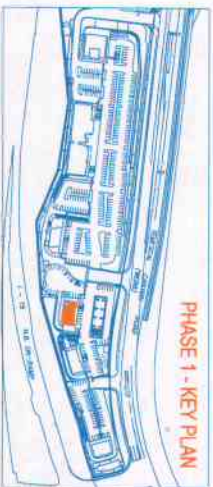




PERSPECTIVE VIEW



PLAN VIEW



**BUILDING 3 - MODEL VIEWS**  
(N.T.S.)

September 13, 2018 16047.06

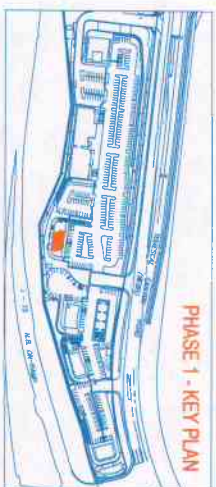
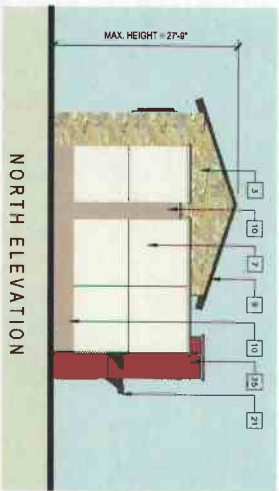
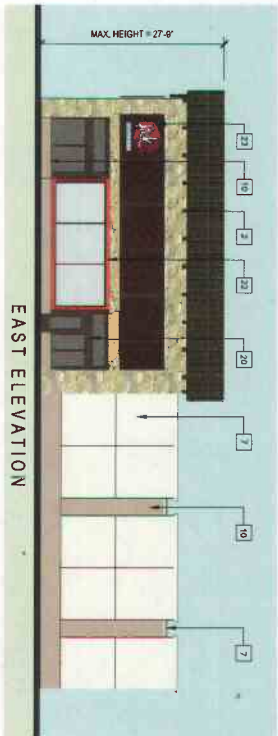
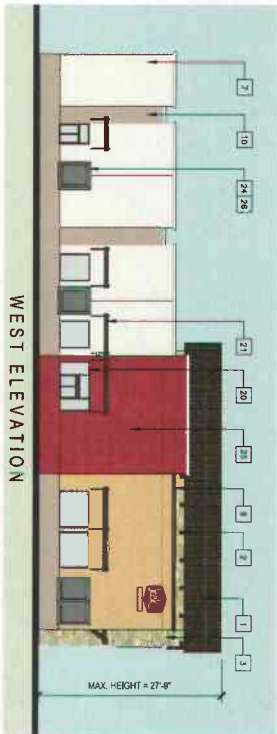
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ARCHITECTURE + PLANNING



A6

SPEEDWAY DEVELOPMENT LLP.  
**TOSCANA VILLAGE AT TEMESCAL VALLEY**

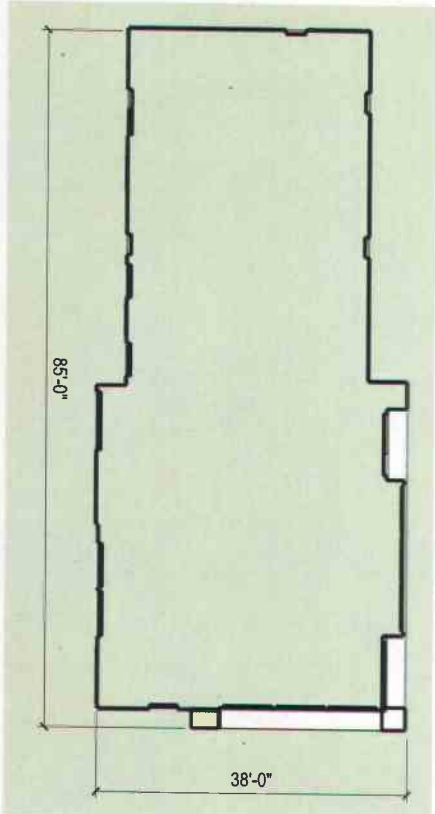
RIVERSIDE COUNTY, CA



**MATERIALS LEGEND**

- 1 CLAY TILE ROOF
- 2 RAFTER TAILS
- 3 STONE VENEER
- 4 PAINT 1 - SW 2863 (STUCCO)
- 5 PAINT 2 - SW 6116 (STUCCO)
- 6 PAINT 3 - SW 9101 (STUCCO)
- 7 PAINT 4 - SW 6385 (STUCCO)
- 8 PAINT 5 - SW 7621 (STUCCO)
- 9 PAINT 6 - SW 6076 (STUCCO TRIM 1)
- 10 PAINT 7 - SW 7506 (STUCCO TRIM 2)
- 11 PAINT 8 - SW 6006 (WROUGHT IRON)
- 12 PRE-CAST MANICOTT PANEL
- 13 PRE-CAST TRIM
- 14 PRE-CAST COLUMNS
- 15 METAL & GLASS CANOPY
- 16 DECORATIVE LIGHTING
- 17 DECORATIVE SHUTTER
- 18 WOOD TRELLIS
- 19 ANODIZED ALUMINUM STOREFRONT
- 20 PAINTED METAL CANOPY
- 21 FEATURE WINDOW PO-OUT W/ ALUMINUM BREAK METAL PANELS
- 22 ILLUMINATED LIGHT BOX
- 23 INTERNALLY ILLUMINATED WALL SIGN DISPLAY POSTER PANEL
- 24 PAINT 9 - SW 6234 RED BAY
- 25 PAINT 10 - RAL 7022 UMBERA GREY

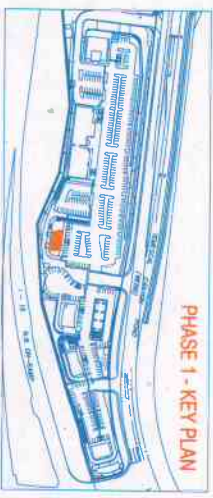
PLAN VIEW



PERSPECTIVE VIEW

SPEEDWAY DEVELOPMENT LLP.  
 TOSCANA VILLAGE AT TEMESCAL VALLEY

RIVERSIDE COUNTY, CA



BUILDING 4 - MODEL VIEWS  
 (N.T.S.)

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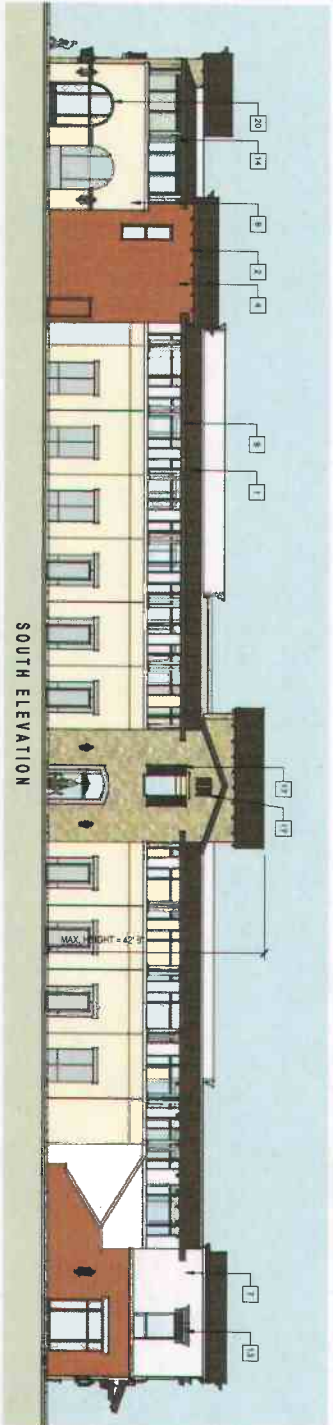
NORTH ELEVATION



EAST ELEVATION



WEST ELEVATION



SOUTH ELEVATION

**MATERIALS LEGEND**

- 1 CLAY TILE ROOF
- 2 PARTIAL TALS
- 3 STONE VENEER
- 4 PAINT 1 - SW 2803 (STUCCO)
- 5 PAINT 2 - SW 6116 (STUCCO)
- 6 PAINT 3 - SW 9101 (STUCCO)
- 7 PAINT 4 - SW 6585 (STUCCO)
- 8 PAINT 5 - SW 7821 (STUCCO)
- 9 PAINT 6 - SW 6076 (STUCCO) (RM 1)
- 10 PAINT 7 - SW 7268 (STUCCO) (RM 2)
- 11 PAINT 8 - SW 6066 (WROUGHT IRON)
- 12 PRE-CAST MANSCOTT PANEL
- 13 PRE-CAST TRIM
- 14 PRE-CAST COLUMNS
- 15 METAL & GLASS CANOPY
- 16 DECORATIVE LIGHTING
- 17 DECORATIVE SPALTER
- 18 DECORATIVE WROUGHT IRON
- 19 WOOD TRELLIS
- 20 ANODIZED ALUMINUM STOREFRONT
- 21 PAINTED METAL CANOPY
- 22 FEATURE WINDOW POP OUT/W
- 23 ALUMINUM BRICK METAL PANELS
- 24 ILLUMINATED LIGHT BOX
- 25 INTERNALLY ILLUMINATED WALL SIGN
- 26 DISPLAY POSTER PANE
- 27 PAINT 9 - SW 627 RED BAY
- 28 PAINT 10 -RAL 7022 LUMINA GREY



SPEEDWAY DEVELOPMENT LLP.  
**TOSCANA VILLAGE AT TEMESCAL VALLEY**

RIVERSIDE COUNTY, CA

**BUILDING 5 - ELEVATIONS**  
 (N.T.S.)

September 13, 2018 16047.06

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 ARCHITECTURE + PLANNING





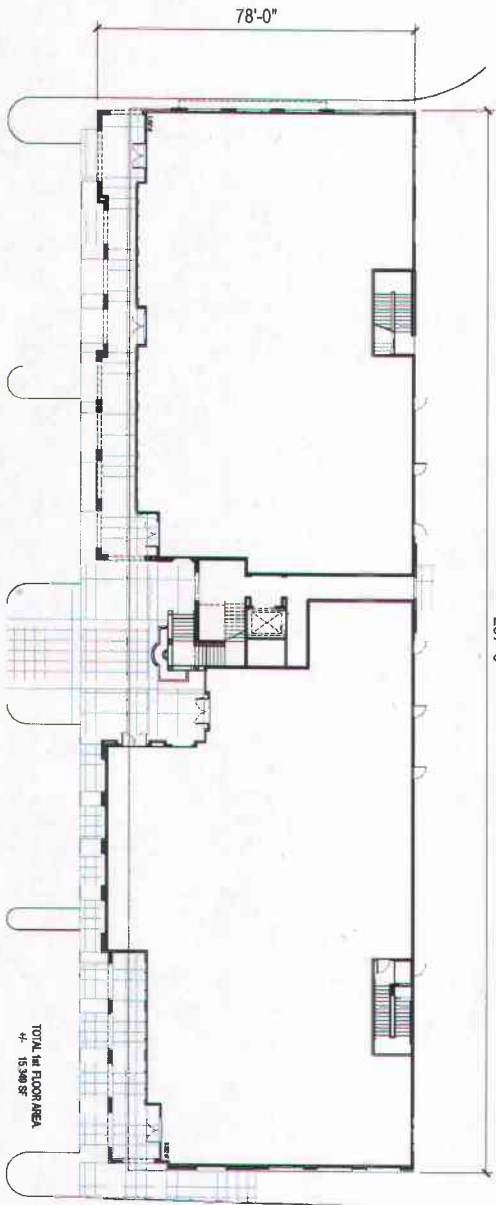
VIEW I



VIEW II

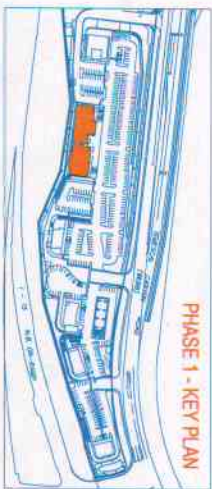


VIEW III



FLOOR PLAN

TOTAL 1st FLOOR AREA  
4 - 15,308 SF



PHASE 1 - KEY PLAN

SPEEDWAY DEVELOPMENT LLP.  
TOSCANA VILLAGE AT TEMESCAL VALLEY

RIVERSIDE COUNTY, CA

BUILDING 5 - MODEL VIEWS

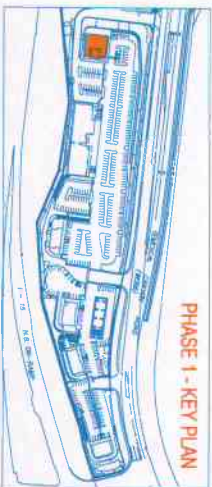
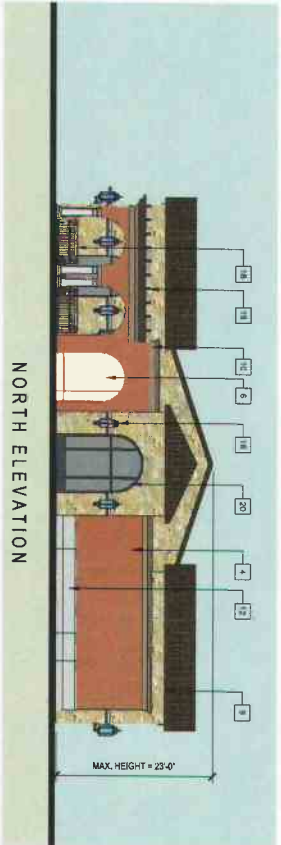
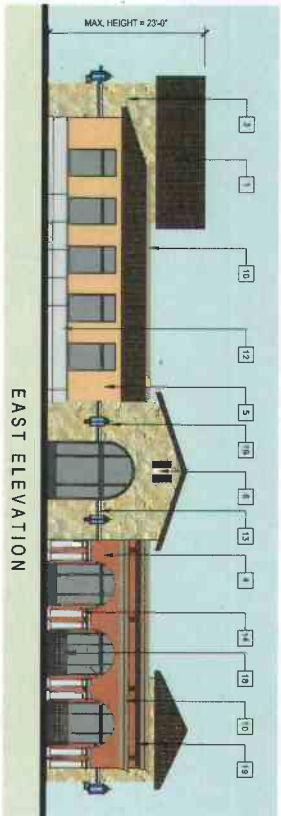
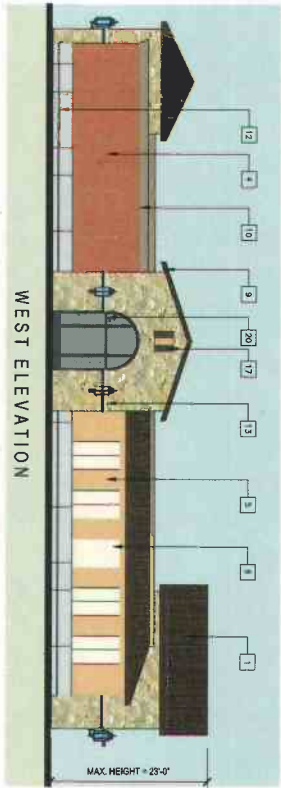
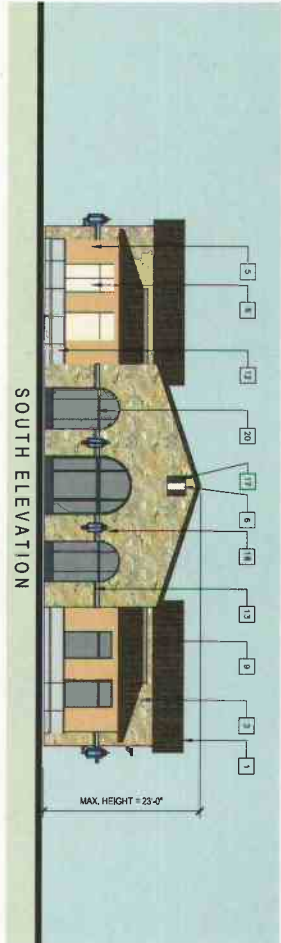
(N.T.S.)

September 13, 2018

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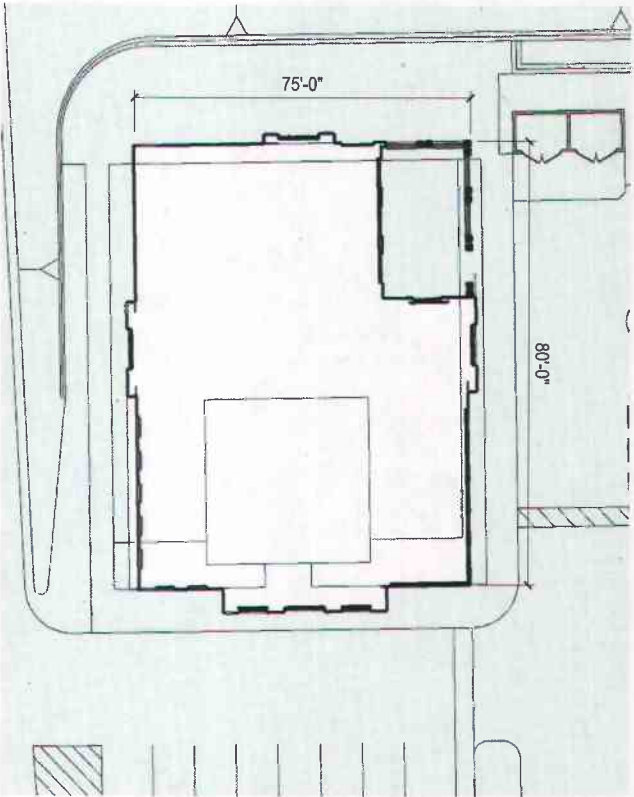
DANIELLIAN ASSOCIATES  
ARCHITECTURE + PLANNING A10





**MATERIALS LEGEND**

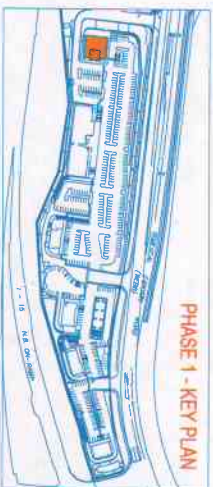
- 1 CLAY TILE ROOF
- 2 RAFTER TAILS
- 3 STONE VENER
- 4 PAINT 1 - SW 2803 (STUCCO)
- 5 PAINT 2 - SW 6116 (STUCCO)
- 6 PAINT 3 - SW 9101 (STUCCO)
- 7 PAINT 4 - SW 5381 (STUCCO)
- 8 PAINT 5 - SW 7021 (STUCCO)
- 9 PAINT 6 - SW 6076 (STUCCO TRIM 1)
- 10 PAINT 7 - SW 7258 (STUCCO TRIM 2)
- 11 PAINT 8 - SW 8005 (WROUGHT IRON)
- 12 PRE-CAST MANISCOT PANEL
- 13 PRE-CAST TRIM
- 14 PRE-CAST COLUMNS
- 15 METAL & GLASS CANOPY
- 16 DECORATIVE LIGHTING
- 17 DECORATIVE SHAFTER
- 18 DECORATIVE WROUGHT IRON
- 19 WOOD TRELLIS
- 20 AVOIDZED ALUMINUM STOREFRONT
- 21 PAINTED METAL CANOPY
- 22 FEATURE WINDOW POP-OUT IN ALUMINUM BREAK METAL PANELS
- 23 ILLUMINATED LIGHT BOX
- 24 INTERNALLY ILLUMINATED WALL SIGN DISPLAY POSTER PANEL
- 25 PAINT 9 - SW 6211 RED BAY
- 26 PAINT 10 - RAL 7022 UMBER GREY



FLOOR PLAN



PERSPECTIVE VIEW



BUILDING 6 - MODEL VIEWS (N.T.S.)

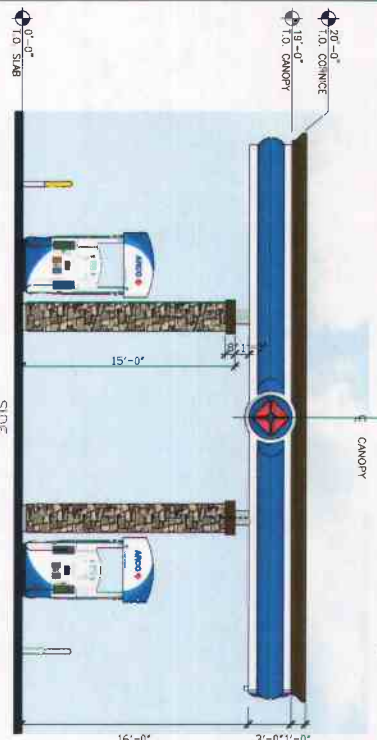
September 13, 2018 16047.06

DANIELIAN ASSOCIATES ARCHITECTURE + PLANNING A12

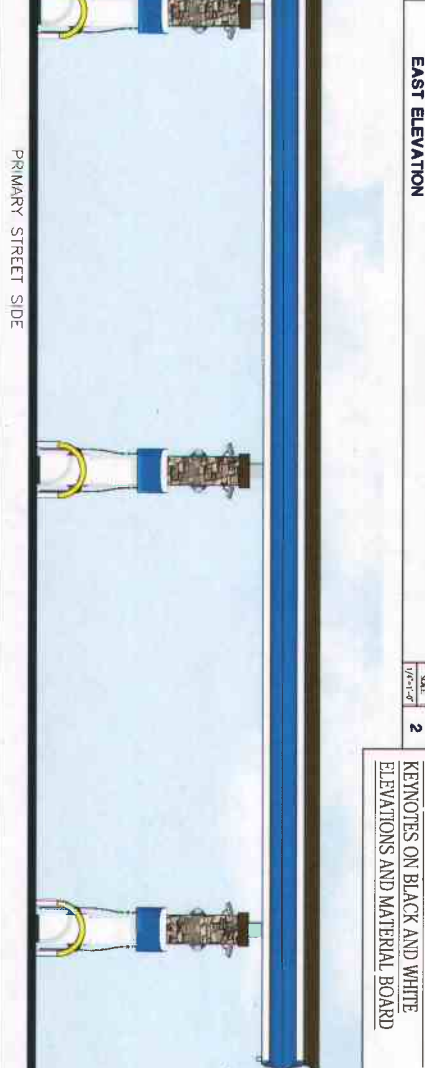
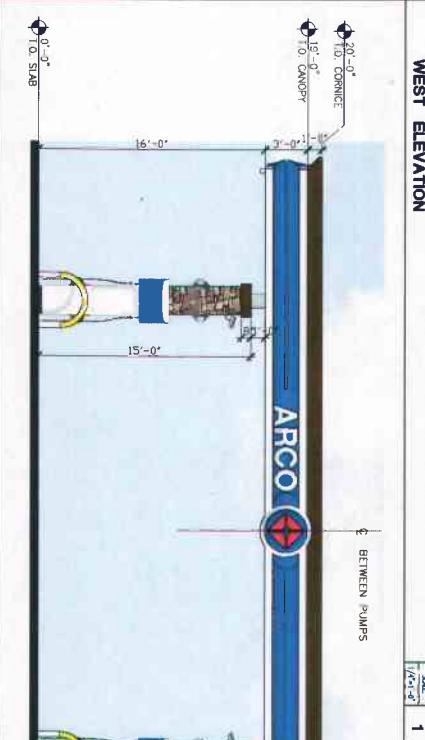


SPEEDWAY DEVELOPMENT LLP.  
TOSCANA VILLAGE AT TEMESCAL VALLEY

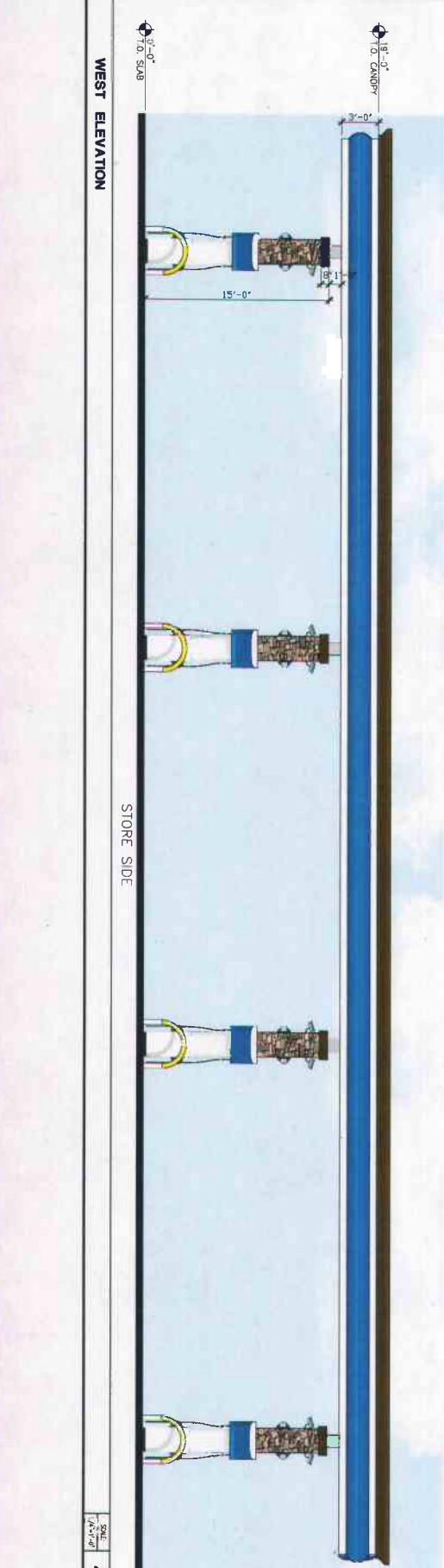
RIVERSIDE COUNTY, CA




NOTE: FINISHES AND PAINT COLOR - SEE KENNOTES ON BLACK AND WHITE ELEVATIONS AND MATERIAL BOARD



SCALE: 1/4" = 1'-0"





ARCO  
BRAND MANAGEMENT COMPANY, LLC

**NOTE: FINISHES AND PAINT COLOR - SEE KENNOTES ON BLACK AND WHITE ELEVATIONS AND MATERIAL BOARD**

SCALE: 1/4" = 1'-0"

1

---

PROJECT: **NOMI TRACK TRAIL**  
8 MPD CANOPY ELEVATIONS

DATE: 07.23.16

SCALE: 1/4" = 1'-0"

SCALE: 1/4" = 1'-0"

3

SCALE: 1/4" = 1'-0"

4

---

**GENERAL NOTES:**

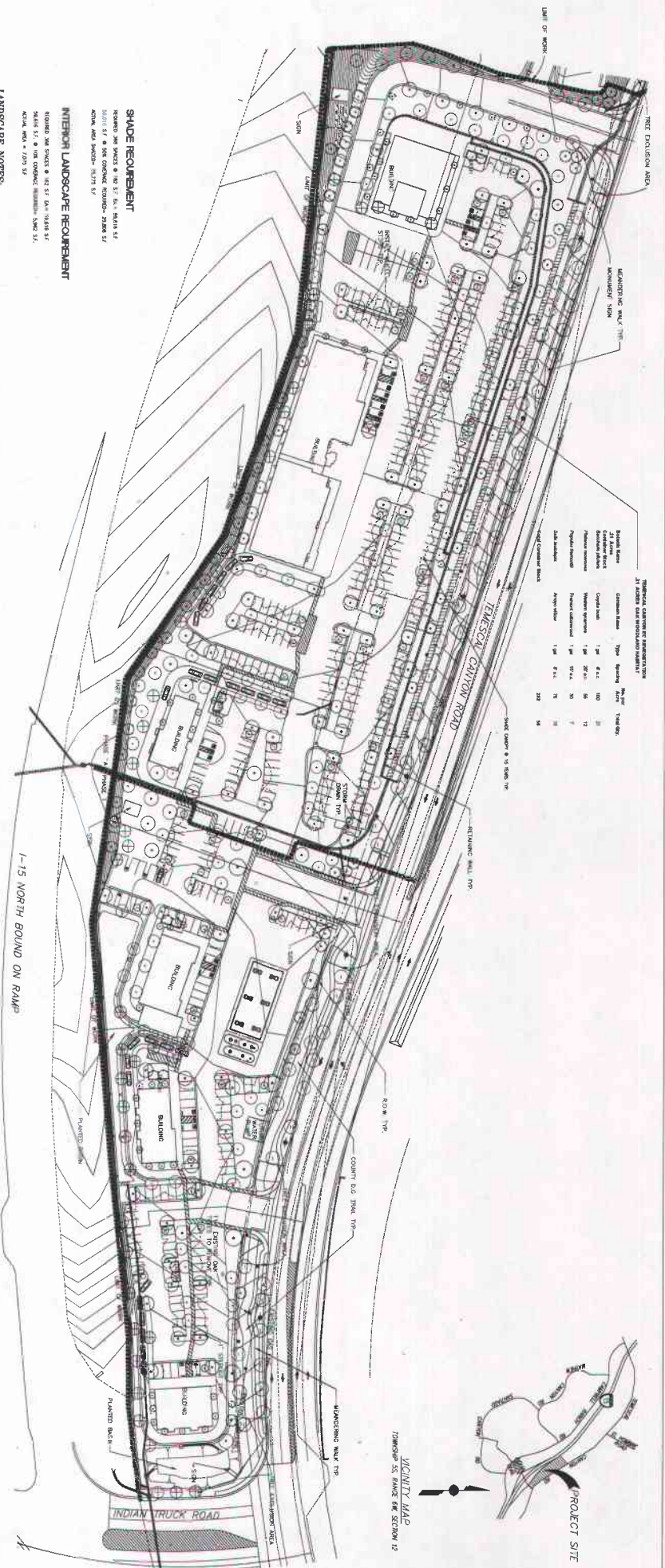
- 1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
- 2. FINISHES AND MATERIALS SHALL BE AS SHOWN ON THE MATERIAL BOARD.
- 3. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS OF THE SITE PRIOR TO CONSTRUCTION.
- 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.
- 5. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AND UTILITIES AT ALL TIMES.
- 6. THE CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES AND STRUCTURES.
- 7. THE CONTRACTOR SHALL MAINTAIN ADEQUATE DRAINAGE AND EROSION CONTROL MEASURES.
- 8. THE CONTRACTOR SHALL MAINTAIN ADEQUATE SAFETY AND SECURITY MEASURES.
- 9. THE CONTRACTOR SHALL MAINTAIN ADEQUATE RECORD DRAWINGS.
- 10. THE CONTRACTOR SHALL MAINTAIN ADEQUATE COMMUNICATIONS WITH THE ARCHITECT.
- 11. THE CONTRACTOR SHALL MAINTAIN ADEQUATE QUALITY CONTROL MEASURES.
- 12. THE CONTRACTOR SHALL MAINTAIN ADEQUATE SCHEDULING AND TIMELINE.
- 13. THE CONTRACTOR SHALL MAINTAIN ADEQUATE BUDGET AND COST CONTROL.
- 14. THE CONTRACTOR SHALL MAINTAIN ADEQUATE RISK MANAGEMENT.
- 15. THE CONTRACTOR SHALL MAINTAIN ADEQUATE LEGAL AND COMPLIANCE MEASURES.

**CLIENT:**  
ARCO NTI  
3000 AM/FM  
BUILDING

**DESIGNER:**  
NOMI TRACK TRAIL  
8 MPD CANOPY ELEVATIONS  
07.23.16  
000.000

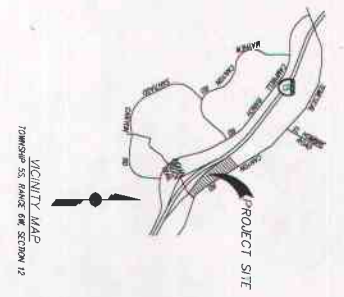
**CA2-2**





**VEGETATION SCHEDULE**

Plant Name	Quantity	Plant Size	Plant Type
1. 10' x 10' x 10'	100	10'	Tree
2. 8' x 8' x 8'	200	8'	Tree
3. 6' x 6' x 6'	300	6'	Tree
4. 4' x 4' x 4'	400	4'	Tree
5. 3' x 3' x 3'	500	3'	Tree
6. 2' x 2' x 2'	600	2'	Tree
7. 1' x 1' x 1'	700	1'	Tree
8. 1' x 1' x 1'	800	1'	Tree
9. 1' x 1' x 1'	900	1'	Tree
10. 1' x 1' x 1'	1000	1'	Tree



**SHADE REQUIREMENT**  
 SHADE 20% NORTH & 25% EAST, 30% SOUTH & 35% WEST  
 SHADE 20% NORTH & 25% EAST, 30% SOUTH & 35% WEST  
 SHADE 20% NORTH & 25% EAST, 30% SOUTH & 35% WEST

**INTERIOR LANDSCAPE REQUIREMENT**  
 ROOMS 200-205: 10' x 10' x 10' x 10' x 10' x 10'  
 ROOMS 206-207: 10' x 10' x 10' x 10' x 10' x 10'  
 ROOMS 208-209: 10' x 10' x 10' x 10' x 10' x 10'

**LANDSCAPE NOTES**  
 1. LANDSCAPE SHALL BE INSTALLED BY THE END OF CONSTRUCTION PERIOD.  
 2. ALL PLANTING SHALL BE DONE BY THE END OF CONSTRUCTION PERIOD.  
 3. ALL PLANTING SHALL BE DONE BY THE END OF CONSTRUCTION PERIOD.

- NOTES**
1. ALL PLANTING SHALL BE DONE BY THE END OF CONSTRUCTION PERIOD.
  2. ALL PLANTING SHALL BE DONE BY THE END OF CONSTRUCTION PERIOD.
  3. ALL PLANTING SHALL BE DONE BY THE END OF CONSTRUCTION PERIOD.
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  13. ALL PLANTING SHALL BE DONE BY THE END OF CONSTRUCTION PERIOD.
  14. ALL PLANTING SHALL BE DONE BY THE END OF CONSTRUCTION PERIOD.

**PLANNING LEGEND**

SYMBOL	DESCRIPTION	SCALE	REMARKS
1	EXISTING BUILDING	1:1	
2	EXISTING DRIVE	1:1	
3	EXISTING DRIVE	1:1	
4	EXISTING DRIVE	1:1	
5	EXISTING DRIVE	1:1	
6	EXISTING DRIVE	1:1	
7	EXISTING DRIVE	1:1	
8	EXISTING DRIVE	1:1	
9	EXISTING DRIVE	1:1	
10	EXISTING DRIVE	1:1	
11	EXISTING DRIVE	1:1	
12	EXISTING DRIVE	1:1	
13	EXISTING DRIVE	1:1	
14	EXISTING DRIVE	1:1	
15	EXISTING DRIVE	1:1	
16	EXISTING DRIVE	1:1	
17	EXISTING DRIVE	1:1	
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21	EXISTING DRIVE	1:1	
22	EXISTING DRIVE	1:1	
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35	EXISTING DRIVE	1:1	
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93	EXISTING DRIVE	1:1	
94	EXISTING DRIVE	1:1	
95	EXISTING DRIVE	1:1	
96	EXISTING DRIVE	1:1	
97	EXISTING DRIVE	1:1	
98	EXISTING DRIVE	1:1	
99	EXISTING DRIVE	1:1	
100	EXISTING DRIVE	1:1	

**PROPOSED LANDSCAPE SCHEDULE**

Plant Name	Quantity	Plant Size	Plant Type
1. 10' x 10' x 10'	100	10'	Tree
2. 8' x 8' x 8'	200	8'	Tree
3. 6' x 6' x 6'	300	6'	Tree
4. 4' x 4' x 4'	400	4'	Tree
5. 3' x 3' x 3'	500	3'	Tree
6. 2' x 2' x 2'	600	2'	Tree
7. 1' x 1' x 1'	700	1'	Tree
8. 1' x 1' x 1'	800	1'	Tree
9. 1' x 1' x 1'	900	1'	Tree
10. 1' x 1' x 1'	1000	1'	Tree

**OWNER / ARCHITECT**  
 THOMAS BROTHERS MAP  
 1000 S. GARDEN AVENUE, SUITE 100  
 ANAHEIM, CA 92805  
 (714) 771-1111

**DESIGNER / ARCHITECT**  
 ALHAMBRA GROUP  
 LANDSCAPE ARCHITECTURE  
 1000 S. GARDEN AVENUE, SUITE 100  
 ANAHEIM, CA 92805  
 (714) 771-1111

**PLOT PLAN 26290**  
 IN THE COUNTY OF RIVERSIDE  
 TOSCANA VILLAGE AT TEMESCAL  
 VALLEY  
**COMPREHENSIVE LANDSCAPE PLAN**



**LEGEND**  
 1. 10' x 10' x 10' x 10' x 10' x 10'  
 2. 8' x 8' x 8' x 8' x 8' x 8'  
 3. 6' x 6' x 6' x 6' x 6' x 6'  
 4. 4' x 4' x 4' x 4' x 4' x 4'  
 5. 3' x 3' x 3' x 3' x 3' x 3'  
 6. 2' x 2' x 2' x 2' x 2' x 2'  
 7. 1' x 1' x 1' x 1' x 1' x 1'  
 8. 1' x 1' x 1' x 1' x 1' x 1'  
 9. 1' x 1' x 1' x 1' x 1' x 1'  
 10. 1' x 1' x 1' x 1' x 1' x 1'

**Color Board**



Toscana Village at Temescal Valley  
 Riverside County, CA  
 Speedway Development, LLP.  
 Existing/Proposed: 09/13/2018

**TOSCANA VILLAGE**  
 Riverside County, CA  
 Speedway Development, LLP

Color and Materials Schedule	Manufacturer/Color
CONCRETE'S SOL	627 ROMA (CONCRETE)
PAINT 1	2603 CONCRETE (MAGNETIC)
PAINT 2	1313 (MAGNETIC)
PAINT 3	3401 (MAGNETIC)
PAINT 4	6401 (MAGNETIC)
PAINT 5	6413 (MAGNETIC)
PAINT 6	6414 (MAGNETIC)
PAINT 7	7200 (MAGNETIC)
PAINT 8	6401 (MAGNETIC)
PAINT 9	6401 (MAGNETIC)
PAINT 10	6401 (MAGNETIC)
STONE VENEER	STONE VENEER (CONCRETE)
PBR CLUST CONCRETE	627 ROMA (CONCRETE)

DATE: 09/13/2018  
 DRAWING: 16047.06  
 SCALE: 1/8" = 1'-0"

# TOSCANA VILLAGE



## Temescal Valley

Temescal Canyon Road & Indian Truck Trail  
Corona, California

PROJECT SIGN & TENANT GUIDELINES

July 26, 2017

PHASE 1 August 15, 2018



880 Columbia Ave.  
Riverside, CA 92507

P: 951.682.9660  
F: 951.682.9665  
[www.fusionesign.com](http://www.fusionesign.com)

15  
FREEWAY

- Revision: 2/7/18
- 7/17/18
- 7/20/18
- 8/14/18
- 10/31/18
- 11/01/18



**PHASE 1B**

NO.	DESCRIPTION	AREA (SQ. FT.)	DATE	STATUS
R-101	RESTAURANT SIGNAGE	30,000 SQ. FT.	07/20/18	IN PROGRESS
R-102	RESTAURANT SIGNAGE	4,000 SQ. FT.	08/14/18	IN PROGRESS
T-101	TENANT IDENTIFICATION	10,000 SQ. FT.	10/31/18	IN PROGRESS
TOTAL		44,000 SQ. FT.		

**PARKING**  
REQUIRED: 21 SPACES  
PROVIDED: 20 SPACES

**PARALLEL CALCULATIONS RESTAURANT**  
RESTAURANT ESTIMATED  
1.2172 LEASING AREA  
1.2172 LEASING AREA  
ASS. VARIATIONS  
1.2172 LEASING AREA  
CALCULATED  
RESTAURANT SIGNAGE 44,000 SQ. FT.  
TENANT IDENTIFICATION 10,000 SQ. FT.  
TOTAL SIGNAGE 54,000 SQ. FT.

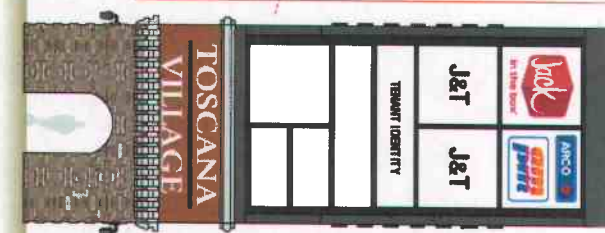
**PHASE 1A**

NO.	DESCRIPTION	AREA (SQ. FT.)	DATE	STATUS
R-101	RESTAURANT SIGNAGE	3,700 SQ. FT.	07/20/18	IN PROGRESS
R-102	RESTAURANT SIGNAGE	4,600 SQ. FT.	08/14/18	IN PROGRESS
R-103	CONVERT. BENCH ONE	1,800 SQ. FT.	08/14/18	IN PROGRESS
R-104	CONVERT. BENCH TWO	1,200 SQ. FT.	08/14/18	IN PROGRESS
R-105	CONVERT. BENCH THREE	1,200 SQ. FT.	08/14/18	IN PROGRESS
TOTAL		12,500 SQ. FT.		

**PARKING**  
REQUIRED: 119 SPACES  
PROVIDED: 119 SPACES

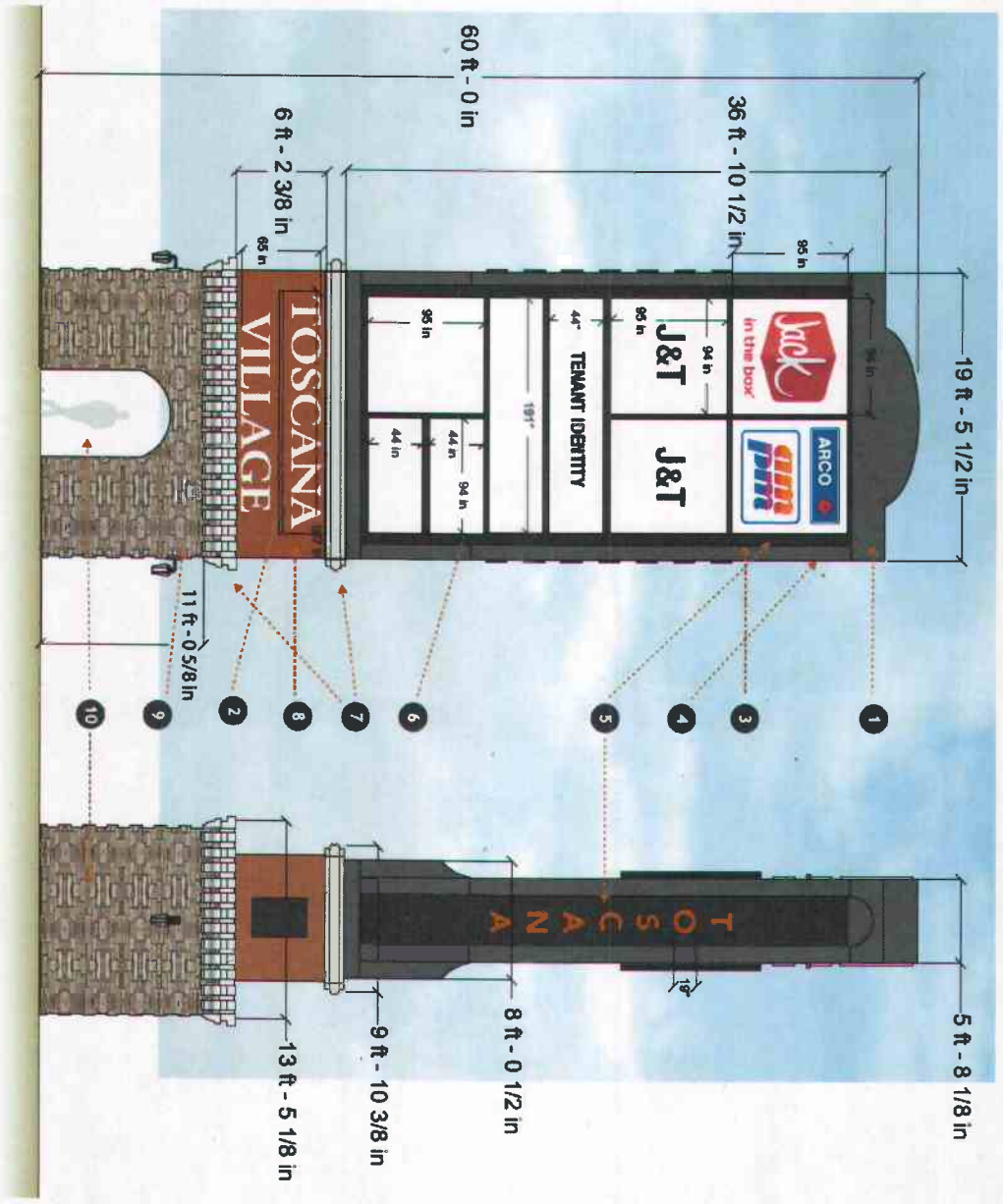
**PARALLEL CALCULATIONS RESTAURANT & FAST FOOD**  
RESTAURANT & FAST FOOD SIGNAGE  
\* SIGNAGE: 12,500 SQ. FT.  
\* SIGNAGE: 12,500 SQ. FT.  
\* SIGNAGE: 12,500 SQ. FT.  
\* SIGNAGE: 12,500 SQ. FT.  
\* SIGNAGE: 12,500 SQ. FT.  
\* SIGNAGE: 12,500 SQ. FT.  
\* SIGNAGE: 12,500 SQ. FT.

- 1. AM/PM
- 2. Arco
- 3. Pad 1
- 4. Pad 2
- 5. Additional Retail
- 6. Additional Retail



PHASE 1	
P2 A/B	Double Faced Pylon Signs
M3 A/B	Double Faced Monument Signs
M4	Single Faced Monument Signs
M5 A/B	Double Faced Monument Signs

- 1. AM/PM
- 2. Arco
- 3. Pad 1
- 4. Pad 2
- 5. Additional Retail
- 6. Additional Retail



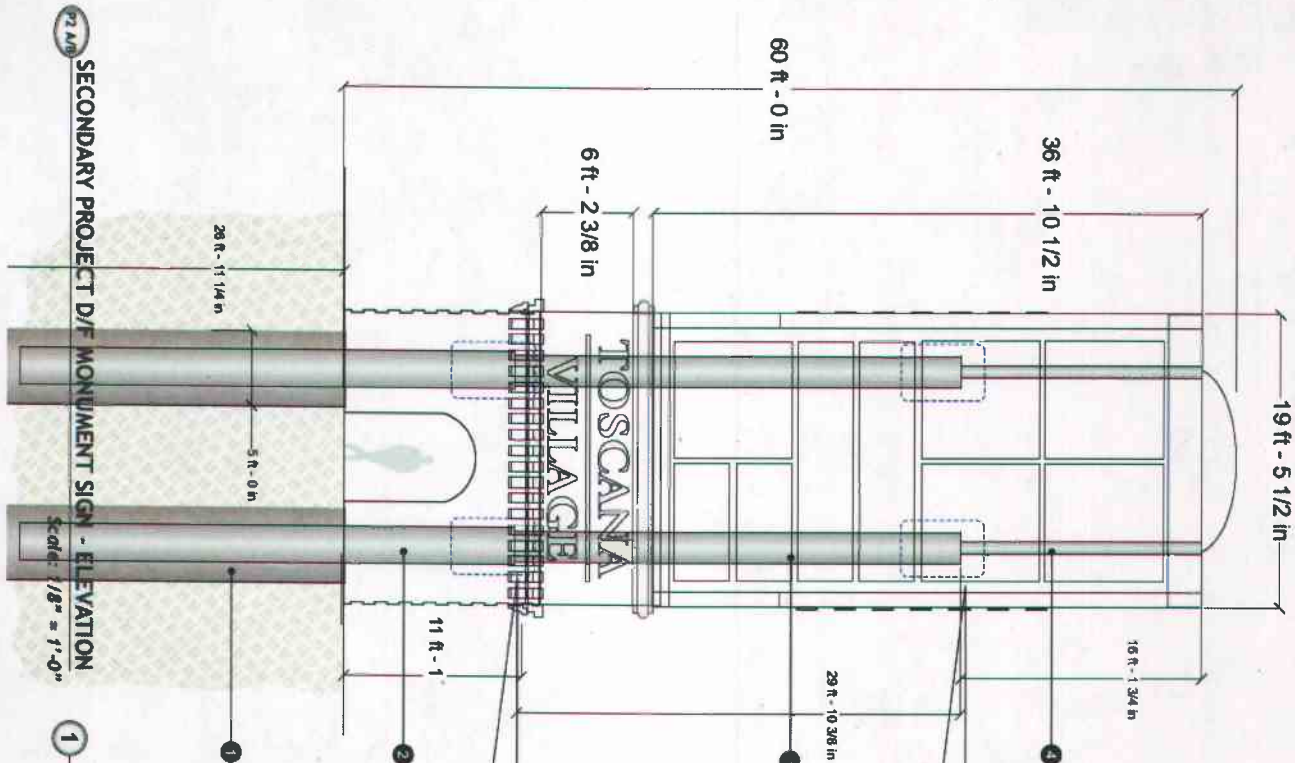
PS1A SECONDARY PROJECT D/F PYLON SIGN - ELEVATION  
Scale: 1/8" = 1'-0"

1 SIDE VIEW  
Scale: 1/8" = 1'-0"

- 1 .080 Aluminum Cladding with Painted Finish
- 2 Aluminum Cabinet Vinyl DVP (Logo Bug)
- 3 Recessed Steel Mesh Accent on Structure
- 4 Project Identity to be halo-illuminated channel letters
- 5 Halo Illuminated Channel Letters Front/Side
- 6 Internally Illuminated Sign Cabinets (Panels)
- 7 Foam Custom Decorative Trim with Painted Stucco Finish
- 8 080 Aluminum Cladding
- 9 El Dorado Stone Limestone\_San-Marino Base
- 10 Stucco Recessed Painted Tan with Access Door

Color Specs TBD

● ● ●

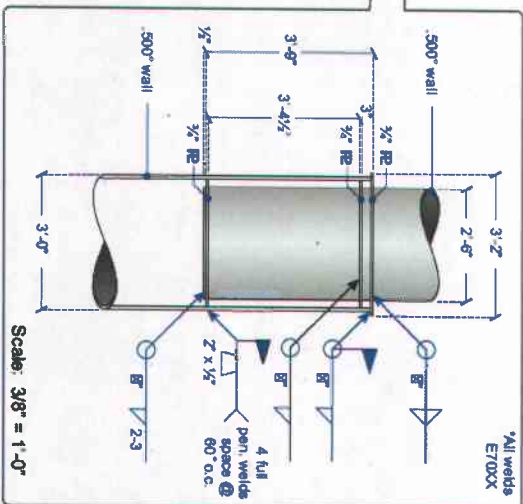
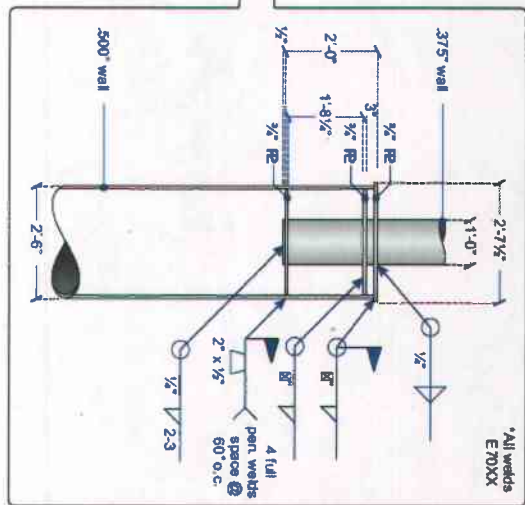


72149 SECONDARY PROJECT D/F MONUMENT SIGN - ELEVATION

Scale: 1/8" = 1'-0"

1 SIDE VIEW

Scale: 1/8" = 1'-0"



1. Poured in place consolidated concrete footing.  
f'c = 2,500 psi
2. 3'-0" diameter x 1/2" wall steel upright pipe support.  
Pipe supports to be infilled with concrete up to grade level.
3. 2'-6" diameter x 1/2" wall steel upright pipe support.
4. 1'-0" diameter x 1/2" wall steel upright pipe support.

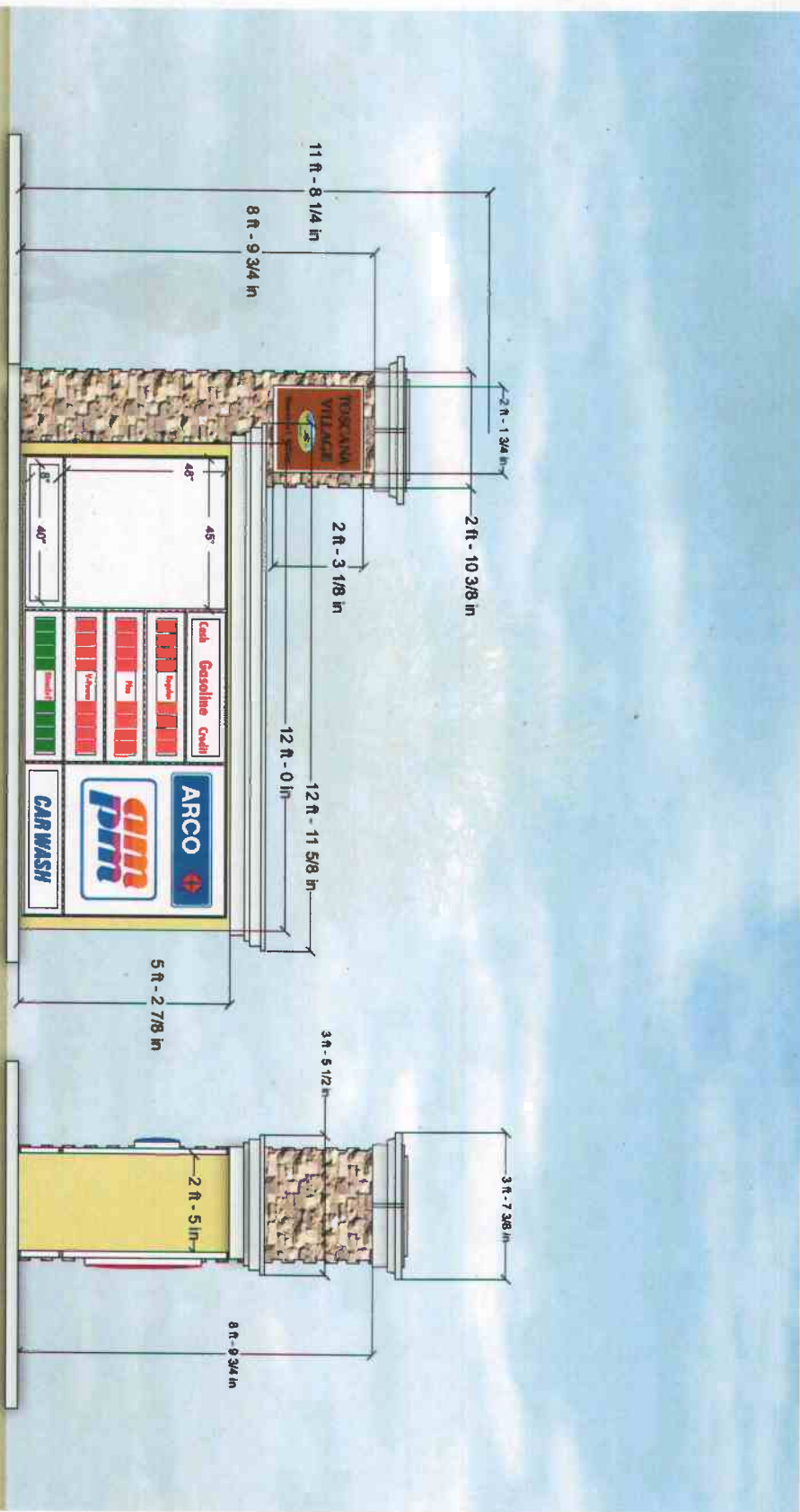
**FUSION**  
SIGNAGE

680 Columbia Ave. - Riverside, CA 92507  
P: 951.682.9660 F: 951.682.6665  
www.fusionsign.com

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As noted  
Design No.:  
Toscan Village  
Date:  
May 12, 2017  
Revised:  
August 15, 2018

Account Manager:  
Loren Hanson  
Designer:  
Arnel Cedejari

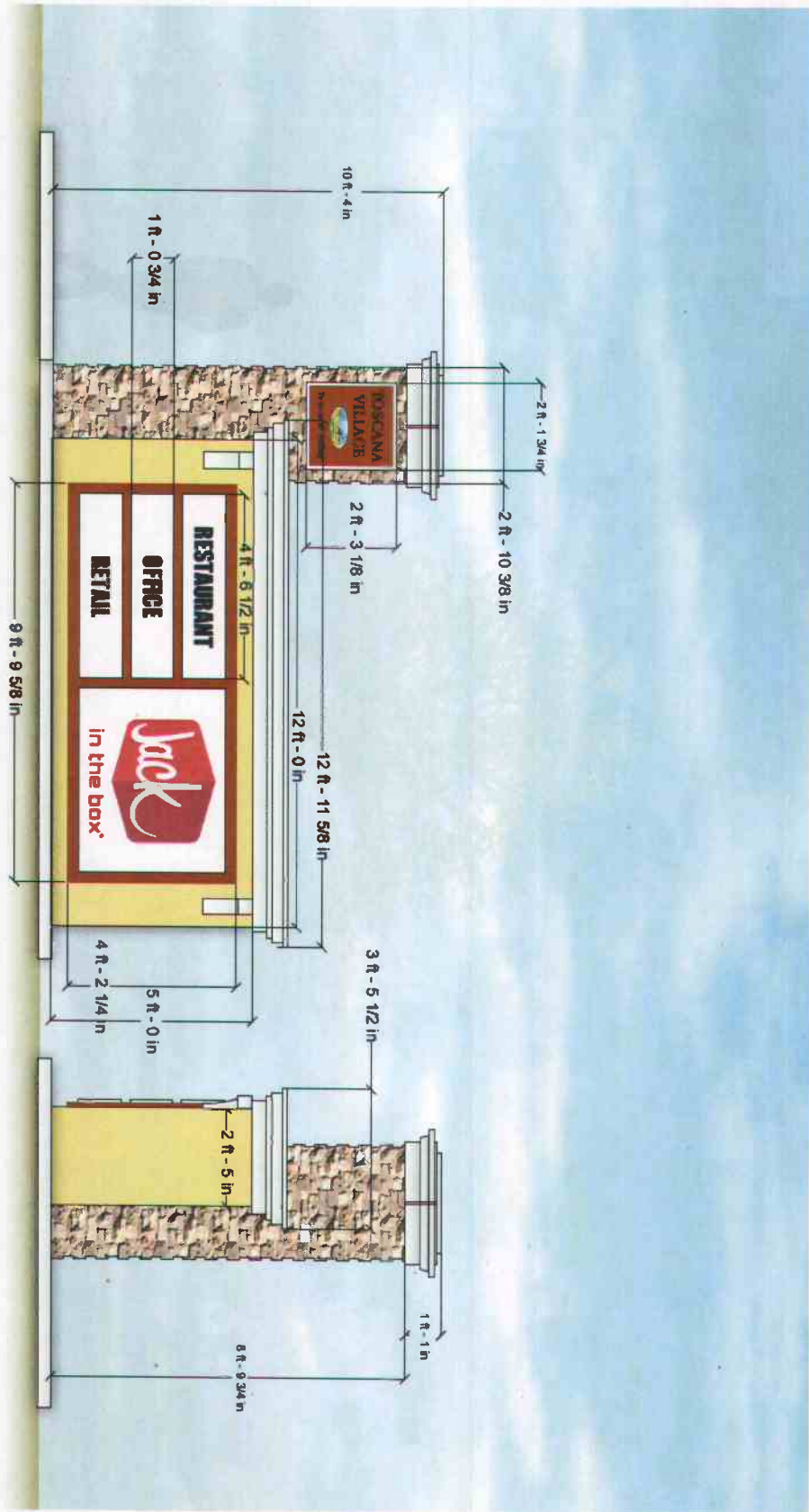


43 2/3 D/F MONUMENT SIGN WITH GASOLINE PRICING - ELEVATION  
SIDE: A

Scale: 3/8" = 1'-0"

1 SIDE VIEW

Scale: 3/8" = 1'-0"



M4 S/F MONUMENT SIGN WITH TENANT PANELS - ELEVATION

Scale: 3/8" = 1'-0"

1 SIDE VIEW

Scale: 3/8" = 1'-0"

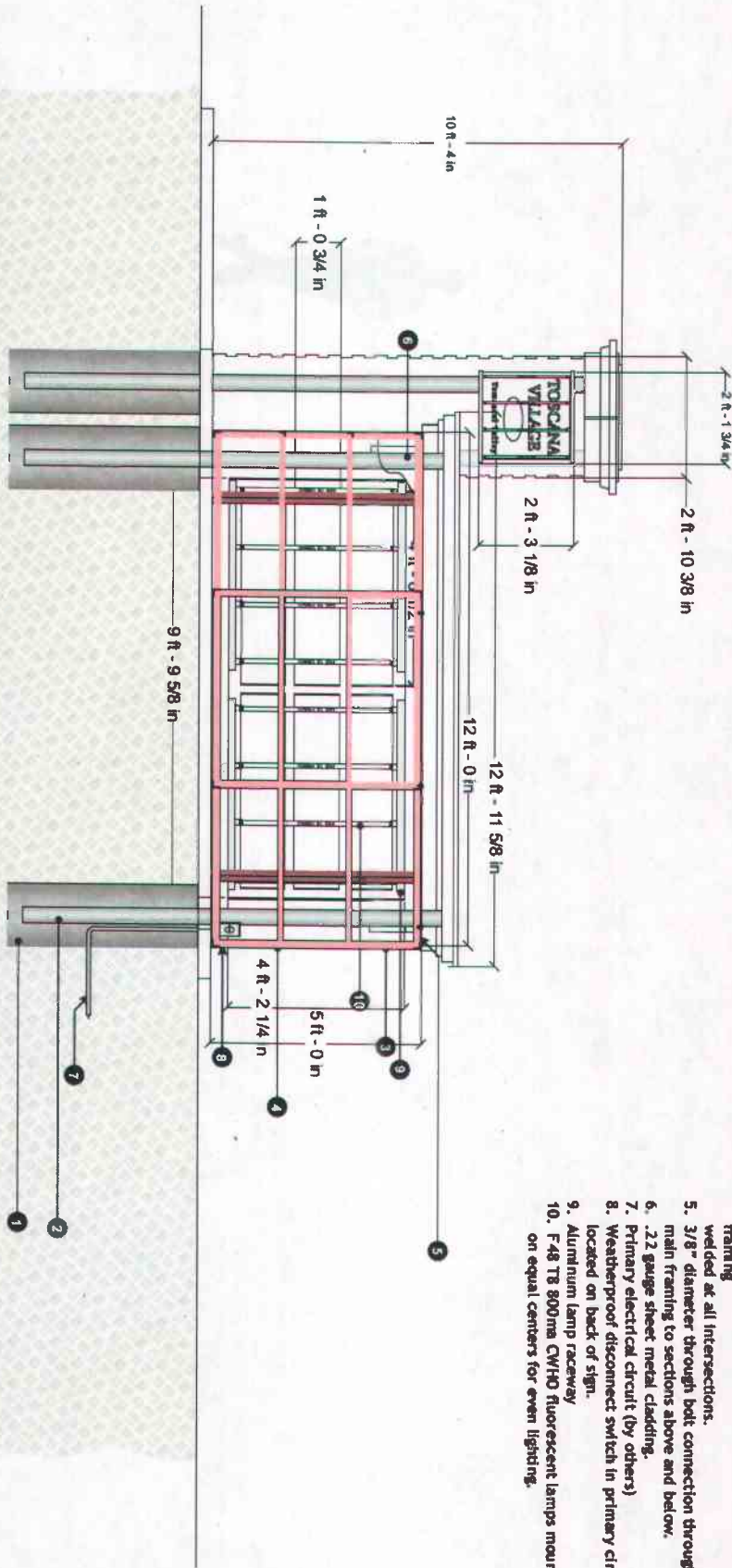


**M4** S/F MONUMENT SIGN WITH TENANT PANELS - ELEVATION  
**M3** OPTION D/F MONUMENT

Scale: 3/8" = 1'-0"

**1** SIDE VIEW

Scale: 3/8" = 1'-0"



**1** FRAMING & FABRICATION DETAILS  
 Scale: 3/8" = 1'-0"

1. Poured in place consolidated concrete footing, f'c = 2,500 psi
2. 6" diameter x 1/8" wall steel upright pipe support.
3. 2" x 2" x .250" wall steel angle main framing welded at all intersections.
4. 1 1/2" x 1 1/4" x .1875" wall steel angle intermediate framing welded at all intersections.
5. 3/8" diameter through bolt connection through main framing to sections above and below.
6. .22 gauge sheet metal cladding.
7. Primary electrical circuit (by others)
8. Weatherproof disconnect switch in primary circuit located on back of sign.
9. Aluminum lamp raceway
10. F48 TB 800ma CWFO fluorescent lamps mounted on equal centers for even lighting.

**FUSION**  
 Sign Co.  
 680 Columbia Ave. - Riverside, CA 92507  
 P: 951.682.9660 F: 951.682.9665  
 www.fusionsign.com

Project:  
 Toscana Village  
 Address:  
 Tameca Canyon Rd @  
 Indian Truck Trail  
 Corona, CA

Account Manager:  
 Loren Hanson

Designer:  
 Arnel Cadatal

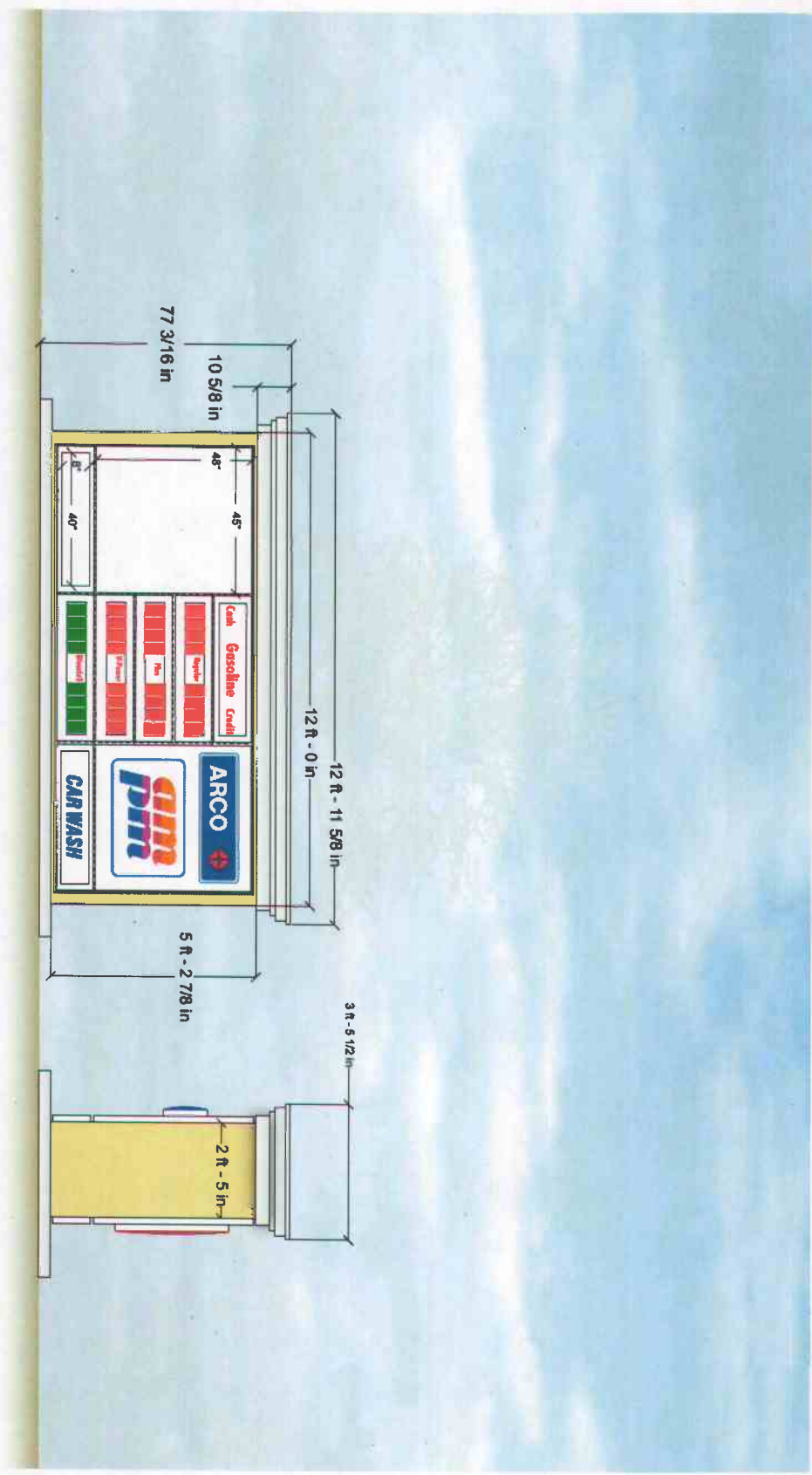
Scale:  
 As noted

Design No.:  
 Toscana Village

Date:  
 May 12, 2017

Revisions:  
 August 15, 2018

This is an official specification & every detail is subject to change without notice. It is intended for your reference & is not to be used as a contract. All dimensions are in feet & inches unless otherwise specified. The contractor shall be responsible for obtaining all necessary permits and for the safety of the work. The designer is not responsible for the construction of the sign. The designer is not responsible for the safety of the sign. The designer is not responsible for the safety of the sign. © 2016



105 1/8  
 D/F MONUMENT SIGN WITH GASOLINE PRICING - ELEVATION  
 SIDE: A

Scale: 3/8" = 1'-0"

1 SIDE VIEW

Scale: 3/8" = 1'-0"

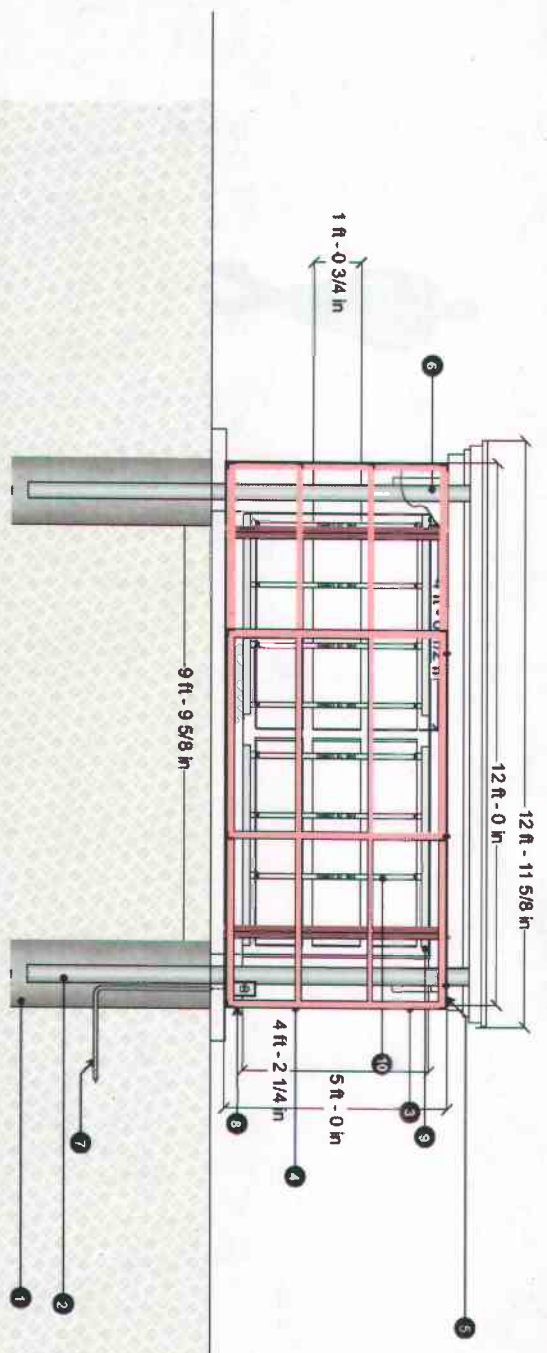
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D/F MONUMENT SIGN WITH TENANT PANELS - ELEVATION

Scale: 3/8" = 1'-0"

1 SIDE VIEW

Scale: 3/8" = 1'-0"



1 FRAMING & FABRICATION DETAILS

Scale: 3/8" = 1'-0"

1. Poured in place consolidated concrete footing.
2. 6" diameter x 1/8" wall steel upright pipe support.
3. 2" x 2" x .250" wall steel angle main framing welded at all intersections.
4. 1 1/2" x 1 1/2" x .1875" wall steel angle intermediate framing welded at all intersections.
5. 3/8" diameter through bolt connection through main framing to sections above and below.
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7. Primary electrical circuit (by others)
8. Weatherproof disconnect switch in primary circuit located on back of sign.
9. Aluminum lamp raceway
10. F48 T8 800ma CW/HD fluorescent lamps mounted on equal centers for even lighting.

**FUSION** Design  
 680 Columbia Ave. - Riverside, CA 92507  
 P: 951.882.9660 F: 951.882.9685  
 www.fusionsign.com

Project: Tozarna Village  
 Address: Tennessee Canyon Rd @ Indian Truck Trail Corona, CA  
 Account Manager: Loren Hanson  
 Designer: Arnel Cadafal  
 Scale: As noted  
 Design No.: Tozarna Village  
 Date: May 12, 2017  
 Revisions: August 15, 2018

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# Toscana Village

## Pylon Survey

Temescal Canyon Road & Indian Truck Trail  
Corona, CA



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