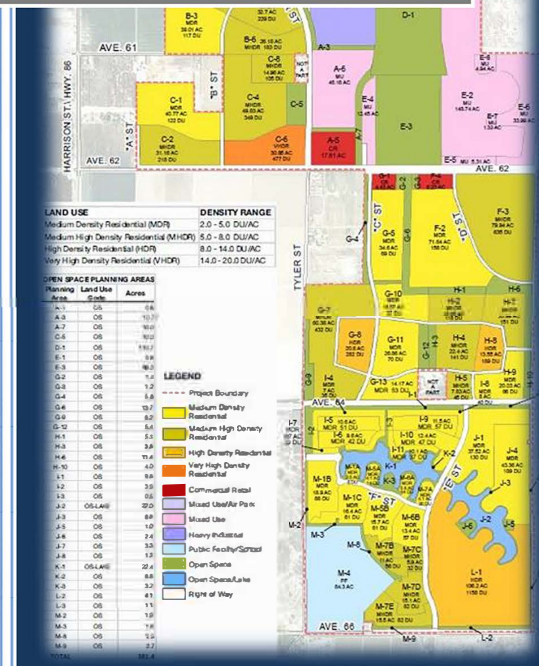
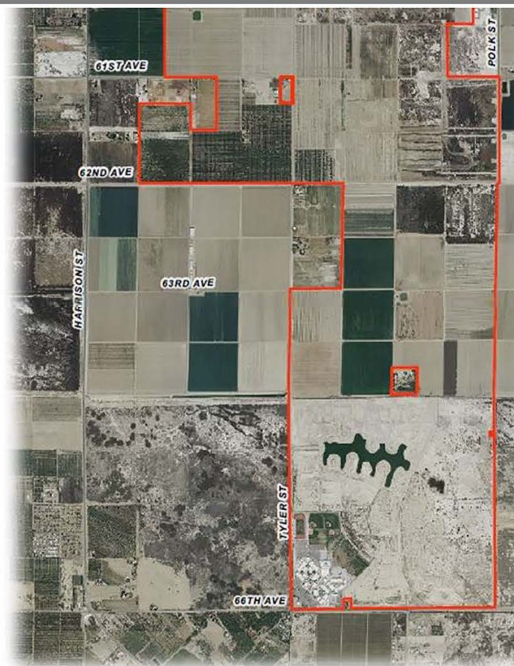


Amendment No. 4 Substantial Conformance No. 3

The Kohl Ranch Specific Plan No. 303



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Amendment No. 1 Adopted: January 28, 2003
Amendment No. 2 Adopted: June 7, 2011
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THE KOHL RANCH

COACHELLA VALLEY, CALIFORNIA

Specific Plan No. 303, Amendment No. 4

Substantial Conformance No. 3

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1 EXECUTIVE SUMMARY

1.1 Introduction

1.1.1 Purpose and Authority of the Specific Plan

The purpose of the Kohl Ranch Specific Plan is to guide development and to stimulate responsible design through customized regulations and guidelines. The original Kohl Ranch Specific Plan was adopted November 16, 1999, Amendment No. 1 was adopted January 28, 2003, and Amendment No. 2 was adopted June 7, 2011. The third amendment to the Specific Plan (SPA3) was prepared pursuant to the authority granted to the County of Riverside by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 to 65457.

1.1.2 Purpose and Authority of the EIR

The Environmental Impact Report (EIR) addresses potential environmental impacts of the Specific Plan for the Kohl Ranch project in the County of Riverside. The California Environmental Quality Act (CEQA) requires that all state and local governmental agencies consider the environmental consequences of projects over which they have discretionary authority. When the original Kohl Ranch Specific Plan was adopted, an EIR was prepared and certified to satisfy CEQA, as set forth in Public Resources Code Section 21000, et seq., the State CEQA Guidelines, 14 California Administrative Code Section 15000, et seq., and the County of Riverside's CEQA Guidelines. The original EIR was certified on November 16, 1999. A draft Addendum is being prepared to review the impacts of changes being made to the original EIR as the result of proposed SPA4. The EIR is the public document designed to provide local and state governmental agency decision makers with an analysis of environmental effects of the proposed project, to indicate possible ways to reduce or avoid environmental damage through mitigation measures and alternatives. The EIR also must disclose significant environmental impacts that cannot be avoided; growth-inducing impacts; effects not found to be significant; and significant cumulative impacts of all past, present, and reasonably foreseeable future projects.

1.1.3 Scope of the EIR

As noted above, the EIR is an informational document used in local and state agency decision-making processes. It is not the purpose of the EIR to either recommend approval or denial of a project or to present political, social, or economic reasons to project approval or denial. Pursuant to CEQA, the County of Riverside served as the Lead Agency for the original EIR and prepared an Initial Study (see Technical Appendix A). For SPA4, an Initial Study was conducted to analyze the potential environmental impacts of SPA4 as compared to the impacts studied in the EIR for the original Specific Plan (see Technical Appendix A). The County determined through the Initial Study for the original project that the adoption of a Specific Plan for the Kohl Ranch project may have significant adverse environmental impacts and that an EIR is required. The Initial Study for the original Specific Plan identified those environmental issues that may be significantly impacted by this project and are addressed in this EIR. The Initial Study for SPA4 did

not identify any environmental issues associated with SPA4 that were not previously identified in the original EIR. These issues include:

- General Plan Land Use Determination
- Land Use Element Consistency
- Existing Land Use and Zoning
- Landform and Topography/Slopes and Erosion
- Soils and Agriculture
- Biology
- Geology and Seismicity
- Hydrology, Flooding, and Drainage
- Air Quality
- Water Quality
- Noise
- Energy Resources
- Open Space and Conservation
- Toxic Substances
- Cultural Resources
- Aesthetics, Visual Analysis, Light and Glare
- Circulation and Traffic
- Water and Sewer
- Fire Services
- Sheriff Services
- Schools
- Parks and Recreation
- Utilities
- Solid Waste
- Health Services
- Disaster Preparedness
- Libraries
- Airports
- Housing Element
- Regional Element
- Administrative Element

Intended Uses of the EIR

On October 14, 1994, the County of Riverside, in its role as Lead Agency for this project, issued a Notice of Preparation (NOP) to the State Clearinghouse, responsible agencies, and other interested parties. The NOP and comments resulting from the distribution of the NOP are contained in Technical Appendix A.

Applications covered by the EIR Addendum, are as follows:

- Tentative Tract Map No. 37269 (TTM37269): proposes a total of 326 residential dwelling lots, one (1) lot for water quality basin, and one (1) lot for a private lagoon with wave making capabilities and future village area for use by the residents. TTM37269 will allow for development of the Thermal Beach Club to include a total of 326 lots consisting of 131 medium density dwelling units and 195 high density dwelling units.
- Plot Plan No. 180037 (PP180037): proposes development and use of an approximately 20 acre lagoon and approximately 34,400 square foot future village area for use by Thermal Beach Club residents and their guests.

A lead agency is the agency with primary responsibility for approval of the project. Other agencies having discretionary approval over a project are "Responsible Agencies" under CEQA. This document will provide environmental information for several other agencies affected by the project, or which are likely to have an interest in the project. Various state and federal agencies exercise control over certain aspects of the project area. The various public, private, and political agencies and jurisdictions with a particular interest in the proposed project include the following:

Federal Agencies

- *Fish and Wildlife Service (FWS)* – Responsible for conserving and protecting wild birds, endangered species, and their habitat.
- *Federal Highway Administration (FHWA)* – Responsible for approving changes to the interstate freeway system.
- *Environmental Protection Agency (EPA)* – Responsible for administration of the Superfund program.

State Agencies

- *California Department of Fish and Wildlife (CDFW)* – Responsible for the protection, conservation, propagation, and enhancement of California's wildlife resources. This department enforces laws and regulations, and issues licenses relative to and cooperates with local agencies in developing projects. This agency will act as a Trustee.
- *California Regional Water Quality Control Board (CRWQCB)* – Responsible for evaluating appropriate uses of water and for issuing National Pollution Discharge Elimination System (NPDES) permits and waste discharge requirements.
- *California Reclamation Board (CRB)* – Responsible for delineation of flooding and regulation of encroachments into designated floodways.
- *California Department of Transportation (Caltrans)* – Responsible for approval of roadway improvements along state highways, including State Routes 86 and 195.
- *California Environmental Protection Agency (CALEPA)* – This agency is the primary state agency concerned with degradation of the environment and how it affects human health. It is responsible for the examination and prevention of pollution of sources of public water supplies; establishment of ambient standards of air quality; monitoring of environmental pollution, regulation of the quality of water supplies and sewage disposal systems; regulation of hazardous waste; regulation of pesticides; regulation and control of radioactive materials; and providing certain laboratory support to other state agencies.
- *California Department of Toxic Substances Control (DTSC)* – This CALEPA agency is the primary state agency that regulates matters related to hazardous waste. It is responsible for the cleanup of hazardous waste sites and permitting, surveillance and enforcement of hazardous waste facilities.
- *State Air Resources Board (CARB)* – This CALEPA agency is responsible for ensuring implementation of the California Clean Air Act, responding to the Federal Clean Air Act and for regulating emissions from consumer products and motor vehicles.
- *California Department of Conservation* – This agency reviews projects for their impacts on agricultural resources.

Local Agencies

- *County of Riverside* – Responsible for land use control, and the provision of urban services on and to the project site. The County will act as the Lead Agency for the proposed project.

- *South Coast Air Quality Management District (SCAQMD)* – Has responsibility for the implementation of the California Clean Air Act. This agency's authority includes Los Angeles and Orange Counties and the western portion of Riverside County.
- *Southern California Association of Governments (SCAG)* – Stimulates intergovernmental cooperation in planning and development activities, and assures better coordination of federally assisted projects. Reviews applications of local and regional agencies for federal grants related to more than 100 programs. Responsible for preparing components of the California Regional Transportation Plan. Programs range from open space planning, waste control and water basin studies to aviation, housing and research in economics and demography.
- *Adjacent Cities* – The cities of Indio, Coachella, and La Quinta are located adjacent to the unincorporated portion of Riverside County where the project is located and will be affected by the proposed project.
- *Coachella Valley Water District (CVWD)* – Responsible for providing domestic water, sanitation, and regional stormwater protection for the site. This area is within District No. 1 of the Coachella Valley Water District for irrigation service. Water from the Coachella Canal is available and shall be used to irrigate golf courses and greenbelts. CVWD can also provide agricultural drainage to this area.
- *Coachella Valley Association of Governments (CVAG)* – Responsible for programming transportation improvements in the project area, preparing demographic forecasts, and solid waste and air quality planning.
- *Riverside County Airport Land Use Commission (ALUC)* – Responsible for reviewing land use proposals for consistency with the Comprehensive Land Use Plan for the Jacqueline Cochran Regional Airport.

Public agencies and interested parties, who did not respond to a request for comment during the preparation of the EIR, will have an opportunity to comment during the public review period for the Draft EIR.

1.2 PROJECT SUMMARY

1.2.1 Project Location

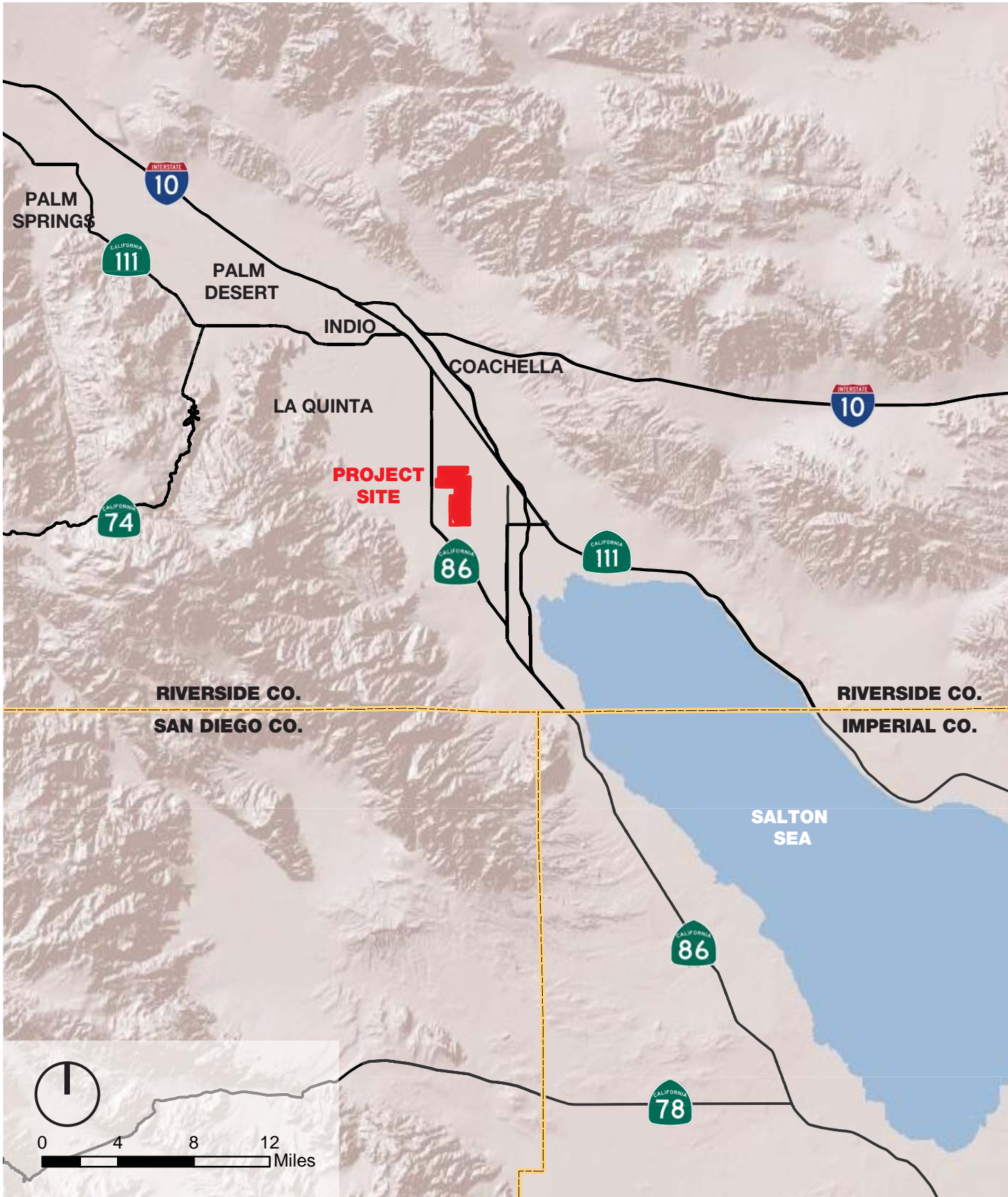
The 2,162.65-acre Kohl Ranch Specific Plan is located in the Coachella Valley portion of Riverside County as depicted by **Figure 1-1, Regional Location**. The site is just south of Jacqueline Cochran Regional Airport, and is roughly east of Harrison Street/Highway 86, west of Highway 111, and north of Highway 195¹. The project is bounded by Avenue 60 on the north, Polk Street on the east, Avenue 66 on the South, and by a line approximately 3,900 feet west of Tyler Street on the west as depicted in **Figure 1-2, Project Vicinity**.

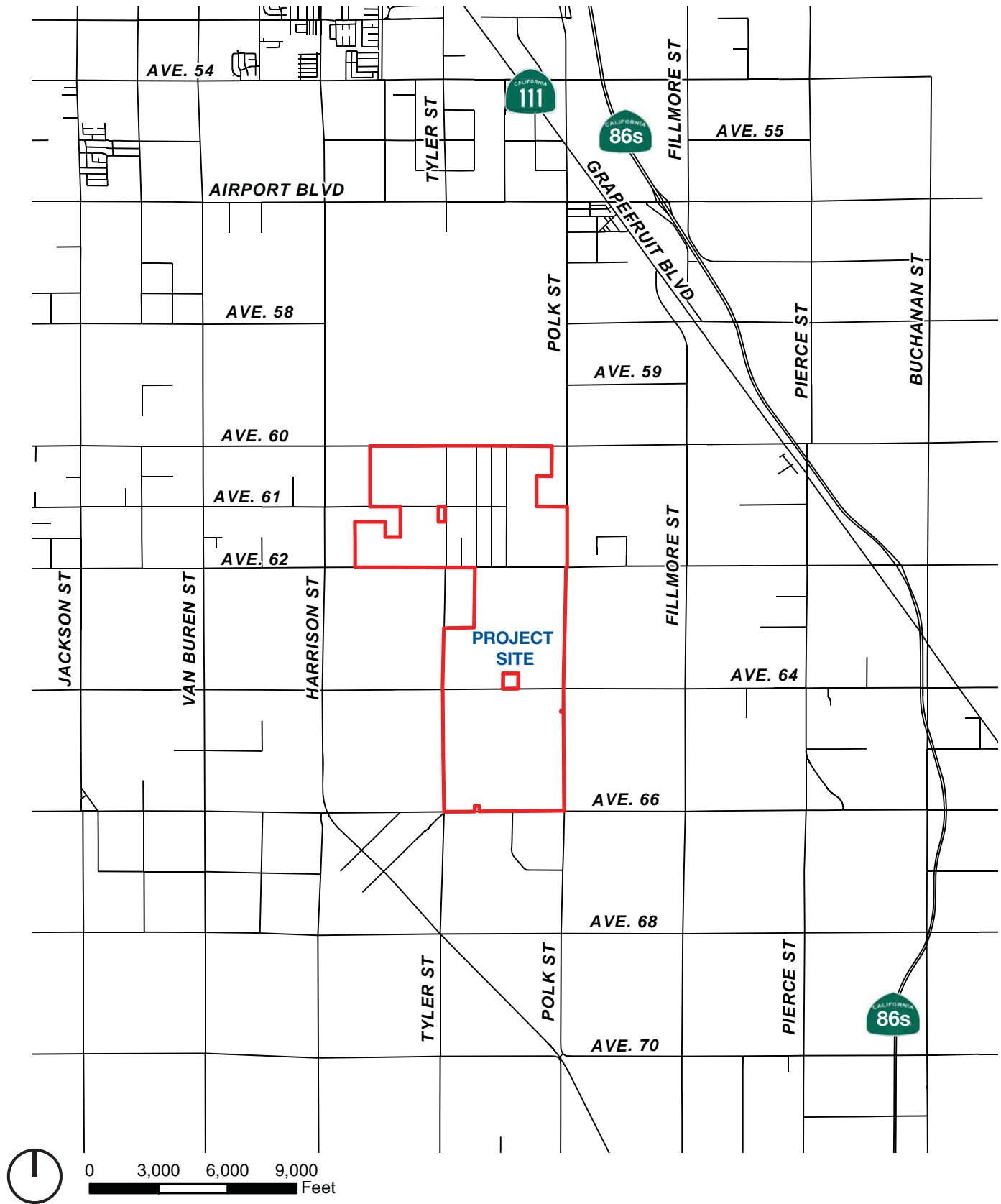
1.2.2 Site Description

The Kohl Ranch site is characterized by flat terrain, with a very gentle slope from northwest to southeast. Elevations range between approximately 125 and 164 feet below sea level. The majority of the site is currently in agricultural use, although a significant portion in the southern section is vacant, disturbed land with sparse, non-native vegetation as depicted in **Figure 1-3, Aerial Photograph**. Existing man-made features include the Avenue 64 Evacuation Channel which flows west to east through the project site, and structures associated with current and past farming activities, including the Kohl Ranch headquarters and an abandoned feed lot. Some limited residential uses occur along the project periphery and Avenue 61. Adjacent, off-site land uses include vacant land, farms and related uses, a former sludge processing operation², residences, and the Jacqueline Cochran Regional Airport. The Torres Martinez Indian Reservation abuts Section 9 on the west, south, and east. These Native American lands are held in individual and tribal ownership.

¹Please note that for clarification purposes, this Specific Plan refers to the "old" Highway 86 as Harrison Street, its local street name. The designation SR-86S refers to the new freeway constructed east of the Whitewater River.

²On November 28, 1994, a U.S. District Court judge issued a preliminary injunction preventing more sludge from being brought to the site. The two companies composting sewage sludge announced the closing of operations at the site.







1.2.3 Proposed Changes to Specific Plan No. 303

The Kohl Ranch Specific Plan No. 303, Amendment 4 (SPA4) is a result of a change in land use allowances located in the northeasterly area of the project site (specifically Planning Area (E-2). SPA4 creates two new Planning Areas (E-2A and E-2B) which include the same allowable uses and development standards as PA E-2 but includes the addition of racetrack recreational units and associated development standards. Racetrack Recreational units are considered a non-residential use identified as a member villa accessible 24 hours a day, seven days a week. The Racetrack Recreational Unit provides a private space for a Thermal Club members to store personal belongings, vehicles, and equipment utilized for racing. The unit can further be utilized for vehicular repairs, other racing-related recreational activities and is located directly adjacent to the racetrack with an open view deck facing the track allowing for viewing of racing activities. The unit would be restricted from permanent residency, and has limitations imposed by the club for duration of stays. An adjustment is being made concurrently with this Specific Plan Amendment to the zoning ordinance through a change of zone (CZ07952), and is reflected in this Specific Plan document.

1.2.4 Project Overview

The Kohl Ranch Specific Plan consists of a balanced array of land uses including residential, mixed use, commercial, industrial, open space/recreation, and public facilities as depicted in **Figure 1-4, Land Use Plan**. Both living and working opportunities will be available within the project. The residential portion includes target of 7,160 dwelling units but with a maximum not exceed 7,171 dwelling units distributed among mixed use and four different density classifications on 1,143.73 acres, with a gross residential density of 4.46 dwelling units per acre³. Although the maximum density and the total number of dwelling units within the high density and very high density residential designations can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Approximately 379.28 acres of open space provide for passive and active recreation, including trails and parks. The plan also allows for the development of large-scale recreational uses such as a golf course and a motor sports race track, which are identified as allowable land uses.

³ The overall gross density for residential areas was determined by dividing the total number of dwelling units by the acreage devoted to residential land uses, open space, public facilities, and local residential streets. The gross density—if determined by dividing the total number of dwelling units by the residential acreage—is 6.27. This density is reduced to 3.32, if the total site acreage is used.

Table 1-A, Land Use Diagram Statistical Summary

Land Use	Acreage	Percent of Total	Percent of Developable Area ¹	Target Dwelling Units
RESIDENTIAL				
Medium Density Residential (MDR)	490.51	22.7	32.1	1,652
Medium High Density Residential (MHDR)	467.76	21.6	30.6	3,245
High Density Residential (HDR)	154.61	7.1	10.1	1,629
Very High Density Residential (VHDR)	30.85	1.4	2.0	477
Total Residential	1,143.73	52.9	74.7	7,001
INDUSTRIAL				
Heavy Industrial (HI)	81.17	3.8	5.3	
Total Industrial	81.17	3.8	5.3	
BUSINESS				
Mixed Use/Air Park (MU/AP)	24.45	1.1	1.6	
Mixed Use	252.73	11.7	16.6	159 ³
Total Business	277.18	12.8	18.2	159³
COMMERCIAL				
Commercial-Retail (CR)	28.27	1.3	1.9	
Total Commercial	28.27	1.3	1.9	
OTHER				
Open Space (OS)	380.14	17.6		
Public Facilities/Schools (PF)	84.30	3.9		
Right-of-Way (ROW)	167.86	7.8		
Total Other	632.30	29.7		
TOTAL	2,162.65	100%	100%	7,160

1. Assumes total of 1,530.35 acres of developable land. This does not include land uses in "other" category.
2. The ROW acreage includes local streets, which are not depicted on the Land Use Plan.
3. This number represents a maximum rather than a target.

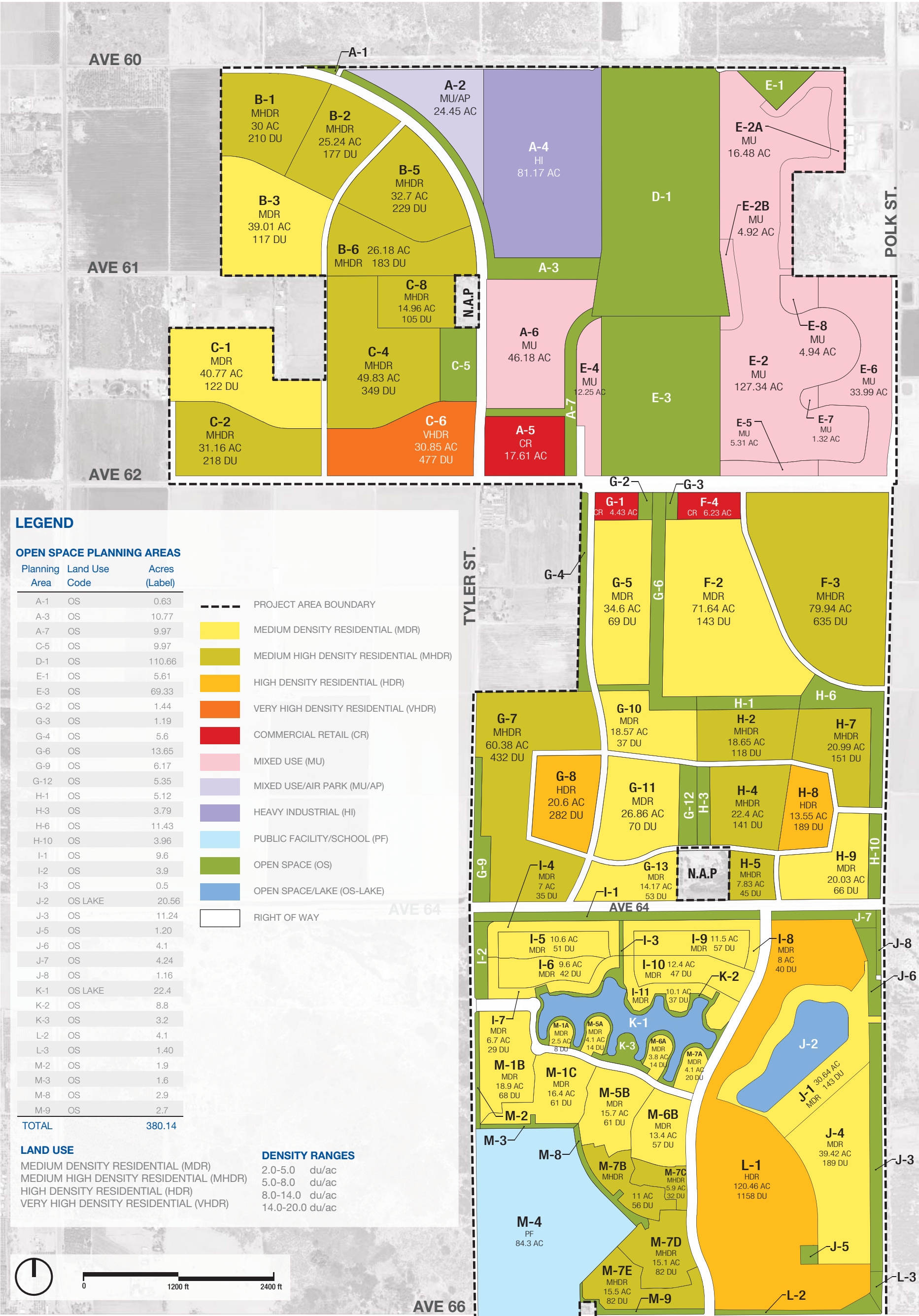


Table 1-B, Land Use Summary by Planning Area

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Mixed Use (MU)		46.18	
A-7	Open Space (OS)		9.97	
B-1	Medium High Density Residential (MHDR)	210	30	5.0–8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0–8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0–5.0
B-5	Medium High Density Residential (MHDR)	229	32.70	5.0–8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0–8.0
C-1	Medium Density Residential (MDR)	122	40.77	5.0–8.0
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0–8.0
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0–8.0
C-5	Open Space (OS)		9.97	
C-6	Very High Density Residential (VHDR)	477	30.85	14.0–20.0
C-8	Medium High Density Residential (MHDR)	105	14.96	5.0–8.0
D-1	Open Space (OS)		110.66	
E-1	Open Space (OS)		5.61	
E-2	Mixed Use (MU)		127.34	
E-2A ²	Mixed Use (MU)		16.48	
E-2B ²	Mixed Use (MU)		4.92	
E-3	Open Space (OS)		69.33	
E-4	Mixed Use (MU)		12.25	
E-5	Mixed Use (MU)	19 ¹	5.31	
E-6	Mixed Use (MU)	120 ¹	33.99	
E-7	Mixed Use (MU)	5 ¹	1.32	
E-8	Mixed Use (MU)	15 ¹	4.94	

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
F-2	Medium Density Residential (MDR)	143	71.64	2.0–5.0
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0–8.0
F-4	Commercial Retail (CR)		6.23	
G-1	Commercial Retail (CR)		4.43	
G-2	Open Space (OS)		1.44	
G-3	Open Space (OS)		1.19	
G-4	Open Space (OS)		5.6	
G-5	Medium Density Residential (MDR)	69	34.6	2.0–5.0
G-6	Open Space (OS)		13.65	
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0–8.0
G-8	High Density Residential (HDR)	282	20.6	8.0–14.0
G-9	Open Space (OS)		6.17	
G-10	Medium Density Residential (MDR)	37	18.57	2.0–5.0
G-11	Medium Density Residential (MDR)	70	26.86	2.0–5.0
G-12	Open Space (OS)		5.35	
G-13	Medium Density Residential (MDR)	53	14.17	2.0–5.0
H-1	Open Space (OS)		5.12	
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0–8.0
H-3	Open Space (OS)		3.79	
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0–8.0
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0–8.0
H-6	Open Space (OS)		11.43	
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0–8.0
H-8	High Density Residential (HDR)	189	13.55	8.0–14.0
H-9	Medium Density Residential (MDR)	66	20.03	2.0–5.0
H-10	Open Space (OS)		3.96	
I-1	Open Space (OS)		9.6	
I-2	Open Space (OS)		3.9	
I-3	Open Space (OS)		0.5	
I-4	Medium Density Residential (MDR)	35	7	2.0–5.0
I-5	Medium Density Residential (MDR)	51	10.6	2.0–5.0

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
I-6	Medium Density Residential (MDR)	42	9.6	2.0–5.0
I-7	Medium Density Residential (MDR)	29	6.7	2.0–5.0
I-8	Medium Density Residential (MDR)	40	8	2.0–5.0
I-9	Medium Density Residential (MDR)	57	11.5	2.0–5.0
I-10	Medium Density Residential (MDR)	47	12.4	2.0–5.0
I-11	Medium Density Residential (MDR)	37	10.1	2.0–5.0
J-1	Medium Density Residential (MDR)	143	30.64	2.0–5.0
J-2	Open Space/ Lake (OS LAKE)		20.56	
J-3	Open Space (OS)		11.24	
J-4	Medium Density Residential (MDR)	189	39.42	2.0–5.0
J-5	Open Space (OS)		1.20	
J-6	Open Space (OS)		4.1	
J-7	Open Space (OS)		4.24	
J-8	Open Space (OS)		1.16	
K-1	Open Space/Lake (OS LAKE)		22.4	
K-2	Open Space (OS)		8.8	
K-3	Open Space (OS)		3.2	
L-1	High Density Residential (HDR)	1,158	120.46	8.0–14.0
L-2	Open Space (OS)		4.1	
L-3	Open Space (OS)		1.40	
M-1A	Medium Density Residential (MDR)	8	2.5	2.0–5.0
M-1B	Medium Density Residential (MDR)	68	18.9	2.0–5.0
M-1C	Medium Density Residential (MDR)	61	16.4	2.0–5.0
M-2	Open Space (OS)		1.9	
M-3	Open Space (OS)		1.6	
M-4	Public Facility (PF)		84.3	
M-5A	Medium Density Residential (MDR)	14	4.1	2.0–5.0
M-5B	Medium Density Residential (MDR)	61	15.7	2.0–5.0
M-6A	Medium Density Residential (MDR)	14	3.8	2.0–5.0
M-6B	Medium Density Residential (MDR)	57	13.4	2.0–5.0
M-7A	Medium Density Residential (MDR)	20	4.1	2.0–5.0

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
M-7B	Medium High Density Residential (MHDR)	56	11	5.0–8.0
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0–8.0
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0–8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0–8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
Right-of-Way			167.86	
TOTAL		7,160	2,162.65	

Note: Planning Areas B-4, B-7, C-3, C-7, and F-1 have been intentionally left out.

1. This number represents a maximum, rather than a target.
2. Planning Area E-2A allows for 58 racetrack recreational units and Planning Area E-2B allows for 16 racetrack recreational units which are not considered dwelling units as they are a non-residential use identified as a member villa accessible 24 hours a day, seven days a week. The Racetrack Recreational Unit provides a private space for a Thermal Club members to store personal belongings, vehicles, and equipment utilized for racing. The unit can further be utilized for vehicular repairs, other racing-related recreational activities and is located directly adjacent to the racetrack with an open view deck facing the track allowing for viewing of racing activities. The unit would be restricted from permanent residency, and has limitations imposed by the club for duration of stays.

The mixed use, commercial, and industrial land use categories will comprise 277, 28, and 81 acres, respectively. Commercial areas will serve the Kohl Ranch project as well as neighboring communities. Mixed Use and industrial uses will be oriented toward the Jacqueline Cochran Regional Airport as well as larger regional markets, and are intended to provide employment opportunities to project area residents. The land uses proposed for the Kohl Ranch Specific Plan are described in **Figure 1-4, Land Use Plan** and are briefly summarized in **Table 1-A**, above.

2. SPECIFIC PLAN ZONING

See Specific Plan Zoning Ordinance (Riverside County Ordinance No. 348.4897) attached to this Specific Plan.

1
2 ORDINANCE NO. 348.4801

3 AN ORDINANCE OF THE COUNTY OF RIVERSIDE

4 AMENDING ORDINANCE NO. 348 RELATING TO ZONING

5 The Board of Supervisors of the County of Riverside Ordains as follows:

6 Section 1. Section 4.2 of Ordinance No. 348, and Official Zoning Plan Map No. 41.090, as
7 amended, are further amended by placing in effect in the Lower Coachella Valley District the zone or zones
8 as shown on the map entitled "Change of Official Zoning Plan Amending Ordinance No. 348, Map No.
9 41.090, Change of Zone Case No. 7852," which map is made a part of this ordinance.

10 Section 2. Article XVIIa, of Section 17.87 of Ordinance No. 348 is amended and restated in its
11 entirety to read as follows:

12 Section 17.87 SP ZONE REQUIREMENTS AND STANDARDS FOR SPECIFIC PLAN NO.
13 303.

14 a. Planning Areas C-5, D-1, G-2, G-3, G-4, G-6, G-9, G-12, H-1, H-3, H-6, H-10, I-1, I-2, I-
15 3, J-2, J-3, J-5, J-6, J-7, J-8, K-1, K-3, L-2, L-3, M-2, M-3, M-8, and M-9. **Open Space**

16 (1) The uses permitted in Planning Areas C-5, D-1, G-2, G-3, G-4, G-6, G-9, G-12, H-
17 1, H-3, H-6, H-10, I-1, I-2, I-3, J-2, J-3, J-5, J-6, J-7, J-8, K-1, K-3, L-2, L-3, M-2, M-3, M-8, and
18 M-9 of Specific Plan No. 303 shall be the same as those uses permitted in Article VIIIe, Section
19 8.100 of Ordinance No. 348 except that the uses permitted pursuant to Section 8.100.a(1) and (2)
20 and Section b.(1) shall not be permitted. In addition, the permitted uses identified under Section
21 8.100.a. shall include public parks; community centers; and when the gross acre of a lot is twenty
22 (20) acres or greater, the uses identified under Article XIII, Section 13.1.b. of Ordinance No. 348
23 shall also be included.

24 (2) The development standards for Planning Areas C-5, D-1, G-2, G-3, G-4, G-6, G-9,
25 G-12, H-1, H-3, H-6, H-10, I-1, I-2, I-3, J-2, J-3, J-5, J-6, J-7, J-8, K-1, K-3, L-2, L-3, M-2, M-3,
26 M-8, and M-9 of Specific Plan No. 303 shall be the same as those standards identified in Article
27 VIIIe, Section 8.101 of Ordinance No. 348.
28

(3) Except as provided above, all other zoning requirements shall be the same as those requirements identified in Article VIIIe of Ordinance No. 348.

b. Planning Areas A-1, A-3, A-7, E-1, and E-3.

Open Space

(1) The uses permitted in Planning Areas A-1, A-3, A-7, E-1, and E-3 of Specific Plan No. 303 shall be the same as those uses permitted in Article VIIIe, Section 8.100 of Ordinance No. 348 except that the uses permitted pursuant to Section 8.100.b.(1) shall not be permitted. In addition, the permitted uses identified under Section 8.100.a. shall include public parks; community centers; facilities related to large scale recreational uses such as a motor sports race track and facilities related thereto, including but not limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle display areas and ancillary uses in support thereof; and when the gross area of a lot is twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.b. of Ordinance No. 348 shall also be included.

(2) The development standards for Planning Areas A-1, A-3, A-7, E-1, and E-3 of Specific Plan No. 303 shall be the same as those standards identified in Article VIIIe., Section 8.101 of Ordinance No. 348.

(3) If Planning Areas A-1, A-3, A-7, E-1, and E-3 are developed with large scale recreational uses such as a motor sports race track and facilities related thereto, the development standards shall be the same as those identified in Article VIIIe., Section 8.101 of Ordinance No. 348 except that the following development standards shall also apply:

(A) The minimum front yard setback for any building shall be 20 feet.

(B) The minimum side yard setback for any building shall be 5 feet.

(4) Except as provided above, all other zoning requirements shall be the same as those requirements identified in Article VIIIe of Ordinance No. 348.

c. Planning Areas A-2.

Mixed Use/Air Park

(1) The permitted uses in Planning Areas A-2 of Specific Plan No. 303 shall be the same as those permitted in Article IX, Section 9.1 of Ordinance No. 348, except that the uses permitted pursuant to Sections 9.1.a. (17), (23), (25), (27), (29), (32), (42), (51), (52), (61), (65), (67), (73), and (93); Sections 9.1.b. (7), (9), (11)a., (18), (19), and (20); and Sections 9.1.d. (4), (5),

(7), (10), (11), (12) and (13) shall not be permitted. In addition, the permitted uses identified under Section 9.1.a. shall include aviation equipment assembly; communication equipment and microwave sales and installation; computer and office equipment sales, service, repair and assembly; conference facilities; country clubs, manufacture of dairy products, not including dairies; emergency and urgent care medical facilities; libraries; manufacture of grain and bakery products; health and exercise centers; hospitals; ice houses; jewelry manufacture and repair; manufacture of wearing apparel and accessories; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture of handbags, luggage, footwear, and other personal leather goods; manufacture of cutlery, tableware, hand tools and hardware; manufacture of plumbing and heating items; vehicle storage and impoundment; manufacture of office and computing machines; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; manufacture of non-alcoholic beverages; manufacture of confectionery products; manufacture and repair of refrigeration and heating equipment; printing of periodicals, books, forms, cards and similar items; public parks and public playgrounds; golf courses; religious institutions; facilities for research and development of precision components and products; and water wells and appurtenant facilities.

In addition, the permitted uses identified under Section 9.1.b. shall include aerial service businesses including advertising, photography and tours; aerospace/aeronautical museums; aircraft equipment sales, service and repair; contractor storage yards; flight schools; intermodal cargo transfer facilities; manufacture of furniture and fixtures, including cabinets, partitions and similar small items; manufacture of bicycles; parcel delivery services; warehousing and distribution; facilities related to large scale recreational uses such as golf courses and a motor sports race track and facilities related thereto, including but not limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle display areas and ancillary uses in support thereof; and when the gross area of a lot is twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.b. of Ordinance No. 348 shall be included.

1 In addition, the permitted uses identified under Section 9.1.d. shall include community
2 centers; schools; meat and poultry processing not including slaughtering or rendering of animals;
3 paper shredding facilities; research and manufacture of drugs and pharmaceuticals; manufacture of
4 soaps, cleaners and toiletries; wrought iron fabrication; machine, welding and blacksmith shops;
5 breweries, distilleries and wineries; paper storage and recycling within a building; recycling
6 processing facilities; paper and paperboard mills; manufacture of containers and boxes; and above
7 ground natural gas storage.

8 (2) The development standards for Planning Areas A-2 of Specific Plan No. 303 shall
9 be the same as those standards identified in Article IX, Section 9.4 of Ordinance No. 348 except
10 that sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks and other
11 sporting activities, shall not be permitted.

12 (3) Except as provided above, all other zoning requirements shall be the same as those
13 requirements identified in Article IX of Ordinance No. 348.

14 d. Planning Areas C-6, G-8, H-8 and L-1.

**High Density Residential & Very
High Density Residential**

15 (1) The uses permitted in Planning Areas C-6, G-8, H-8 and L-1 of Specific Plan No.
16 303 shall be the same as those uses permitted in Article VII, Section 7.1 of Ordinance No. 348,
17 except that the uses permitted pursuant to Section 7.1.a.(3); (4) and (10); Section 7.1.b(9); and
18 Section 7.1.c(1) shall not be permitted.

19 In addition, the permitted uses identified under Section 7.1.b. shall include two family
20 dwellings developed pursuant to Subsections AA. through DD. of this section; lakes, including
21 those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water and
22 noncommercial fishing; water wells and appurtenant facilities; and when the gross area of a lot is
23 twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.a. and b. of
24 Ordinance No. 348 shall also be included, except that the uses permitted pursuant to Section
25 13.1.a.(15) shall not be permitted.

26 (2) The development standards for Planning Areas C-6, G-8, H-8 and L-1 of Specific
27 Plan 303 shall be the same as those standards identified in Article VII, Sections 7.2 through 7.11
28

1 except that the development standards set forth in Sections 7.3, 7.4, 7.5, 7.6, and 7.10 shall be
2 deleted and replaced by the following:

3 A. Lot area shall be not less than four thousand (4,000) square feet, unless
4 cluster development subject to the development standards set forth in subsections AA.
5 through DD. of this section is utilized. The minimum lot area shall be determined by
6 excluding that portion of a lot that is used solely for access to the portion of a lot used as
7 building site.

8 B. The front yard shall be not less than 16 feet, measured from the existing
9 street line or from any future street line as shown on any Specific Plan of Highways,
10 whichever is nearer the proposed structure.

11 C. The minimum average width of that portion of a lot to be used as a
12 building site shall be forty feet (40'), with a minimum average depth of one hundred feet
13 (100') unless cluster development subject to the development standards set forth in
14 subsections AA. through DD. of this section is utilized. "Flag" lots shall not be permitted.

15 D. The minimum frontage of a lot shall be forty feet (40') except that lots
16 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty feet (30')
17 unless cluster development subject to the development standards set forth in subsections
18 AA. through DD. of this section is utilized. Lot frontage along curvilinear streets may be
19 measured at the building setback in accordance with zone development standards.

20 E. Side yards on interior and through lots shall be not less than ten percent
21 (10%) of the width of the lot, but not less than three feet (3') in width in any event, and
22 need not exceed a width of five feet (5') unless cluster development subject to the
23 development standards set forth in subsections AA. through DD. of this section is utilized.
24 Side yards on corner and reverse corner lots shall be not less than ten feet (10') from the
25 existing street line or from any future street line as shown on any Specific Plan of
26 Highways, whichever is nearer the proposed structure, upon which the main building sides
27 unless cluster development subject to the development standards set forth in subsections
28

1 AA. through DD. of this section is utilized. Where a zero lot line design is utilized, the
2 alternate side yard shall be not less than ten feet (10') in width.

3 F. The rear yard shall not be less than ten feet (10') unless cluster
4 development subject to the development standards set forth in subsection AA. through DD.
5 of this section is utilized.

6 G. Every main building erected or structurally altered shall have a lot or
7 building site of not less than one thousand one hundred (1,100) square feet for each
8 dwelling unit in such main building unless cluster development subject to the development
9 standards set forth in subsections AA. through DD. of this section is utilized.

10 In addition, when a cluster development design is utilized, the following
11 development standards shall be applicable:

12 AA. The minimum overall area for each individual unit within a two-
13 family dwelling exclusive of the area set aside for street rights of way shall be two
14 thousand (2,000) square feet.

15 BB. The minimum lot area for two-family lots used as a residential
16 building site shall be two thousand (2,000) square feet. The minimum lot area
17 shall be determined by excluding that portion of a lot that is used solely for access
18 to the portion of a lot used as a building site. For each two family dwelling,
19 common open space shall be provided equal to the difference between the lot area
20 for such two family dwelling and eight thousand (8,000) square feet.

21 CC. Side yards on interior and through lots shall be not less than three
22 feet (3') for one-story buildings; not less than ten feet (10') for two-story buildings;
23 and not less than fifteen feet (15') for three-story buildings. Side yards on corner
24 and reversed corner lots shall be not less than ten feet (10') from the existing street
25 line as shown on any Specific Plan of Highways, whichever is nearer the proposed
26 structure, upon which the main building sides, except that where the lot is less than
27 fifty feet (50') wide the yard need not exceed twenty percent (20%) of the lot
28 width.

DD. The rear yard shall not be less than ten feet (10') for one-story buildings; not less than fifteen feet (15') for two-story buildings; and not less than twenty feet (20') for three-story buildings.

(3) Except as provided above, all other zoning requirements shall be the same as those requirements identified in Article VII of Ordinance No. 348.

e. Planning Areas B-1, B-2, B-5, B-6, C-2, C-4, C-8, F-3, G-7, H-2, H-4, H-5, H-7, M-7B, M-7C, M-7D, and M-7E.

Medium High Density Residential

(1) The uses permitted in Planning Areas B-1, B-2, B-5, B-6, C-2, C-4, C-8, F-3, G-7, H-2, H-4, H-5, H-7, M-7B, M-7C, M-7D, and M-7E of Specific Plan No. 303 shall be the same as those uses permitted in Article VII, Section 7.1 of Ordinance No. 348, except that the uses permitted pursuant to Section 7.1.a.(3); Section 7.1.b(9); and 7.1.c(1) shall not be permitted. In addition, the permitted uses identified under Section 7.1.b. shall include two family dwellings developed pursuant to Subsections AA. through FF. of this section; community centers, lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water and non-commercial fishing; water wells and appurtenant facilities; and when the gross area of a lot is twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.b. of Ordinance No. 348 shall also be included.

(2) The development standards for Planning Areas B-1, B-2, B-5, B-6, C-2, C-4, C-8, F-3, G-7, H-2, H-4, H-5, H-7, M-7B, M-7C, M-7D, and M-7E of Specific Plan No. 303 shall be the same as those standards identified in Article VII, Sections 7.2 through 7.11, except that the development standards set forth in Sections 7.3, 7.5, 7.6, and 7.11 shall be deleted and replaced by the following:

A. Lot area shall be not less than five thousand (5,000) square feet, unless cluster development subject to the development standards set forth in subsections AA. through FF. of this section is utilized. The minimum lot area shall be determined by excluding that portion of a lot that is used solely for access to the portion of a lot used as building site.

1 B. The minimum average width of that portion of a lot to be used as a
2 building site shall be fifty feet (50'), with a minimum average depth of one hundred feet
3 (100') unless cluster development subject to the development standards set forth in
4 subsections AA. through FF. of this section is utilized. "Flag" lots shall not be permitted.

5 C. The minimum frontage of a lot shall be fifty (50') except that lots fronting
6 on knuckles or cul-de-sacs may have a minimum frontage of thirty feet (30') unless cluster
7 development subject to the development standards set forth in subsections AA. through FF.
8 of this section is utilized. Lot frontage along curvilinear streets may be measured at the
9 building setback in accordance with zone development standards.

10 D. Side yards on interior and through lots shall be not less than ten percent
11 (10%) of the width of the lot, but not less than three feet (3') in width in any event, and
12 need not exceed a width of five feet (5') unless cluster development subject to the
13 development standards set forth in subsections AA. through FF. of this section is utilized.
14 A zero lot line design may be used, in which event the alternate side yard shall be not less
15 than ten feet (10') in width. Side yards on corner and reverse corner lots shall be not less
16 than ten feet (10') from the existing street line or from any future street line as shown on
17 any Specific Plan of Highways, whichever is nearer the proposed structure, upon which the
18 main building sides, unless cluster development subject to the development standards set
19 forth in subsections AA. through FF. of this section is utilized.

20 E. The rear yard shall not be less than ten feet (10') unless cluster
21 development subject to the development standards set forth in subsections AA. through FF.
22 of this section is utilized.

23 In addition, when a cluster development design is utilized, for either single family or two
24 family dwellings, the following development standards shall be applicable:

25 AA. The minimum overall area for each single-family dwelling unit or
26 each individual unit within a two-family dwelling, exclusive of the area set aside
27 for street rights of way shall be four thousand (4,000) square feet.
28

1 BB. The minimum lot area for individual single-family and two-family
2 lots used as a residential building site shall be four thousand (4,000) square feet.
3 The minimum lot area shall be determined by excluding that portion of a lot that is
4 used solely for access to the portion of a lot used as a building site. For each
5 dwelling unit, common open space shall be provided equal to the difference
6 between the single-family or two-family lot area and five thousand (5,000) square
7 feet for each single-family dwelling or ten thousand five hundred (10,500) square
8 feet for each two-family dwelling.

9 CC. The minimum average width of that portion of a lot to be used as a
10 building site shall be fifty feet (50'), with a minimum average depth of ninety feet
11 (90'). "Flag" lots shall not be permitted.

12 DD. The minimum frontage of a lot shall be fifty feet (50'), except that
13 lots fronting on knuckles or culs-de-sac may have a minimum frontage of thirty
14 feet (30'). Lot frontage along curvilinear streets may be measured at the building
15 setback in accordance with zone development standards.

16 EE. Side yards for single-family dwellings on interior and through lots
17 shall be not less than ten percent (10%) of the width of the lot, but not less than
18 three feet (3') in width in any event, and need not exceed a width of five feet (5').
19 Side yards for single-family dwellings on corner and reverse corner lots shall be
20 not less than ten feet (10') from the existing street line or from any future street line
21 as shown on any Specific Plan of Highways, whichever is nearer the proposed
22 structure, upon which the main building sides. Where a zero lot line design is
23 utilized for single-family dwellings, the alternate side yard shall be not less than
24 ten feet (10') in width. Side yards for two-family dwellings on interior and through
25 lots shall be not less than five feet (5') for one-story buildings; not less than ten feet
26 (10') for two-story buildings; and not less than fifteen feet (15') for three-story
27 buildings. Side yards for two-family dwellings on corner and reverse corner lots
28 shall be measured from the existing street line or from any future street line as

1 shown on any Specific Plan of Highways, whichever is nearer the proposed
2 structure, upon which the main building sides.

3 FF. The rear yard for single-family dwellings shall be not less than ten
4 feet (10'). The rear yard for two-family dwellings shall be not less than ten feet
5 (10') for one-story buildings, not less than fifteen feet (15') for two-story buildings;
6 and not less than twenty feet (20') for three-story buildings.

7 (3) Except as provided above, all other zoning requirements shall be the same as
8 those requirements identified in Article VII of Ordinance No. 348.

9 f. Planning Areas A-5, G-1 and F-4.

Commercial Retail

10 (1) The uses permitted in Planning Areas A-5, G-1 and F-4 of Specific Plan No. 303
11 shall be the same as those permitted in Article IX, Section 9.1 of Ordinance No. 348, except that
12 the uses permitted pursuant to Sections 9.1.a. (29), (51) and (93), b.(11)a., (12), (18), (19), and
13 (20), d.(2), (3), (4), (5), (6), (9), (10), (11), (12) and (13), shall not be permitted.

14 In addition, the permitted uses identified under Section 9.1.a. shall include public parks and
15 public playgrounds; golf courses; country clubs; animal hospitals with all kennels entirely indoors;
16 health clubs; computer sales and repair stores; parcel delivery services; libraries; religious
17 institutions; community centers; schools; and water wells and appurtenant facilities.

18 In addition, when the gross area of a lot is twenty (20) acres or greater, the permitted uses
19 identified under Section 9.1.b. shall include the uses permitted under Article XIII, Section 13.1.b.
20 of Ordinance No. 348.

21 In addition, the permitted uses identified under Section 9.1.d. shall include electric vehicle
22 charging stations.

23 (2) The development standards for Planning Areas A-5, G-1 and F-4 of Specific Plan
24 No. 303 shall be the same as those standards identified in Article IX, Section 9.4 of Ordinance No.
25 348.

26 (3) Except as provided above, all other zoning requirements shall be the same as those
27 requirements identified in Article IX of Ordinance No. 348.
28

g. Planning Areas B-3, C-1, F-2, G-5, G-10, G-11, G-13, H-9, I-4, I-5, I-6, I-7, I-8, I-9, I-10, I-11, J-1, J-4, M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-6B, and M-7A.

(1) The uses permitted in Planning Areas B-3, C-1, F-2, G-5, G-10, G-11, G-13, H-9, I-4, I-5, I-6, I-7, I-8, I-9, I-10, I-11, J-1, J-4, M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-6B, and M-7A of Specific Plan No. 303 shall be the same as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348, except that the uses permitted pursuant to Section 6.1.b(5); and Section 6.1.c(1). shall not be permitted. In addition, the permitted uses identified under Section 6.1.a. shall include two family dwellings developed pursuant to subsection AA. through GG. of this section; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; water wells and appurtenant facilities; and when the gross acre of a lot is twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.a. and b. of Ordinance No. 348 shall also be included, except that the uses permitted pursuant to Section 13.1.a(15) shall not be permitted.

In addition the permitted uses identified under Section 6.1.b. shall include day care centers; libraries; religious institutions; community centers; and schools.

(2) The development standards for Planning Areas B-3, C-1, F-2, G-5, G-10, G-11, G-13, H-9, I-4, I-5, I-6, I-7, I-8, I-9, I-10, I-11, J-1, J-4, M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-6B, and M-7A of Specific Plan No. 303 shall be the same as those standards identified in Article VI, Section 6.2, except that the development standards set forth in Article VI, Section 6.2.b, c., d., e.(2), e.(3) and g shall be deleted and replaced by the following:

A. Lot area shall be not less than six thousand (6,000) square feet, unless cluster development subject to the development standards set forth in subsection AA. through GG. of this section is utilized. The minimum lot area shall be determined by excluding that portion of a lot that is used solely for access to the portion of a lot used as building site.

B. The minimum average width of that portion of a lot to be used as a building site shall be sixty feet (60'), with a minimum average depth of one hundred feet

1 (100') unless cluster development subject to the development standards set forth in
2 subsections AA. through GG. of this section is utilized. "Flag" lots shall not be permitted.

3 C. The minimum frontage of a lot shall be sixty feet (60') except that lots
4 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty-five feet (35')
5 unless cluster development subject to the development standards set forth in subsections
6 AA. through GG. of this section is utilized. Lot frontage along curvilinear streets may be
7 measured at the building setback in accordance with zone development standards.

8 D. Side yards on interior and through lots shall be not less than ten percent
9 (10%) of the width of the lot, but not less than three feet (3') in width in any event, and
10 need not exceed a width of five feet (5') unless cluster development subject to the
11 development standards set forth in subsection AA. through GG. of this section is utilized.
12 Side yards on corner and reverse corner lots shall be not less than ten feet (10') from the
13 existing street line or from any future street line as shown on any Specific Plan of
14 Highways, whichever is nearer the proposed structure, upon which the main building sides
15 unless cluster development subject to the development standards set forth in subsections
16 AA. through GG. of this section is utilized.

17 E. The rear yard shall not be less than ten feet (10') unless cluster
18 development subject to the development standards set forth in subsections AA. through
19 GG. of this section is utilized.

20 In addition, when a cluster development design is utilized for single family or two
21 family dwellings, the following development standards shall be applicable:

22 AA. The minimum overall area for each single-family dwelling unit or
23 each individual unit within a two-family dwelling, exclusive of the area set aside
24 for street rights of way shall be five thousand (5,000) square feet.

25 BB. The minimum lot area for individual single-family lots used as a
26 residential building site shall be five thousand (5,000) square feet. The minimum
27 lot area for two-family lots shall be five thousand (5,500) square feet. The
28 minimum lot area shall be determined by excluding that portion of a lot that is used

1 solely for access to the portion of a lot used as a building site. For each dwelling
2 unit, common open space shall be provided equal to the difference between the
3 single-family or two-family lot area and six thousand (6,000) square feet for each
4 single-family dwelling or twelve thousand (12,000) square feet for each two-
5 family dwelling.

6 CC. The minimum average width of that portion of a lot to be used as a
7 building site shall be fifty-five feet (55'), with a minimum average depth of one
8 hundred feet (100'). "Flag" lots shall not be permitted.

9 DD. The minimum frontage of a lot shall be fifty-five feet (55'), except
10 that lots fronting on knuckles or cul-de-sac may have a minimum frontage of thirty
11 feet (30'). Lot frontage along curvilinear streets may be measured at the building
12 setback in accordance with zone development standards.

13 EE. Side yards on interior and through lots shall be not less than ten
14 percent (10%) of the width of the lot, but not less than three feet (3') in width in
15 any event, and need not exceed a width of five feet (5'). Side yards on corner and
16 reverse corner lots shall be not less than ten (10') from the existing street line or
17 from any future street line as shown on any Specific Plan of Highways, whichever
18 is nearer the proposed structure, upon which the main building sides. Where a zero
19 lot line design is utilized the alternate side yard shall be not less than ten feet (10')
20 in width.

21 FF. The rear yard for single-family dwellings shall be not less than ten
22 feet (10'). The rear yard for two-family dwellings shall be not less than ten feet
23 (10') for one-story buildings, not less than fifteen feet (15') for two-story buildings,
24 and not less than twenty feet (20') for three-story buildings.

25 GG. In no case shall more than sixty percent (60%) of any lot be
26 covered by buildings or structures.

27 (3) Except as provided above, all other zoning requirements shall be the same as those
28 requirements identified in Article VI of Ordinance No. 348.

h. Planning Area A-4

(1) The uses permitted in Planning Area A-4 of Specific Plan 303 shall be the same as those uses permitted in Article XII, Section 12.2 of Ordinance No. 348, except that the uses permitted pursuant to Section 12.2.c. (3), (7), (11) and (12); Section 12.2.d.; Section 12.2.e., 12.2.f. and 12.2.g. shall not be permitted. In addition, the permitted uses identified under Section 12.2.b. shall include water wells and appurtenant facilities, facilities related to large scale recreational uses such as golf courses and a motor sports race track and facilities related thereto, including but not limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle display areas, underground fuel storage and ancillary uses in support thereof; and when the gross acre of a lot is twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.a. and b. of Ordinance No. 348 shall also be included, except that the uses permitted pursuant to Section 13.1.a.(15) shall not be permitted.

In addition, the permitted uses identified under Section 12.2.b shall include aerial services including advertising, photography and tours; aerospace/aeronautical museums; aircraft taxiways; catering services/flight kitchens; conference facilities; golf courses and appurtenant facilities; convenience stores; dry cleaners; flight schools; hospitals; hotels and motels; intermodal cargo transfer terminals; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; paper recycling facilities; parcel delivery services; and rental car agencies including the storage of rental cars.

In addition, the permitted uses identified under Section 12.2.c. shall include cogeneration plants; structures and facilities necessary and incidental to the development, generation and transmission of electric power and gas such as power plants, booster or conversion plants, transmission lines, pipelines and the like; and incarceration and detention facilities.

(2) The development standards for Planning Area A-4 of Specific Plan No. 303 shall be the same as those standards identified in Article XII, Section 12.4 of Ordinance No.348, provided however that Article XII, Section 12.4(b)(3) shall apply only to setbacks calculated from public streets. Article XII, Section 12.4.a. is modified to provide that the minimum lot area shall be seven thousand (7,000) square feet with no minimum average width. There shall be no

1 minimum setback from any private street. Article XII, Section 12.4c.(2) is modified to provide that
2 an observation tower built within Planning Areas A-4 and built as part of a large scale recreational
3 use shall not exceed 70 feet in height and sports lighting, consisting of exterior nighttime lighting
4 for ballfields, racetracks and other sporting activities, shall not be permitted.

5 (3) Except as provided above, all other zoning requirements shall be the same as
6 those requirements identified in Article XII of Ordinance No.348.

7 i. Planning Area A-6, E-2, and E-4

Mixed Use

8 (1) The uses permitted in Planning Areas A-6, E-2 and E-4 of Specific Plan 303 shall
9 be the same as those uses permitted in Article IX, Section 9.1a of Ordinance No. 348, except that
10 the uses permitted pursuant to Article IX, Sections 9.1.a. (17), (23), (25), (27), (29), (32), (42),
11 (51), (52), (61), (65), (67), (73), (83), (93) and (96-within Airport Land Use Compatibility Zone
12 C); allowed under Sections 9.1.b. (7), (9), (10), (11.c), (13), (14), (15), (16), (18), (19), and (20);
13 and allowed under Sections 9.1.d. (2), (4), (5), (9), (10), (12), (13), and (16) shall not be permitted;
14 and uses permitted pursuant to Article IX, Section 9.1.a (35) shall not be permitted in Planning
15 Areas E-2 and E-4.

16 In addition, the uses identified under Section 9.1.b as allowable with a plot plan shall
17 include aerial services including advertising, photography and tours; aerospace/aeronautical
18 museums; aircraft equipment sales, service, and repair; aircraft taxiways; airports; aviation
19 equipment assembly; body and fender shops; building materials and sales yard; building movers
20 storage yard; catering services/flight kitchens; cold storage plant; communications and microwave
21 installations; computer and office equipment sales, service, repair and assembly; conference
22 facilities; contractor storage yards; country clubs; dry cleaners; emergency and urgent care medical
23 facilities; facilities related to large scale recreational uses such as a motor sports race track and
24 facilities related thereto, including but not limited to race track, private garages, clubhouse, tuning
25 shop, observation tower, museum, vehicle display areas, underground fuel storage and ancillary
26 uses in support thereof; flight schools; hardware and home improvement centers; health and
27 exercise centers; heliports; ; intermodal cargo transfer terminals; libraries; industrial and
28 manufacturing uses involving food products including beverages, including alcoholic beverages,

1 canning and preserving fruits and vegetables, dairy products-not including dairies, grain and bakery
2 products, ice, meat and poultry products-including meat packing but not slaughtering, sugar and
3 confectionary products, and wineries, distilleries, and breweries; textile products including cotton,
4 wool, and synthetic weaving and finishing mills, wearing apparel and accessory products, knitting
5 mills, floor covering mills, and yard and thread mills; lumber and wood products including saw and
6 planing mills, manufacture of containers and crates, fabrication of wood building structures,
7 lumber yards, manufacture of furniture and fixtures including cabinets, partitions, and similar
8 items; paper products including paper and paperboard mills, manufacture of containers and boxes,
9 paper shredding, printing and publishing of newspaper, periodicals, books, forms cards and similar
10 items, binding of books and other publications; chemicals and related products including
11 manufacture of organic and inorganic compounds-not including those of a hazardous nature,
12 manufacture of drugs and pharmaceuticals, soaps, cleaners, and toiletries, manufacture of
13 agricultural chemicals-not including pesticides and fertilizers, paints and varnishes; rubber and
14 plastic and synthetic products including manufacture of tires and tubes, fabrication of rubber,
15 plastics, and synthetic products; leather products including tanning and finishing of leather,
16 manufacture of handbags, luggage, footwear, and other personal leather goods; stone clay, glass,
17 and concrete products including stone cutting and related activities, pottery and similar items, glass
18 blowing, pressing and cutting, glassware products, manufacture of concrete, gypsum, plaster and
19 mineral products; metal products including manufacture of cans and containers, cutlery, tableware,
20 hand tools and hardware, plumbing and heating items, wrought iron fabrication, manufacture and
21 assembly of fencing, machine, welding, and blacksmith shops, metal stamps and forged metal
22 products, fabrication of metal buildings, manufacture of ordnance and firearms, not including
23 explosives, jewelry; primary metal industries including foundries, rolling and drawing metals,
24 casting metals, blast furnaces, smelting of metals; machinery including engines, turbines, and parts,
25 farm, garden construction, industrial machinery, office and computing machines, manufacture and
26 repair of refrigeration and heating equipment, equipment sales, rental, and storage; electrical
27 equipment including electrical and electronic apparatus and components, appliances, lighting and
28 wiring, radio, television and communications equipment, musical and recording equipment,

1 musical and recording equipment; transportation and related industries including vehicles, aircraft,
2 boats and parts manufacture, railroad equipment, motorcycles, bicycles, and parts, travel trailers
3 and recreational vehicles manufacture, draying, freighting, and trucking operations, railroad yards
4 and stations, vehicle storage and impoundment, trailer and boat storage; engineering of scientific
5 instruments including manufacture and repair of measuring devices, watches, clocks and related
6 items; manufacture and repair of optical goods, medical instruments, supplies and equipment,
7 engineering, survey and drafting instruments and photography equipment; manufacture, assembly,
8 testing and repair of components, devices, equipment and systems of an electrical, electronic, or
9 electro-mechanical nature; manufacture of wearing apparel and accessories; mini warehouses;
10 nurseries and garden supply; outdoor film studio; paper recycling facilities; parcel delivery
11 services; parking lots and parking structures; public parks and public playgrounds; public utility
12 substations and storage yards; recycling of wood, metal and construction wastes;; repair of jewelry;
13 research and development facilities for biomedical, chemical, electronic, mechanical and other
14 scientific purposes; research and development facilities for precision components and products;
15 sand blasting; trailer and truck sales and rentals; vehicle and motorcycle repair; water wells and
16 appurtenant facilities; and warehousing and distribution.

17 In addition, the uses conditionally permitted identified under Section 9.1.d shall include
18 hospitals; abattoirs; above ground natural gas storage less than 6,000 gallons; acid and abrasives
19 manufacturing; auto wrecking and junk yards; concrete batch plants; cotton ginning; disposal
20 service operations; electric vehicle charging stations; fertilizer production, and processing organic
21 or inorganic; gas, steam, and oil drilling operations; recycling processing facilities; processing and
22 rendering of fats and oils; and sewerage treatment plants.

23 (2) The development standards for Planning Areas A-6 ,E-2, and E-4 of Specific Plan
24 No. 303 shall be the same as those standards identified in Article XII, Section 12.4 of Ordinance
25 No. 348 except those development standards set forth in Article XII, a, b, c(2), and k shall be
26 deleted and replaced by the following:

- 27 a. Lot Size. Minimum lot area shall be seven thousand (7,000) square feet with no
28 minimum average width.

1 b. Standard Setbacks.

- 2 1. Where the front, side, or rear yard adjoins a street, the minimum setback shall
- 3 be 25 feet from any public street.
- 4 2. No minimum setback is required from any private street.
- 5 3. Front Yard: No minimum.
- 6 4. Rear Yard: No minimum.
- 7 5. Side Yard: No minimum.

8 c. Height Requirements. Buildings shall not exceed 50 feet unless a height up to 75 feet

9 is approved pursuant to Article XVIII, Section 18.34. An observation tower built as

10 part of large scale recreational use shall not exceed 70 feet in height.

11 d. Lighting. All lighting fixtures, including spot lights, electrical reflectors and other

12 means of illumination for signs, structures, landscaping, parking, loading, unloading

13 and similar areas, shall be focused, directed, and arranged to prevent glare or direct

14 illumination on streets or adjoining property. Sports lighting, consisting of exterior

15 nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be

16 permitted.

17 (3) Except as provided above, all other zoning requirements shall be the same as those

18 requirements identified in Article XII of Ordinance No. 348.

19 j. Planning Area E-6

Mixed Use

20 (1) The uses permitted in Planning Area E-6 of Specific Plan 303 shall be the same as

21 those uses permitted in Article IX, Section 9.1 of Ordinance No. 348, except that the uses permitted

22 pursuant to Article IX, Sections 9.1.a. (17), (23), (25), (27), (29), (32), (42), (51), (52), (61), (65),

23 (67), (73), (83), and (93); allowed under Sections 9.1.b. (7), (9), (10), (11.c), (13), (14), (15), (16),

24 (18), (19), and (20); and allowed under Sections 9.1.d. (2), (4), (5), (9), (10), (12), (13), and (16)

25 shall not be permitted.

26 In addition, the uses identified under Section 9.1.b as allowable with a plot plan shall

27 include aerial services including advertising, photography and tours; aerospace/aeronautical

28 museums; aircraft equipment sales, service, and repair; aircraft taxiways; airports; aviation

1 equipment assembly; body and fender shops; building materials and sales yard; building movers
2 storage yard; catering services/flight kitchens; cold storage plant; communications and microwave
3 installations; computer and office equipment sales, service, repair and assembly; conference
4 facilities; contractor storage yards; country clubs; dry cleaners; emergency and urgent care medical
5 facilities; facilities related to large scale recreational uses such as a motor sports race track and
6 facilities related thereto, including but not limited to race track, private garages, single family
7 residential including duplex units defined as a structure with two dwelling units placed beside one
8 another sharing a common wall, clubhouse, tuning shop, observation tower, museum, vehicle
9 display areas, underground fuel storage and ancillary uses in support thereof; flight schools; health
10 and exercise centers; intermodal cargo transfer terminals; libraries; industrial and manufacturing
11 uses involving food products including beverages- including alcoholic beverages, canning and
12 preserving of fruits and vegetables, dairy products-not including dairies, grain and bakery
13 products, ice, meat and poultry products-including meat packing but not slaughtering, sugar and
14 confectionary products, and wineries, distilleries, and breweries; textile products including cotton,
15 wool, and synthetic weaving and finishing mills, wearing apparel and accessory products, knitting
16 mills, floor covering mills, and yard and thread mills; lumber and wood products including saw and
17 planing mills, manufacture of containers and creates, fabrication of wood building structures,
18 lumber yards, manufacture of furniture and fixtures including cabinets, partitions, and similar
19 items; paper products including paper and paperboard mills, manufacture of containers and boxes,
20 paper shredding, printing and publishing of newspaper, periodicals, books, forms cards and similar
21 items, binding of books and other publications; chemicals and related products including
22 manufacture of organic and inorganic compounds-not including those of a hazardous nature,
23 manufacture of drugs and pharmaceuticals, soaps, cleaners, and toiletries, manufacture of
24 agricultural chemicals-not including pesticides and fertilizers, paints and varnishes; rubber and
25 plastic and synthetic products including manufacture of tires and tubes, fabrication of rubber,
26 plastics, and synthetic products; leather products including tanning and finishing of leather,
27 manufacture of handbags, luggage, footwear, and other personal leather goods; stone clay, glass,
28 and concrete products including stone cutting and related activities, pottery and similar items, glass

blowing, pressing and cutting, glassware products, manufacture of concrete, gypsum, plaster and mineral products; metal products including manufacture of cans and containers, cutlery, tableware, hand tools and hardware, plumbing and heating items, wrought iron fabrication, manufacture and assembly of fencing, machine, welding, and blacksmith shops, metal stamps and forged metal products, fabrication of metal buildings, manufacture of ordnance and firearms, not including explosives, jewelry; primary metal industries including foundries, rolling and drawing metals, casting metals, blast furnaces, smelting of metals; machinery including engines, turbines, and parts, farm, garden construction, industrial machinery, office and computing machines, manufacture and repair of refrigeration and heating equipment, equipment sales, rental, and storage; electrical equipment including electrical and electronic apparatus and components, appliances, lighting and wiring, radio, television and communications equipment, musical and recording equipment, musical and recording equipment; transportation and related industries including vehicles, aircraft, boats and parts manufacture, railroad equipment, motorcycles, bicycles, and parts, travel trailers and recreational vehicles manufacture, draying, freighting, and trucking operations, railroad yards and stations, vehicle storage and impoundment, trailer and boat storage; engineering of scientific instruments including manufacture of wearing apparel and accessories; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; mini warehouses; nurseries and garden supply; outdoor film studio; paper recycling facilities; parcel delivery services; parking lots and parking structures; public parks and public playgrounds; public utility substations and storage yards; recycling of wood, metal and construction wastes; repair of jewelry; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; research and development facilities for precision components and products; sand blasting; trailer and truck sales and rentals; vehicle and motorcycle repair; water wells and appurtenant facilities; and warehousing and distribution.

1 In addition, the uses conditionally permitted identified under Section 9.1.d shall include
2 hospitals, abattoirs; above ground natural gas storage less than 6,000 gallons; concrete batch plants;
3 cotton ginning, disposal service operations; electric vehicle charging stations; fertilizer production,
4 and processing organic and inorganic; gas, steam and oil drilling operations; processing and
5 rendering of fats and oils; recycling processing facilities; and sewerage treatment plants.

6 (2) The development standards for Planning Area E-6 of Specific Plan No. 303 shall
7 be the same as those standards identified in Article XII, Section 12.4 except those development
8 standards set forth in Article XII, a, b, c(2), and k shall be deleted and replaced by the following:

9 a. If residential uses are located contiguous to nonresidential uses, then the following
10 standards shall apply:

11 1. Lot Size. Minimum lot area shall be seven thousand (7,000) square feet with
12 no minimum average width.

13 b. Standard Setbacks.

14 1. Where the front, side, or rear yard adjoins a street, the minimum setback shall
15 be 25 feet from any public street.

16 2. No minimum setback is required from any private street.

17 3. Front Yard: No minimum.

18 4. Rear Yard: No minimum.

19 5. Side Yard: No minimum.

20 c. Building Height. Buildings shall not exceed 50 feet unless a height up to 75 feet is
21 approved pursuant to Article XVIII, Section 18.34. An observation tower built within
22 as part of large scale recreational use shall not exceed 70 feet in height.

23 d. Lighting. All lighting fixtures, including spot lights, electrical reflectors and other
24 means of illumination for signs, structures, landscaping, parking, loading, unloading
25 and similar areas, shall be focused, directed, and arranged to prevent glare or direct
26 illumination on streets or adjoining property. Sports lighting, consisting of exterior
27 nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be
28 permitted.

1
2 (3) Except as provided above, all other zoning requirements shall be the same as those
3 requirements identified in Article XII of Ordinance No. 348.

4 k. Planning Areas E-5, E-7 and E-8

Mixed Use

5 (1) The uses permitted in Planning Areas E-5, E-7 and E-8 of Specific Plan 303 shall
6 be the same as those uses permitted in Article IX, Section 9.1 of Ordinance No. 348, except that the
7 uses permitted pursuant except that the uses permitted pursuant to Article IX, Sections 9.1.a. (17),
8 (23), (25), (27), (29), (32), (35), (42), (51), (52), (61), (65), (67), (73), (83), (93), and (96); Sections
9 9.1.b. (7), (9), (10), (11.a.) (11.b), (11.c), (13), (14), (15), (16), (18), (19), and (20); and Sections
10 9.1.d. (2), (4), (5), (9), (10), (12), (13), and (16) shall not be permitted.

11 In addition, the uses identified under Section 9.1.b as allowable with a plot plan shall
12 include aerial services including advertising, photography and tours; aerospace/aeronautical
13 museums; aircraft equipment sales, service, and repair; aircraft taxiways; airports; aviation
14 equipment assembly; body and fender shops; building materials and sales yard; building movers
15 storage yard; catering services/flight kitchens; cold storage plant; communications and microwave
16 installations; computer and office equipment sales, service, repair and assembly; conference
17 facilities; contractor storage yards;; country clubs; dry cleaners; emergency and urgent care medical
18 facilities; facilities related to large scale recreational uses such as a motor sports race track and
19 facilities related thereto, including but not limited to race track, private garages, single family
20 residential including duplex units defined as a structure with two dwelling units placed beside one
21 another sharing a common wall, clubhouse, tuning shop, observation tower, museum, vehicle
22 display areas, underground fuel storage and ancillary uses in support thereof; flight schools;
23 hardware and home improvement centers; health and exercise centers; heliports;; intermodal cargo
24 transfer terminals; libraries; industrial and manufacturing uses involving food products including
25 beverages, including alcoholic beverages, canning and preserving fruits and vegetables, dairy
26 products-not including dairies, grain and bakery products, ice, meat and poultry products-including
27 meat packing but not slaughtering, sugar and confectionary products, and wineries, distilleries, and
28 breweries; textile products including cotton, wool, and synthetic weaving and finishing mills,

1 wearing apparel and accessory products, knitting mills, floor covering mills, and yard and thread
2 mills; lumber and wood products including saw and planing mills, manufacture of containers and
3 creates, fabrication of wood building structures, lumber yards, manufacture of furniture and
4 fixtures including cabinets, partitions, and similar items; paper products including paper and
5 paperboard mills, manufacture of containers and boxes, paper shredding, printing and publishing of
6 newspaper, periodicals, books, forms cards and similar items, binding of books and other
7 publications; chemicals and related products including manufacture of organic and inorganic
8 compounds-not including those of a hazardous nature, manufacture of drugs and pharmaceuticals,
9 soaps, cleaners, and toiletries, manufacture of agricultural chemicals-not including pesticides and
10 fertilizers, paints and varnishes; rubber and plastic and synthetic products including manufacture of
11 tires and tubes, fabrication of rubber, plastics, and synthetic products; leather products including
12 tanning and finishing of leather, manufacture of handbags, luggage, footwear, and other personal
13 leather goods; stone clay, glass, and concrete products including stone cutting and related activities,
14 pottery and similar items, glass blowing, pressing and cutting, glassware products, manufacture of
15 concrete, gypsum, plaster and mineral products; metal products including manufacture of cans and
16 containers, cutlery, tableware, hand tools and hardware, plumbing and heating items, wrought iron
17 fabrication, manufacture and assembly of fencing, machine, welding, and blacksmith shops, metal
18 stamps and forged metal products, fabrication of metal buildings, manufacture of ordnance and
19 firearms, not including explosives, jewelry; primary metal industries including foundries, rolling
20 and drawing metals, casting metals, blast furnaces, smelting of metals; machinery including
21 engines, turbines, and parts, farm, garden construction, industrial machinery, office and computing
22 machines, manufacture and repair of refrigeration and heating equipment, equipment sales, rental,
23 and storage; electrical equipment including electrical and electronic apparatus and components,
24 appliances, lighting and wiring, radio, television and communications equipment, musical and
25 recording equipment, musical and recording equipment; transportation and related industries
26 including vehicles, aircraft, boats and parts manufacture, railroad equipment, motorcycles,
27 bicycles, and parts, travel trailers and recreational vehicles manufacture, draying, freighting, and
28 trucking operations, railroad yards and stations, vehicle storage and impoundment, trailer and boat

1 storage; engineering of scientific instruments including manufacture and repair of measuring
2 devices, watches, clocks and related items; manufacture and repair of optical goods, medical
3 instruments, supplies and equipment, engineering, survey and drafting instruments and
4 photography equipment; manufacture, assembly, testing and repair of components, devices,
5 equipment and systems of an electrical, electronic, or electro-mechanical nature; manufacture of
6 wearing apparel and accessories; mini warehouses; nurseries and garden supply; outdoor film
7 studio; paper recycling facilities; parcel delivery services; parking lots and parking structures;
8 public parks and public playgrounds; public utility substations and storage yards; recycling of
9 wood, metal and construction wastes;; repair of jewelry; research and development facilities for
10 biomedical, chemical, electronic, mechanical and other scientific purposes; research and
11 development facilities for precision components and products; sand blasting; trailer and truck sales
12 and rentals; vehicle and motorcycle repair; water wells and appurtenant facilities; and warehousing
13 and distribution.

14 In addition, the uses conditionally permitted identified under Section 9.1.d shall include
15 hospitals, abattoirs; above ground natural gas storage less than 6,000 gallons; concrete batch plants;
16 cotton ginning, disposal service operations; electric vehicle charging stations; fertilizer production,
17 and processing organic and inorganic; gas, steam and oil drilling operations; processing and
18 rendering of fats and oils; recycling processing facilities; and sewerage treatment plants.

19 (2) The development standards for Planning Areas E-5, E-7 and E-8 of Specific Plan
20 No. 303 shall be the same as those standards identified in Article XII, Section 12.4 , except those
21 development standards set forth in Article XII, a, b, c(2) and k shall be deleted and replaced by the
22 following:

23 a. If residential uses are located contiguous to nonresidential uses, then the following
24 standards shall apply:

25 1. Lot Size. Minimum lot area shall be seven thousand (7,000) square feet with
26 no minimum average width.

27 b. Standard Setbacks.
28

1 1. Where the front, side, or rear yard adjoins a street, the minimum setback shall
2 be 25 feet from any public street.

3 2. No minimum setback is required from any private street.

4 3. Front Yard: No minimum.

5 4. Rear Yard: No minimum.

6 5. Side Yard: No minimum.

7 c. Building Height. Buildings shall not exceed 50 feet unless a height up to 75 feet is
8 approved pursuant to Article XVIII, Section 18.34. An observation tower built within
9 as part of large scale recreational use shall not exceed 70 feet in height.

10 d. Lighting. All lighting fixtures, including spot lights, electrical reflectors and other
11 means of illumination for signs, structures, landscaping, parking, loading, unloading
12 and similar areas, shall be focused, directed, and arranged to prevent glare or direct
13 illumination on streets or adjoining property. Sports lighting, consisting of exterior
14 nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be
15 permitted.

16 (3) Except as provided above, all other zoning requirements shall be the same as those
17 requirements identified in Article XII of Ordinance No. 348.

18 1. Planning Area M-4.

Public Facilities

19 (1) The uses permitted in Planning Area M-4 of Specific Plan No. 303 shall be the
20 same as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348. In addition, the
21 permitted uses identified under Section 6.1.a. shall include government offices, courthouses, police
22 stations, fire stations, libraries, museums, and public schools.

23 (2) The development standards for Planning Area M-4 of Specific Plan No. 303 shall
24 be the same as those standards identified in Article VI, Section 6.2 of Ordinance No. 348.

25 (3) Except as provided above, all other zoning requirements shall be the same as those
26 requirements identified in Article XI of Ordinance No. 348.

27 Section 3. This ordinance shall take effect 30 days after its adoption.
28

BOARD OF SUPERVISORS OF THE COUNTY
OF RIVERSIDE, STATE OF CALIFORNIA

By: Marion Ashley
Chairman **MARION ASHLEY**

ATTEST:

KECIA HARPER-IHEM
Clerk of the Board

By: Kecia Harper-Ihem
Deputy

APPROVED AS TO FORM:
3/4/2015

By: Melissa R. Cushman
MELISSA R. CUSHMAN
Deputy County Counsel

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STATE OF CALIFORNIA)
)
COUNTY OF RIVERSIDE) SS

I HEREBY CERTIFY that at a regular meeting of the Board of Supervisors of said county held on March 24, 2015, the foregoing ordinance consisting of 3 Sections was adopted by the following vote:

AYES: Jeffries, Tavaglione, Washington, Benoit and Ashley
NAYS: None
ABSENT: None

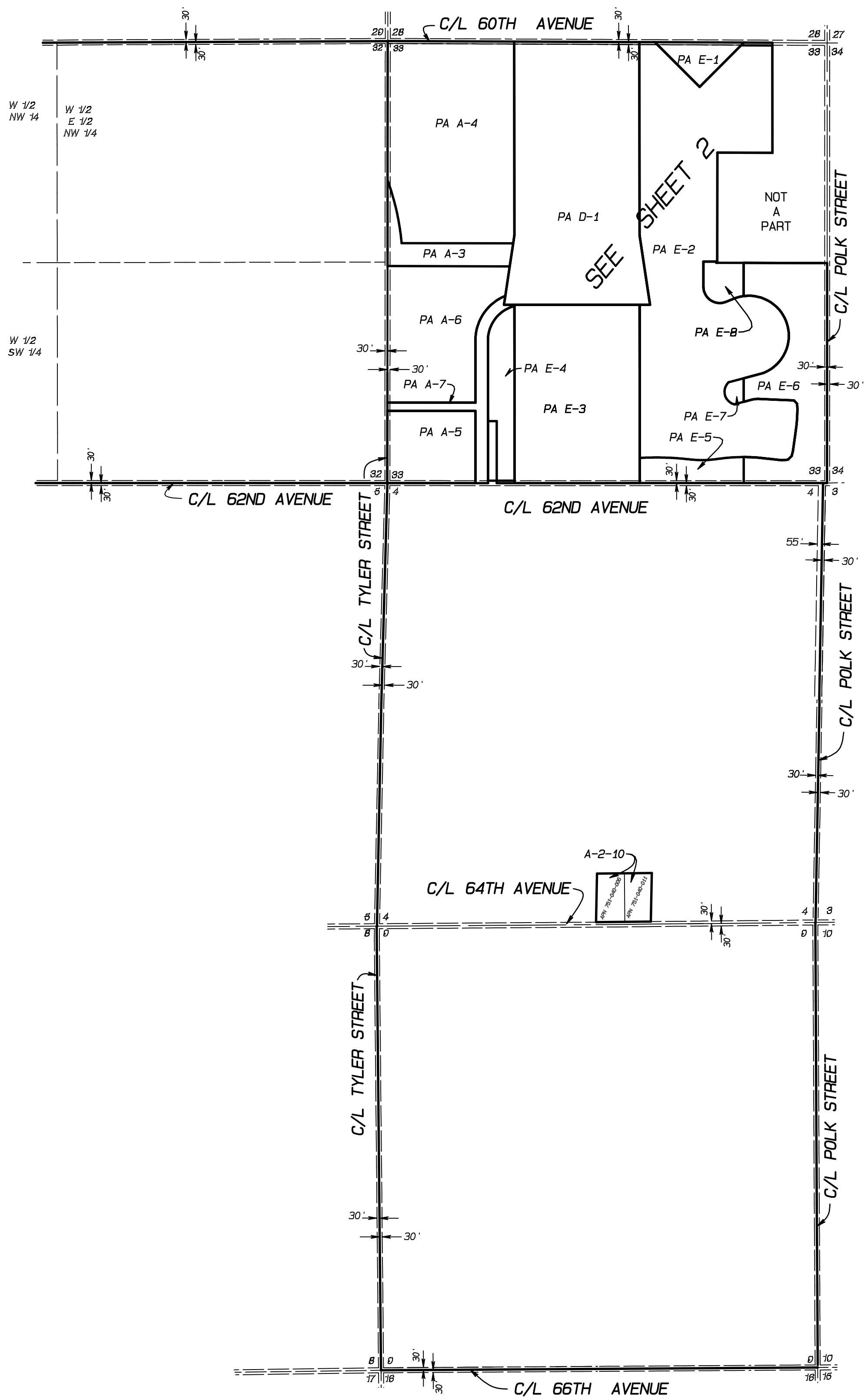
DATE: March 24, 2015

KECIA HARPER-IHEM
Clerk of the Board

BY: 
Deputy

SEAL

SEC. 33, T.6S., R.8E, S.B.B. & M.
SEC. 9, T.7S., R.8E., S.B.B. & M.



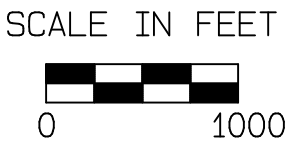
- SP ZONE
- A-2-10

SPECIFIC PLAN (S.P. 303)
HEAVY AGRICULTURE 10 ACRE MINIMUM

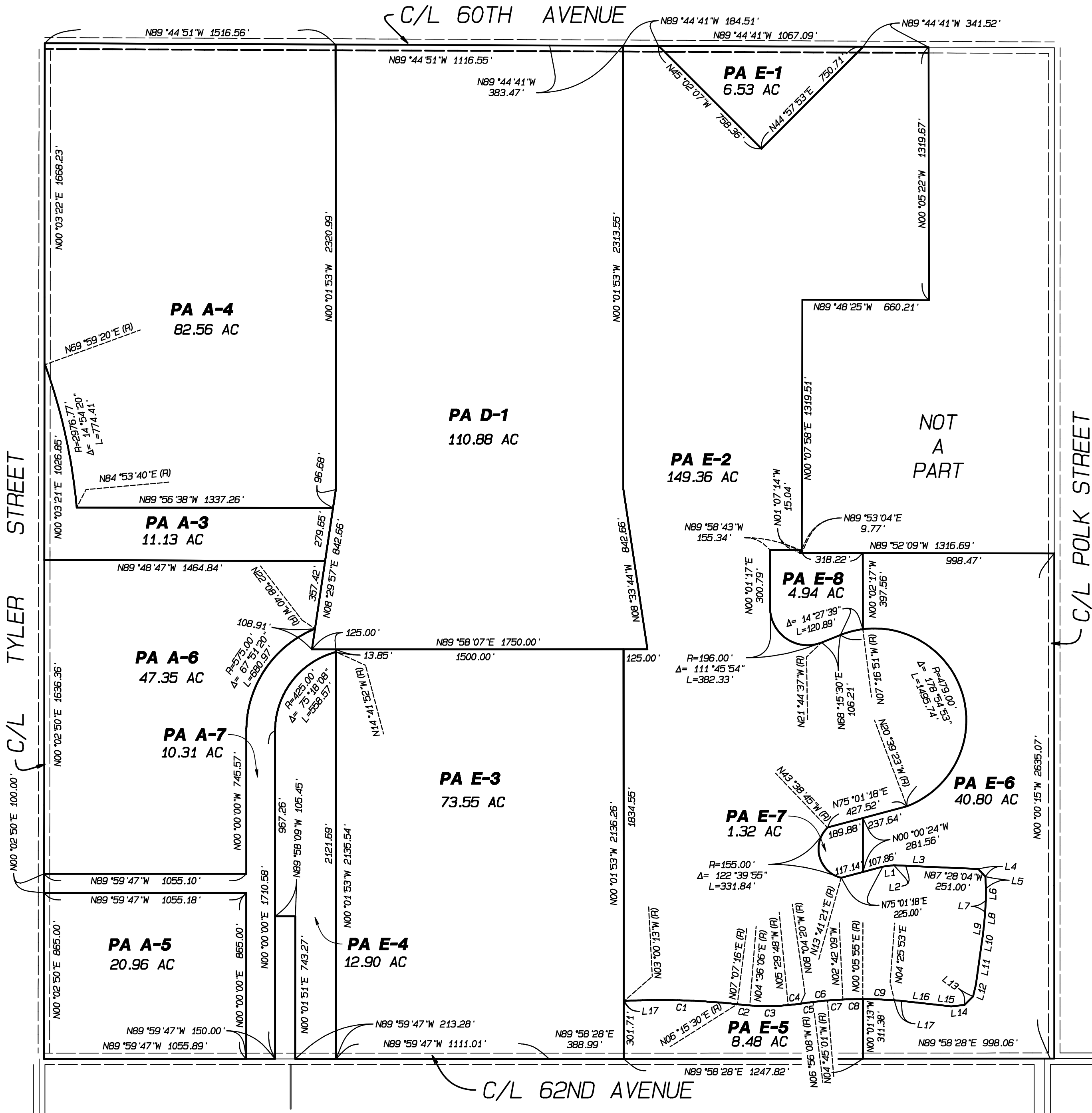
MAP NO. 41.090

CHANGE OF OFFICIAL ZONING PLAN
LOWER COACHELLA VALLEY DISTRICT

CHANGE OF ZONE CASE NO. 07852
AMENDING ORDINANCE NO. 348
ADOPTED BY ORDINANCE NO. 348.4801
ADOPTION DATE MARCH 24, 2015
RIVERSIDE COUNTY BOARD OF SUPERVISORS



SEC. 33, T.6S., R.8E, S.B.B. & M.
SEC. 9, T.7S., R.8E., S.B.B. & M.



LINE	BEARING	DISTANCE
L1	N82°37'30"E	57.86'
L2	N87°28'04"W	62.00'
L3	N87°28'04"W	127.00'
L4	N43°20'08"W	54.71'
L5	N03°10'55"W	62.00'
L6	N00°49'08"E	61.99'
L7	N03°11'44"E	62.00'
L8	N05°04'47"E	65.00'
L9	N06°57'50"E	62.00'
L10	N07°56'05"E	65.00'
L11	N07°56'08"E	189.00'
L12	N13°10'29"E	62.85'
L13	N45°34'03"E	63.12'
L14	N83°55'38"E	70.37'
L15	N83°44'26"W	65.39'
L16	N83°47'16"W	196.15'
L17	N85°04'53"W	36.65'

CURVE	RADIUS	DELTA	LENGTH
C1	3655.29'	9°15'43"	590.88'
C2	1201.61'	3°06'52"	65.32'
C3	1136.27'	10°05'54"	200.27'
C4	1244.00'	3°00'50"	65.44'
C5	2499.91'	1°30'12"	65.59'
C6	1978.09'	2°11'00"	75.38'
C7	1569.74'	2°55'02"	79.92'
C8	2246.22'	2°40'58"	105.17'
C9	2206.42'	4°14'59"	163.66'



SCALE IN FEET



SP ZONE

SPECIFIC PLAN (S.P. 303)

MAP NO. 41.090

CHANGE OF OFFICIAL ZONING PLAN
LOWER COACHELLA VALLEY DISTRICT

CHANGE OF ZONE CASE NO. 07852
AMENDING ORDINANCE NO. 348
ADOPTED BY ORDINANCE NO. 348.4801
ADOPTION DATE MARCH 24, 2015
RIVERSIDE COUNTY BOARD OF SUPERVISORS

3. SPECIFIC PLAN

The Eastern Coachella Valley has experienced significant population growth and residential development in recent years, as evidenced by the addition of 78,019 residents in the cities of Coachella, Indio, La Quinta, and Palm Desert between 2000 and 2008 (a 57 percent increase)¹. In addition, the level of planning activity in the project area (e.g., Airport Master Plan) reflects the strategic location of the eastern Coachella Valley and the Kohl Ranch site with respect to the Los Angeles, Orange County, and San Diego metropolitan areas, the Inland Empire and Mexico. Access to the regional transportation network affords the site maximum potential to reach these extensive markets. The land use designations associated with the Kohl Ranch Specific Plan are intended to provide a balanced mix of land uses throughout the project site and within individual neighborhoods and are distributed throughout the site so as to support ongoing planning efforts in the project vicinity, and to further the goals of the Coachella Valley Enterprise Zone.

The diverse business and employment opportunities, living environments, recreational and visual amenities, and roadway and other infrastructure improvements that would be planned and permitted under the Specific Plan would enhance the attractiveness of the Jacqueline Cochran Regional Airport vicinity, and would reinforce the desirability of the area for potential future development opportunities within and surrounding the airport. The proposed project would also facilitate development of the site by a single industrial user or multiple users interested in the Coachella Valley as the location for a major jobs-generating enterprise. The Specific Plan would be compatible with the policies in the Comprehensive General Plan which recognize the growth potential of the project area as a result of the airport expansion and which recommend that industrial uses be among those to locate in this area. The Specific Plan designation also would provide a mechanism for ensuring that major ongoing planning efforts for the area are properly coordinated and mutually supportive.

¹ State of California, Department of Finance, *E-4 Population Estimates for Cities, Counties and the State, 2001–2009, with 2000 Benchmark*. Sacramento, California, May 2009. (Accessed on 7/17/10 at <http://www.dof.ca.gov/research/demographic/reports/estimates/e-4/2001-09/>)

3.1 PROJECT-WIDE PLANNING STANDARDS

The following section of the Specific Plan details the land uses and development standards for the Kohl Ranch community as a whole. Illustrations are provided to portray the various planning areas, infrastructure plans, and design treatments such as buffers, landscaping and streetscapes. The visual images and themes addressed in this section set the overall tone for the project.

Planning objectives for the Kohl Ranch Specific Plan include:

- Increased market potential and attractiveness of the Jacqueline Cochran Regional Airport and vicinity;
- Support for planning, economic development and development efforts, in accordance with the goals of the Master Plan for the expansion of Jacqueline Cochran Regional Airport;
- Flexibility to respond to changing market conditions, through designation of golf course as an alternate land use;
- A balanced, living and working environment that provides a mix of land uses including a variety of housing products and employment opportunities; and
- Cohesive, balanced neighborhoods, relating to overall project phasing, which can be developed separately, or together.

To the extent there is any inconsistency between the Specific Plan and the Zoning Ordinance as to any uses or development standards, the Zoning Ordinance controls.

3.1.1 Comprehensive Land Use Plan

The Kohl Ranch Specific Plan consists of a balanced array of land uses including residential, mixed use, commercial, industrial, open space/recreation, and public facilities. Both living and working opportunities will be available within the thirteen project neighborhoods, with the specific mix of uses dependent upon the neighborhood location, relationship to adjacent uses such as Jacqueline Cochran Regional Airport, and access to the regional transportation network. In general, the neighborhoods in the northern portion of the site have the highest concentration of employment uses, which can best maximize the advantages of proximity to the airport, and which are least likely to be affected by airport-related impacts such as noise. The land uses within these neighborhoods are designed to locate employment opportunities in close proximity to residential areas, thereby reducing the number of vehicle trips required, and to create centers of local activity that prosper from the mix of commercial, industrial, recreation, and business uses. These northernmost neighborhoods provide jobs and services to areas both on and off site.

The residential portion of the land use plan includes a target of 7,160 dwelling units but with a maximum not to exceed 7,171 dwelling units. These units are to be distributed among mixed use and four different density classifications on approximately 1,397 acres with an additional 380.14

acres of open space. The mixed Use, commercial, and industrial land use categories will comprise approximately 277, 28, and 81 acres, respectively. Commercial areas will serve the Kohl Ranch project as well as neighboring communities. Mixed Use and industrial uses will be oriented toward the Jacqueline Cochran Regional Airport as well as larger regional markets and are intended to provide employment opportunities to project area residents.

A target maximum number of dwelling units is specified for each planning area where permissible. The target is based on the applicable density range and may or may not represent the maximum number of units allowed within the density range for any single planning area. Therefore, any given implementing subdivision may increase the number of units beyond the target density up to the maximum allowed for its planning area without a specific plan amendment, provided that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171 dwelling units. Further, the number of dwelling units contained in an implementing subdivision application may exceed the maximum number of allowable units specified in the Specific Plan for any single planning area, by not more than ten percent (10%) without an amendment to this plan, provided that the maximum number of dwelling units in other planning areas are reduced by an equivalent number of units such that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171 dwelling units. See Section 3.1.11, Specific Plan Administration, for the approach to tracking units during the build out of the project. Density transfers will be required to receive administrative approval. The land uses proposed for the Kohl Ranch Specific Plan are identified in **Table 3.1-A, Land Use Plan Statistical Summary**, **Table 3.1-B, Land Use Summary by Planning Area**, and depicted in **3.1-1, Land Use Plan**.

Table 3.1-A, Land Use Plan Statistical Summary

Land Use	Acreage	Percent of Total	Percent of Developable Area ¹	Target Dwelling Units
RESIDENTIAL				
Medium Density Residential (MDR)	490.51	22.7	32.1	1,650
Medium High Density Residential (MHDR)	467.76	21.6	30.6	3,245
High Density Residential (HDR)	154.61	7.1	10.1	1,629
Very High Density Residential (VHDR)	30.85	1.4	2.0	477
Total Residential	1,143.73	52.9	74.7	7,001
INDUSTRIAL				
Heavy Industrial (HI)	81.17	3.8	5.3	
Total Industrial	81.17	3.8	5.3	
BUSINESS				
Mixed Use/Air Park (MU/AP)	24.45	1.1	1.6	
Mixed Use	252.73	11.7	16.6	159 ³
Total Business	277.18	12.8	18.2	159³
COMMERCIAL				
Commercial Retail (CR)	28.27	1.3	1.9	
Total Commercial	28.27	1.3	1.9	
OTHER				
Open Space (OS)	380.14	17.6		
Public Facilities/Schools (PF)	84.30	3.9		
Right-of-Way (ROW)	167.86	7.8		
Total Other	632.30	29.7		
TOTAL	2,162.65	100%	100%	7,160

1. Assumes total of 1,530.35 acres of developable land. This does not include land uses in "other" category.
2. The ROW acreage includes local streets, which are not depicted on the Land Use Plan. See **Figure 3.1-2, Circulation Plan**.
3. This number represents a maximum, rather than a target.

Table 3.1-B, Land Use Summary By Planning Area

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Mixed Use (MU)		46.18	
A-7	Open Space (OS)		9.97	
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0-8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0
C-1	Medium Density Residential (MDR)	122	40.77	2.0-5.0
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0
C-5	Open Space (OS)		9.97	
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0
C-8	Medium High Density Residential (MHDR)	105	14.96	5.0-8.0
D-1	Open Space (OS)		110.66	
E-1	Open Space (OS)		5.61	

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
E-2	Mixed Use (MU)		127.34	
E-2A ²	Mixed Use (MU)		16.48	
E-2B ²	Mixed Use (MU)		4.92	
E-3	Open Space (OS)		69.33	
E-4	Mixed Use (MU)		12.25	
E-5	Mixed Use (MU)	19 ¹	5.31	
E-6	Mixed Use (MU)	120 ¹	33.99	
E-7	Mixed Use (MU)	5 ¹	1.32	
E-8	Mixed Use (MU)	15 ¹	4.94	
F-2	Medium Density Residential (MDR)	143	71.64	2.0-5.0
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0
F-4	Commercial Retail (CR)		6.23	
G-1	Commercial Retail (CR)		4.43	
G-2	Open Space (OS)		1.44	
G-3	Open Space (OS)		1.19	
G-4	Open Space (OS)		5.6	
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0
G-6	Open Space (OS)		13.65	
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0
G-9	Open Space (OS)		6.17	

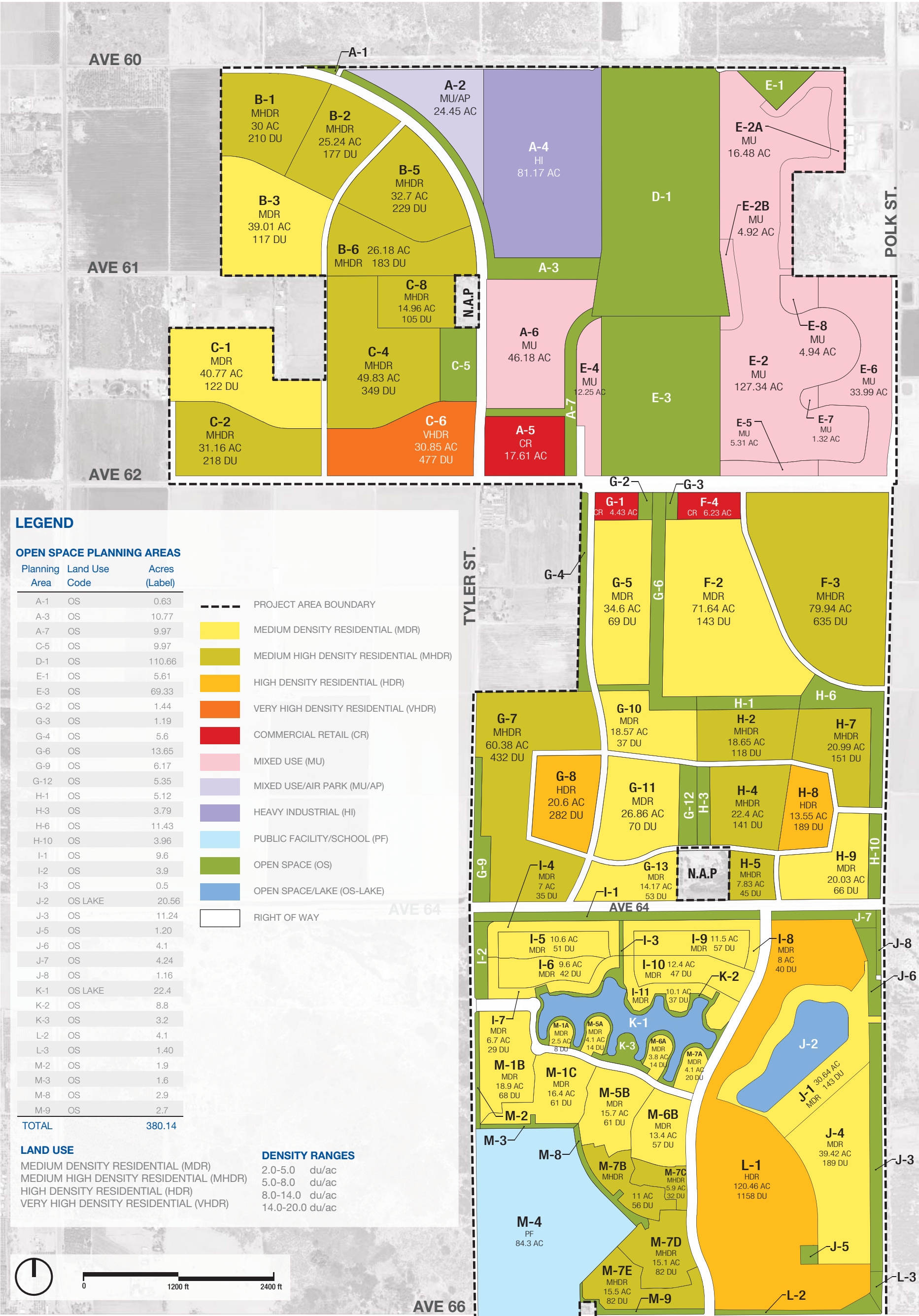
Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0
G-12	Open Space (OS)		5.35	
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0
H-1	Open Space (OS)		5.12	
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0
H-3	Open Space (OS)		3.79	
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0
H-6	Open Space (OS)		11.43	
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0-8.0
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0
H-10	Open Space (OS)		3.96	
I-1	Open Space (OS)		9.6	
I-2	Open Space (OS)		3.9	
I-3	Open Space (OS)		0.5	
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0
I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0
J-1	Medium Density Residential (MDR)	143	30.64	2.0-5.0
J-2	Open Space/Lake (OS-LAKE)		20.56	
J-3	Open Space (OS)		11.24	
J-4	Medium Density Residential (MDR)	189	39.42	2.0-5.0
J-5	Open Space (OS)		1.20	
J-6	Open Space (OS)		4.1	
J-7	Open Space (OS)		4.24	
J-8	Open Space (OS)		1.16	
K-1	Open Space/Lake (OS-LAKE)		22.4	
K-2	Open Space (OS)		8.8	
K-3	Open Space (OS)		3.2	
L-1	High Density Residential (HDR)	1,158	120.46	8.0-14.0
L-2	Open Space (OS)		4.1	
L-3	Open Space (OS)		1.40	
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
M-2	Open Space (OS)		1.9	
M-3	Open Space (OS)		1.6	
M-4	Public Facility (PF)		84.3	
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0-8.0
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0-8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
Right-of-Way			167.86	
TOTALS		<u>7.160</u>	<u>2.162.65</u>	

Note: Planning Area's B-4, B-7, C-3, C-7, and F-1 have been intentionally left out.

1. This number represents a maximum rather than a target.
2. Planning Area E-2A allows for 58 racetrack recreational units and Planning Area E-2B allows for 16 racetrack recreational units which are not considered dwelling units as they are a non-residential use identified as a member villa accessible 24 hours a day, seven days a week. The Racetrack Recreational Unit provides a private space for a Thermal Club members to store personal belongings, vehicles, and equipment utilized for racing. The unit can further be utilized for vehicular repairs, other racing-related recreational activities and is located directly adjacent to the racetrack with an open view deck facing the track allowing for viewing of racing activities. The unit would be restricted from permanent residency, and has limitations imposed by the club for duration of stays.



Residential

Residential areas within the project will provide a target of 7,160 dwelling units but with a maximum not to exceed 7,171 dwelling units, with an overall gross density of 4.46 dwelling units per gross residential acre¹. The residential element of the plan comprises 1,143.73 acres or 52.9 percent of the total project site. Four residential land uses are proposed, Medium Density Residential, Medium High Density Residential, High Density Residential and Very High Density Residential, establishing appropriate transitions to adjacent non-residential land uses, both on and off site. It should be noted that residential planning areas can be developed to a lower density than that specified by the planning area land use designation, without requiring an amendment to the Specific Plan. Additionally, the maximum density and the total number of dwelling units within the high and very density residential designations can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Residential uses north of Avenue 62 and west of Tyler Street are intended to house employees of the nearby commercial, business and industrial uses.

Each of the residential land use designations (i.e., Medium Density Residential, Medium High Density Residential, High Density Residential and Very High Density Residential) can be developed with several product types, including those which permit clustering of residential density within planning areas. Residential product types include: Detached Dwelling Units, Cluster Detached Dwelling Units, and Cluster Attached Dwelling Units.

The residential designations each have a unique set of development standards designed to accommodate a variety of housing product types. These different residential designations are intended to provide flexibility to develop in areas that are affected by the open space requirements and noise impacts of the Jacqueline Cochran Regional Airport safety zones and the limitations imposed by the utility easement beltway in the central portion of the site. By allowing for clustering of development within planning areas, it is possible to retain residential densities while providing adequate open space for drainage ways, recreational and other lakes and golf courses, the power line easement, and the airport zones.

Under the Specific Plan, it is possible to develop one or more of the product types within an individual planning area or neighborhood, provided that the development is consistent with the project's Design Guidelines.

¹ The overall gross density for residential areas was determined by dividing the total number of dwelling units by the acreage devoted to residential land uses, open space, public facilities, and local residential streets. The gross density—if determined by dividing the total dwelling units by the residential acreage—is 6.27. This density is reduced to 3.32 if the entire site acreage is used.

Medium Density Residential (2.0–5.0 du/acre)

Medium Density Residential units are proposed throughout the project site. A target of 1,650 dwelling units on 490.51 acres is planned. This is based on a density range of 2.0–5.0 du/ac. The purpose of this designation is to provide a medium density residential alternative in the plan. The intent is to: 1) locate lower density land uses at the project periphery adjacent to off-site lands currently in low intensity uses such as agriculture; 2) provide a transition between open space/recreational areas and medium density land uses; 3) provide for limited residential uses where permitted within the Extended Runway Centerline (ERC) airport safety zone; and 4) create a housing opportunity offering greater private open space amenity than is available in the higher density residential zones.

In addition to One-Family Dwellings development, additional uses within the Medium Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; water wells and appurtenant facilities; day care centers; libraries; religious institutions; community centers; and schools. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Medium High Density Residential (5.0–8.0 du/acre)

Medium High Density Residential units are proposed throughout the project site. A target of 3,245 dwelling units on 467.76 acres is planned. This is based on a density of range of 5.0–8.0 du/acre. The purpose of this designation is to provide a lower density creative residential alternative in the plan. The intent is to: 1) create the potential for single-family, first home buyers; 2) offer a desirable housing product for retirees and second home buyers; and 3) accommodate residents seeking more private open space amenities than are available in the High and Very High Density Residential areas.

In addition to Multiple Family Dwellings development, additional uses within the Medium High Density Residential land use designation may include such uses as dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; water wells and appurtenant facilities; day care centers; libraries; religious institutions; community centers; and schools. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

High Density Residential (8.0–14.0 du/acre)

High Density Residential land uses are located in close proximity to community amenities and trails systems for the project. A target of approximately 1,629 dwelling units on 154.61 acres is planned, assuming a density range of 8.0–14.0 du/acre. The purpose of this designation is to enhance the range of housing types provided within the project area. The intent is to: 1) achieve an affordable living environment for those working in the project vicinity; 2) provide a more compact housing product within close proximity to commercial services and employment opportunities; 3) create pedestrian-oriented communities with common open space and recreation facilities; and 4) orient residential areas around community focal points such as churches and community centers. The maximum density and the target number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

In addition to Multiple Family Dwellings development, additional uses within the High Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; and water wells and appurtenant facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Very High Density Residential (14.0–20.0 du/acre)

Very High Density Residential land uses are located in close proximity to community commercial cores established for the project. A target of approximately 477 dwelling units on 30.85 acres is planned, assuming a density range of 14.0–20.0 du/acre. The purpose of this designation is to enhance the range of housing types provided within the project area. The intent is to: 1) achieve an affordable living environment for those working in the project vicinity; 2) provide a more compact housing product within close proximity to commercial services and employment opportunities; 3) create pedestrian-oriented communities with common open space and recreation facilities; and 4) orient residential areas around community focal points such as churches and community centers. The maximum density and the target number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

In addition to Multiple Family Dwellings development, additional uses within the Very High Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; and water wells and appurtenant facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Industrial

One industrial land use category is proposed offering a range of employment opportunities that respond to market conditions and the skills base of the local labor force. A total of approximately 81.17 acres of heavy industrial uses are concentrated in the northern portion of the site, and in close proximity to the Jacqueline Cochran Regional Airport and Polk Street, a designated arterial highway providing access to the airport and points east.

The purpose of this designation is to allow for more intense, industrial/manufacturing uses or other uses compatible with the regional airport. The intent is to: 1) encourage land uses which provide relatively greater levels of employment than is typically associated with industrial uses such as warehousing and distribution; 2) allow for uses that require outdoor storage; and 3) maximize use of the regional transportation system, including air, rail and the road network.

In addition to Manufacturing-Heavy development, additional uses within the Heavy Industrial land use designation may include such uses as water wells and appurtenant facilities; aerial services including advertising, photography and tours; aerospace/aeronautical or automotive museums; aircraft taxiways; catering services/flight kitchens; conference facilities; golf courses and appurtenant facilities; convenience stores; dry cleaners; flight schools; hospitals; hotels and motels; intermodal cargo transfer terminals; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; paper recycling facilities; parcel delivery services; and rental car agencies including the storage of rental cars; motor sports race track and facilities necessary and incidental thereto including, a race track, private garages, fuel storage, buildings for vehicle display, tuning shop, and observation tower; cogeneration plants; structures and facilities necessary and incidental to the development, generation and transmission of electric power and gas such as power plants, booster or conversion plants, transmission lines, pipelines and the like; and incarceration and detention facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Business

Mixed Use/Air Park

The Mixed Use/Air Park designation is located in the northernmost portion of the project site, just south of the Jacqueline Cochran Regional Airport. Approximately 24.45 acres are devoted to this land use. The purpose of this designation is to accommodate a wide range of land uses including office, service, commercial, airport-related, incubator business, and very light industrial. The intent is to: 1) complement expansion plans for the Jacqueline Cochran Regional Airport by providing needed services and commercial uses located near the Tyler Street airport entrance; 2) create a supportive environment for start-up businesses; 3) devise standards that encourage combinations of uses such as office/sales/storage/assembly; and 4) provide support services for business and industrial uses in the project area.

In addition to General Commercial development, additional uses within the Mixed Use/Air Park land use designation may include such uses as aviation equipment assembly; communication equipment and microwave sales and installation; computer and office equipment sales, service, repair and assembly; conference facilities; country clubs, manufacture of dairy products, not including dairies; emergency and urgent care medical facilities; libraries; manufacture of grain and bakery products; health and exercise centers; hospitals; ice houses; jewelry manufacture and repair; manufacture of wearing apparel and accessories; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture of handbags, luggage, footwear, and other personal leather goods; manufacture of cutlery, tableware, hand tools and hardware; manufacture of plumbing and heating items; vehicle storage and impoundment; manufacture of office and computing machines; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; manufacture of non-alcoholic beverages; manufacture of confectionery products; manufacture and repair of refrigeration and heating equipment; printing of periodicals, books, forms, cards, and similar items; public parks and public playgrounds; golf courses; religious institutions; facilities for research and development of precision components and products; facilities related to alternative energy development; large-scale recreational uses such as a motor sports race track and facilities related thereto, including but not limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle display areas, and ancillary uses in support thereof; water wells and appurtenant facilities; aerial service businesses including advertising, photography and tours; aerospace/aeronautical museums; aircraft equipment sales, service and repair; contractor storage yards; flight schools; intermodal cargo transfer facilities; manufacture of furniture and fixtures, including cabinets, partitions, and similar small items; manufacture of bicycles; parcel delivery services; warehousing and distribution; community centers; schools; meat and poultry processing not including slaughtering or rendering of animals; paper shredding facilities; research and manufacture of drugs and pharmaceuticals; manufacture of soaps, cleaners and toiletries; wrought iron fabrication; machine, welding and blacksmith shops; breweries, distilleries and wineries; paper storage and recycling within a building; recycling processing facilities; paper and paperboard mills; manufacture of containers and boxes; and above ground natural gas storage. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater.

Section 2 – Specific Plan Zoning Ordinance an approved list of allowable uses.

Mixed Use

The Mixed Use designation is located in the northernmost portion of the project site, just south of the Jacqueline Cochran Regional Airport. Approximately 252.73 acres are devoted to this land use. The purpose of this designation is to accommodate a wide range of land uses including office, service, commercial, airport-related, incubator business, mixed-use residential units, very light industrial, and large scale recreational development. The intent is to: 1) complement expansion plans for the Jacqueline Cochran Regional Airport by providing needed services and commercial uses located near the Tyler Street airport entrance; 2) devise standards that encourage combinations of uses such as office/sales/storage/assembly; 3) provide support services for business and industrial uses in the project area; and 4) provide standards unique to development of large scale recreational use.

Additional uses within the Mixed Use land use designation, depending on the Planning Area, may include such uses as above ground natural gas storage less than 6,000 gallons; aerial service businesses including advertising, photography and tours; aircraft equipment sales, service and repair; contractor storage yards; aircraft taxiways; aviation equipment assembly; breweries, distilleries and wineries; catering services/flight kitchens; community centers; computer and office equipment sales, service, repair and assembly; conference facilities; convenience stores; country clubs; dry cleaners; emergency and urgent care medical facilities; facilities for research and development of precision components and products; flight schools; hospitals; hotels and motels; ice houses; intermodal cargo transfer facilities; jewelry manufacture and repair; facilities related to large scale recreational uses such as a motor sports race track and facilities related thereto, including but not limited to race track, private garages, single-family residential including duplex units defined as a structure with two dwelling units placed beside one another sharing a common wall, racetrack recreational units, clubhouse, tuning shop, observation tower, museum, vehicle display areas, underground fuel storage and ancillary uses in support thereof; libraries; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture and repair of refrigeration and heating equipment; manufacture of bicycles; manufacture of confectionery products; manufacture of dairy products, not including dairies; manufacture of furniture and fixtures, including cabinets, partitions, and similar small items; manufacture of grain and bakery products; manufacture of cutlery, tableware, hand tools and hardware; manufacture of non-alcoholic beverages; manufacture of office and computing machines; manufacture of plumbing and heating items; manufacture of wearing apparel and accessories; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; outdoor film studio; paper storage and recycling within a building; parcel delivery services; public parks and public playgrounds; religious institutions; rental car agencies including the storage of rental cars; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; research and manufacture of drugs and pharmaceuticals; schools; warehousing and distribution; water wells and appurtenant facilities. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses that may be permitted with an entitlement as specified therein.

Due to the unique nature of large scale recreational development north of Avenue 62, the Mixed Use designation allows for two or more different types of uses contiguous to one another, planned as a unit. For instance, with racetrack development north of Avenue 62, certain

planning areas could develop as residential product types adjacent to racetrack facilities. These planning areas will also allow for duplex units, or structures with two dwelling units placed beside one another sharing a common wall.

A maximum of 159 dwelling units on 46 acres is planned as part of these mixed-use development areas.

Commercial-Retail

A Commercial Retail land use designation, comprising a total of 28.27 acres, is proposed for the project. The purpose of this designation is to provide a range of commercial uses in support of broader employment and residential uses within the Plan. The intent is to: 1) provide limited neighborhood commercial uses to serve residential communities; 2) incorporate commercial uses within walking distance of residential neighborhoods and employment uses; 3) locate highway-oriented commercial uses at the project periphery or along major through roadways, with easy access to and visibility from the arterial highway system; 4) accommodate "big box" commercial uses on larger sites, to serve regional markets; and 5) provide appropriate circulation, parking and loading areas to handle traffic generated by commercial land uses.

In addition to general commercial development, additional uses within the Commercial Retail land use designation may include such uses as public parks and public playgrounds; golf courses; country clubs; animal hospitals with all kennels entirely indoors; health clubs; computer sales and repair stores; parcel delivery services; libraries; religious institutions; community centers; schools; and water wells and appurtenant facilities; and electric vehicle charging stations. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. The Specific Plan Zoning Ordinance provides the allowable uses and Development Standards.

Public Facilities/Schools

This land use designation totals 84.30 acres and consists of property owned by the Coachella Valley Unified School District as a site for multiple schools. SPA4 recognizes that these schools have been constructed and are operational.

In addition to One-Family Dwelling development, additional uses within the Public Facilities/Schools land use designation may include such uses as government offices, courthouses, police stations, fire stations, libraries, museums, public schools, water wells and appurtenant facilities; day care centers; religious institutions; community centers; private schools and communication facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Open Space

The purpose of this designation is to provide open space and recreational amenities to serve populations in the project neighborhoods. Approximately 380.14 acres of open space is provided. The intent is to: 1) allow for a variety of passive and active recreational experiences for neighborhood residents and visitors, ranging in scale from community parks to more intimate neighborhood parks; 2) incorporate walking and bicycle trails and linkages between residential neighborhoods and commercial/employment centers, where appropriate; 3) comply with land use restrictions for Jacqueline Cochran Regional Airport safety zones which require the

maintenance of open space clear of obstructions; 4) accommodate stormwater runoff originating both on and off site which must be controlled and conveyed through the property; and 5) provide a variety of landscape settings ranging from highly landscaped to native communities for visual interest as well as buffering between adjoining land uses.

The Specific Plan allows for large-scale recreational uses such as a motor sports race track in the northern portion of the site as a permitted land use. Any such large-scale recreation use, if developed, would be sited around the drainage corridors. The Specific Plan also allows clubhouses and their associated shopping and dining facilities. Additionally, uses such as lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water will be a permitted use in the southern portion of the site. The intent is to: 1) diversify the recreational opportunities available to area residents and visitors; 2) create an attractive amenity around the clubhouses that is enhanced through landscaping and design treatments; 3) develop a central meeting place (clubhouse) that offers uses that are incidental to the primary recreation use; and 4) stimulate job growth and creation of new employment opportunities.

Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Roads

Major road rights-of-way totaling approximately 167.86 acres within the project site will be implemented in conjunction with the project.

3.1.2 Project-Wide Development Standards

To ensure a logical, orderly, and sensitive development of land uses proposed for the Kohl Ranch, special development criteria, standards, and mitigation measures have been created for each Planning Area. These area-specific standards, discussed in detail in Section 3.2 – Land Use, Planning, and Development Standards by Neighborhood, provide for appropriate transitions to neighboring land uses.

In addition to these specific techniques, project-wide development standards also have been developed as part of the Comprehensive Land Use Plan and are designed to complement the individual conditions within each Planning Area. These general standards are:

- 1) The total Specific Plan shall be developed with a target of 7,160 dwelling units but with a maximum not to exceed 7,171 dwelling units on 1,143.73 acres of residential uses and approximately 48 acres of mixed use, as illustrated on the Specific Land Use Plan. However, the maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. General uses permitted will include residential, industrial, commercial, business, open space/golf course, recreational and other lakes, country club, and public facilities.
- 2) Uses and development standards shall be in accordance with the County of Riverside Zoning Ordinance No. 348 for this Specific Plan in Section 2 – Specific Plan Zoning

Ordinance, and shall be defined by Specific Plan objectives, future detailed plot plans and potential conditional use permits as appropriate.

- 3) Standards relating to signage, landscaping, parking and other related design elements shall conform to the County of Riverside Zoning Ordinance No. 348 as amended for this Specific Plan in Section 2, and Coachella Valley Water District Ordinance 1302.1.
- 4) All project lighting shall be in accordance with County of Riverside standards.
- 5) The development of property shall be in accordance with the mandatory requirements of all Riverside County ordinances including Ordinance No. 348 as amended for this Specific Plan in Section 2, Ordinance No. 460, and state laws; and shall conform substantially with the adopted Specific Plan as filed in the office of the Riverside County Planning Department, unless otherwise amended.
- 6) All development on the site will be consistent with this Specific Plan No. 303 and subsequent amendments, as on file with the Riverside County Planning Department, and with all applicable laws of the State of California.
- 7) Prior to issuance of a building permit for construction of any use contemplated by this approval, the applicant shall first obtain clearance from the County of Riverside Planning Department verifying that all pertinent conditions of the Specific Plan approval have been satisfied for the phase of development in question.
- 8) Lots created pursuant to this Specific Plan and any subsequent tentative maps, shall be in conformance with the development standards of the Specific Plan herein applied to the property.
- 9) Agriculture shall continue to be an allowable use in in accordance with Specific Plan Zoning Ordinance.
- 10) Utility service to this Specific Plan shall be provided by individual authorized service providers or through the creation of a Public Utilities District (PUD).
- 11) A target maximum number of dwelling units is specified for each residential planning area. The target is based on the applicable density range and may or may not represent the maximum number of units allowed within the density range for any single planning area. Therefore, any given implementing subdivision may increase the number of units beyond the target density up to the maximum allowed for its planning area without a specific plan amendment, provided that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171 dwelling units. Further the number of dwelling units contained in an implementing subdivision application may exceed the maximum number of allowable units specified in the Specific Plan for any single planning area by not more than ten percent (10%) without an amendment to this plan, provided that the maximum number of dwelling units in other planning areas are reduced by an equivalent number of units such that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171 dwelling units. See Section 3.1.11, Specific Plan Administration, for the approach to tracking units during the build out of the project. Density transfers will be required to receive administrative approval.
- 12) A total of 5 acres of parkland shall be provided for each one-thousand (1,000) in population within the Kohl Ranch Specific Plan boundary or payment of a fee in-lieu

thereof, or a combination of both for neighborhood or community park and recreational facilities. Fees shall be paid in accordance with Riverside County Ordinance 460.

3.1.3 Circulation Plan

The Kohl Ranch Specific Plan circulation system is designed to provide direct and convenient access to all portions of the project site, and to provide efficient connections to major transportation corridors in the project vicinity such as the new SR-86S Freeway (east of the Whitewater River)². In addition, the proposed circulation system has been coordinated with other planning efforts in the area, such as the planned development at Jacqueline Cochran Regional Airport and the South Valley Implementation Plan. Based on a thorough traffic impact analysis (Appendix J of EIR No. 396), with recommended improvements, the study area intersections at buildout will operate at acceptable levels of service consistent with Riverside County policies. Precise access locations and the phasing of roadway improvements shall be determined at the plot plan or tentative tract map level, subject to approval by the Riverside County Transportation Department.

The project traffic analysis estimates that 88,464 trip-ends per day will be generated by the project at buildout (Appendix J of EIR No. 396). With an internal trip discount of ten percent (the percentage of trips assumed not to leave the Kohl Ranch), it is reduced to 79,618 external trip-ends.

The roadway designations are depicted in **Figure 3.1-2, Circulation Plan**. Roadway cross-sections are depicted in **Figure 3.1-3, Roadway Cross Sections**.

On-site, Avenue 62 is currently identified in the Coachella Valley Association of Governments (CVAG) Transportation Project Prioritization Study (TPPS), between Harrison Street and Polk Street. This is a priority list for transportation projects in the Coachella Valley. Transportation Uniform Mitigation Fees (TUMF), Measure A and other funding sources are administered through CVAG to assist local jurisdictions with roadway improvements. Prioritization on the list is based on the following factors: roadway surface condition, system continuity, level of service, and accident rate. The TPPS is typically updated every five (5) years. Avenue 62 is the only roadway on site that appears on the TPPS list.

Public Transit

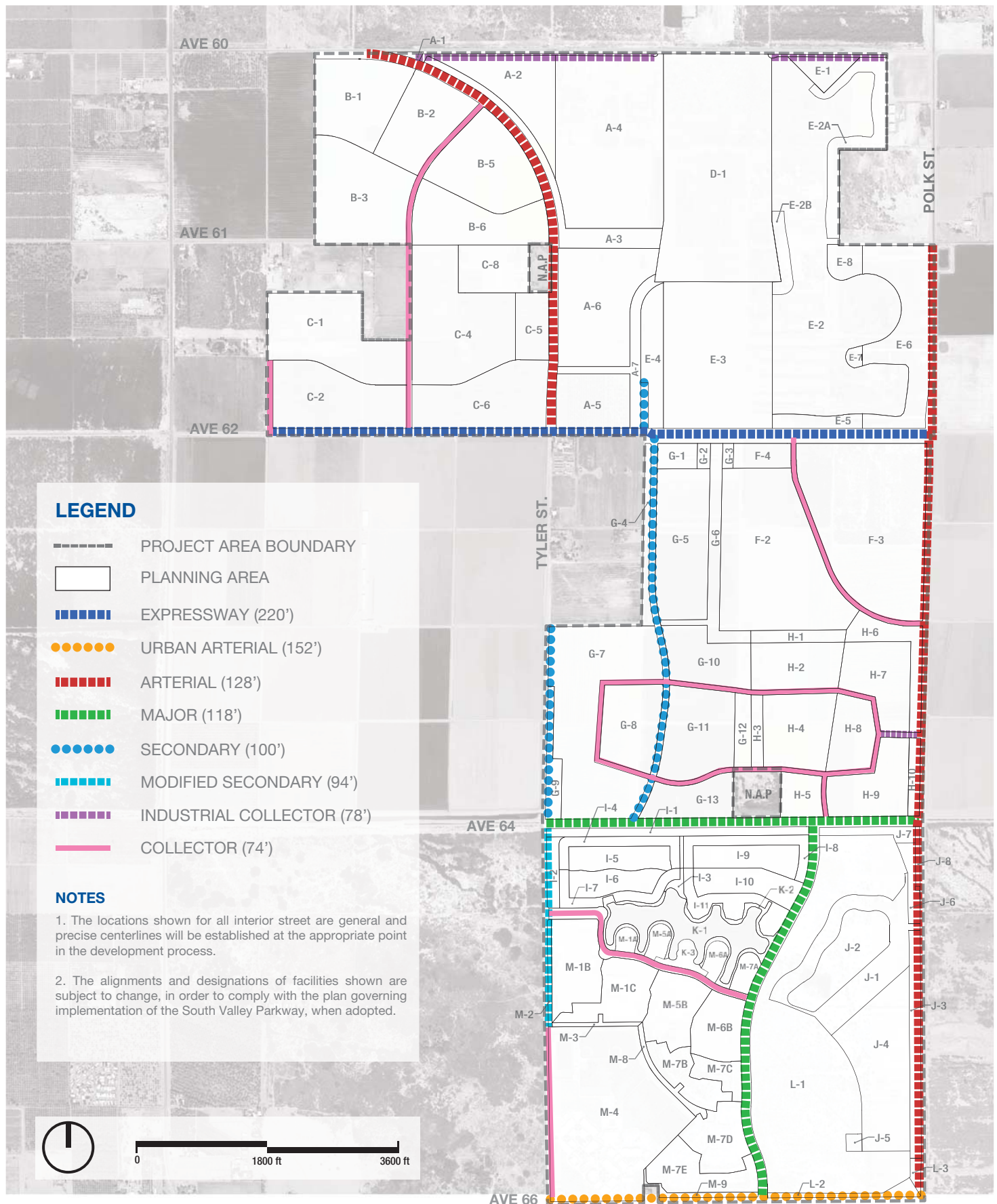
The SunLine Transit Agency (STA) is the regional public transportation operator in the Coachella Valley and will service the project area. According to the SunLine Transit Facilities Design Manual dated December 2006 and the Bus Route Map for Line 91, effective January 2, 2011, one bus route (Line 91) currently operates along Avenue 66 providing both eastbound and westbound service seven days a week. The nearest bus stop is located at Avenue 66 and Middleton Street located at the southwest corner of the project boundary. EIR 396 includes mitigation measures that require coordination with SunLine Transit, bus stops and bus turn-outs.

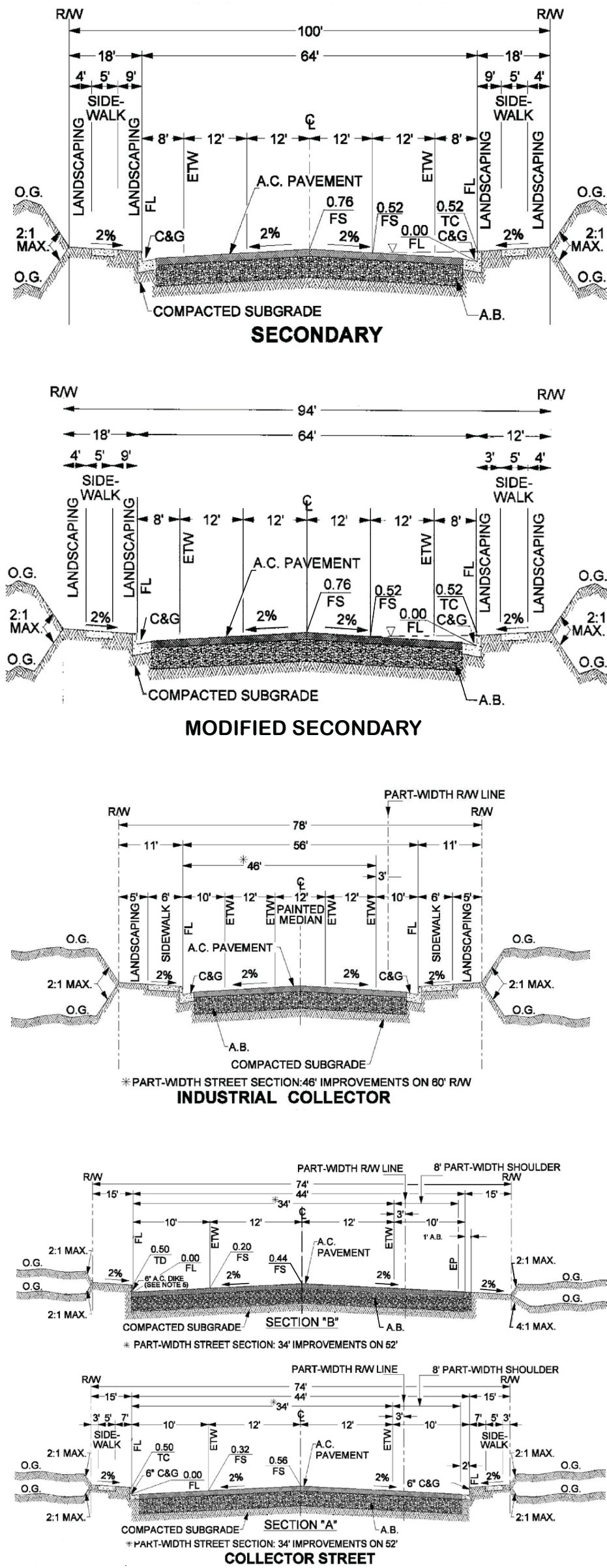
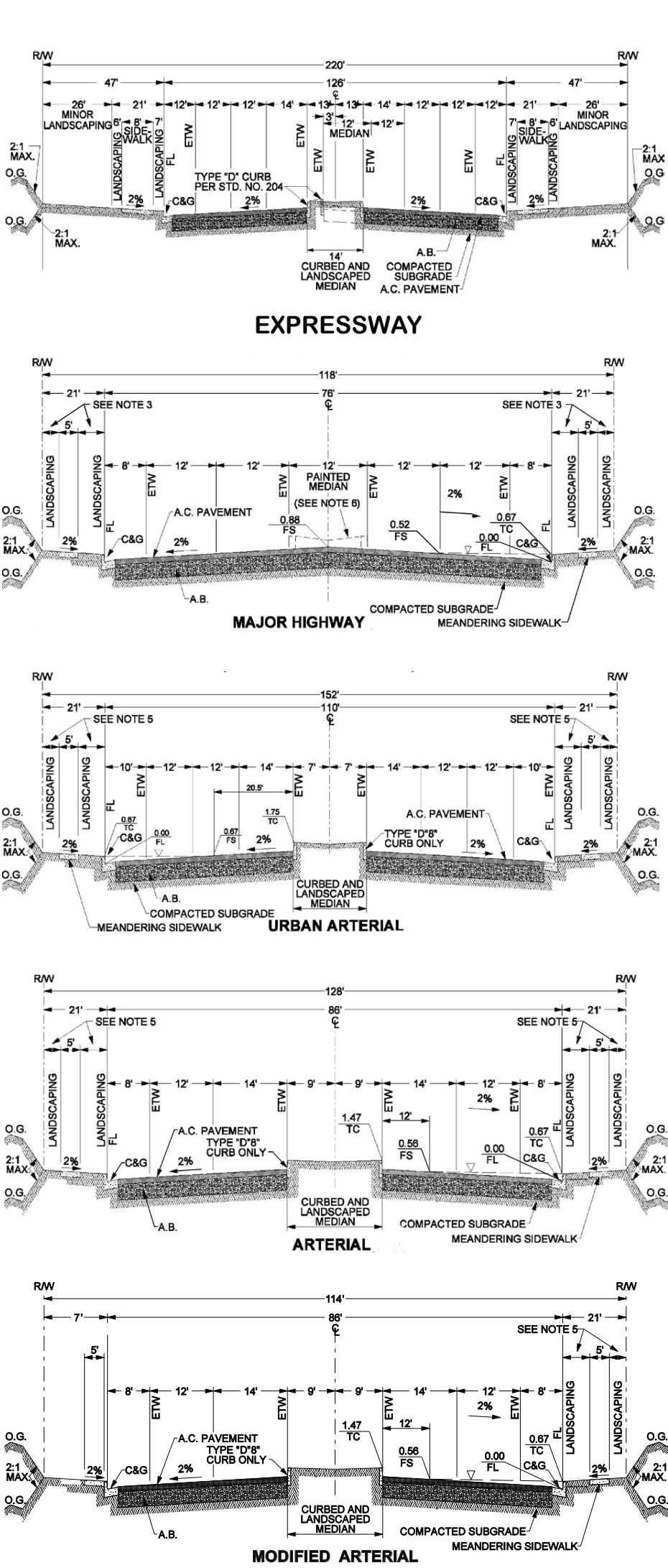
² For clarification, please note that the designation SR-86S refers to the new freeway constructed east of the Whitewater River. The "old" Highway 86 is located west of the project site and is referred to in this Specific Plan as Harrison Street, its local street name.

Circulation Development Standards

- 1) Any application for any subdivision within the specific plan boundary shall comply with the standards of Ordinance 460.
- 2) Each subdivision shall comply with the on-site and off-site street improvement recommendations and mitigation measures outlined in the subsequent traffic studies for each individual project.
- 3) All roadways intersecting four lane facilities or greater shall be a minimum of 74 feet of right-of-way and constructed in accordance with Standard 103, Ordinance 461 from the four-lane facility to the nearest intersection.
- 4) All typical sections shall be per Ordinance 461, or as approved by the Transportation Department.
- 5) All intersection spacing and/or access openings shall be per Standard 114, Ordinance 461, or as approved by the Transportation Department.
- 6) No textured pavement accents will be allowed within County rights-of-way.
- 7) Mid-block cross-walks are not allowed.
- 8) Driveways and access points. No driveways or access points as shown in this specific plan are approved. All access points shall conform to Transportation Department standard access spacing, depending upon the street's classification.
- 9) Drainage. The drainage facilities necessary for this project will generally be outside of the road rights-of-way and maintained by an owners association, county service area, or by the Coachella Valley Water District. Drainage facilities required for road purposes will be maintained by the transportation department or CVWD.
- 10) Commercial. Per the General Plan, "Neighborhood Commercial Uses must be located along Secondary or greater highways, at or near intersections with Secondary Highways."
- 11) Schools/Parks. The Transportation Department's policy regarding streets adjacent to school sites and park sites requires a minimum of 66' right-of-way (Standard 104).
- 12) Any landscaping within public road rights-of-way will require approval by the Transportation Department and assurance of continuing maintenance through the establishment of a landscape maintenance district or similar mechanism as approved by the Transportation Department.
- 13) All trails developed as part of this specific plan shall be designated as shown on **Figure 3.1-10, Public Facility Sites** or as agreed to by Riverside County Regional Parks and Recreation District; and shall be located within or adjacent to the right-of-way pursuant to **Figure 3.4-18, Tyler Street Trail** and **Figure 3.4-19 Avenue 62 Trail**. Class I bikeways shall be developed in accordance with the standards contained within Chapter 1000 of the California Department of Transportation – Highway Design Manual (Fourth Edition).
- 14) All roadway widths will be at the discretion of the Transportation Department, once the ultimate land uses for the individual planning areas are determined.

- 15) All projects, including subdivisions within the specific plan boundary, shall be subject to a Mitigation Monitoring Program, included as part of EIR No. 396 and any updated environmental assessments associated therewith.
- 16) Prior to the issuance of building permits, the landowner shall convey an avigation easement to Jacqueline Cochran Regional Airport, which shall be recorded, or shall provide evidence that such an easement covering the property has already been recorded..





NOTES

3. SEE STANDARD NO. 113 FOR LANDSCAPED MEDIAN REQUIREMENTS, INCLUDING MEDIAN CURB.

5. DISTANCE FROM SIDEWALK TO R/W, AND TO FLOW LINE, VARIES. SEE STANDARD NO. 404 FOR DETAILS OF MEANDERING SIDEWALK.

3.1.4 Drainage Plan

Drainage flows enter the property from both the northwest and southwest corners of the site and along the southern, western and northern boundary. Flows drain through the property and outlet along the southeast boundary between Avenues 66 and "D" Street as depicted in **Figure 3.1-4, Conceptual Drainage Plan**. Development of the project will require the collection and conveyance of flood flows through the project in a manner which will ensure the protection of the developed properties from a 100-year flood. In addition, storm flows will be redispersed along the eastern boundary to approximate existing flow conditions, to avoid adversely impacting downstream properties. The proposed flood control system is designed to collect the storm flows as they enter the site in collection basins, transport the flows through the site in graded swales and drainage pipes and discharge the flows over weirs, on the east side of the project.

In order to provide the required level of on-site flood protection, the storm flows will be intercepted on the east side of Tyler Street and on the south side of Avenue 60. The storm flows will then be conveyed through the project area through utilization of drainage pipe. In order to approximate existing historical runoff conditions, the difference between the on-site developed and undeveloped runoff flows will be controlled by use of on-site retention basins. The size and depth of these basins will be determined when a final development plan is prepared. Since each collection basin will be designed to handle predicted flood flows the width of the collection basins will vary. Generally, these collection basins will be trapezoidal in shape with 3:1 side slopes and a top width between 100 and 200 feet. Once the storm flows have passed through the graded swales and drainage pipes they will enter the dispersal basins along the eastern boundary of the project. These basins will vary in top width from 200 to 300 feet and will have a mild gradient toward the south. As peak flows progress in a southerly direction, they will spill over a side weir designed to outlet storm flows toward the east in a manner consistent with existing conditions. Retained water will be pumped in a sheet flow dispersal at rates less than presently occur.

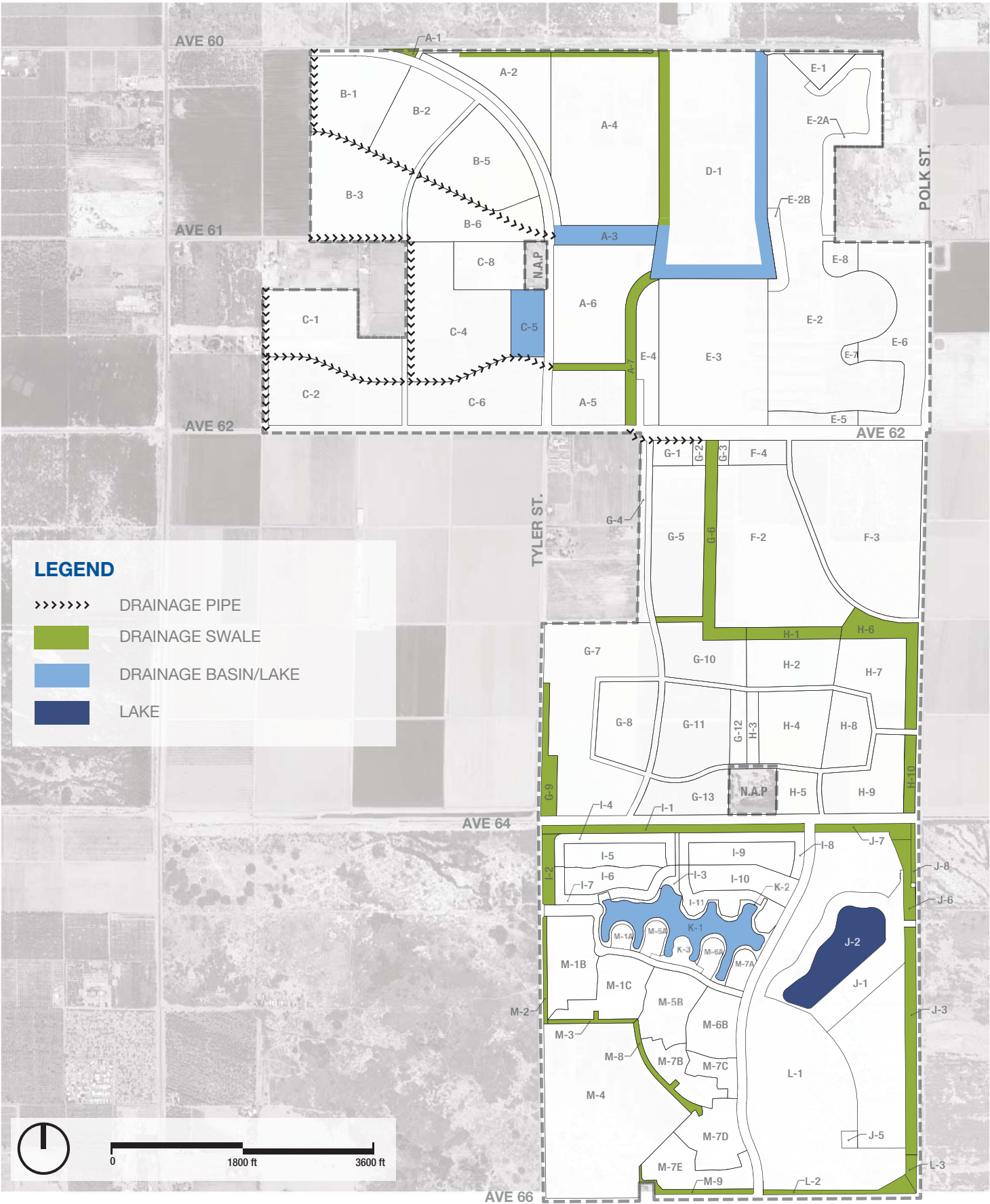
On-site runoff will be intercepted and conveyed through the development by means of a conventional catch basin, swales and storm drain system, in accordance with Coachella Valley Water District (CVWD) standards, so that the increase in on-site runoff resulting from the development will be detained on-site and allowed to percolate into the ground or be captured and reused. The collector storm drain system will be designed to utilize street flow carrying capacity and flows into catch basins and inlets when the quantity exceeds the top of curb.

A high degree of protection from the 100-year flood will be provided to all building pads on the project site as the recommended Flood Control Plan is implemented. Moreover, downstream properties will no longer have to contend with the uncertainty of the existing uncontrolled storm flows, and will have the benefit of controlled flows from the project area. Ultimately, the Eastern Coachella Valley Stormwater Management Plan will implement facilities that will eliminate offsite flows from impacting the Kohl Ranch. Future implementing Projects that develop prior to construction of these facilities shall account for 100-year flood condition in the interim.

Drainage Development Standards

- 1) All drainage facilities will be designed and constructed in accordance with the Riverside County and Coachella Valley Water District (CVWD) standards and specifications, and the Standard Specifications for Public Works Construction, (GREEN BOOK).
- 2) Drainage facilities will be subject to the review and approval of the Riverside County Transportation Department.
- 3) Design of drainage facilities will be reviewed by CVWD in conjunction with their review of the sewer and water facilities.
- 4) Drainage plans shall be submitted to CVWD for review and approval. This is to ensure that all proposed facilities are compatible with existing CVWD and U.S. Bureau of Reclamation (USBR) facilities.
- 5) The capital cost of all on-site facilities will be the responsibility of the applicant. Such facilities will be dedicated to Riverside County and a homeowners or County Service Area for maintenance and operations.
- 6) All areas within the Specific Plan area will be required to prepare a site specific Water Quality Management Plan (WQMP) in accordance with the requirements of the National Pollutant Discharge Elimination System (NPDES) standards. Industrial developments will require an additional SWPPP to operate.
- 7) All projects proposing construction activities including: cleaning, grading, or excavation that results in the disturbance of at least five acres total land area, or activity which is part of a larger common plan of development of five acres or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the specific plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program. Mitigation measures may include, but not be limited to: on-site retention; covered storage of all outside storage facilities; vegetated swales; monitoring programs; etc.
- 8) The drainage plan for Kohl Ranch shall take into account the existing agricultural drainage facilities in this area. Possible conflicts with these facilities shall be evaluated by the developer's engineer and CVWD.

FIGURE 3.1-4 **CONCEPTUAL DRAINAGE PLAN**



3.1.5 Conceptual Community Structure Plan

The Community Structure Plan identifies the major project open space and recreation areas which define the project neighborhoods, and the landscape treatments for project roadways and entries.

Open Space and Recreation

The open space areas are an integral part of the Kohl Ranch Specific Plan. They provide recreational amenities to residents and workers, and are used to screen development edges from potential noise and visual impacts associated with surrounding arterials. The open space system also provides aesthetically pleasing views both from within the project and from surrounding roadways and adjacent properties.

The open space and recreation system for the Kohl Ranch Specific Plan includes neighborhood and community parks, large-scale recreational uses, passive open space and a project-wide trail system. The open space system is organized around, and if necessary, will be adjusted to accommodate any changes in the project drainage network and the Jacqueline Cochran Regional Airport safety zones which limit the land uses and densities which can be located on certain portions of the site. Approximately 380.14 acres are devoted to the Open Space land use category, including Open Space-Lake. Under a golf course scenario, this would increase by approximately 150 acres.

Large-Scale Recreational Uses

Allowable land uses could accommodate a golf course that would parallel the drainage system, and serve as a central view focus and buffer between land uses for the majority of residential unit in the southern portion of the project site. The major recreational use in this area could be a golf course, which could alternatively be developed for recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, and non-potable irrigation water). The golf course would be equipped with a driving range and clubhouse facility. The golf course is a permitted use allowable under the provisions of the Section 2 - Specific Plan Zoning. If developed, it would be sited to maximize the golf course frontage and views for residential units. While the golf course would be privately owned, it is envisioned that the golf course (or in the alternative, recreational lakes) would serve neighborhood residents.

In the northern portion of the project site, a private motor sports race track, and uses incidental thereto, would be allowed uses within the open space designation in Neighborhood E. The facilities allowed within the open space planning areas in Neighborhood E would include a race track, private garages and uses incidental thereto such as a tuning shop and observation tower.

Local Parks

Seven local parks are planned to serve residents and/or employees of the project as depicted in **Figure 3.1-5, Community Structure**. If large-scale recreational uses such as a golf course or race track are built, Parks 4, 5, and 6 may be incorporated into the adjacent large-scale recreational facility. The parks are located throughout the site, and are connected by the project-wide trail system (**Figure 3.1-10, Public Facilities Sites**). Under the golf course alternative, an enhanced amenity package is planned which would supplement the recreation opportunities provided by the local parks. **Table 3.1-C, Potential Park Uses** on the following page describes potential park uses for the project, with and without the golf course scenario, **Table 3.1-D, Potential Park Uses (Golf Course Scenario)**, and with or without the racing facility **Table 3.1-E, Potential Park Uses (Racetrack Scenario)**.

- Park 1 – The local park in the northwestern portion of the site located adjacent to Tyler Street, serves the residential uses to the north of Avenue 62. The size of the park would total approximately 10 acres. This park is located to provide easy access to the large area of contiguous natural open space just south of the airport via a project trail along the north side of Tyler Street.
- Park 2 – The community park located to the south of Avenue 62 is approximately 4.0 acres. This park is linked to the other local parks by trails, some of which follow the drainage channels/paseos.
- Park 3 – A 9.1-acre park is located in the center of the site between two residential neighborhoods. This park can be accessed from the north, south, east, and west by the trail system, which also connects the park with public facility sites to the south.
- Parks 4, 5, and 6 – The local parks in the southern portion of the site are intended to be private to serve residents and are connected to each other and the residential neighborhoods they serve via a trail system planned around the lakes. Under the golf course scenario, these parks would be constructed as parts of the golf course.

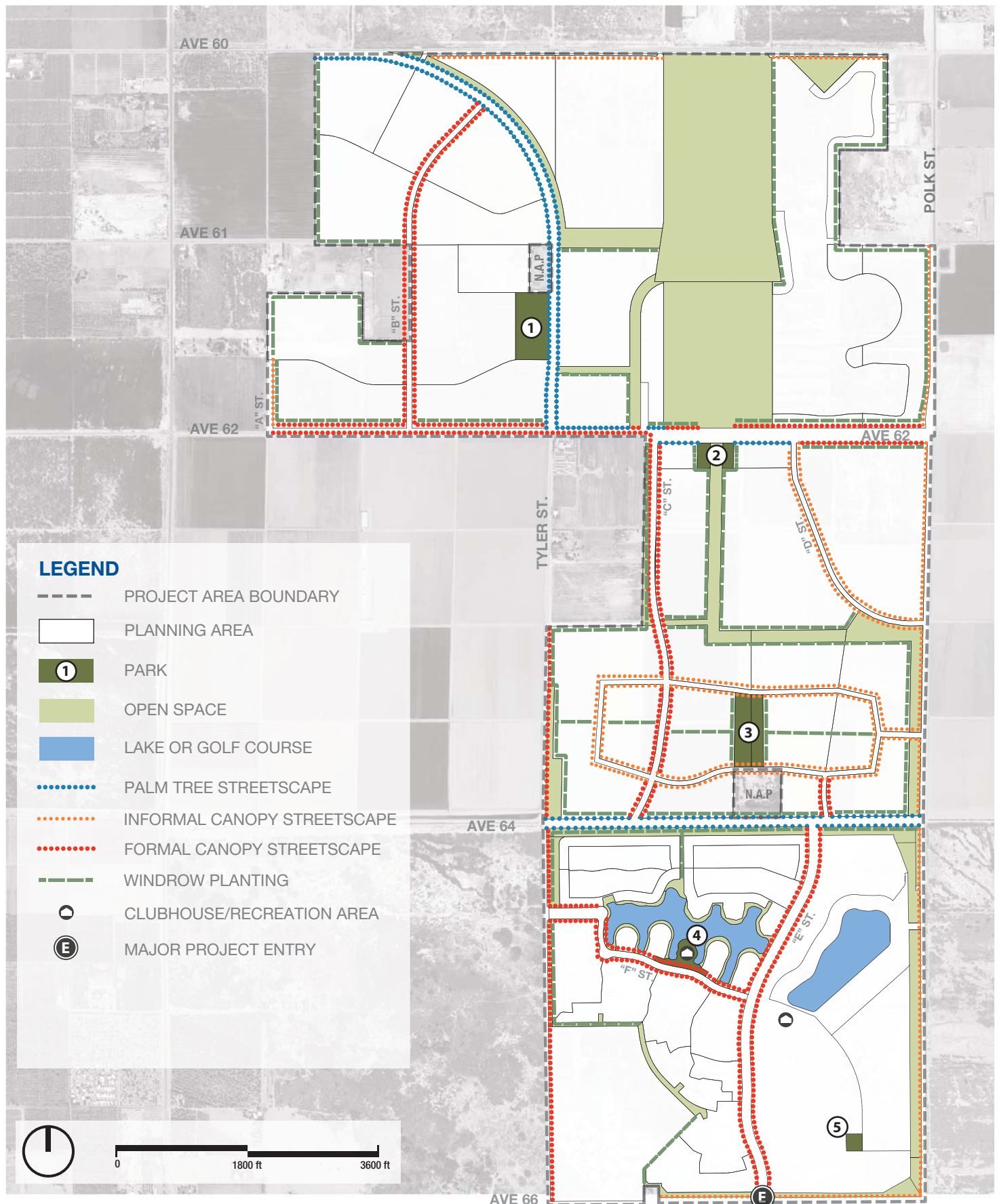


Table 3.1-C, Potential Park Uses¹

Park	Description	Size	Potential Park Uses
1	<ul style="list-style-type: none"> Neighborhood park in northwest portion of site in Neighborhood C Serves residential uses to the north of Avenue 62 Connected to large area of natural open space via Trail along Tyler Street 	9.97 acres	Ball fields Picnic area Tot lot Play equipment
2	<ul style="list-style-type: none"> Community park south of Avenue 62 in Neighborhood G Within airport safety zone Portion of park within drainage area Linked to other local parks by trails 	4.0 acres	Ball field Picnic area Tot lot Play equipment Tennis courts Recreation center
3	<ul style="list-style-type: none"> Neighborhood park in the center of the site in Neighborhoods G and H Trail system connects park with public facility sites to the north and south 	9.1 acres	Ball fields Picnic area Shuffle board Horse shoes
4 – 5	<ul style="list-style-type: none"> Small Neighborhood parks serving multiple neighborhoods in southern portion of the site (Neighborhoods J and K) Within drainage area Connected to public facilities and other parks through trail system 	4.4 acres	Picnic area Lawn bowling Shuffle board Horse shoes Senior center

1. Potential park uses identified in the table are provided as examples of the types of uses that would be appropriate in each park under the three scenarios. Actual park uses and facilities may differ from those listed.

Table 3.1-D, Potential Park Uses (Golf Course Scenario)

Park	Description	Size	Potential Park Uses
1	Same as Table 3.1-C	9.97 acres	Same as Table 3.1-C
2	Same as Table 3.1-C	4.0 acres	Same as Table 3.1-C
3	Same as Table 3.1-C	9.1 acres	Same as Table 3.1-C
4 – 5	Parks could be converted to golf course use	0.0 acres	Not applicable

Table 3.1-E, Potential Park Uses (Race Track Scenario)

Park	Description	Size	Potential Park Uses
1	Same as Table 3.1-C	9.97 acres	Same as Table 3.1-C
2	Same as Table 3.1-C	4.0 acres	Same as Table 3.1-C
3	Same as Table 3.1-C	9.1 acres	Same as Table 3.1-C
4 – 5	Same as Table 3.1-C	4.4 acres	Same as Table 3.1-C

Trail System

The proposed development is planned with an off-street (pavement) trail system that parallels streets and drainage ways throughout the site. **Figure 3.1-10, Public Facilities Sites** shows proposed trail locations, however, the final location of the off-street trail system may be adjusted to accommodate final engineering of the drainage system and development plans. A conceptual design for drainage swales and pipeline is shown in **Figure 3.1-4, Conceptual Drainage Plan**. Street-parallel trails will be constructed per the designations identified on **Figure 3.1-10, Public Facilities Sites** and illustrated in the cross sections found in **Figure 3.4-18, Tyler Street Trail** and **Figure 3.4-19, Avenue 62 Trail** found in Section 3.4 – Landscape Design Guidelines. The off-street system is supplemented by sidewalks and bike lanes within the paved roadway. In addition to providing access to the local parks, the trail system also provides a pedestrian linkage to the schools and to key commercial sites. The proposed trail system provides connections through the site to implement its portion of the County-planned trail system for the area.

Naturalized Open Space

The system of drainage channels throughout the project site will offer a scenic amenity to residents of the Kohl Ranch Specific Plan. The channels will be graded and will contain native desert vegetation that will also provide a buffer between land uses. The acreage of this natural open space will vary depending upon whether the golf courses or other recreational uses are built. Planning Area D-1 (110.66 acres) will remain in open space due to airport-related restrictions.

Landscape Concept

The landscape concept for the Kohl Ranch Specific Plan provides criteria for the treatment of areas within roads and easements, medians, land use transitions, development edges and project entries. The landscape concept has been conceived to organize the development and to establish a unified landscape framework that provides continuity throughout the project area and supports the community themes. Proposed landscape materials are intended to direct and guide the resident or visitor to the development, screen sensitive views, and frame or create focal points and views as the motorist and pedestrian moves through and around the project.

The landscape concept consists of a hierarchy of recommended plantings that correspond to the roadway classifications and project entry statements. More detailed descriptions of these landscape features are provided in Section 3.4 – Landscape Design Guidelines. The landscape concept is based upon the use of natural desert vegetation both for its visual effect and drought tolerant characteristics. The use of water efficient irrigation practices is encouraged, with canal water used for golf courses and public open space areas.

Five basic streetscape planting schemes are proposed:

- Palm Tree Streetscape;
- Formal Canopy Streetscape;
- Informal Canopy Streetscape;
- Windrow Planting; and
- Expressway Planting.

Project Entries

Major project entries generally are identified by groves of palm trees, which create visual interest through the introduction of a vertical element and a beacon that is visible at a distance across the flat landscape. The palm tree plantings are carried into the site along major project roadways, using a less dense spacing between plantings than the concentrated plantings at project entries. Major project entries are identified at the following locations:

- Avenue 60 at northern project boundary;
- Avenue 62 and Polk Street; and
- "E" Street and Avenue 66.

Minor project entries will be treated with a scaled-down version of the palm tree concept. Entries and intersections will be precisely located as internal roadways are designed.

Streetscapes

In order to achieve a cohesive overall circulation system for the Kohl Ranch Specific Plan and provide a strong community structure, a consistent streetscape treatment is recommended for public and private rights-of-way. The general landscape concept combines the use of palms trees with citrus understory, massed in selected locations, interchanged with formal and informal canopy plantings and use of other desert plant material. The streetscapes are broken down into three major categories with special features and plant forms as indicated.

The proposed plantings along major project roadways are based upon the roadway significance and strategic location within the project site. Because of its importance as a connection and northern project entry, Tyler Street, north of Avenue 62, is proposed to employ the palm tree planting concept, with breaks in the plantings where tall trees are prohibited through the airport safety zones. If the race track scenario is developed, Windrow Plantings will be used along its frontage to screen from residential uses to the west.

Formal canopy streetscape plantings are planned along Avenue 62, "B" Street, "C" Street, "E" Street, "F" Street, Tyler Street (south of Avenue 62) and several local roadways that provide internal circulation between neighborhoods. These streets provide definition to the project neighborhoods. Informal canopy streetscape plantings are recommended for local roads that are more interior to the site.

Buffers and Land Use Transitions

Windrow plantings are recommended to serve as buffers between land use types, along drainage channels and project edges, and within residential neighborhoods to mitigate against high desert winds. Windrows also can be used to provide structure to project neighborhoods and can be integrated with the project trail system, but must be sited to take into consideration airport safety zones and screening of the race track, if developed.

Community Structure Development Standards

- 1) All detailed landscaping programs for planning areas and roadways will be prepared by a qualified landscape architect for review by the County.
- 2) Common open space areas within each planning area may be devoted to passive or active uses, and will, to the extent feasible, be coordinated with any open space in adjoining planning areas to create a continuous network. The exact design and layout of facilities will be accomplished in conjunction with detailed future tract layouts.
- 3) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped common areas shall be submitted to the Riverside County Planning Department for approval. The improvement plans shall include, but not be limited to, the following:
 - Final grading plan;
 - Irrigation plans prepared by a landscape architect;
 - A landscaping plan with specifications for mulching and staking methods; locations, type, size and quantity of plantings;
 - Fence treatment plans; and
 - Special treatment/buffer area treatment plans.
- 4) The applicant developer and/or builder shall be responsible for maintenance and upkeep of all common landscaped areas and irrigation systems within its ownership parcels until such time as these operations are the responsibility of other parties.
- 5) At the time of recordation of any final subdivision map which contains a common open space area, the subdivision shall have those common areas transferred to the master property owners association or an appropriate public maintenance agency who will take responsibility for maintenance.
- 6) Irrigation of common open space areas and golf courses will be accomplished with canal water and/or reclaimed water to the extent possible.
- 7) For further landscape development standards, please refer to Section 3.4 – Landscape Design Guidelines.

3.1.6 Water and Sewer Plan

The project study area is completely within the service boundary of the Coachella Valley Water District (CVWD) which provides water and sewer service to the project area. The water and sewer plan is based on an analysis of water and sewer infrastructure which evaluates availability of services, calculates water demands and sewer generation quantities, and proposes infrastructure facilities which allow for the advancement of development. The purpose of the water and sewer plan is to provide for the backbone improvements which will be capable of serving the ultimate project, in addition to the individual portions independent of their progression.

Existing Water and Sewer Conditions

There are some domestic water distribution facilities within the project boundary. Domestic water provided within the CVWD service boundary comes from ground water which is chlorinated, the Colorado River and the State Water Project. Previous studies conducted in the area indicate that some ground water must be treated for arsenic to meet the State Drinking Water Standards. A Domestic Water, Non-potable Water, Canal Water and Sanitation System Installation and Service Agreement (Document No. 2002-538101) is currently on file at CVWD which provides for mitigation of arsenic when required, through payment of fees for arsenic treatment facilities. CVWD is located at 85-995 Avenue 52 in Coachella, California.

There is an existing tank site southwest of the intersection of Avenue 68 and Harrison Street with a 24-inch waterline that brings water to the project. An 18-inch water line is located in Tyler Street at the intersection of Tyler and Avenue 66 which extends north along the project boundary to Neighborhood I. A 24-inch water line is also located at the intersection of Tyler Street and Avenue 66. This line runs east along the existing school site boundary (Planning Area M-4). An existing arsenic treatment plant is located on the northeast corner of Tyler Street and Avenue 66.

There are some sewer facilities located within the project boundary. An existing sewer line currently services the existing school site (Planning Area M-4) and runs through Neighborhoods J, L, and M to an existing sewage pump station located in Planning Area J-4. An existing 12-inch sewer force main runs north to Avenue 62 and intersects with an existing CVWD force main. It is then pumped to Wastewater Reclamation Plant No. 4.

CVWD operates and maintains its Wastewater Reclamation Plant No. 4 located between Avenue 62 on the north, Avenue 64 on the south, adjacent to the Whitewater River on the east, and approximately 600 feet east of Fillmore Street on the west. This plant has a design capacity of approximately 5.0 MGD, and is estimated to be currently operating at half capacity. The plant currently treats to a secondary treatment level using stabilization ponds for finishing. The ponds are used for flow stabilization by allowing the depth to increase during peak wet water events.

Proposed Water System and Water Conservation Measures

This project will utilize a “dual water system” consisting of potable and non-potable water systems. The potable system will provide water for domestic purposes and provide water for fire protection. The non-potable system will provide water for irrigation purposes.

The proposed potable water system improvements as depicted in **Figure 3.1-6, Conceptual Potable Water Plan** will operate within a single pressure zone serving ground elevations from 88 to 167 feet below sea level. Hydraulic control for the project should be set using a reservoir high water level of approximately 83 feet above sea level. An existing tank site has been constructed at CVWD’s recommended pad elevation of approximately 60 feet above sea level.

The potable water system is proposed to be fed by wells to be located within or near the project boundary. Four wells are anticipated to pump peak daily demands through an on-site system from 12-inch to 30-inch diameter transmission lines looped for maximum efficiency and fire flow delivery. Internal water lines feeding individual houses will range from 6-inch to 8-inch. Excess water during low and average flows will be conveyed to reservoirs to be located southwesterly as reflected in **Figure 3.1-7, Off-Site Water Improvements**, of the project. An existing 24-inch diameter transmission main will convey flows to the project when operating from gravity flow. Per CVWD’s Master Water Plan, transmission mains are proposed adjacent to, and through the Specific Plan. The Specific Plan water system shall connect to any existing adjacent water facilities that have available capacity.

The potable water system will be designed to deliver gravity flow during high power cost windows. Well pumps will be controlled by level sensing devices and transmitters on the reservoirs. The water system will utilize a certain volume from the tanks, prior to the well pumps cycling, to ensure that adequate circulation of water in the tanks is maintained.

In accordance with the Water Conservation Plan, water conservation measures will be incorporated into the project plan to include water saving devices and systems. Further, the use of surface water will be used for irrigation purposes. Water conservation for the Kohl Ranch project will be achieved through:

- Dual-source water system to reduce groundwater consumption;
- Recycled and/or canal water for irrigation;
- Drip and other efficient irrigation;
- Intelligent irrigation controllers;
- Native and non-native drought-tolerant planting materials;
- Tiered water rates; and
- Use of proposed lake to double as a forebay for the non-potable water treatment.

Groundwater use will be reduced through the initial use of Colorado River water for non-potable water uses as part of a dual-source water system. The design will include a potable water system designed for indoor use only, as well as a separate non-potable water system for outdoor landscaping. The potable water system will be connected to the CVWD domestic water system, which relies upon groundwater. The non-potable system will use Colorado River water delivered by the existing agricultural canal system for landscaping and outdoor use. The majority of the landscaping throughout the project site is designated (in general terms) as “desert landscaping.” And, the project’s proposed high density residential uses reduce the amount of landscaped open space per unit, limits the number of individual pools, and encourages the use

of public facilities.

This non-potable system will be designed to meet recycled water standards in anticipation of the possible future availability of tertiary water when it becomes available from the nearby Water Reclamation Plant 4 (WRP-4). The project will be sewered and the majority of the demand on the Aquifer from the project will be from indoor use. In the event recycled wastewater from WRP-4 becomes available, project demand on the Aquifer will be further reduced; recycled water can be used to replace Colorado River water or groundwater for agriculture use in the area or on project landscaping. Additional Colorado River water could be freed up for agricultural use or to recharge the Aquifer.

CVWD has three WRP's that produced recycled water for a combined total of 8,289 acre-feet in 2008, which was used to reduce the demand on the groundwater basin. In addition, Desert Water Agency, a partnership with CVWD in the management of the Aquifer, operates a reclamation plant in Palm Springs and produced 3,000 acre-feet of recycled water per year, which also reduces the demand on the groundwater basin. This dual water system is being required of new, larger developments in the Lower Valley, including Kohl Ranch, as a result of SB1557, which finds that the use of potable domestic water for nonpotable uses is wasteful if suitable nonpotable water sources are available.

Additionally, CVWD is working with the cities in its service area to limit the amount of water that can be used for outdoor landscaping. As the result of the adoption of state-wide indoor water conservation measures requiring low flush toilets, shower and faucet flow restrictors, and other devices, the amount of water used inside homes has been significantly reduced. With the large number of new homes constructed, these conservation programs have reduced impacts of new development on the Aquifer.

The non-potable water system water source as reflected in **Figure 3.1-8, Conceptual Non-Potable Water System**, will be diverted canal water that would be delivered into an on-site system of lakes and pond(s). Hydropneumatic booster station(s) will pump water from the lakes and pond(s) throughout the development for irrigation purposes. The on-site water system consists of 8-inch to 42-inch diameter transmission lines looped for maximum efficiency and delivery at appropriate service pressures.

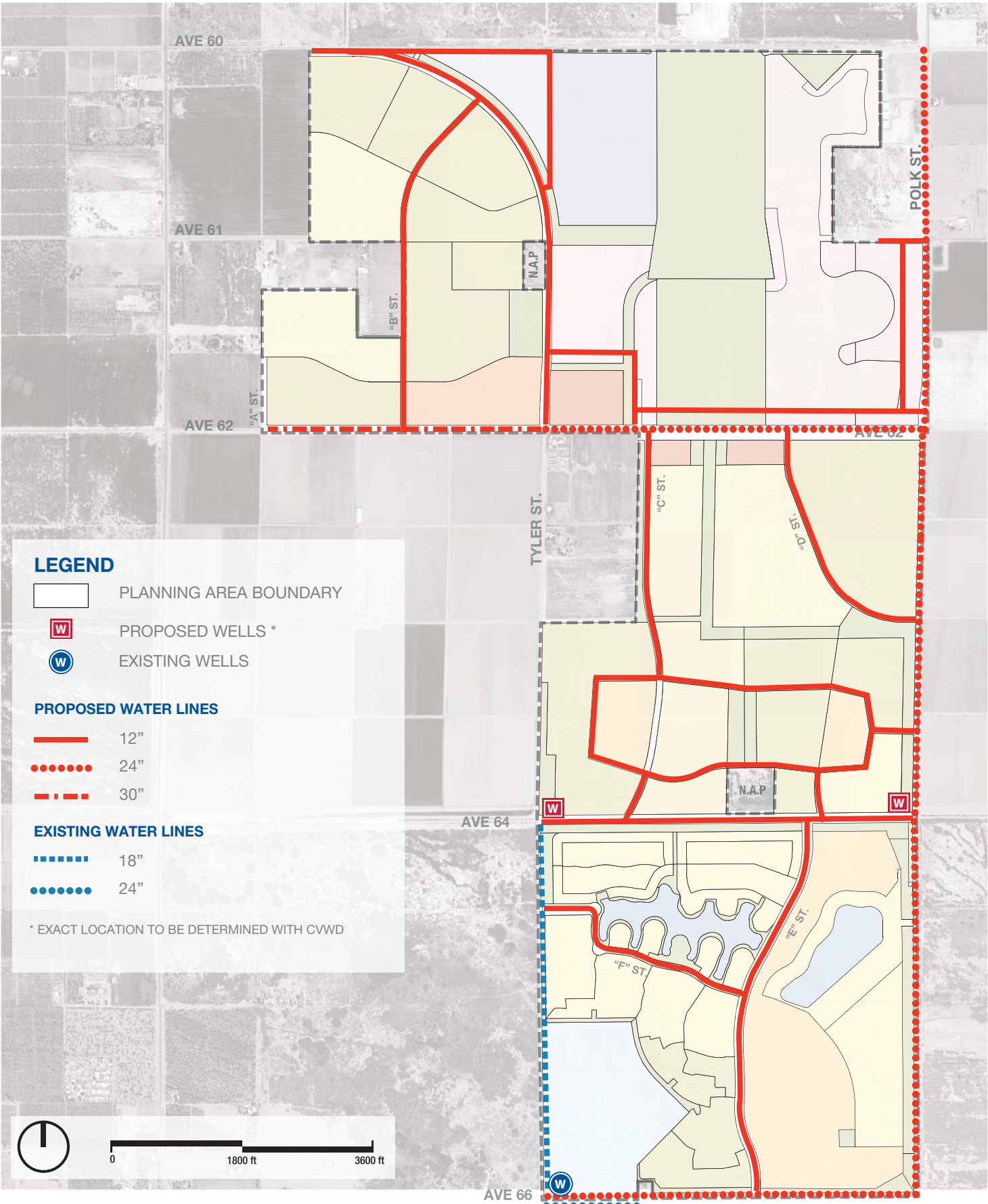
Proposed Sewer

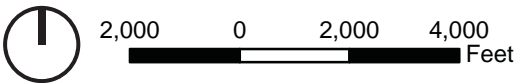
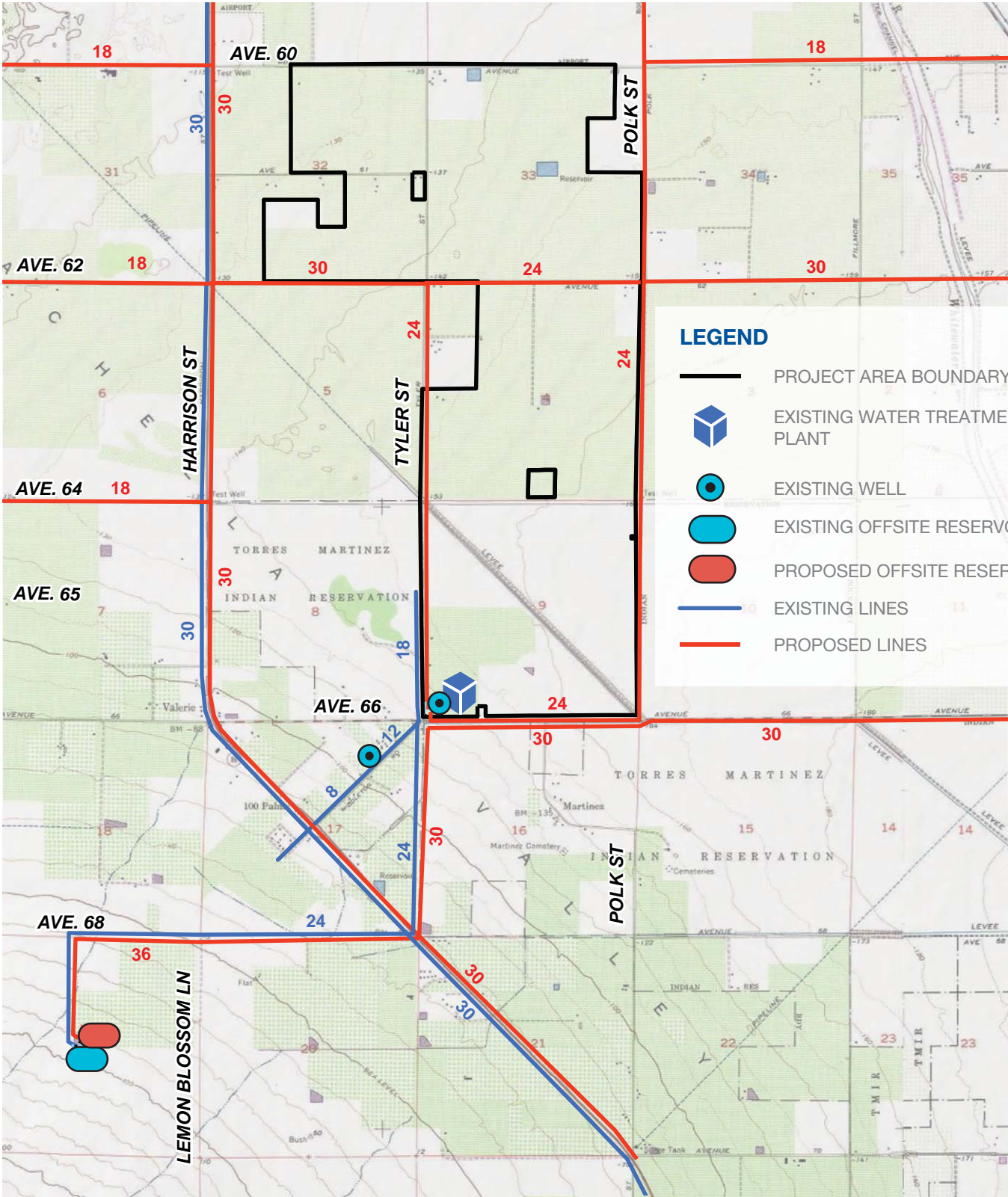
The Specific Plan's sewer improvements as reflected in **Figure 3.1-9, Sewer Plan**, are based on an analysis of sewer infrastructure that elevates availability of services, calculates sewage generation, and proposes infrastructure facilities which allow for the advancement of the development. The purpose of the sewer plan is to provide for the backbone improvements that will be capable of serving the ultimate project, in addition to the individual portions independent of their progression.

The Specific Plan proposed sewer system conveys flows to the existing sewage pump station and ultimately to CVWD's treatment plant. The Specific Plan sewer system shall connect to any adjacent existing sewer facility that has capacity.

Water and Sewer Development Standards

- 1) All water and sewer facilities will be designed and constructed in accordance with the CVWD standards and specifications, American Water Works Association (AWWA), American National Standards Institute (ANSI) and the Standard Specifications for Public Works Construction, (GREEN BOOK).
- 2) The capital cost of on-site and off-site facilities necessary to serve the project site will be the responsibility of the applicant. Such facilities will be dedicated to CVWD, after construction, for the District to maintain and operate. Fee credits may be available for transmission, storage, and wells. As discussed above, a Domestic Water, Non-potable Water, Canal Water and Sanitation System Installation, and Service Agreement (Document No. 2002-538101) is currently on file at CVWD which provides for mitigation of arsenic through payment of fees for arsenic treatment facilities.
- 3) In accordance with the Water Conservation Plan (see Appendix F), water conservation measures will be incorporated into the project plan. The following measures will be utilized to reduce the impact on the underground basin and increase efficiency in water usage:
 - (a) A dual water system (potable/non-potable) will be installed to service landscaped areas.
 - (b) Irrigation lines will utilize canal water and/or treated effluent to irrigate landscape areas.
 - (c) Water conservation plumbing fixtures or water saving devices and systems shall be used in all construction.
 - (d) Where possible, the existing tile drains will be maintained to prevent high salt water from migrating to the groundwater basin.
 - (e) The project development shall conform with state, county, and CVWD regulations regarding water conservation and reclamation.
- 4) All development is required to landscape in accordance with **Table 3.4-A, Kohl Ranch Plant Palette** found in Section 3.4, and CVAG Ordinance 1302.1, Landscape and Irrigation System Design Criteria. This will ensure additional water conservation through the use of desert-appropriate landscape materials.





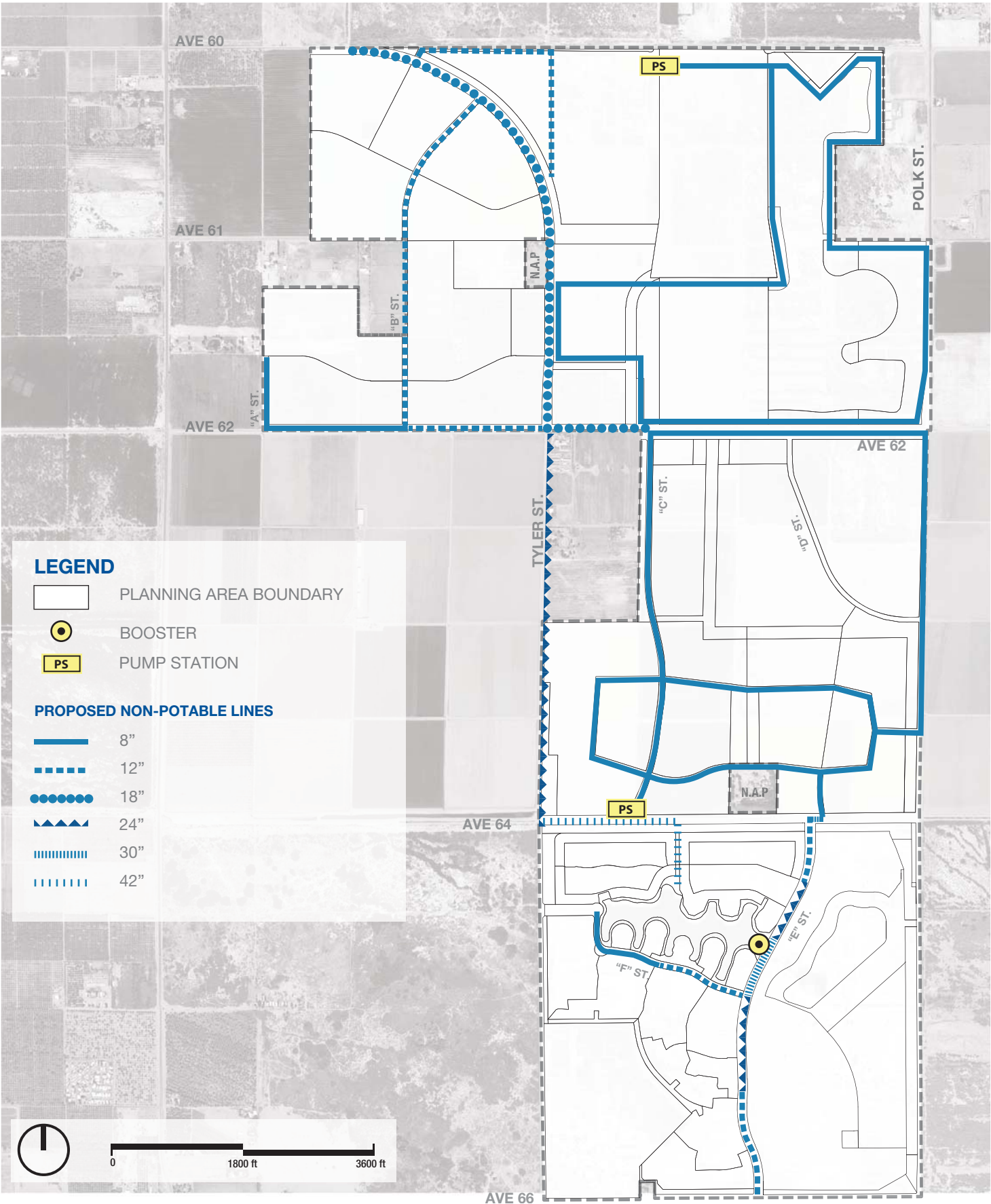
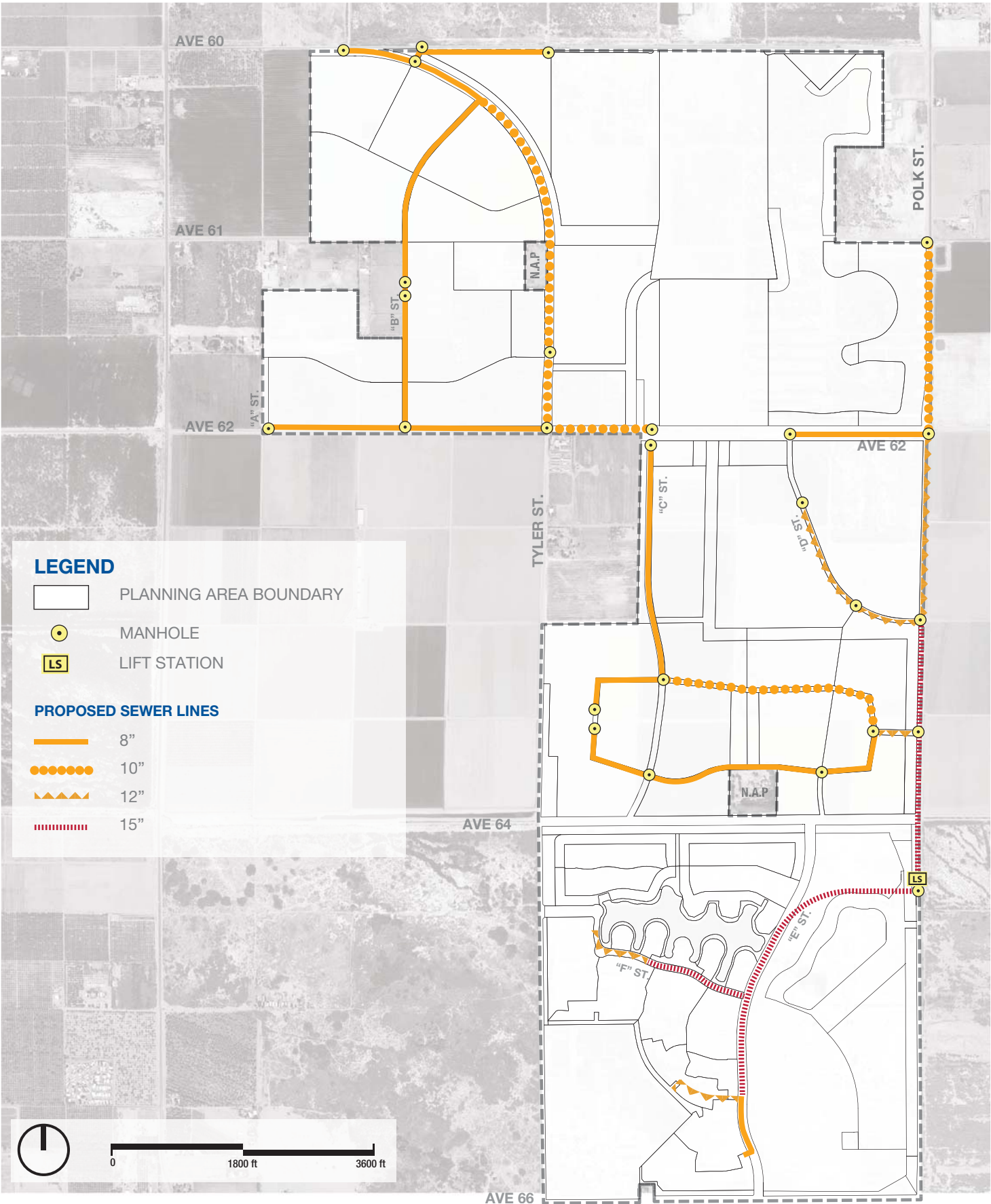


FIGURE 3.1-9 **CONCEPTUAL SEWER SYSTEM**



3.1.7 Public Facilities Sites and Project Phasing

In order to insure timely development of public facilities, a phasing plan has been prepared for parks, schools and other public facilities as depicted in **Figure 3.1-10, Public Facilities Sites**.

Project Phasing Plan

Four phasing scenarios have been established for the proposed project as depicted in **Figure 3.1-11, Phasing Plan**. Each scenario represents an area of the site that is likely to develop as a unit. The purpose of a scenario is to establish the infrastructure required to initiate development, assuming that the scenario site is the portion of the total project site to be developed first. However, the scenario numbers do not necessarily indicate the sequence in which the site will be built out. For example, Phasing Scenario 2 and Phasing Scenario 4 could be constructed first. It is anticipated that the project will be developed over a twenty-five year period, in response to market demands, and according to a logical extension of roadways, public utilities and infrastructure. Efforts by local government to stimulate economic development in the project area, through the planned development at Jacqueline Cochran Regional Airport, can be expected to serve as an impetus to development of revenue-producing land uses in the northern portion of the project site. The project phasing scenarios are further described below.

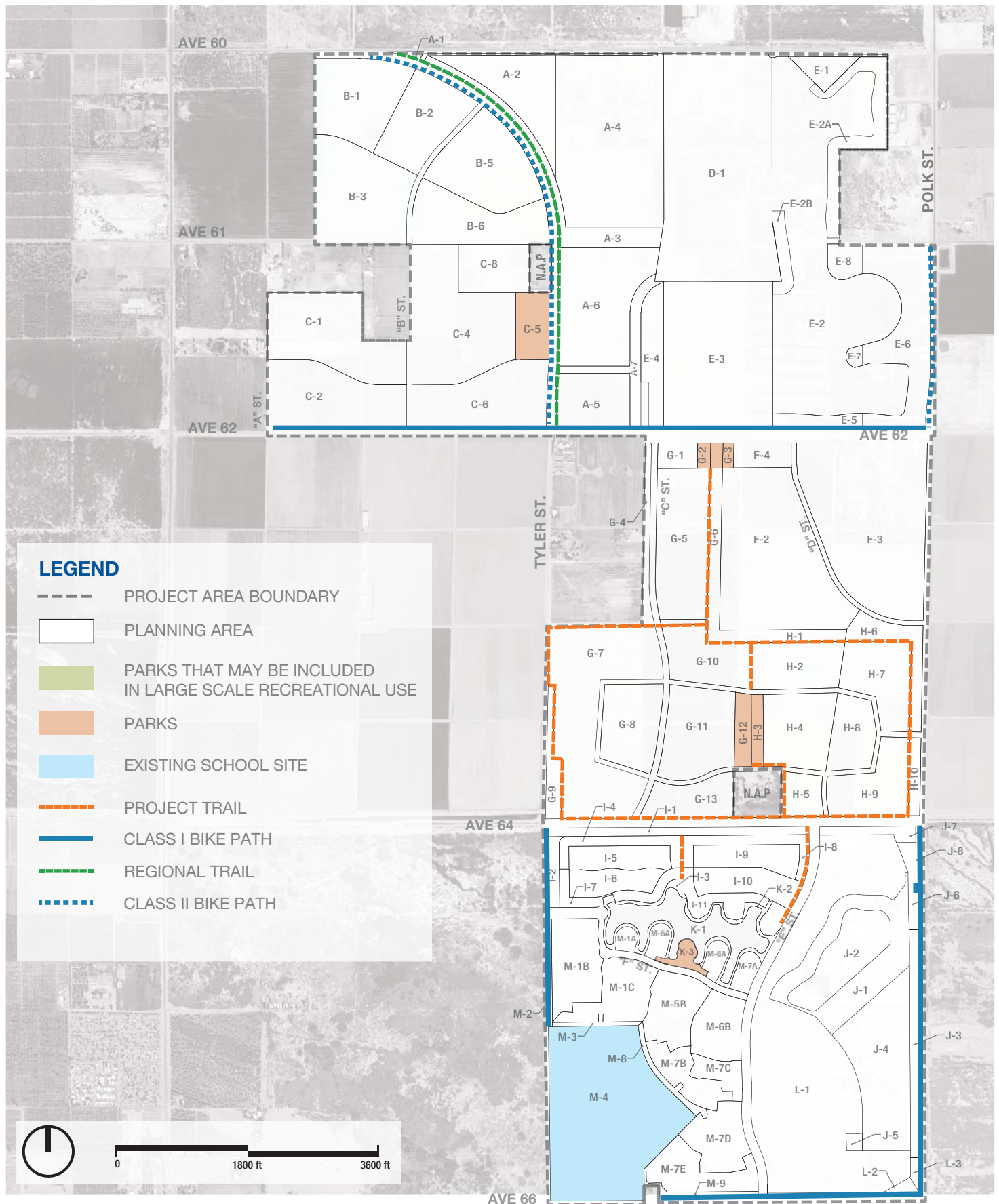
Public Facilities Phasing Schedule

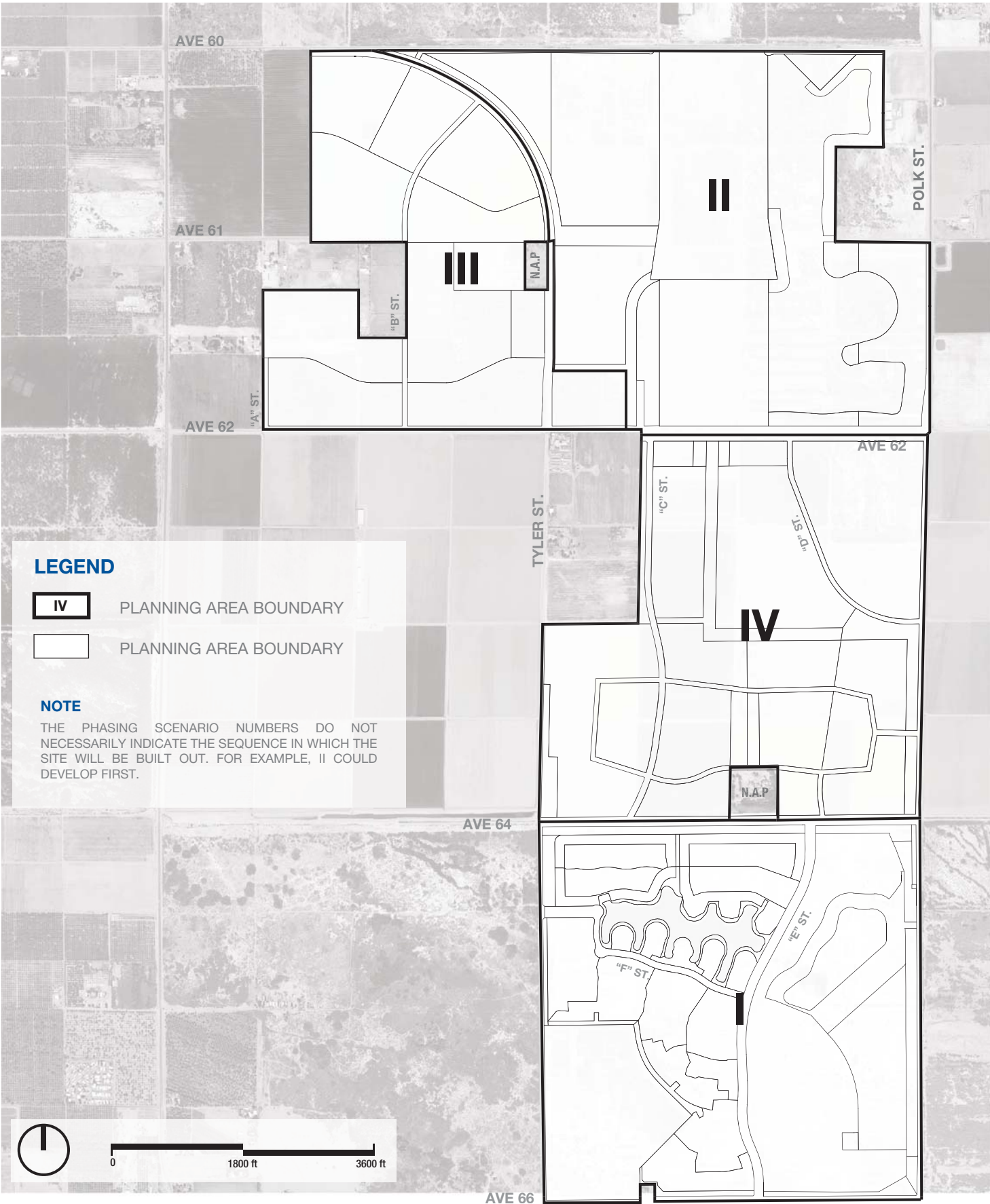
Public facilities shall be phased as indicated in **Table 3.1-F, Public Facilities Phasing Schedule**, below.

Table 3.1-F, Public Facilities Phasing Schedule

Planning Area	Public Facility	Size of Site (acres)	Milestones and Requirements (to be determined)
PHASING SCENARIO 1			
K-1/J-2	Lake	42.96	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
K-3	Park	3.2	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
J-5	Park	1.2	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
PHASING SCENARIO 2			
-	-	-	Payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or industrial subdivision map is filed for Neighborhood E.
PHASING SCENARIO 3			
C-5	Park	9.97	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood C.
PHASING SCENARIO 4			
G-3	Park	1.19	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G.
G-2/G-6	Park	15.09	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G.
G-12/H-3	Park	9.14	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G or H.

Note: The park acreages assume that the golf course or race track would not be built. Planning Area M-4 is an existing school.





Phasing Scenario 1

Phasing Scenario 1 encompasses the southern segment of the project area. The infrastructure improvements described in this section assume that Scenario 1 would be the first area of the site to develop. Transportation improvements would involve both major and secondary roadways, including partial width of Polk Street from Avenue 66 to Avenue 64; Avenue 64 from Polk Street to Tyler Street; "F" Street between "E" Street and Tyler Street; and "E" Street between Avenue 64 and Avenue 66. Grading activities for this Scenario would be accomplished by stockpiling export on the Scenario 4 site to the north. Drainage for this scenario would require that off-site flows be picked up along the westerly boundary in master planned facilities, conveyed through the site and then released at a controlled rate along the eastern boundary of Polk. Sewer facilities would gravity flow to the easterly portion of the scenario site to the existing sewage pump station. From that location, an existing sewer force main will convey the flows to the existing waste water lines along Avenue 62. Water facilities to support the development of this scenario begin with the extension by others, of the existing 24-inch diameter waterline in Avenue 66 to the east at the intersection of Avenue 66 and Polk Street. A 24-inch diameter waterline will be constructed by others, in Polk Street between Avenue 66 and Avenue 64. A 12-inch diameter waterline will be constructed in Avenue 64 between Tyler Street and Polk Street in "E" Street and "F" Street. Two wells exist to provide adequate water supply and backup (one on the school site and one on Middleton Street just to the southwest of the project area). Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-G, Phasing Scenario 1 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	I-4	7.0	35
	I-5	10.6	51
	I-6	9.6	42
	I-7	6.7	29
	I-8	8.0	40
	I-9	11.5	57
	I-10	12.4	47
	I-11	10.1	37
	J-1	30.64	143
	J-4	39.42	189
	M-1A	2.5	8
	M-1B	18.9	68
	M-1C	16.4	61
	M-5A	4.1	14
	M-5B	15.7	61
	M-6A	3.8	14
	M-6B	13.4	57
	M-7A	4.1	20

Land Use	Planning Areas	Acreage	Units
<i>Subtotal</i>		224.86	973
Medium High Density Residential	M-7B	11	56
	M-7C	5.9	32
	M-7D	15.1	82
	M-7E	15.5	82
<i>Subtotal</i>		47.5	252
High Density Residential	L-1	120.46	1,158
<i>Subtotal</i>		120.46	1,158
Open Space	I-1	9.6	
	I-2	3.9	
	I-3	0.5	
	J-2	20.56	
	J-3	11.24	
	J-5	1.2	
	J-6	4.1	
	J-7	4.24	
	J-8	1.16	
	K-1	22.4	
	K-2	8.8	
	K-3	3.2	
	L-2	4.1	
	L-3	1.4	
	M-2	1.9	
	M-3	1.6	
	M-8	2.9	
	M-9	2.7	
<i>Subtotal</i>		105.5	
Scenario 1 Total		498.32	2,383

Note: Planning Area M-4 (84.3 acres) is an existing school site.

Phasing Scenario 2

Phasing Scenario 2 is situated in the northeast corner of the project area. The infrastructure improvements described in this section assume that Scenario 2 would be the first area of the site to develop. Transportation improvements would involve expressway, arterial and secondary roadways, including partial width of Polk Street north of Avenue 62 to project boundary; and Avenue 62 between "C" Street and Polk Street. Grading activities for this scenario would be limited to the Scenario 2 site. There would be limited (if any) import/export required off-site. Drainage for Scenario 2 would require that off-site flows be picked up along the western and northern Scenario 2 boundary and in an interim swale along the westerly boundary of Tyler Street and conveyed to the proposed on-site open space channel areas (Planning Areas A-3 and A-7). Off-site flows from these open space areas would be conveyed along a drainage channel running along the west edge of Scenario 2 daylighting south of site through Scenario 4 that returns to a sheet flow condition, with no additional drainage facilities required. Sewer facilities would gravity flow to the southeast corner of the drainage areas to a gravity sewer presently being constructed by CVWD. Water facilities to support the development of this scenario begin with the extension of the existing 24-inch diameter waterline in Avenue 66 east to Polk Street and north in Polk Street to Avenue 60. A proposed 24-inch waterline in Avenue 62 between Tyler Street and Polk Street is also needed. Connecting to these 24-inch lines will be 12-inch lines in Tyler Street, Avenue 60 and internal to the Scenario 2 area to serve the area. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-H, Phasing Scenario 2 Details

Land Use	Planning Areas	Acreage	Units
Mixed Use/Air Park	A-2	24.45	
<i>Subtotal</i>		24.45	
Mixed Use	A-6	46.18	
	E-2	127.34	
	E-2A	16.48	
	E-2B	4.92	19
	E-4	12.25	120
	E-5	5.31	5
	E-6	33.99	15
	E-7	1.32	
	E-8	4.94	
<i>Subtotal</i>		252.73	159
Heavy Industrial	A-4	81.17	
<i>Subtotal</i>		81.17	
Open Space	A-1	0.63	
	A-3	10.77	
	A-7	9.97	

Land Use	Planning Areas	Acreage	Units
	D-1	110.66	
	E-1	5.61	
	E-3	69.33	
<i>Subtotal</i>		206.97	
Scenario 2 Total		565.32	159

Phasing Scenario 3

Phasing Scenario 3 is located at the northwestern corner of the project area. The infrastructure improvements described in this section assume that Scenario 3 would be the first area of the site to develop. Transportation improvements would involve expressway, arterial and secondary roadways, including "A" Street north of Avenue 62; "B" Street between Avenue 62 and Tyler Street; Tyler Street north of Avenue 62; and partial width of Avenue 62. Grading activities would be limited to the Scenario 3 site. Drainage for Scenario 3 would require that off-site flows be picked up along the western project boundary, conveyed through the site and then released to the adjacent Scenario 2 area. Two small off-site retention facilities may be required east of Scenario 3. The retention facilities would have overflow weirs on the downstream side to return the storm flow to its original sheet flow condition. Sewer facilities would gravity flow to the southeast corner of the site to a gravity sewer in Avenue 62 presently being constructed by CVWD. Water facilities to support the development of this scenario begin with either the extension of the existing 18-inch diameter waterline at Tyler Street and "F" Street to the intersection of Avenue 62 and Tyler Street, or a 24-inch line will be extended westerly from Polk Street in Avenue 62 to its intersection with Tyler Street. From the intersection of Avenue 62 and Tyler Street, a 12-inch diameter waterline will be constructed north on Tyler Street to Avenue 60. From the intersection of Avenue 62 and Tyler Street, a 30-inch waterline will be constructed west to the project boundary. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-I, Phasing Scenario 3 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	B-3	39.01	117
	C-1	40.77	122
<i>Subtotal</i>		79.78	239
Medium High Density Residential	B-1	30	210
	B-2	25.24	177
	B-5	32.70	229
	B-6	26.18	183
	C-2	31.16	218
	C-4	49.83	349
	C-8	14.96	105

Land Use	Planning Areas	Acreage	Units
<i>Subtotal</i>		<i>210.07</i>	<i>1,471</i>
Very High Density Residential	C-6	30.85	477
<i>Subtotal</i>		<i>30.85</i>	<i>477</i>
Commercial Retail	A-5	17.61	
<i>Subtotal</i>		<i>17.61</i>	
Open Space	C-5	9.97	
<i>Subtotal</i>		<i>9.97</i>	
Scenario 3 Total		348.28	2,187

Phasing Scenario 4

Phasing Scenario 4 site is located in the central portion of the project area and abuts the southern boundary of the Phasing Scenario 2 and the northern boundary of Phasing Scenario 2. The infrastructure improvements described in this section assume that Scenario 4 would be the first area of the site to develop. Transportation improvements would involve expressway, major and secondary roadways, including partial width of Polk Street between Avenue 62 and Avenue 64; Avenue 64 between Tyler Street and Polk Street; "C" Street from Avenue 64 to Avenue 62; "D" Street from Polk to Avenue 62; Avenue 62 between "C" Street; and Tyler Street between Avenue 64 and the northwest corner of Phase 4. Grading activities for this scenario would be limited to the Scenario 4 site, with the exception of a portion of open space area D-1 in the Scenario 2 site and a portion of open space areas K-1 and J-2 in the Scenario 1 site, which would be used for a borrow/stockpile area or detention/retention areas. Drainage for this scenario would require that off-site flows be picked up in interim swales along the northerly and westerly boundaries, conveyed through the site and then released to the Scenario 1 site immediately to the south. In addition, master planned off-site retention facilities would be required immediately to the south. The retention facilities would have overflow weirs on the downstream side to return the storm flow to its original sheet flow condition. Sewer facilities would either gravity flow to the north to a CVWD gravity line in Avenue 62 or flow through the site to the southeast corner of the scenario to an existing sewer pump station at Polk and Avenue 64. From there, an existing force main will convey the flows to the existing wastewater treatment facility. Water facilities to support the development of this scenario begin with the extension of an existing 24-inch diameter waterline at the intersection of Avenue 66 and Tyler Street to Polk Street and north in Polk Street to Avenue 62. A 24-inch diameter waterline will be constructed in Avenue 62 between Polk Street and "C" Street. A 12-inch diameter waterline will be constructed in Avenue 64 between Tyler Street and Polk Street and within the phase. Two wells will be constructed along the waterline alignment to provide adequate water supply and backup. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-J, Phasing Scenario 4 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	F-2	71.64	143
	G-5	34.6	69
	G-10	18.57	37
	G-11	26.86	70
	G-13	14.17	53
	H-9	20.03	66
<i>Subtotal</i>		<i>185.87</i>	<i>438</i>
Medium High Density Residential	F-3	79.94	635
	G-7	60.38	432
	H-2	18.65	118
	H-4	22.4	141
	H-5	7.83	45
	H-7	20.99	151
<i>Subtotal</i>		<i>210.19</i>	<i>1,522</i>
High Density Residential	G-8	20.6	282
	H-8	13.55	189
<i>Subtotal</i>		<i>34.15</i>	<i>471</i>
Commercial Retail	F-4	6.23	
	G-1	4.43	
<i>Subtotal</i>		<i>10.66</i>	
Open Space	G-2	1.44	
	G-3	1.19	
	G-4	5.6	
	G-6	13.65	
	G-9	6.17	
	G-12	5.35	
	H-1	5.12	
	H-3	3.79	
	H-6	11.43	
	H-10	3.96	
<i>Subtotal</i>		<i>57.7</i>	
Scenario 4 Total		498.57	2,431

Project Phasing Standards

- 1) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped areas for that stage of development, shall be submitted to the County Planning Department for approval. The improvement plans shall include, but not be limited to the following:
 - Final grading plan.
 - Irrigation plans certified by a landscape architect.
 - A hardscaping plan with location, type and quantity of potential recreational amenities/facilities prepared by a licensed landscape architect.
 - Wall or fencing treatment details consistent with the Specific Plan.

Plans for landscaping, grading and irrigation systems shall be submitted to the Coachella Valley Water District for review. This is to ensure efficient water management.

- 2) Construction of the development permitted hereby, including recordation of final subdivision maps, may be done progressively in stages, provided adequate vehicular access is constructed for all dwelling units in each stage of development and further provided that such phase of development conforms substantially with the Specific Plan Phasing Program.

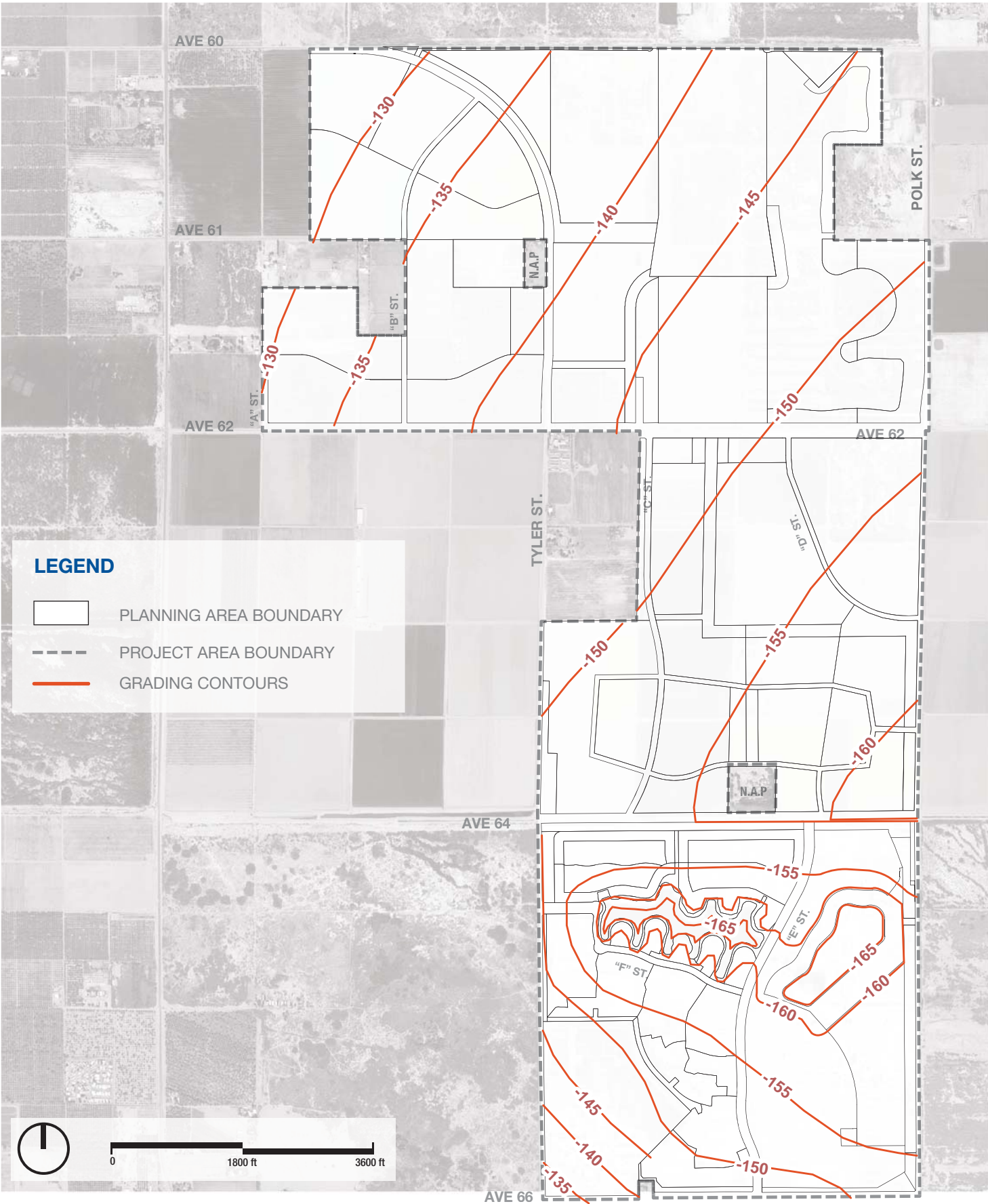
3.1.8 Illustrative Grading Plan

The study area is relatively flat with approximately 45 feet of topographical relief. The existing topography tends to slope from the northwest to the southeast at a nominal rate of between 0.30 percent and 0.40 percent. Earthwork characteristics and grading recommendations as presented in the Geotechnical Feasibility Report were utilized in this study (EIR396 – Appendix B).

The grading concept illustrates site development feasibility and provides a “balanced” earthwork scenario not dependent upon import or export of material. Grading is designed to conform to the drainage conveyance requirements while following existing topographical patterns as reflected in **Figure 3.1-12, Grading Plan**. All development areas are designed with positive drainage towards acceptable drainage conveyances. Prior to the issuance of a grading permit from Riverside County, the developer's engineer shall submit the grading plans to the Coachella Valley Water District for review and approval. This is to ensure that all existing facilities and easements are taken into account prior to construction.

Grading Plan Development Standards

- 1) All grading activities shall be in substantial conformance with the overall Conceptual Grading Plan, the California Building Code and Riverside County Ordinance No. 457.
- 2) Prior to any development within any area of the Specific Plan, an overall Conceptual Grading Plan for the portion in process shall be submitted for Planning Department approval. The Grading Plan for each such area shall be used as a guideline for subsequent detailed grading plans for individual stages of development within that area, and shall include preliminary pad and roadway elevations.
- 3) Unless otherwise approved by the County of Riverside, Building and Safety Department, all cut and fill slopes shall be constructed at inclinations of no steeper than two (2) horizontal feet to one (1) vertical foot.
- 4) A grading permit shall be obtained from the County of Riverside, as required by the County Grading Ordinance, prior to grading.
- 5) Soil stabilizers should be used to control dust as required by SCAQMD Rule 403.
- 6) Erosion control practices shall be implemented during grading activities.
- 7) The grading contractor shall be required to obtain an encroachment permit from CVWD prior to entering any right-of-ways which belong to CVWD or USBR.
- 8) All projects proposing construction activities including: clearing, grading, or excavation that results in the disturbance of at least five acres total land area, or activity which is part of a larger common plan of development of five acres or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the specific plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program.



3.1.9 Comprehensive Maintenance Plan

Maintenance of open space, recreational facilities and roadways is important in establishing the appearance and quality of the Kohl Ranch Specific Plan area. It is anticipated that maintenance responsibilities for the project will be shared by several entities, as outlined below.

Parks and Recreation

Local parks will be dedicated to the Coachella Valley Recreation and Parks District (CVRPD), the new special district created to maintain Open Space as described in 8.b, below, or will remain private. The CVRPD, the new district, or Home Owners Association (HOA) will be responsible for park maintenance and recreation programs associated with these facilities.

Open Space

To maintain project open space (arterial and local streetscapes, trails, drainage courses, and natural open space), a new special district is required, such as a landscaping and lighting district, community services district, or county service area. This decision will be made at a future stage in project development, in concert with County agencies or HOA. The special district would contract with an existing local agency or a private firm for actual services.

Street Lighting

To operate and maintain local street lighting, a new special district is required, such as a landscaping and lighting district, community services district, or county service area. This decision will be made at a future stage in project development, in concert with County agencies. The special district would contract with an existing local agency or a private firm for actual services.

Drainage Facilities

CVWD shall not be responsible for the construction, operation or maintenance of stormwater/drainage facilities within the project area. CVWD shall review the hydrology design of the project and evaluate how drainage from the site could affect regional stormwater facilities. The drainage plan shall be subject to mitigation based upon the findings of the CVWD.

Riverside County and a property owners association or County Service Area are assumed to be responsible for maintaining any storm drains, canals or basins within the project area. Riverside County will not be responsible for drainage maintenance unless specifically indicated by the County.

Project Roadways

All public project roadways and private streets will be designed and constructed to standards acceptable to the County. All public roadways will be entered into the Riverside County System of roads for operation and maintenance as approved by the Board of Supervisors.

3.1.10 Airport Zones and Utility Easement Beltway

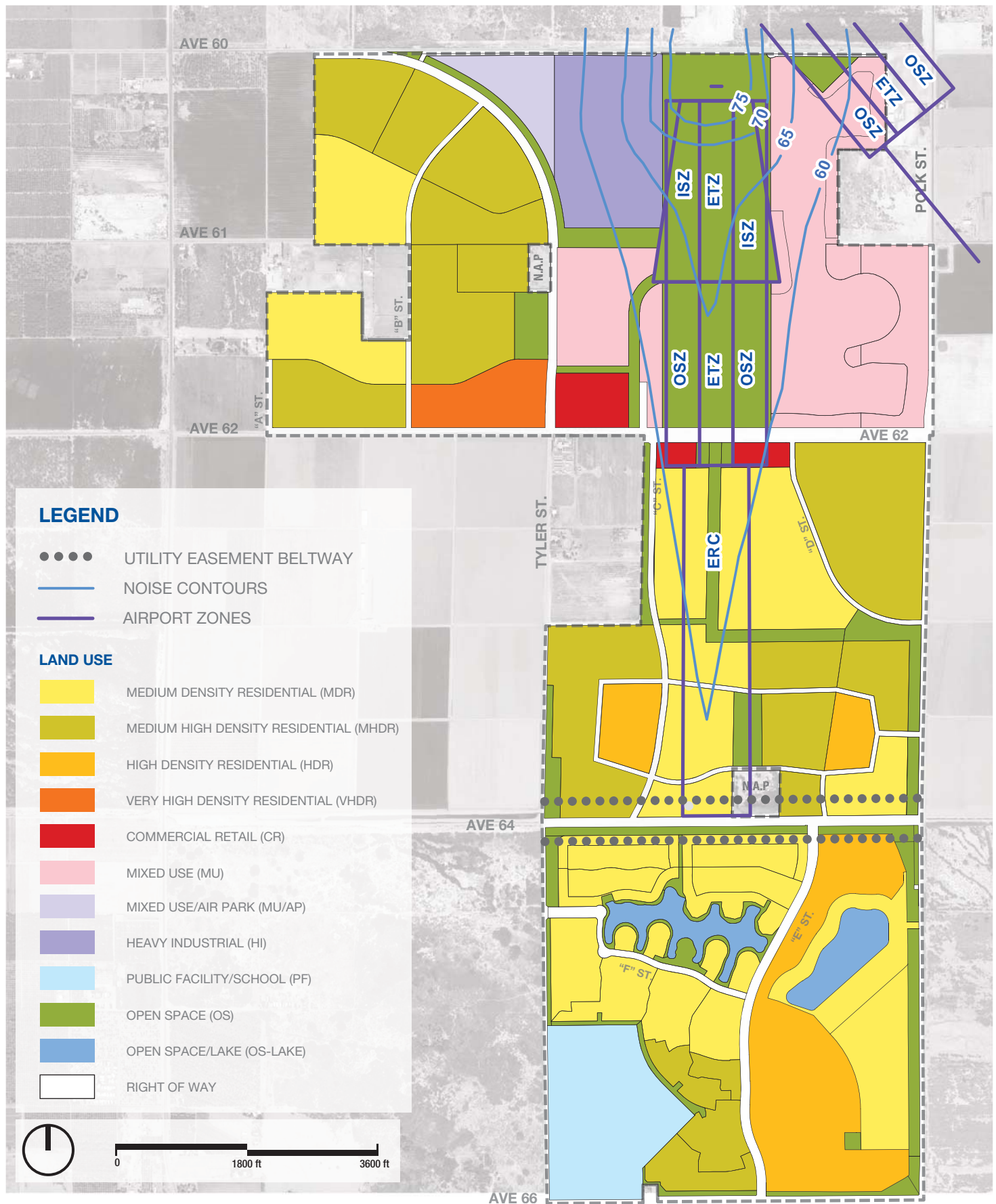
Jacqueline Cochran Regional Airport Land Use Compatibility

The Kohl Ranch site is located immediately south of Jacqueline Cochran Regional Airport, a general aviation transport airport. Based on proximity of the site to the airport, the Specific Plan has been designed to meet the following objectives:

- reflect current use of and future plans for the airport;
- respond to constraints posed by the airport influence area, and ensure land use compatibility with respect to noise, safety and height; and
- maximize opportunities presented by proximity to this resource through careful design of the project circulation system and strategic location of employment-based uses.

In October 1990, the Riverside County Department of Aviation prepared the Thermal Airport Master Plan. In December 2004, the Riverside County Economic Development Agency prepared a new Airport Master Plan for the renamed Jacqueline Cochran Regional Airport. The Master Plan identifies the potential for limited commercial air carrier and air cargo service and evaluates several alternatives for expansion of the airport to meet current and future demands through the year 2013. Plans for both airside and landside improvements are identified, including: extension of Runway 17-35 across Avenue 60 onto the Kohl Ranch property; retention of Runway 12-30 as the crosswind runway; general aviation terminal through expansion/redevelopment of existing facilities; separate air carrier and air cargo area; additional T-hangars; and heliport. The Master Plan calls out property acquisition of approximately 128 acres south of Avenue 60 for expansion of runway 17-35. The Master Plan also delineates Airport Safety Zones and noise contours related to planned airport operations as reflected in **Table 3.1-K, Airport Safety Zones** and depicted on **Figure 3.1-13, Airport Safety Zones and Utility Easement Beltway**.

In accordance with State requirements, the County Airport Land Use Commission (ALUC) prepared the Comprehensive Land Use Plan (CLUP) for Thermal Airport in August 1992 and updated it in 2005. The CLUP designates an airport influence area and includes land use compatibility guidelines that address airport noise, safety, height restrictions, and general concerns related to aircraft overflight. The "airport influence area" around Jacqueline Cochran Regional Airport is divided into six compatibility zones. Five zones affect the Kohl Ranch Specific Plan, as shown on **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**.



THE KOHL RANCH
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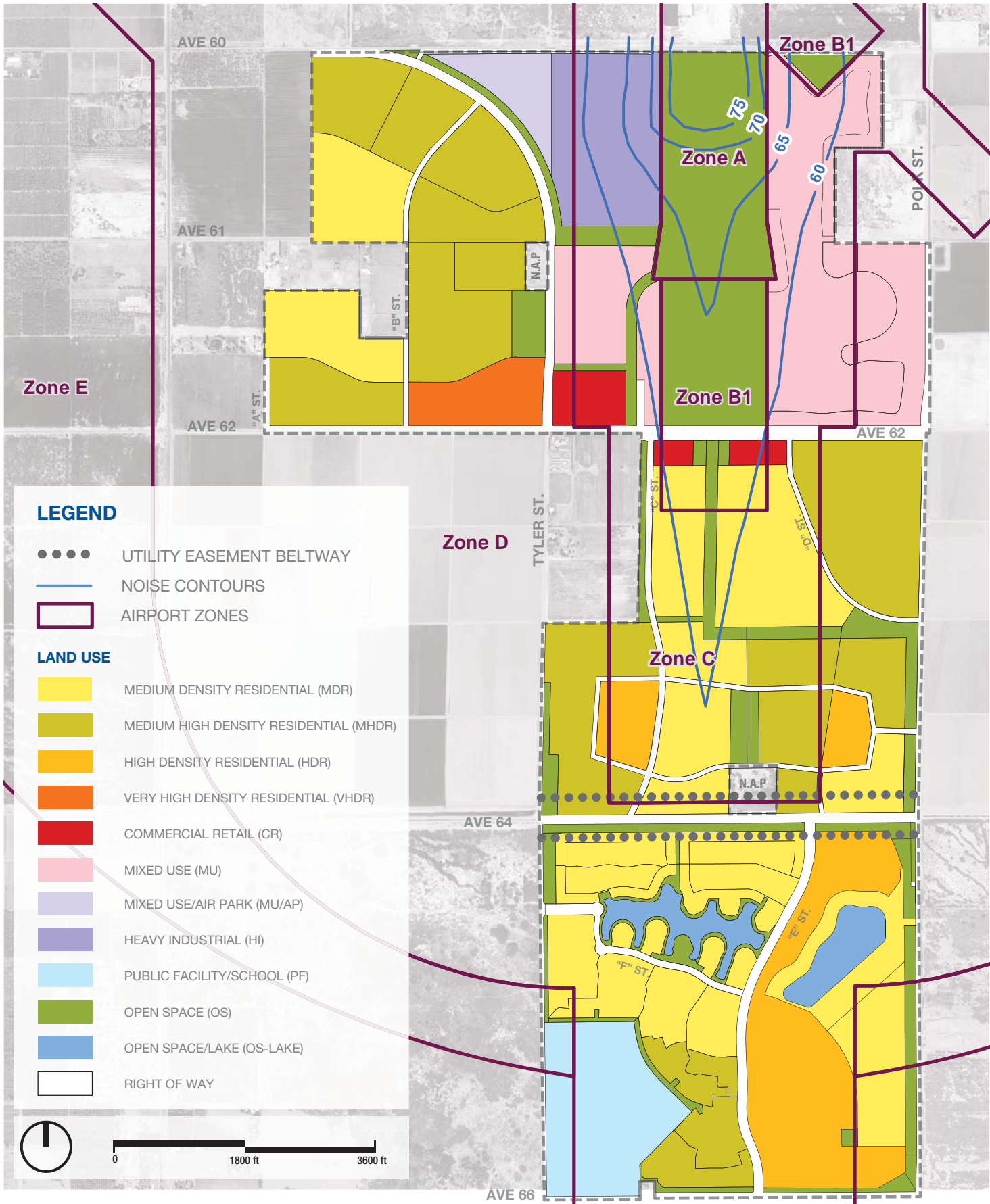


Table 3.1-K, Airport Safety Zones

Zone		Maximum Density	Maximum Lot Coverage by Structures	Land Use
Symbol	Name			
ISZ	Inner Safety Zone	0 (No structures permitted)	0 (No structures permitted)	No petroleum or explosives storage. No petroleum or natural gas pipelines. No above-ground power lines.
OSZ	Outer Safety Zone	Uses in structures?: 25 persons/acre Uses not in structures: 50 persons/acre	25% of net area	No residential, hotels, places of public assembly, public utility stations/plants, and facilities that process flammable materials, or that could be damaged in an aircraft accident, among others.
ETZ	Emergency Touchdown Zone	0 (No structures permitted)	0 (No structures permitted)	No structures or other objects that would represent obstructions are allowed, such as but not limited to: large trees, heavy fences and walls, tall and steep berms and retaining walls, non-frangible streetlight and sign standards, billboards.
ERC	Extended Runway Centerline Zone	3 du/net acre. Uses in structures?: 100 persons/acre.	50% of gross area or 65% of net area, whichever is greater.	No uses involving as the primary activity, manufacture, storage or distribution of explosives or flammable materials.

Source: Jacqueline Cochran Regional Airport Master Plan, December 14, 2004.

The areas of the Kohl Ranch project amended/modified by SPA4 and CZ07952 were determined by ALUC to be inconsistent with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (JCRALUCP) as amended in 2006, on the basis that the proposal would result in an exceedance of the permissible numbers of dwelling units within a portion of the property located in Compatibility Zone C, which limits residential density to one dwelling unit per five acres.. However, Section 21676.5 of the California Public Utilities Code states that the County of Riverside may overrule the ALUC's inconsistency determination if, after a public hearing, the County makes findings that the project is consistent with the purposes set forth in Public Utilities Code Section 21670.

On November 6, 2018, the Riverside County Board of Supervisors approved Specific Plan Amendment No. 4 and found it consistent with the purposes set forth in Public Utilities Code Section 21670 and approved to overrule the ALUC inconsistency determination because, the proposed Project is entirely within Compatibility Zone C and the lots within Compatibility Zone C that would be allowed for overnight occupancy are located on the edge of the zone, adjacent to Compatibility Zone D, where all the existing lots are allowed to have overnight occupancy. The remaining lots in Zone C, on the west side of the racetrack, directly adjacent to Compatibility Zone A, would continue to be prohibited from having overnight occupancy. Therefore, the Project minimizes the potential safety impacts by not allowing overnight stays in the areas closest to the runway thereby reducing the potential for a serious hazard to the public in the event of a potential aircraft accident on the Project site. Indeed, the Racetrack

Recreational Units are located along the perimeter of Thermal Motorsport Racetrack and are consistent and complimentary with surrounding land uses to the east, south, and west that include various amenities and uses that support the racetrack. Therefore, as demonstrated, the site is consistent with Public Utility Code section 21670(a)(1) and (2). Moreover, the Project does not represent a hazard to aircraft in typical flight because the Project does not propose the construction of any new buildings that were not previously considered under the Kohl Ranch Specific Plan, and all buildings are required to comply with all applicable FAA requirements. Therefore, the Project would not result in a safety hazard to aircraft in flight, consistent with Public Utility Code section 21670(a)(1) and (2).

Airport Noise Compatibility Guidelines

The CLUP presents noise compatibility guidelines for the Jacqueline Cochran Regional Airport vicinity, as follow:

- Discourage new single-family dwellings and prohibit mobile homes within the 60 CNEL contour. Where homes are permitted within the 60 CNEL contour, the need for sound insulation should be studied and noise easements should be acquired.
- Within the 65 CNEL, new residential construction should not be undertaken. New hotels or motels are permitted if the need for sound insulation is evaluated.
- Institutional uses should be discouraged within the 65-70 CNEL range. If no alternative location is available, the need for sound insulation should be studied.
- Commercial, industrial and recreational uses are considered compatible with noise levels between the 65 and 70 CNEL.

The Kohl Ranch property is impacted by noise from Jacqueline Cochran Regional Airport. To address airport noise in the Specific Plan, primarily open space uses are located within the 65, 70, and 75 CNEL noise contours. A limited amount of airpark/mixed use and industrial uses are within the 65 CNEL contour, consistent with the guidelines. Land uses within the 60 CNEL contour include open space, industrial and commercial retail uses. Some medium density residential uses also are within the 60 CNEL contour. Cluster provisions have been incorporated into Section 2.0 – Specific Plan Zoning so that residential uses can be located outside areas impacted by airport noise. In addition, common walls within medium high, high and very high density units are anticipated to attenuate airport noise within the 60 CNEL contour.

Airport Vicinity Height Guidelines

The Federal government has developed standards for determining obstructions in navigable airspace. Federal Aviation Regulations Part 77 defines a variety of imaginary surfaces at certain altitudes around airports. The Part 77 surfaces include the primary surface, approach surface, transitional surface, horizontal surface, and conical surface. Collectively, the Part 77 surfaces around an airport define a bowl-shaped area with ramps sloping up from each runway end. The Part 77 standards are not absolute height restrictions, but instead identify elevations at which structures may present a potential safety problem. Penetrations of the Part 77 surface generally are reviewed on a case by case basis. The CLUP uses the Part 77 criteria as the basis for height limitations in the vicinity of Jacqueline Cochran Regional Airport.

Height limitations are not anticipated to pose a development constraint for the Kohl Ranch site. Based on the Part 77 surfaces identified in the CLUP, structures above approximately 85 feet in height in the vicinity of Avenue 62, 150 feet in height in the vicinity of Avenue 64, and 315 feet in height in the vicinity of Avenue 66, could potentially create a problem. Development in these areas is not expected to exceed the relevant height limitations.

Airport Safety Compatibility Guidelines

The CLUP for Jacqueline Cochran Regional Airport establishes land use compatibility guidelines for land use compatibility zones that affect the project site. These guidelines are summarized below in **Table 3.1-L, Basic Compatibility Criteria**.

In addition to the guidelines for the five zones, the CLUP prohibits particularly hazardous land uses in all designated safety zones. These land uses include uses which would impede the ability of the pilot to see the airfield as identified in Footnote 1 to **Table 3.1-L**.

Open space is concentrated within the most restrictive safety zones, the Emergency Touchdown Zone (ETZ) and the Inner Safety Zone (ISZ). Land uses proposed in the Specific Plan within the Outer Safety Zone (OSZ) are limited to open space, commercial, and mixed use land uses. The Extended Runway Centerline (ERC) zone, while less restrictive, limits the density of uses within the zone. Consequently, open space and medium density residential uses have been sited within this area. Residential cluster provisions incorporated into Section 2.0 – Specific Plan Zoning, encourage common open space areas to be located within the ERC, with transfer of residential density to areas outside the safety zone.

Airport Compatibility Design Standards for Mixed Use

Mixed Use structures in Planning Areas E-2 through E-8 of The Kohl Ranch Specific Plan, allowing for overnight occupancy shall incorporate the following features:

- Signage depicting areas allowing overnight occupancy from areas of non-overnight occupancy.

Airport Compatibility Design Guidelines for Lakes

Recreational lakes within The Kohl Ranch Specific Plan shall be designed to incorporate the following features in order to reduce or eliminate the habitat and foraging opportunities for birds in the water areas:

- Use of linear shaped water areas.
- Formed, vertical banks that make access to land at the water's edge difficult.
- Restrict any vegetation that offers shelter and nesting opportunities.
- Do not include islands in water areas.
- Eliminate shallows and muddy areas.
- Control availability of forage – fish, frogs, algae and insects.
- Provide for human activity in or near the water.

Through the incorporation of these features into the design of the recreational lakes, the lakes will not attract the type or concentration of bird activity that would create a hazard to aircraft in flight.

Utility Easement Beltway

A utility easement beltway traverses the site in an east-west direction, roughly along the Avenue 64 right-of-way. This beltway contains easements for CVWD irrigation pipes, the Avenue 64 Evacuation Channel, and a power line easement for an existing 161 kV power line. This area is proposed to remain in street right of way and open space, and to incorporate an element of the project-wide trail system, connecting with parks and public facilities throughout the site. Residential development in the planning areas immediately to the north of the easement beltway would be clustered outside of the beltway, to maintain the residential densities in these areas and preserve open space areas.

Table 3.1-L, Basic Compatibility Criteria

Zone	Locations	Maximum Densities / Intensities					Additional Criteria	
		Residential (d.u./ac) ¹	Average ⁶	Single Acre ⁷	with Bonus ⁸	Req'd Open Land ³	Prohibited Uses ⁴	Other Development Conditions ⁵
A	Runway Protection Zone and within Building Restriction Line	0	0	0	0	All Remaining	<ul style="list-style-type: none"> › All structures except ones with location set by aeronautical function › Assemblages of people › Objects exceeding FAR Part 77 height limits › Storage of hazardous materials › Hazards to flight ⁹ 	<ul style="list-style-type: none"> › Aviation easement dedication
B1	Inner Approach/Departure Zone	0.05 (average parcel size ≥20.0 ac.)	25	50	65	30%	<ul style="list-style-type: none"> › Children's schools, day care centers, libraries › Hospitals, nursing homes › Places of worship › Bldgs with >2 aboveground habitable floors › Highly noise-sensitive outdoor nonresidential uses ¹⁰ › Aboveground bulk storage of hazardous materials ¹¹ › Critical community infrastructure facilities ¹² › Hazards to flight ⁹ 	<ul style="list-style-type: none"> › Locate structures maximum distance from extended runway centerline › Minimum NLR of 25 dB in residences (including mobile homes) and office buildings ¹³ › Airspace review required for objects >35 feet tall ¹⁴ › Aviation easement dedication
B2	Adjacent to Runway	0.1 (average parcel size ≥10.0 ac.)	100	200	260	No Req't	Same as Zone B1	<ul style="list-style-type: none"> › Locate structures maximum distance from runway › Minimum NLR of 25 dB in residences (including mobile homes) and office buildings ¹³ › Airspace review required for objects >35 feet tall ¹⁴ › Aviation easement dedication
C	Extended Approach/Departure Zone	0.2 (average parcel size ≥5.0 ac.)	75	150	195	20%	<ul style="list-style-type: none"> › Children's schools, day care centers, libraries › Hospitals, nursing homes › Bldgs with >3 aboveground habitable floors › Highly noise-sensitive outdoor nonresidential uses ¹⁰ › Hazards to flight ⁹ 	<ul style="list-style-type: none"> › Minimum NLR of 20 dB in residences (including mobile homes) and office buildings ¹³ › Airspace review required for objects >70 feet tall ¹⁵ › Deed notice required
D	Primary Traffic Patterns and Runway Buffer Area	(1) ≤0.2 (average parcel size ≥5.0 ac.) or ¹⁶ (2) ≥5.0 (average parcel size ≤0.2 ac.)	100	300	390	10%	<ul style="list-style-type: none"> › Highly noise-sensitive outdoor nonresidential uses ¹⁰ › Hazards to flight ⁹ 	<ul style="list-style-type: none"> › Airspace review required for objects >70 feet tall ¹⁵ › Children's schools, hospitals, nursing homes discouraged ¹⁷ › Deed notice required
E	Other Airport Environs	No Limit	No Limit ¹⁸		No Req't		<ul style="list-style-type: none"> › Hazards to flight ⁹ 	<ul style="list-style-type: none"> › Airspace review required for objects >100 feet tall ¹⁵ › Major spectator-oriented sports stadiums, amphitheaters, concert halls discouraged beneath principal flight tracks ¹⁸
*	Height Review Overlay	Same as Underlying Compatibility Zone		Not Applicable			Same as Underlying Compatibility Zone	<ul style="list-style-type: none"> › Airspace review required for objects >35 feet tall ¹⁴ › Aviation easement dedication

See Chapter 3 for airport-specific additions or exceptions to these policies

NOTES:

- ¹ Residential development must not contain more than the indicated number of dwelling units (excluding secondary units) per gross acre. Clustering of units is encouraged. See Policy 4.2.5 for limitations. Gross acreage includes the property at issue plus a share of adjacent roads and any adjacent, permanently dedicated, open lands. Mixed-use development in which residential uses are proposed to be located in conjunction with nonresidential uses in the same or adjoining buildings on the same site shall be treated as nonresidential development. See Policy 3.1.3(d).
- ² Usage intensity calculations shall include all people (e.g., employees, customers/visitors, etc.) who may be on the property at a single point in time, whether indoors or outside.
- ³ Open land requirements are intended to be applied with respect to an entire zone. This is typically accomplished as part of a community general plan or a specific plan, but may also apply to large (10 acres or more) development projects. See Policy 4.2.4 for definition of open land.
- ⁴ The uses listed here are ones that are explicitly prohibited regardless of whether they meet the intensity criteria. In addition to these explicitly prohibited uses, other uses will normally not be permitted in the respective compatibility zones because they do not meet the usage intensity criteria.
- ⁵ As part of certain real estate transactions involving residential property within any compatibility zone (that is, anywhere within an airport influence area), information regarding airport proximity and the existence of aircraft overflights must be disclosed. This requirement is set by state law. See Policy 4.4.2 for details. Easement dedication and deed notice requirements indicated for specific compatibility zones apply only to new development and to reuse if discretionary approval is required.
- ⁶ The total number of people permitted on a project site at any time, except rare special events, must not exceed the indicated usage intensity times the gross acreage of the site. Rare special events are ones (such as an air show at the airport) for which a facility is not designed and normally not used and for which extra safety precautions can be taken as appropriate.
- ⁷ Clustering of nonresidential development is permitted. However, no single acre of a project site shall exceed the indicated number of people per acre. See Policy 4.2.5 for details.
- ⁸ An intensity bonus may be allowed if the building design includes features intended to reduce risks to occupants in the event of an aircraft collision with the building. See Policy 4.2.6 for details.
- ⁹ Hazards to flight include physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations. Land use development that may cause the attraction of birds to increase is also prohibited. See Policy 4.3.7.
- ¹⁰ Examples of highly noise-sensitive outdoor nonresidential uses that should be prohibited include amphitheatres and drive-in theaters. Caution should be exercised with respect to uses such as poultry farms and nature preserves.
- ¹¹ Storage of aviation fuel and other aviation-related flammable materials on the airport is exempted from this criterion. Storage of up to 6,000 gallons of nonaviation flammable materials is also exempted. See Policy 4.2.3(c) for details.
- ¹² Critical community facilities include power plants, electrical substations, and public communications facilities. See Policy 4.2.3(d) for details.
- ¹³ NLR = Noise Level Reduction, the outside-to-inside sound level attenuation that the structure provides. See Policy 4.1.6.
- ¹⁴ Objects up to 35 feet in height are permitted. However, the Federal Aviation Administration may require marking and lighting of certain objects. See Policy 4.3.6 for details.
- ¹⁵ This height criterion is for general guidance. Shorter objects normally will not be airspace obstructions unless situated at a ground elevation well above that of the airport. Taller objects may be acceptable if determined not be obstructions. See Policies 4.3.3 and 4.3.4.
- ¹⁶ Two options are provided for residential densities in *Compatibility Zone D*. Option (1) has a density limit of 0.2 dwelling units per acre (i.e., an average parcel size of at least 5.0 gross acres). Option (2) requires that the density be *greater than* 5.0 dwelling units per acre (i.e., an average parcel size *less than* 0.2 gross acres). The choice between these two options is at the discretion of the local land use jurisdiction. See Table 2B for explanation of rationale. All other criteria for *Zone D* apply to both options.
- ¹⁷ Discouraged uses should generally not be permitted unless no feasible alternative is available.
- ¹⁸ Although no explicit upper limit on usage intensity is defined for *Zone E*, land uses of the types listed—uses that attract very high concentrations of people in confined areas—are discouraged in locations below or near the principal arrival and departure flight tracks. This limitation notwithstanding, no use shall be prohibited in *Zone E* if its usage intensity is such that it would be permitted in *Zone D*.

Source: Riverside County Airport Land Use Compatibility Plan Policy Document (adopted October 2004)

3.1.11 Specific Plan Implementation and Administrative Process

Pursuant to Government Code Section 65451 all specific plans must contain a "program of implementation measures including regulations, programs, public works projects, and financing measures" necessary to implement the specific plan. This section outlines the methods by which development in the specific plan will be processed, including infrastructure financing mechanisms and other necessary programs.

As it relates to the implementation and administrative process, Riverside County Zoning Ordinance 348 shall be adhered to at all times.

Specific Plan Time Frames

Riverside County requires the adoption of a phasing plan for each Specific Plan (SP). Each plan must be monitored for reasonable progress toward implementation. The phasing program for the Kohl Ranch Specific Plan is described above in Section 3.1.7 – Public Facility Sites and Project Phasing. The project developer will work with the County to adhere to the phasing plan and to ensure the logical and timely completion of the project.

Financing Plan

The developer shall be responsible for financing construction of the infrastructure improvements required to support the Project, such as perimeter and internal streets, water lines, sewers, and storm drains. All necessary infrastructure improvements shall be developed in conjunction with the roadway improvements. The financing of construction, operation, and maintenance of public improvement and facilities will include funding through a combination of financing mechanisms. However, the developer shall be ultimately responsible for all fair share costs associated with implementing the Project, including but not limited to the costs of providing infrastructure and complying with mitigation measures, conditions of approval, and other requirements of the Project.

Financing may involve a combination of impact fees and exacting, special assessment districts, landscaping and lighting districts, and other mechanisms agreed to by the developer and the County of Riverside as noted below. Developer funded improvements may be subject to a reimbursement agreement or credits against fees pursuant to provisions of a development agreement or conditions of approval. The County and developer will cooperate to ensure that the public facilities are built in accordance with all requirements of the Specific Plan and EIR. A development agreement and conditions of approval may be used to facilitate this process.

Developer Funding

In many cases, certain onsite facilities are tied directly to individual projects. In these cases, it is reasonable to expect the developer (or property owner) to pay the entire cost of the facility in order to secure development rights. Onsite local streets, utility connections from main trunk lines, and drainage facilities are good examples of facilities that are normally required concurrent with development of an individual parcel funded by the developer.

Special Assessment Districts

A special assessment district is a type of benefit district that requires a vote by the property owners to encompass a defined and limited geographic area. The County or other agencies may form a special assessment district under one of several different statutory acts to construct

public improvements such as streets, storm drains, sidewalks, streetlights, sewers, parks landscape, and other similar capital facilities. The special assessment district can issue bonds to finance those improvements and levy a special assessment to pay debt service on those bonds.

A special assessment district may fund improvements within the entire Specific Plan area or smaller areas in the Specific Plan where special improvements are constructed that directly benefit only certain property owners. Special assessments districts may only be used to pay for projects that are of specific and direct benefit to the property owner being assessed. The amount of the assessment must directly relate to the amount of benefit received by the property owner.

Landscaping and Lighting Districts

Landscaping and lighting districts (LLD) may be used for installation, maintenance, and servicing of landscaping and lighting through annual assessments on benefiting properties. LLDs may also provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities.

Other Funding Sources

Other sources may be available to finance improvement projects, such as government grants, private developer coalitions, or various types of bonds not listed above.

Dwelling Unit Tracking System

Each Residential Planning Area within the Kohl Ranch Specific Plan has an identified density range consistent with the County General Plan, Flexibility Range, and a Target number of dwelling units as identified in **Table 3.1-M, Residential Planning Area Flexibility Range**. The Flexibility Range is consistent with a variation of no more than 10% from the allowable density range. The Flexibility Range was determined based on several factors: (1) ultimately the Planning Area acreage may change as a result of final engineering, (2) the flexibility may be necessary to accommodate airport restrictions which continue to change over time, and (3) based on the fact that there is an overall unit cap within the SP. Flexibility can be given to the density range and target number of dwelling units within each Residential Planning Area, under certain circumstances, as described below. Flexibility can be given to the density range and target number of dwelling units within each residential Planning Area, under certain circumstances, as described below.

Additionally, all Planning Area acreages may be revised when more accurate information is available. Up to a 15 percent variation in Planning Area acreage may be allowed without a Specific Plan Amendment or determination of substantial project conformance with adopted specific plan, but will be required to receive administrative approval. A 16 to 30 percent variation in Planning Area acreage may be allowed with a 'determination of substantial project conformance with adopted specific plan,' and any acreage change in Planning Area acreage of over 30 percent will require a Specific Plan Amendment. A Planning Area acreage change would not result in a Flexibility Range change, without requiring a Specific Plan Amendment. This accommodation should cover reasonable changes in planning area size caused by roadway realignments such as the one that precipitated changes reflected in Specific Plan No. 303 Amendment 2 (SP00303A2).

Any developer proposing residential development within the Kohl Ranch Specific Plan, either through individual or multiple Planning Areas, will be required to report how many dwelling units will be developed within each Planning Area, as shown on **Table 3.1-N, Reporting Mechanism**, and if any modifications to the Land Use Table are necessary. This report will need to be submitted to the County of Riverside Planning Department to ensure the total number of dwelling units will not exceed 7,171. **Table 3.1-O, Master Log**, will be the running tabular record of all proposed and developed dwelling units, for use by the Planning Department.

Table 3.1-M, Residential Planning Area Flexibility Range

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range DU/AC	Flexibility Range DU/AC
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0	4.5-8.8
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0	4.5-8.8
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0	1.8-5.5
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0-8.0	4.5-8.8
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0	4.5-8.8
C-1	Medium Density Residential (MDR)	122	40.77	2.0-5.0	1.8-5.5
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0	4.5-8.8
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0	4.5-8.8
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0	12.6-22.0
C-8	Medium High Density Residential (MHDR)	105	14.96	5.0-8.0	4.5-8.8
F-2	Medium Density Residential (MDR)	143	71.64	2.0-5.0	1.8-5.5
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0	4.5-8.8
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0	1.8-5.5
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0	4.5-8.8
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0	7.2-15.4
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0	1.8-5.5
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0	1.8-5.5
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0	1.8-5.5
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0	4.5-8.8
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0	4.5-8.8
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0	4.5-8.8
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0-8.0	4.5-8.8
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0	7.2-15.4
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0	1.8-5.5
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0	1.8-5.5
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0	1.8-5.5
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0	1.8-5.5
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0	1.8-5.5
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0	1.8-5.5

I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0	1.8-5.5
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0	1.8-5.5
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0	1.8-5.5
J-1	Medium Density Residential (MDR)	143	30.64	2.0-5.0	1.8-5.5
J-4	Medium Density Residential (MDR)	189	39.42	2.0-5.0	1.8-5.5
L-1	High Density Residential (HDR)	1,158	120.46	8.0-14.0	7.2-15.4
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0	1.8-5.5
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0	1.8-5.5
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0	1.8-5.5
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0	1.8-5.5
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0	1.8-5.5
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0	1.8-5.5
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0	1.8-5.5
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0	1.8-5.5
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0	4.5-8.8
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0-8.0	4.5-8.8
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0-8.0	4.5-8.8
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0	4.5-8.8
TOTAL		<u>7,001</u>	<u>1,143.73</u>		

Table 3.1-N, Reporting Mechanism

Application Case #							
PA	Acres in PA	Density Range		Flexibility Range		Proposed Number of Dwelling Units	Justification Statement (required if outside dwelling unit range)

Table 3.1-O, Master Log

Planning Case No, TTP, or TPM	Residential PA	Gross Acres	Target Number of SU	Density Ranch DU	Flexibility Range DU	Actual DU Implemented At:			Unit Pool Balance	Unit Balance
						TTP or TTP Approval	Final Map Recordation	Actual Building Permit		

Development within the Dwelling Unit Range

The Density Range is established at the time of the adoption of the Specific Plan. Any proposed development within the Density Range is in conformance with the Specific Plan. When completing the Reporting Mechanism, and if the proposed development is within the Density Range, a justification statement is not required, and a Specific Plan Amendment or Specific Plan Substantial Conformance Report is not required, as well.

Development Below or Above Density Range but Within the Flexibility Range

The Flexibility Range is defined as the range within 10 percent of the allowable Density Range for each Planning Area. The Flexibility Range gives more flexibility to the Density Range for each Planning Area to account for minor modifications, and is permitted under certain circumstances, without requiring a Specific Plan Amendment, but a Specific Plan Substantial Conformance Report is required.

The Developer, in requesting use of the Flexibility Range, shall state those special circumstances in **Table 3.1-N**, which would justify the use of the Flexibility Range. Examples that would justify use of the Flexibility Range include, but are not limited to: a decrease or increase in Planning Area size at the final engineering level; or the decrease of residential development acreage as a result of the development of parks, trails, or public facilities. The Planning staff shall determine if the request for the Flexibility Range is justified, and in granting approval of the Flexibility Range, staff shall find as follows:

- 1) That the Planning Area for the desired Flexibility Range is adequate in size and shape to accommodate proposed development; and
- 2) That the site for the proposed development contains streets and public facilities properly designed to carry the type and quantity of traffic generated by the subject development; and
- 3) That the proposed development will not impair the integrity and character of the Planning Area in which it is located or otherwise have an adverse effect on adjacent property or the permitted use thereof; and
- 4) That the proposed use is not contrary to this Specific Plan.

Development Outside of Flexibility Range

If the proposed development is below or above the existing allowable Density Range and the Flexibility Range, a Specific Plan Amendment will be required.

The Difference of Dwelling Units Developed Compared to the Target

Each residential Planning Area has a designated Dwelling Unit Target, which in total comprises 7,160 dwelling units. When a Planning Area is developed, a Residential Development Report, **Table 3.1-N, Reporting Mechanism**, is submitted to the Planning Department to record the number of dwelling units. Any units over or under the Dwelling Unit Target number of dwelling units will be added or taken away from the remaining balance of units to ensure the 7,171 maximum dwelling unit total for the Specific Plan is not exceeded and added or subtracted from the Unit Pool. When the Unit Pool reaches $50 \pm$ units, a redistribution to/from other planning areas shall be completed by the Master Developer and submitted to the County Planning Department.

3.2 LAND USE, PLANNING & DEVELOPMENT STANDARDS

Development standards for the Kohl Ranch have been established at four levels: Section 3.1 – Project-Wide Planning Standards, Section 3.3 – Design Guidelines, Section 3.4 – Landscape Design Guidelines, and Land Use, Planning & Development Standards, which are provided in this section.

Neighborhood Concept

The development proposal for the Kohl Ranch Specific Plan is founded upon the concept of self-sufficient neighborhoods, each defined by strong edges and an internal circulation system, and identified by a similar orientation, intent, and theme. Where appropriate, design guidelines are tailored to reinforce the unique character and theme of the project neighborhoods. Linking all of the neighborhoods is an extensive open space and trail system, relating to the airport safety zones and drainage ways, which provide residents and workers with active and passive recreational opportunities and scenic amenity. This open space network also serves to buffer incompatible uses and neighborhoods from one another as reflected in **Figure 3.2-1, Neighborhoods**. As described earlier in this chapter, neighborhoods in the northern portion of the site generally have a high concentration of employment-based uses, while neighborhoods in the southern portion are predominantly residential.

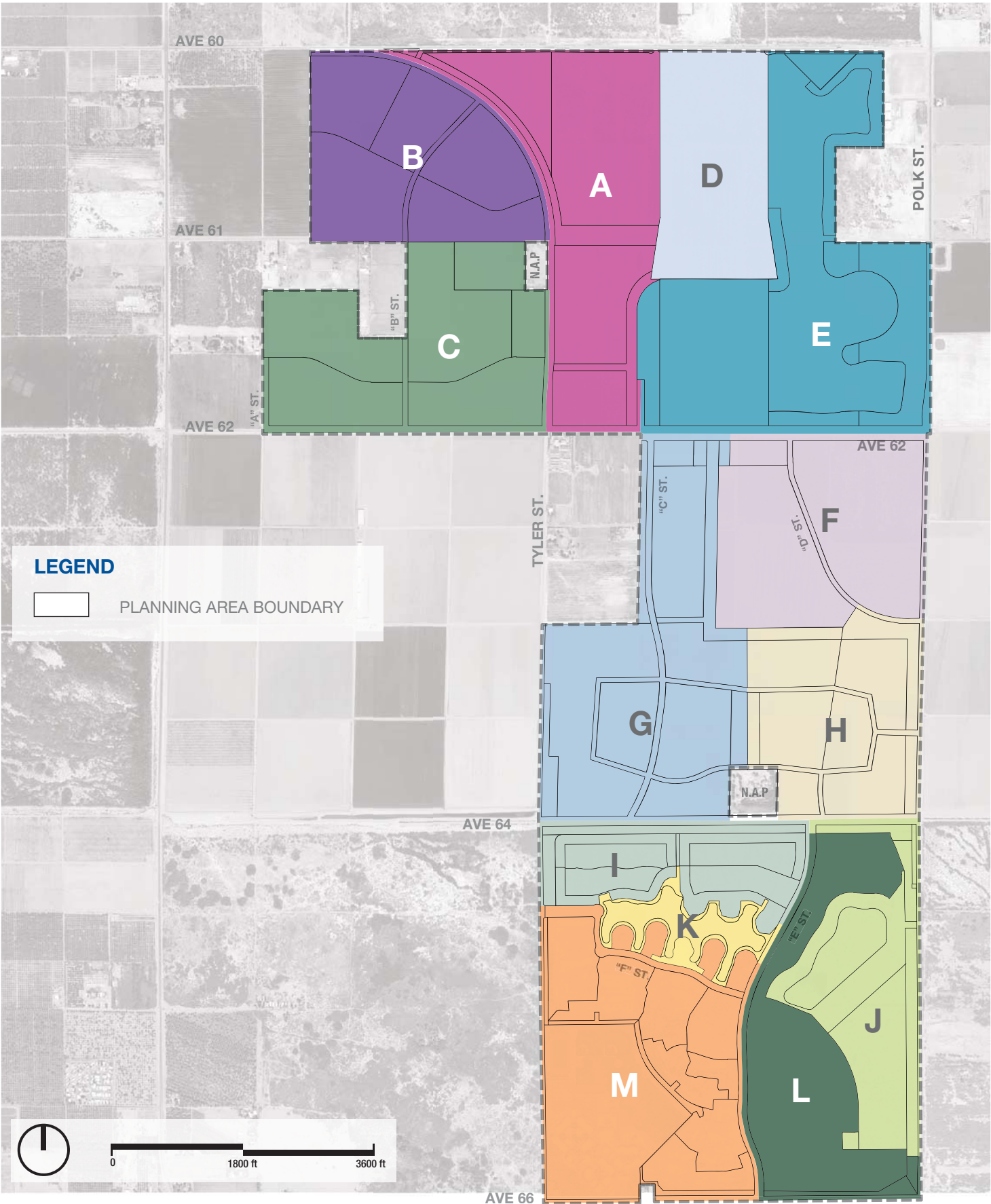
The Kohl Ranch Specific Plan identifies 95 planning areas on the site grouped within 13 larger areas referred to as neighborhoods. Neighborhoods define logical development areas based on land use and the relationship to planned roadways and infrastructure. The thirteen project neighborhoods were conceived to create cohesive, well-balanced communities, upon completion of the project. The neighborhoods relate generally to the phasing plan for the project, so that infrastructure and amenities can be readily provided as each neighborhood is developed. However, the neighborhood boundaries are slightly different than the boundaries of the project phasing scenarios which are based on the logical provision of infrastructure as well as overall market forces.

Definitions of the project planning areas are based upon logical, separate units of development, adjoining land uses, available infrastructure and physical landscape features.

Flexibility of the Standards

The Land Use, Planning & Development Standards described here and in Section 2 – Specific Plan Zoning Ordinance, are designed to provide a high degree of flexibility for future development of the Kohl Ranch site. This flexibility is critical, in order to respond to changing market forces during the life of the project, and to accommodate future conditions related to development of Jacqueline Cochran Regional Airport

This flexibility is reflected in provisions for alternate allowable land uses that are specified in the descriptions of neighborhoods and planning areas below. Such alternate land uses include golf courses and related facilities, other large recreational uses such as race track facilities and lakes. The conditions under which alternative land uses are allowed, along with the planned use of buffers and organization of land uses throughout the project will ensure that development of the project can be managed over time and that incompatibilities between adjacent land uses can be avoided.



Flexibility is also evident in the plan through the incorporation of provisions which allow the clustering of residential uses, through the transfer of private open space to common open space. As a result, overall residential densities remain the same, and a shared amenity is created. The cluster option is particularly well-suited to land within airport-restricted zones, and to planning areas that are adjacent to drainage facilities which may be expanded under a golf course scenario.

Organization of this Section

Detailed descriptions of the neighborhoods are provided below, followed by neighborhood-wide planning standards which describe airport-related constraints, allowable land uses, neighborhood access, and streetscape, entry and intersection treatments. Following the neighborhood descriptions and planning standards are descriptions and standards for each individual planning area. A brief description, summary of land use and development standards specified in Section 2 – Specific Plan Zoning Ordinance, and applicable planning standards are provided for each planning area. For each planning area, these standards describe such features as alternate allowable land uses and conditions, access, and edge and buffer treatments.

A foldout, plan view graphic is provided for each of the thirteen neighborhoods. Each of these graphics is located at the beginning of the corresponding neighborhood section, and should be kept open during review of the planning area descriptions and standards which precede it. These graphics indicate the location of all planning areas, access points, and all landscape features including streetscapes, intersection details, project entries, edges, and buffers.

Appropriate references in the text are made to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines. The graphics for the Design Guidelines are derived from the Community Structure Plan and are intended to convey the project intent with respect to open space, landscaping, site planning and architecture. These elements will create the framework for development of the site. Although development may conform closely to elements of the illustrative plans provided in this section, it is anticipated that actual development will respond to market conditions.

A Specific Plan Zoning Ordinance was prepared in conjunction with this Specific Plan document. The zoning provisions within that ordinance establish allowable uses and development standards for each planning area. The zoning provisions should be used in conjunction with the planning standards for each planning area.

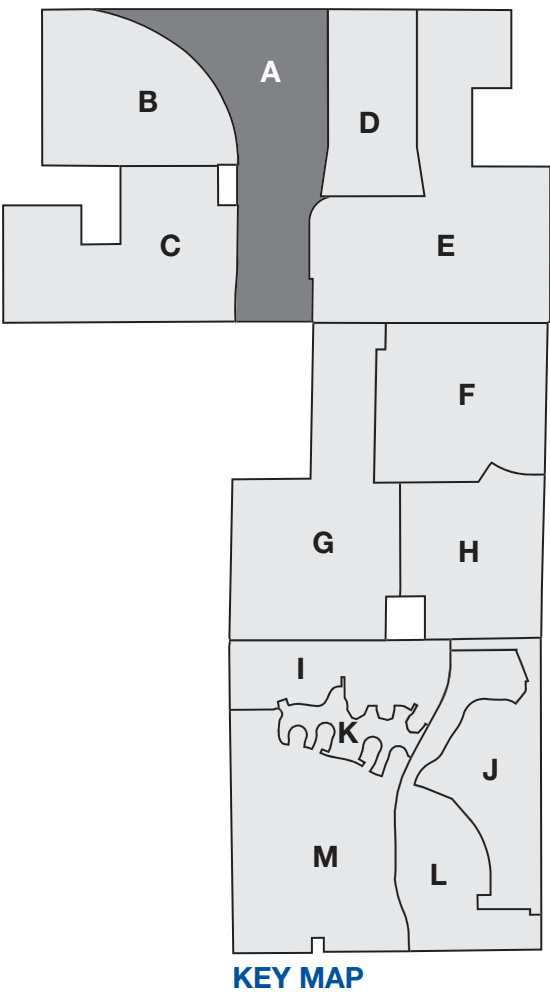
3.2.1 Neighborhood A

Neighborhood A, which consists of seven planning areas, is located in the north-central portion of the project site, just south of Avenue 60 and east of Tyler Street as depicted in **Figure 3.2-2, Neighborhood A**. The neighborhood totals approximately 190 acres including 21.37 acres of open space, 24.45 acres of mixed use/air park, 17.61 acres of commercial retail, 81.17 acres of heavy industrial, and 46.18 acres of mixed use. The neighborhood has a natural outward orientation and response to the regional context. Land uses targeted for Neighborhood A are intended to serve an area larger than the neighborhood itself, providing jobs and services to areas north and west of the site.

The uses provided for within Neighborhood A relate to planned uses at the Jacqueline Cochran Regional Airport located immediately to the north. A mix of commercial, industrial, mixed use, and service uses are intended to support airport development. The open space areas in the neighborhood include elements of the project-wide trail system that are generally coincident with drainage ways and major streets. Primary access to the neighborhood is provided via Avenue 60 and Tyler Street. A major project entry is provided at the intersection of Tyler Street and Avenue 60.

NEIGHBORHOOD A PLANNING STANDARDS

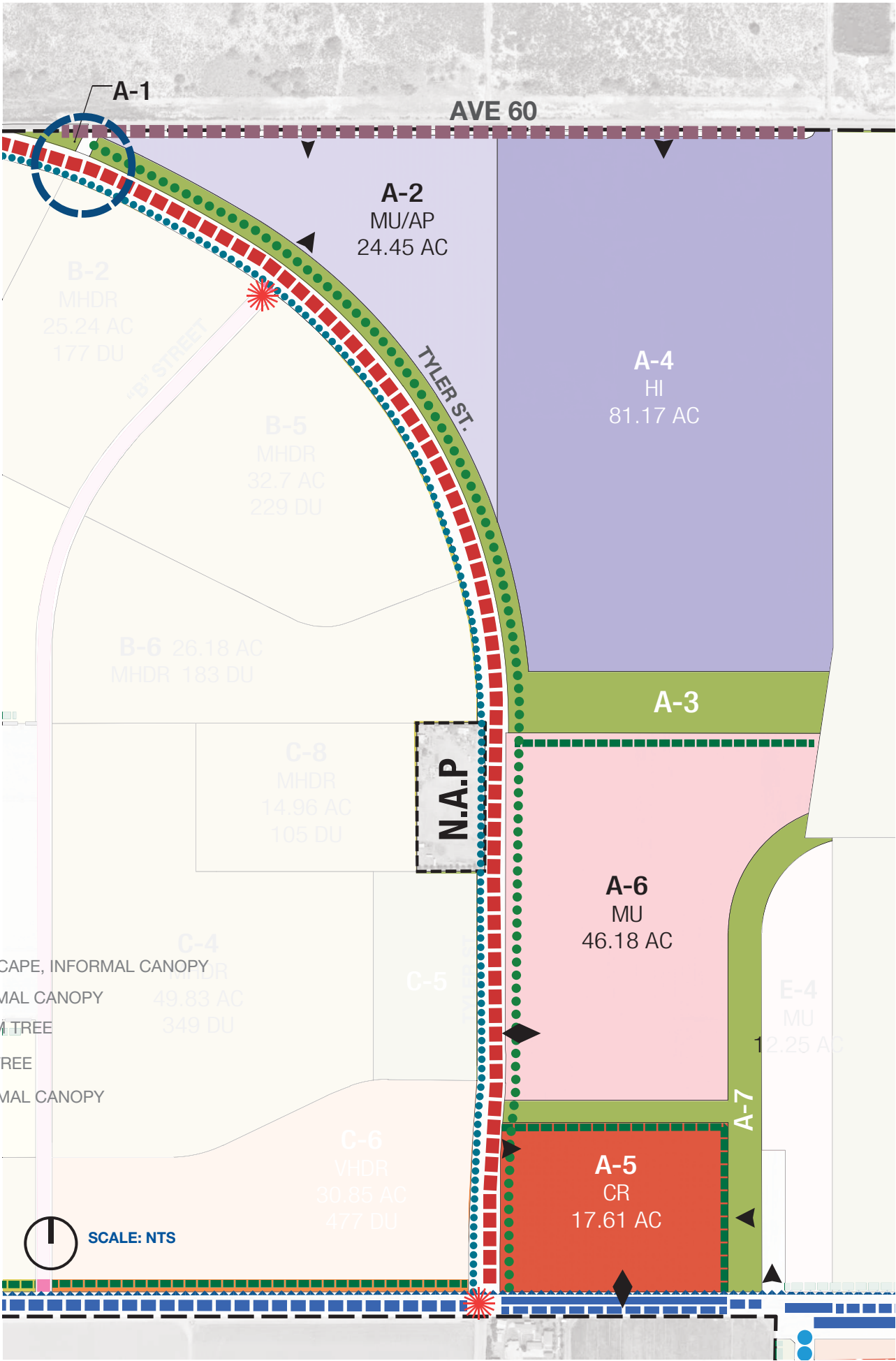
- 1) Portions of the commercial retail, mixed use, and industrial land uses within this neighborhood are located within Zone C of the Airport Compatibility Land Use Plan (CLUP). The mixed use/air park area and portions of the commercial-retail, mixed use and industrial uses are located within Zone D as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with these zones are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- 2) A large-scale recreational use such as a motor sports race track is a permitted use in two of the seven planning areas.
- 3) Tyler Street provides access to Planning Areas A-1 through A-7.
- 4) The northwest corner of Planning Area A-2 at Avenue 60/Tyler Street will be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- 5) The intersections of Tyler Street and Avenue 62 and Tyler Street and “B” Street will be developed in accordance with **Figure 3.4-5, Special Intersection Detail**.
- 6) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**.
- 7) Tyler Street, north of Avenue 62 will be developed in accordance with **Figure 3.4-8, Arterial Streetscape 1**.
- 8) Avenue 60 (along north edge of Planning Areas A-2 and A-4 will be developed in accordance with **Figure 3.4-16, Industrial Collector Streetscape**.
- 9) The east side of Tyler Street will be developed in accordance **Figure 3.4-19, Tyler Street Trail**.
- 10) The north side of Avenue 62 will be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.



KEY MAP

LEGEND

- PROJECT AREA BOUNDARY
- INDUSTRIAL COLLECTOR STREETSCAPE, INFORMAL CANOPY
- EXPRESSWAY STREETSCAPE, FORMAL CANOPY
- EXPRESSWAY STREETSCAPE, PALM TREE
- ARTERIAL STREETSCAPE 1, PALM TREE
- COLLECTOR STREETSCAPE 1, FORMAL CANOPY
- WINDROW PLANTING
- CLASS I BIKE PATH
- REGIONAL TRAIL
- CLASS II BIKE PATH
- FULL ACCESS
- RIGHT IN/RIGHT OUT ACCESS
- INTERSECTION DETAIL



Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Heavy Industrial (HI)		46.18	
A-7	Open Space (OS)		9.97	
TOTAL			190.78	

NEIGHBORHOOD A PLANNING AREAS**A-1: OPEN SPACE**

Planning Area A-1 provides for development of 0.63 acres of open space. This area is to be used as a collection point for storm flows entering the project site.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-1 is located in Zone D of the CLUP. Development in this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area A-1 is accessed from Tyler Street.
- (3) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (4) Large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (5) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-19, Tyler Street Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

A-2: MIXED USE/AIR PARK

Planning Area A-2 provides for 24.45 acres of mixed use/air park development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-2 is located in Zone D of the CLUP. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-2 is accessed from Tyler Street.
- (4) The edge treatment for the interface between Planning Area A-2's mixed use/air park and the drainage uses in the adjacent planning area, is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-3: OPEN SPACE

Planning Area A-3 provides for the development of 10.77 acres of open space which will be part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-3 is located in Zones C and D of the CLUP. Development in these zones will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-3 is accessed from Tyler Street.
- (4) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The edge treatment for the interface between the drainage uses in this planning area and the heavy industrial and mixed use/air park land uses in the adjacent planning areas are described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (6) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-18, Tyler Street Trail**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

A-4: HEAVY INDUSTRIAL

Planning Area A-4 provides for 81.17 acres of heavy industrial development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-4 is located in Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-4 is accessed from Tyler Street.
- (4) The edge treatment for the interface between Planning Area A-4's heavy industrial use and the drainage uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-5: COMMERCIAL RETAIL

Planning Area A-5 provides for 17.61 acres of commercial development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-5 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area A-5 is accessed from Tyler Street and Avenue 62.
- (3) The edge treatment for the interface between the commercial use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-19, Tyler Street Trail**. A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-6: MIXED USE

Planning Area A-6 provides for 46.18 acres of mixed-use development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-6 is located within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development in these zones will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-6 is accessed from Tyler Street.
- (4) The edge treatment for the interface between the drainage uses and the heavy industrial land uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-18, Tyler Street Trail**.
- (6) An observation tower built within Planning Area A-6 and built as part of a large scale recreational use shall not exceed 70 feet.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 3.3 – Design Guidelines for Mixed Use Development and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

A-7: OPEN SPACE

Planning Area A-7 provides for 9.97 acres of open space which will be part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area A-7 is located within Zone C of the CLUP for the airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-7 is accessed from Tyler Street and Avenue 62.
- (4) The edge treatment for the interface between the drainage in this planning area and the heavy industrial and commercial land uses in the adjacent planning areas is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

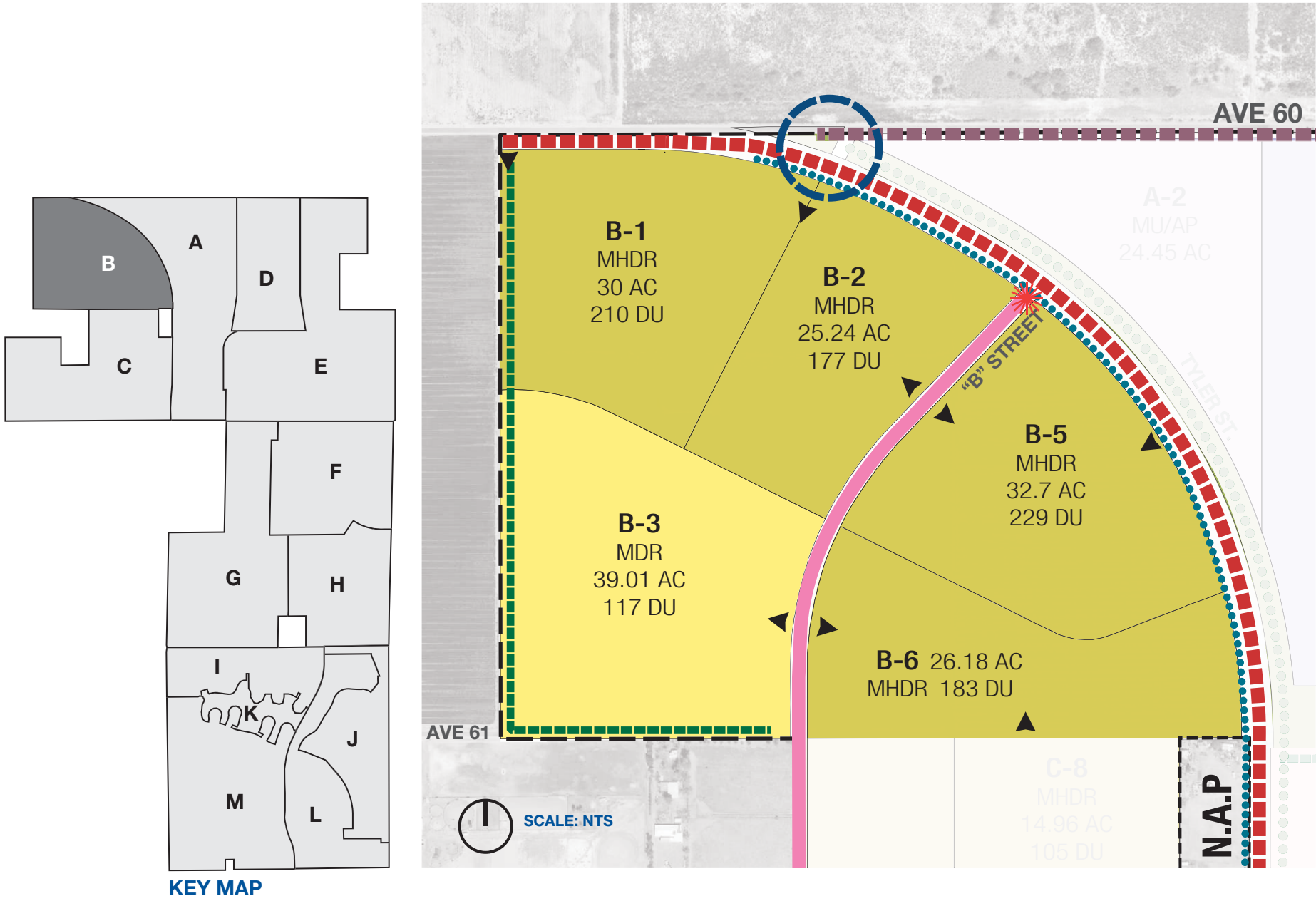
A-8: *Planning Area A-8 has been combined with adjacent planning area E-4 to provide a new Planning Area E-4 described below in Section 3.2.8.*

3.2.2 Neighborhood B

Neighborhood B contains five planning areas and is comprised of 153.13 acres as reflected in **Figure 3.2-3, Neighborhood B**. It is located in the northwestern portion of the project site, just south of "B" Street and Tyler Street. Proposed land uses are focused along Tyler Street, and include Medium Density Residential (39.01 acres) and Medium High Density Residential (114.12 acres). A target of 916 residential units is planned for Neighborhood B. These units are intended to provide housing for employees of the business, commercial and industrial areas in the northern portion of the project site. Primary access to the neighborhood is provided via Tyler Street and "B" Street. A major project entry is provided at the Avenue 60/Tyler Street intersection.

NEIGHBORHOOD B PLANNING STANDARDS

- (1) The entire neighborhood is located within Zone D of the CLUP for the airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1 – Project Wide Planning Standards of this Specific Plan as depicted in **Table 3.1-G, Basic Compatibility Criteria**.
- (2) Avenue 60 provides access to Planning Areas B-1.
- (3) Tyler Street provides access to Planning Areas B-2, B-5 and B-6.
- (4) "B" Street provides access to Planning Areas B-2, B-3, B-5 and B-6.
- (5) A Major Project Entry is located in the northwest corner of the neighborhood at Avenue 60/Tyler Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) A Special Intersection Detail is provided for the intersection of "B" Street and Tyler Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (7) Tyler Street will be developed in accordance with **Figure 3.4-8, Arterial Streetscape 1**.
- (8) "B" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.



LEGEND

- PROJECT AREA BOUNDARY
- INDUSTRIAL COLLECTOR STREETSCAPE, INFORMAL CANOPY
- ARTERIAL STREETSCAPE 1, PALM TREE
- COLLECTOR STREETSCAPE 1, FORMAL CANOPY
- WINDROW PLANTING
- CLASS II BIKE PATH
- FULL ACCESS
- INTERSECTION DETAIL
- MAJOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
B-1	Medium High Density Residential (MHDR)	210	30	5.0–8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0–8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0–5.0
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0–8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0–8.0
TOTAL		916	153.13	

NEIGHBORHOOD B PLANNING AREAS**B-1: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area B-1 provides for the development of 30.00 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 210 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-1 is accessed from Tyler Street.
- (2) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (3) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-2: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area B-2 provides for the development of 25.24 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning). A target of 177 dwelling units is planned within a density range 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-2 is accessed from Tyler Street and “B” Street.
- (2) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (3) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-3: MEDIUM DENSITY RESIDENTIAL

Planning Area B-3 provides for the development of 39.01 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 117 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-3 is accessed from “B” Street.
- (2) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer.**
- (3) Please refer to Section 33.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-4: *Planning Area number has intentionally been skipped in the numbering sequence.*

B-5: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area B-5 provides for the development of 32.7 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development. A target of 229 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-5 is accessed from “B” Street and Tyler Street.
- (2) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (3) Please refer to Section 3.1 – Project-Wide Planning Standards.

B-6: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area B-6 provides for the development of 26.18 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 183 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area B-6 is accessed from “B” Street and Tyler Street.
- (2) This planning area will be developed with the project edge buffer conditions as indicated in in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project-Wide Planning Standards.

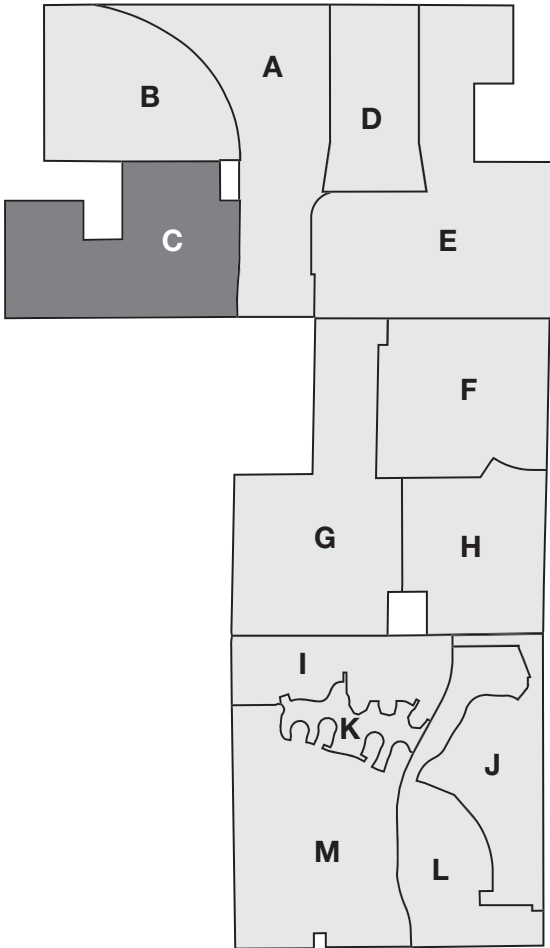
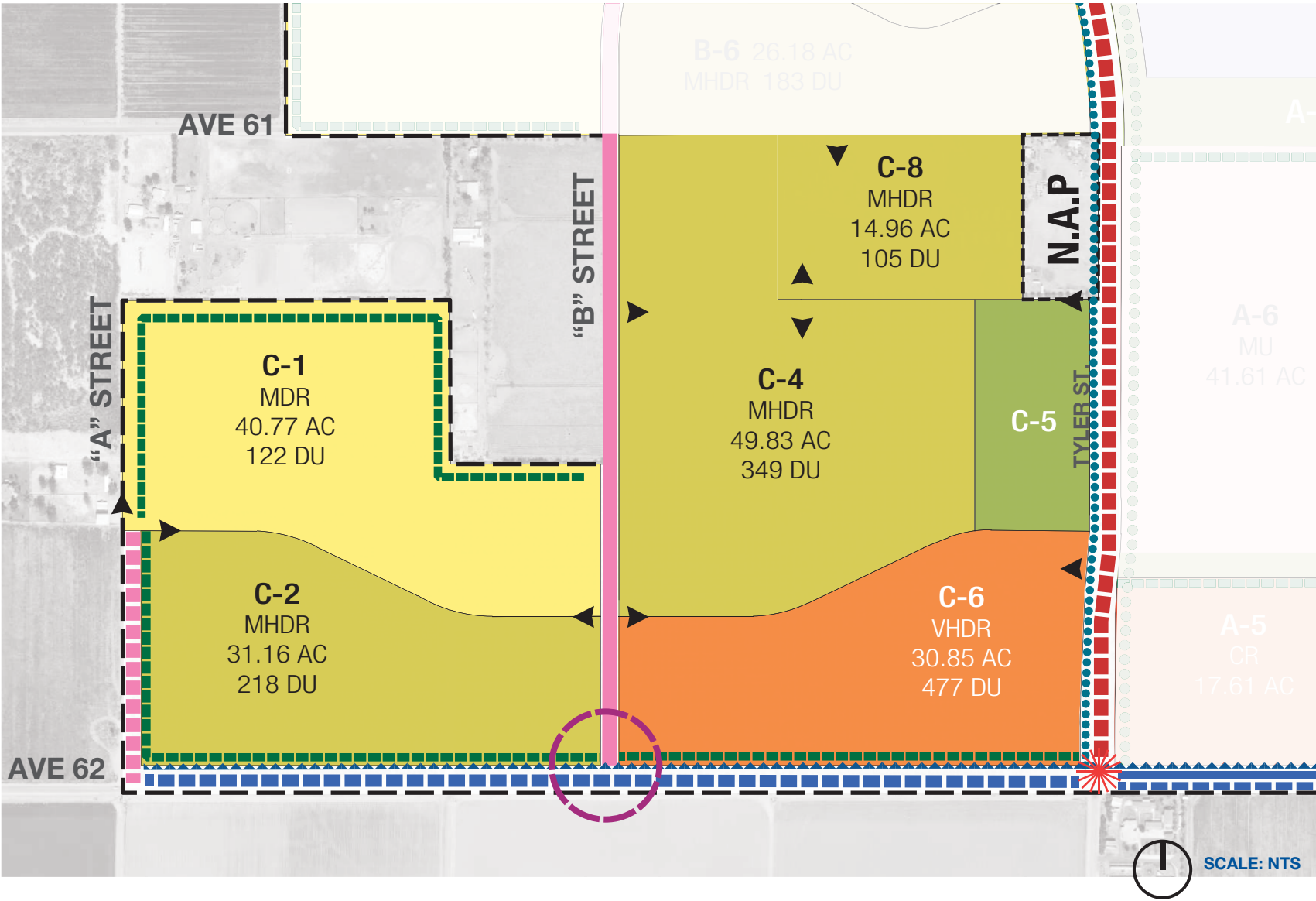
PLANNING AREA B-7: *Planning Area number has intentionally been skipped in the numbering sequence.*

3.2.3 Neighborhood C

Neighborhood C is located just north of Avenue 62 in the northwestern portion of the project site as reflected in **Figure 3.2-4, Neighborhood C**. This 177.54-acre neighborhood contains the following land uses within its six planning areas: Medium Density Residential (40.77 acres), Medium High Density Residential (95.95 acres), Very High Density Residential (30.85 acres) and Open Space (9.97 acres). A target of 1,271 dwelling units is planned for Neighborhood C. The predominantly residential uses are intended to house employees working in the employment-based neighborhoods located nearby. The maximum density and the total number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. The proximity of these residential areas to job opportunities is designed to reduce the number and length of home-to-work trips that would otherwise be generated by these land uses. The higher density residential uses are located in closer proximity to commercial uses, while the lower density residential uses are located at the project periphery. If developed with a golf course, a permitted use under provisions of Section 2 - Specific Plan Zoning Ordinance, residents would be provided with a high-quality recreational and scenic amenity. A number of the neighborhood planning areas would be afforded golf course frontage. In the absence of the golf course, drainage ways would function to provide more natural open space, providing structure and definition to the neighborhood. In addition, a local park would be developed to serve neighborhood residents and act as a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for development of a kart track or driver training facility. Primary access to the neighborhood is from "A" Street, "B" Street, Tyler Street and Avenue 62.

NEIGHBORHOOD C PLANNING STANDARDS

- (1) Neighborhood C is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with this zone are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use is a permitted use in all planning areas as defined in Section 2 – Specific Plan Zoning Ordinance.
- (3) Avenue 62 provides access to Planning Areas C-2 and C-6.
- (4) "A" Street provides access to Planning Areas C-1 and C-2.
- (5) "B" Street provides access to Planning Areas C-1, C-2, C-4, C-6 and C-8.
- (6) Tyler Street provides access to Planning Areas C-5 and C-6.
- (7) A Minor Entry is located at "B" Street and Avenue 62 to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (8) "B" Street will be developed in accordance with **Figures 3.4-17, Collector Streetscape 1**.
- (9) "A" Street will be developed in accordance with **Figures 3.4-18, Collector Streetscape 2**.
- (10) Tyler Street north of Avenue 62 will be developed in accordance with **Figures 3.4-8, Aerial Streetscape 1**.
- (11) Avenue 62 will be developed in accordance with **Figures 3.4-6, Expressway Streetscape**.
- (12) A local park will be provided in Planning Area C-5, to serve neighborhood residents and provide a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for development of a kart track.
- (13) The north side of Avenue 62 will be developed with **Figure 3.4-20, Avenue 62 Trail** as reflected in **Figure 3.1-10, Public Facilities**.



KEY MAP

LEGEND

- PROJECT AREA BOUNDARY
- INDUSTRIAL COLLECTOR STREETScape, INFORMAL CANOPY
- EXPRESSWAY STREETScape, FORMAL CANOPY
- EXPRESSWAY STREETScape, PALM TREE
- ARTERIAL STREETScape 1, PALM TREE
- COLLECTOR STREETScape 1, FORMAL CANOPY
- COLLECTOR STREETScape 2, INFORMAL CANOPY
- SECONDARY STREETScape
- WINDROW PLANTING
- CLASS I BIKE PATH
- REGIONAL TRAIL
- CLASS II BIKE PATH
- FULL ACCESS
- INTERSECTION DETAIL
- MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
C-1	Medium Density Residential (MDR)	122	40.77	5.0–8.0
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0–8.0
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0–8.0
C-5	Open Space (OS)		9.97	
C-6	Very High Density Residential (VHDR)	477	30.85	14.0–20.0
C-8	Medium High Density Residential (RM)	105	14.96	5.0–8.0
TOTAL		1271	177.54	

NEIGHBORHOOD C PLANNING AREAS**C-1: MEDIUM DENSITY RESIDENTIAL**

Planning Area C-1 provides for the development of 40.77 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 122 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area C-1 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-1 is accessed from “A” Street and “B” Street.
- (4) The Project Edge Treatment for this planning area is illustrated in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

C-2: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area C-2 provides for the development of 31.16 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 218 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area C-2 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-2 is accessed from “A” Street, “B” Street and Avenue 62.
- (4) The Project Edge Treatment for this planning area is illustrated in **Figure 3.4-29, out Parcel/Adjacent Property Buffer**.
- (5) A Minor Entry is planned for the intersection of “B” Street and Avenue 62 as shown in **Figure 3.4-4, Minor Project Entry**.
- (6) A Class I Bike Path is located in this planning area along the north side of Avenue 62 as illustrated in **Figure 3.4-20, Avenue 62 Trail**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

C-3: *Planning Area number has intentionally been skipped in the numbering sequence.*

C-4: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area C-4 provides for the development of 49.83 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional lots and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning). A target of 349 dwelling units is planned within a density range of 5.0–8.0 du/acre).

Land Use and Development Standards

Please refer to Ordinance No. 348 (see Section 2 –Specific Plan Zoning.

Planning Standards

- (1) Planning Area C-4 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-4 is accessed from "B" Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

C-5: OPEN SPACE

Planning Area C-5 provides for the development of 9.97 acres of open space that will be developed as a local park and provide a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for mixed-use development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area C-5 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-5 is accessed from Tyler Street.
- (4) This planning area will be developed in accordance with **Figure 3.4-29, Out Parcel/Adjacent Property Buffer.**
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

C-6: VERY HIGH DENSITY RESIDENTIAL

Planning Area C-6 provides for the development of 30.85 acres of very high density residential land uses with minimum lot sizes of 4,000 square feet for conventional lots and 2,000 square feet under cluster development. A target of 477 dwelling units is planned within a density range of 14.0–20.0 du/acre. The maximum density and the total number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area C-6 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area C-6 is accessed from “B” Street, Tyler Street and Avenue 62.
- (3) A Minor Entry is planned for the intersection of “B” Street and Avenue 62 to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) The Commercial Buffer treatment for the interface between the residential uses in this planning area and the commercial uses located across the street is illustrated in **Figure 3.4-31, Commercial Buffer Streetscape**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

C-7: *Planning Area number has intentionally been skipped in the numbering sequence.*

C-8: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area C-8 provides for the development of 14.96 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development. A target of 105 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

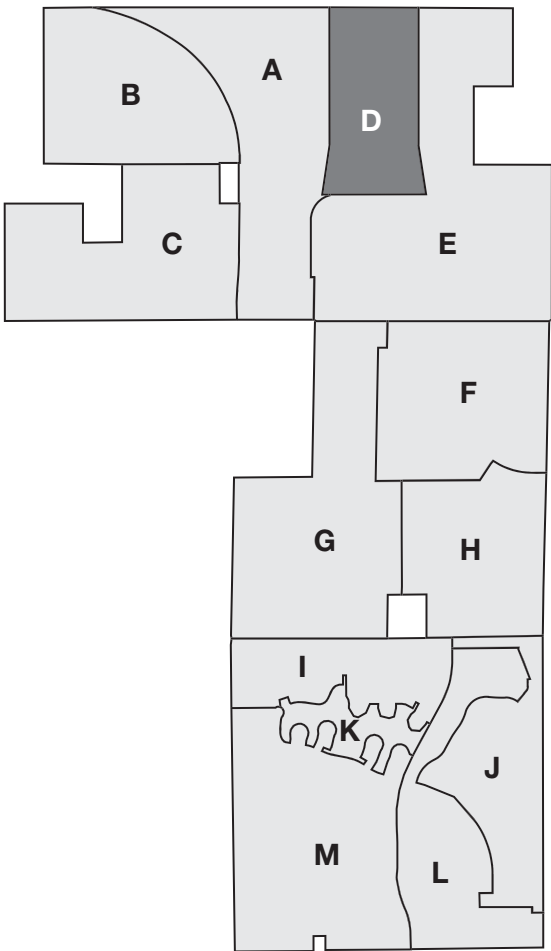
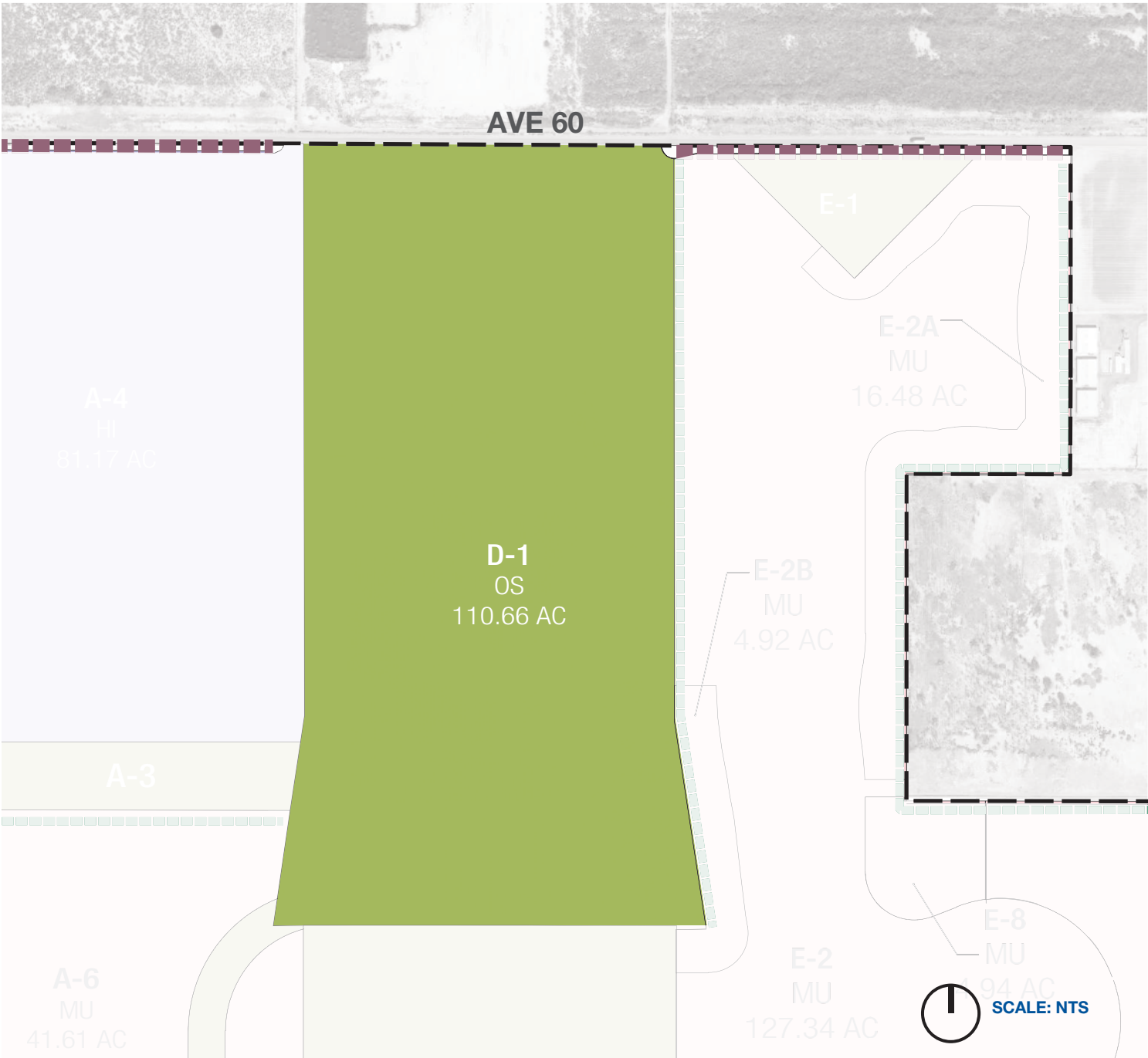
- (1) Planning Area C-8 is located within Zone D of the CLUP for the Desert Resorts Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area C-8 is accessed from "B" Street through interior loop roads.
- (3) This planning area will be developed with the Buffer Condition along the adjacent property to the east which is depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.4 Neighborhood D

Neighborhood D is located in the northern portion of the project site, between Avenue 60 and Avenue 62 as depicted in **Figure 3.2-5, Neighborhood D**. This neighborhood is intended to function as a buffer between the business, commercial, recreational uses and the airport runway to the north. The neighborhood is 110.66 acres in size and consists of open space uses. All of the open space is within airport influenced areas and is designed to handle storm drainage. Primary access to this neighborhood is provided via Avenue 60 from the west or the east, but Avenue 60 will not traverse the northern edge of the Neighborhood due to possible airport expansion.

NEIGHBORHOOD D PLANNING STANDARDS

- (1) The open space neighborhood is constrained by the OSZ, Inner Safety Zone (ISZ) and ETZ airport zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, Neighborhood D is located within Zone A of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Avenue 60 provides access to Planning Area D-1.



KEY MAP

- LEGEND**
- PROJECT AREA BOUNDARY
 - INDUSTRIAL COLLECTOR STREETSCAPE, INFORMAL CANOPY

NEIGHBORHOOD D PLANNING AREAS**D-1: OPEN SPACE**

Planning Area D-1 provides for 110.66 acres of open space. A portion of this area will be used to handle storm flows. Much of the planning area is described in the Jacqueline Cochran Regional Airport Master Plan as proposed for acquisition for the runway clear zone. This planning area shall remain in open space uses.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area D-1 is located within the Jacqueline Cochran Regional Airport ETZ and ISZ airport safety zones as reflected on **Figure 3.1-13 Airport Zones** and **Table 3.1-F, Airport Safety Zones**. Development within these zones will comply with all applicable restrictions and requirements. Additionally, Neighborhood D is located within Zone A of the CLUP for the Jacqueline Cochran Regional Airport as reflected in reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with this zone are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Planning Area D-1 is accessed from Avenue 60.
- (3) The Buffer Condition between Planning Area D-1 and adjacent industrial uses will be as illustrated in **Figure 3.4-6, Expressway Streetscape**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.5 Neighborhood E

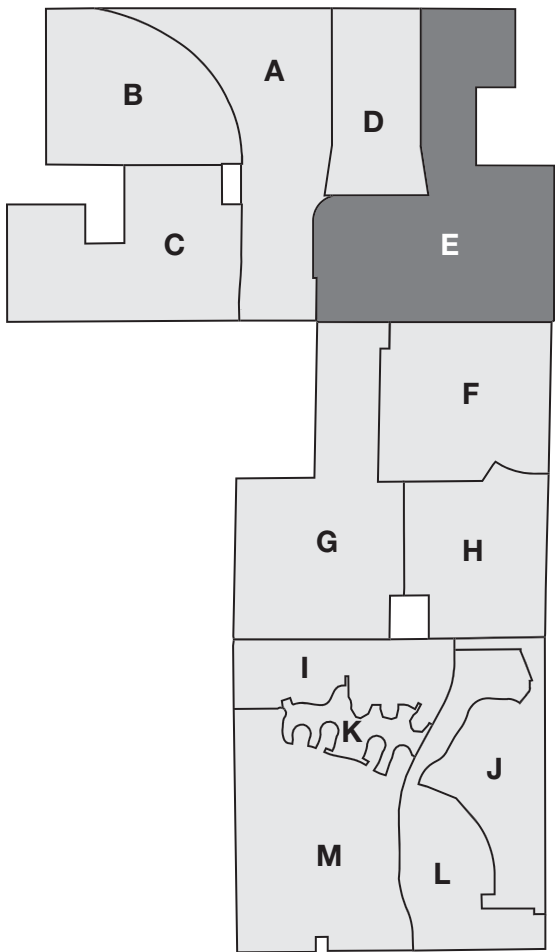
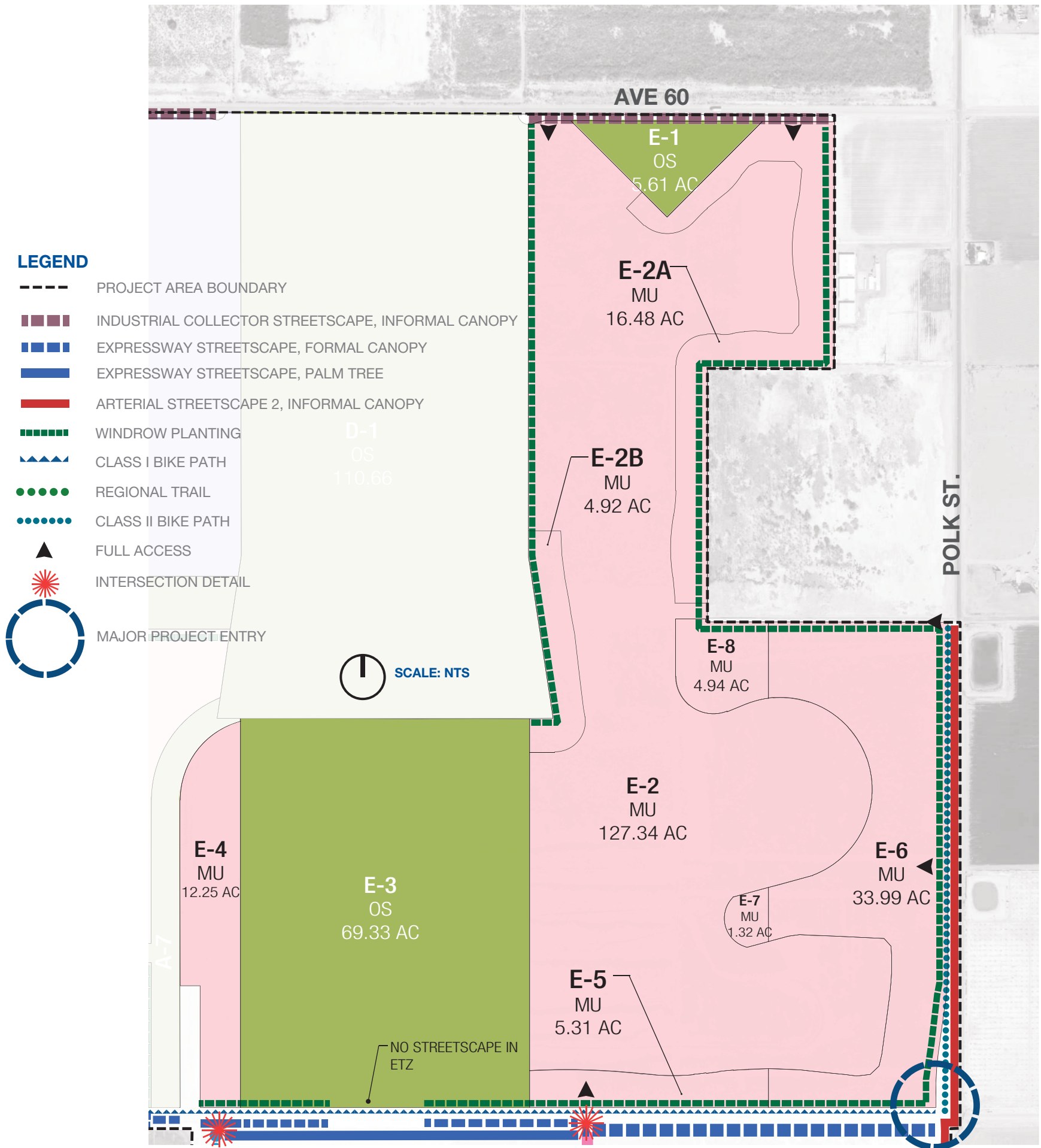
Neighborhood E is situated in the northeastern corner of the project site and is bounded by Jacqueline Cochran Regional Airport and Avenue 60 on the north, Polk Street on the east, and Avenue 62 on the south as depicted in **Figure 3.2-6, Neighborhood E**. This neighborhood has a strong regional orientation as a result of its excellent access to the regional circulation system and to Jacqueline Cochran Regional Airport. The neighborhood also looks eastward by virtue of its potential connection to the proposed interchange at Avenue 62 and the new Highway 86, just a few miles to the east. Primary access is provided by Polk Street, Avenue 60 and Avenue 62. The land uses proposed in this 281.49-acre neighborhood include mixed-use (206.55 acres), and open space (74.94 acres).

A racetrack has been approved for this neighborhood with its own separate set of conditions of approval and design manual for features specific to its development. The open space in the northeastern portion of the neighborhood responds to airport-related constraints, and will become part of the larger recreational land use.

Land designated for mixed use development abutting the airport property offers the potential for airport uses to extend southward into the site. This area also has potential for rail service to be provided from the north via a rail spur from the Southern Pacific main line. In addition to the jobs that could be created by heavy industry, Neighborhood E allows for a large-scale recreational use such as a motor sports race track which will also create jobs. Agricultural operations to the east of Polk Street offer the potential for development of compatible agriculture-related uses on the property. A Major Entry is located at the intersection of Polk Street and Avenue 62.

NEIGHBORHOOD E PLANNING STANDARDS

- (1) Portions of Neighborhood E are constrained by the OSZ and ETZ airport safety zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within Zones B1, C, and D of the CLUP as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan. As Neighborhood E allows for mixed-use residential uses in designated planning areas, **Figure 3.2-6A, Lots Allowing Overnight Use**, identifies which lots within those planning areas allow for such use. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive zone policy applies. For example, Lot 55 on **Figure 3.2-6A** lies within both Zone C and D. Because Zone D is the less restrictive zone, Zone D ALUC policies apply.
- (2) Tyler Street provides access to Planning Areas E-2, E-2A, E-2B, E-3, E-4, E-5, E-6, E-7, and E-8 through Planning Area A-6.
- (3) Avenue 60 provides access to Planning Area E-1 and E-2, E-2A, E-2B.
- (4) Polk Street provides access to Planning Area E-2, E-2A, E-2B, E-5, E-6, E-7, and E-8.
- (5) A Major Project Entry is located at Tyler Street and Polk Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (7) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**, with the exception of the ETZ in which no trees are to be planted.
- (8) A large-scale recreational use such as a motor sports race track is a permitted land use in seven of the eight planning areas as defined in Section 2 – Specific Plan Zoning Ordinance.
- (9) The north side of Avenue 62 will be developed as depicted in **Figure 3.1-10, Public Facilities** in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (10) The west side of Polk Street will be developed with a Class II Bike Path as described **Figure 3.1-10, Public Facilities**.



KEY MAP

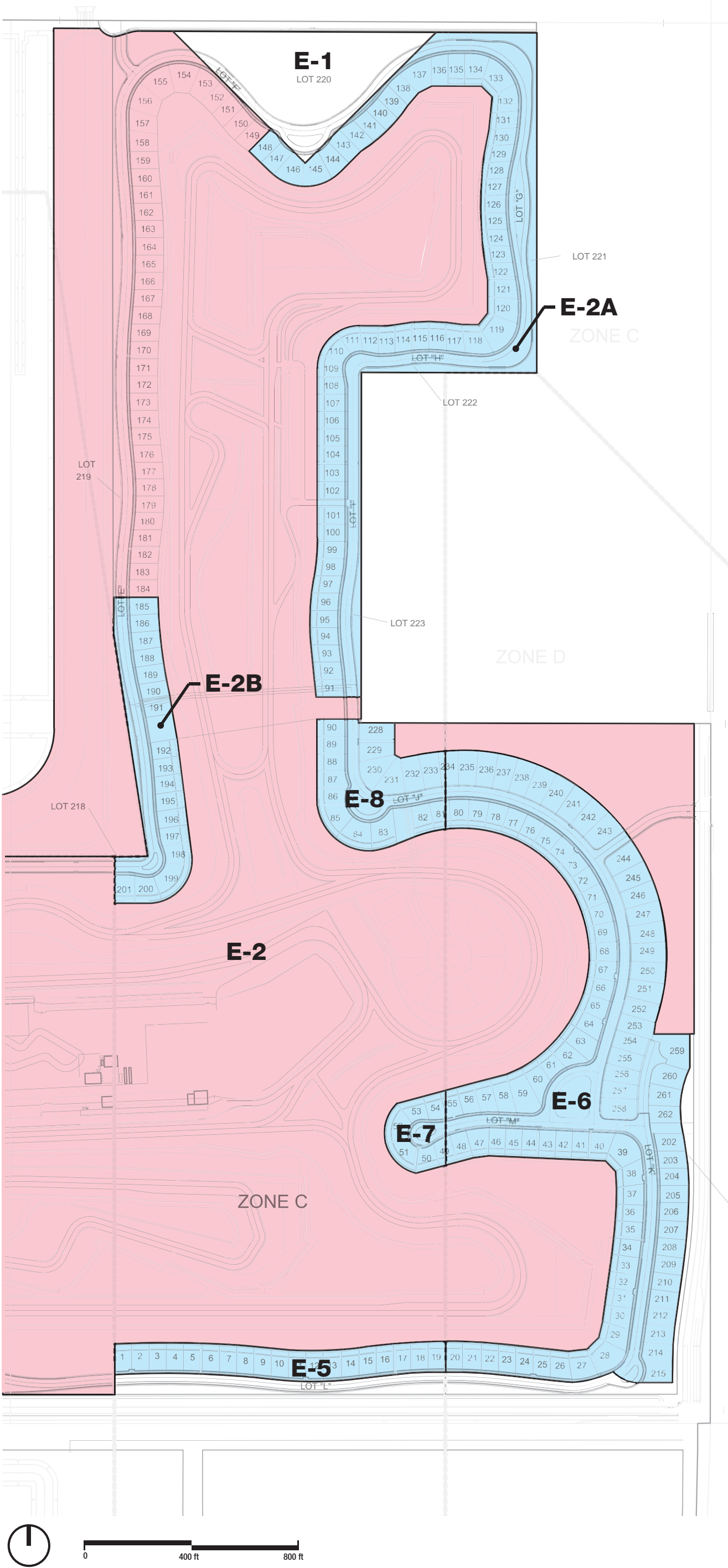
Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
E-1	Open Space (OS)		5.61	
E-2	Mix Use (MU)		127.34	
E-2A	Mix Use (MU)		16.48	
E-2B	Mix Use (MU)		4.92	
E-3	Open Space (OS)		69.33	
E-4	Commercial Retail (CR)		12.25	
E-5	Mix Use (MU)		5.31	
E-6	Mix Use (MU)		33.99	
E-7	Mix Use (MU)		1.32	
E-8	Mix Use (MU)		4.94	
TOTAL			281.49	

FIGURE 3.2-6A **LOTS ALLOWING OVERNIGHT USE**

LEGEND

NO OVERNIGHT

OVERNIGHT ALLOWED



NEIGHBORHOOD E PLANNING AREAS**E-1: OPEN SPACE**

Planning Area E-1 provides for the development of 5.61 acres of open.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-1 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and the ETZ and OSZ airport safety zones. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area.
- (3) Planning Area E-1 is accessed from Avenue 60.
- (4) Large-scale recreational uses such as a golf course and a motor sports race track are permitted uses in this planning area.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

E-2: MIXED USE

Planning Area E-2 provides for the development of 127.34 acres of mixed-use nonresidential development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-2 is located within Zone C of the Jacqueline Cochran Regional Airport CLUP. This planning area will comply with all applicable restrictions and requirements of that zone.
- (2) Access to Planning Area E-2 is from Tyler and Polk Streets.
- (3) The edge treatment for the interface between the planned mixed use and adjacent open space is illustrated in **Figure 3.4-28, Airpark Drainage Buffer**. The edge treatment will be as described in **Figure 3.4-24, Golf Course/ Racetrack Edge Condition**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area. Signage shall be posted that overnight occupancy is prohibited in non-residential structures. No minimum setback or buffer is required for Planning Area E-2 from adjacent residential uses.
- (6) An observation tower built within Planning Area E-2 and built as part of a large scale recreational use shall not exceed 70 feet.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Project Wide Development Standards.

E-2A: MIXED USE

Planning Area E-2A provides for the development of 16.48 acres of mixed-use development to include the development of recreational units which are non-residential member villas providing a private space for a Thermal Motorsports Park Club member to store personal belongings, vehicles, and equipment utilized for racing; utilized for vehicular repairs and other racing-related recreational activities; and are located adjacent to the racetrack with an open view deck facing the track to allow viewing of racing activities. These units are accessible 24 hours a day, seven days a week, however, they are restricted from permanent residency and have limitations imposed by the Thermal Motorsports Park Club for duration of stays. A maximum of 58 racetrack recreational units is planned.

Land Use and Development Standards

Please refer to Ordinance No. 348 and Specific Plan Amendment No. 4, Specific Plan Zoning Ordinance.

Planning Standards

- (1) Planning Area E-2A is located within Zone C of the Jacqueline Cochran Regional Airport CLUP. This planning area will comply with all applicable restrictions and requirements of that zone.
- (2) Access to Planning Area E-2A is from Tyler and Polk Streets.
- (3) The edge treatment for the interface between the planned mixed use and adjacent open space is illustrated in **Figure 3.4-28, Airpark Drainage Buffer**. The edge treatment will be as described in **Figure 3.4-24, Golf Course/ Racetrack Edge Condition**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area including racetrack recreational units defined as non-residential use identified as a member villa accessible 24 hours a day, seven days a week. The Racetrack Recreational Unit provides a private space for a Thermal Club members to store personal belongings, vehicles, and equipment utilized for racing. The unit can further be utilized for vehicular repairs, other racing-related recreational activities and is located directly adjacent to the racetrack with an open view deck facing the track allowing for viewing of racing activities. The unit would be restricted from permanent residency, and has limitations imposed by the club for duration of stays..
- (6) Standards for Racetrack Recreational Unit development:
 - a. A maximum of 58 racetrack recreational units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-2A.
 - b. Structures must provide vehicle storage area on the ground floor of structure and may include the following:

Garage Level (1st Floor):

 - Cars
 - Lifts
 - Work areas
 - Storage
 - Mechanical
 - Main Entry
 - Half baths
 - Bath with locker rooms
 - Washer/dryer area
 - Elevator
 - Stairs
 - Media Room
 - Kitchenette area

Second Level (and/or mezzanine):

- Elevator
- Stairs
- Storage
- Mechanical
- Bedrooms and closets
- Bath area
- Laundry
- Family Room/Game Room
- Kitchen
- Outside Patios
- Outside Planters

Third Level (Main Living Level):

- Bedrooms and Closets
- Bath
- Elevator
- Stairs
- Laundry
- Kitchen
- Dining
- Great Room
- Outside Patios
- Mechanical
- Storage
- Game Room
- BBQ area
- Outside pools and hot tub
- Planters

- c. Access to structures shall be from common access areas or corridors
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
 - (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
 - (9) Please refer to Section 2.0 – Zoning Ordinance for development of other allowable uses and associated developments standards.
 - (10) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
 - (11) Please refer to Section 3.1 – Project Wide Development Standards.

E-2B: MIXED USE

Planning Area E-2B provides for the development of 4.92 acres of mixed-use development to include the development of recreational units which are non-residential member villas providing a private space for a Thermal Motorsports Park Club member to store personal belongings, vehicles, and equipment utilized for racing; utilized for vehicular repairs and other racing-related recreational activities; and are located adjacent to the racetrack with an open view deck facing the track to allow viewing of racing activities. These units are accessible 24 hours a day, seven days a week, however, they are restricted from permanent residency and have limitations imposed by the Thermal Motorsports Park Club for duration of stays A maximum of 16 racetrack recreational units is planned.

Land Use and Development Standards

Please refer to Ordinance No. 348 and Specific Plan Amendment No. 4, Section 2 – Specific Plan Zoning Ordinance.

Planning Standards

- (1) Planning Area E-2B is located within Zone C of the Jacqueline Cochran Regional Airport CLUP. This planning area will comply with all applicable restrictions and requirements of that zone.
- (2) Access to Planning Area E-2B is from Tyler and Polk Streets.
- (3) The edge treatment for the interface between the planned mixed use and adjacent open space is illustrated in **Figure 3.4-28, Airpark Drainage Buffer**. The edge treatment will be as described in **Figure 3.4-24, Golf Course/ Racetrack Edge Condition**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area including racetrack recreational units defined as non-residential use identified as a member villa accessible 24 hours a day, seven days a week. The Racetrack Recreational Unit provides a private space for a Thermal Club members to store personal belongings, vehicles, and equipment utilized for racing. The unit can further be utilized for vehicular repairs, other racing-related recreational activities and is located directly adjacent to the racetrack with an open view deck facing the track allowing for viewing of racing activities. The unit would be restricted from permanent residency, and has limitations imposed by the club for duration of stays..
- (6) Standards for Racetrack Recreational Unit development:
 - a. A maximum of 16 racetrack recreational units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-2B.
 - b. Standard Setbacks
 - i. No minimum setback is required from any private street.
 - ii. Front Yard: 30-foot minimum
 - iii. Rear Yard: No minimum
 - iv. Side Yard: 5-feet.
 - c. Structures must provide vehicle storage area on the ground floor of structure and may include the following:
 - Garage Level (1st Floor):
 - Cars
 - Lifts
 - Work areas
 - Storage
 - Mechanical
 - Main Entry
 - Half baths
 - Bath with locker rooms
 - Washer/dryer area
 - Elevator
 - Stairs
 - Media Room
 - Kitchenette area
 - Second Level (and/or mezzanine):
 - Elevator
 - Stairs
 - Storage
 - Mechanical

- Bedrooms and closets
- Bath area
- Laundry
- Family Room/Game Room
- Kitchen
- Outside Patios
- Outside Planters

Third Level (Main Living Level):

- Bedrooms and Closets
- Bath
- Elevator
- Stairs
- Laundry
- Kitchen
- Dining
- Great Room
- Outside Patios
- Mechanical
- Storage
- Game Room
- BBQ area
- Outside pools and hot tub
- Planters

- d. Access to structures shall be from common access areas or corridors
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 2.0 – Zoning Ordinance for development of other allowable uses and associated developments standards.
- (10) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (11) Please refer to Section 3.1 – Project Wide Development Standards.

E-3: OPEN SPACE

Planning Area E-3 provides for the development of 69.33 acres of open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards – General

- (1) Planning Area E-3 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and the ETZ and OSZ airport safety zones. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area.
- (3) Planning Area E-3 is accessed from Tyler Street.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail** with the exception of the ETZ where no trees shall be planted.
- (5) A Class I Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) The edge treatment for the interface between the open spaces uses and industrial uses in the adjacent planning area is illustrated in **Figure 3.4-28 Airpark Drainage Buffer**. If the race track is developed, the edge treatment will be as described in **Figure 3.4-24 Golf Course/Racetrack Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

E-4: MIXED USED

Planning Area E-4 provides for 12.25 acres of nonresidential mixed-use land development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-4 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-4 accessed from Tyler Street.
- (3) The edge treatment for the interface between the mixed use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-28 Airpark Drainage Buffer**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class I Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

E-5: MIXED USE

Planning Area E-5 provides for 5.31 acres of mixed-use land development with minimum lot sizes of 7,000 square feet as defined in Section 2 – Specific Plan Zoning.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-5 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-5 accessed from Polk and Tyler Streets.
- (3) The edge treatment for the interface between the mixed use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-28 Airpark Drainage Buffer**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class I Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Standards for Residential development:
 - a. A maximum of 19 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-5.
 - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
 - c. No buffer is required in Planning Area E-5 for residential uses contiguous to Planning Area E-2.
 - d. Residential structures must provide vehicle storage area on the ground floor of structure.
 - e. Access to residential structures shall be from common access areas or corridors.
 - f. Structures on lots with a net area of 7,540 square feet or less shall not exceed the follow standards without further ALUC review:
 - i. 7,150 square feet; or
 - ii. two stories or 42 feet in height.
 - g. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
 - i. Floor Area Ratio – 0.95;
 - ii. Lot Coverage – 0.5; and
 - iii. 42 feet in height.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Project Wide Development Standards.

E-6: MIXED USE

Planning Area E-6 provides for 33.99 acres of residential and nonresidential mixed-use land development as defined in Section 2 – Specific Plan Zoning.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-6 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-6 accessed from Polk and Tyler Streets.
- (3) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) A Class II Bike Path is located in this planning area along west side of Polk Street. A Class I Bike Path is also located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (5) A Major Project Entry is located at the southeast corner of this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) A total of 8 acres of nonresidential development may occur in the northeastern corner of Planning Area E-6 in addition to residential development.
- (7) Standards for Residential development:
 - a. A maximum of 120 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-6.
 - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
 - c. No buffer is required in Planning Area E-6 for residential uses contiguous to Planning Area E-2.
 - d. Standard Setbacks
 - i. No minimum setback is required from any private street.
 - ii. Front Yard: 30-foot minimum
 - iii. Rear Yard: No minimum
 - iv. Side Yard: 5-feet.
 - e. Residential structures must provide vehicle storage area on the ground floor of structure.
 - f. Access to residential structures shall be from common access areas or corridors.
 - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the following standards without further ALUC review:
 - i. 7,150 square feet; or
 - ii. two stories or 42 feet in height.
 - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
 - i. Floor Area Ratio – 0.95;
 - ii. Lot Coverage – 0.5; and
 - iii. 42 feet in height.
- (8) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (9) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (10) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (11) Please refer to Section 3.1 – Project Wide Development Standards.

E-7: MIXED USE

Planning Area E-7 provides for 1.32 acres of mixed-use land development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area E-7 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-7 accessed from Polk and Tyler Streets.
- (3) Standards for Residential development:
 - a. A maximum of 5 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-7.
 - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
 - c. No buffer is required in Planning Area E-7 for residential uses contiguous to Planning Area E-2.
 - d. Standard Setbacks
 - i. No minimum setback is required from any private street.
 - ii. Front Yard: 30-foot minimum
 - iii. Rear Yard: No minimum
 - iv. Side Yard: 5-feet.
 - e. Residential structures must provide vehicle storage area on the ground floor of structure.
 - f. Access to residential structures shall be from common access areas or corridors
 - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the follow standards without further ALUC review:
 - i. 7,150 square feet; or
 - ii. two stories or 42 feet in height.
 - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
 - i. Floor Area Ratio – 0.95;
 - ii. Lot Coverage – 0.5; and
 - iii. 42 feet in height.
- (4) Please refer to Section 3.3 – Design and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

E-8: MIXED USE

Planning Area E-8 provides for 4.94 acres of mixed-use land development.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

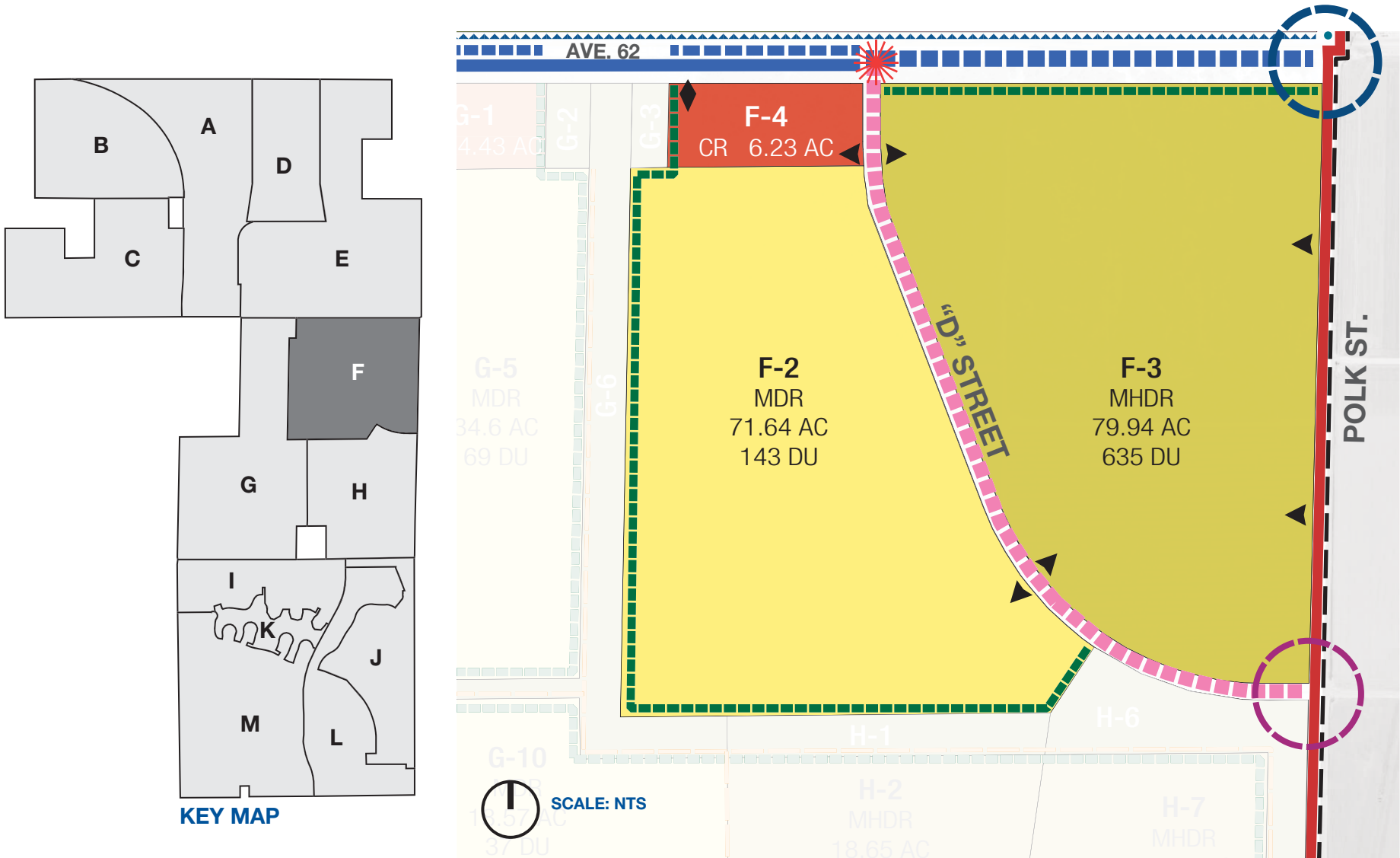
- (1) Planning Area E-8 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-8 accessed from Polk and Tyler Streets.
- (3) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Standards for Residential development:
 - a. A maximum of 15 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-8.
 - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
 - c. No buffer is required in Planning Area E-8 for residential uses contiguous to Planning Area E-2.
 - d. Standard Setbacks
 - i. No minimum setback is required from any private street.
 - ii. Front Yard: 30-foot minimum
 - iii. Rear Yard: No minimum
 - iv. Side Yard: 5-feet.
 - e. Residential structures must provide vehicle storage area on the ground floor of structure.
 - f. Access to residential structures shall be from common access areas or corridors
 - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the follow standards without further ALUC review:
 - i. 7,150 square feet; or
 - ii. two stories or 42 feet in height.
 - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
 - i. Floor Area Ratio – 0.95;
 - ii. Lot Coverage – 0.5; and
 - iii. 42 feet in height.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards

3.2.6 Neighborhood F

Neighborhood F totals 157.81 acres, and is located along Polk Street, just south of Avenue 62 as depicted in **Figure 3.2-7, Neighborhood F**. It consists of 71.64 acres of medium density residential, 79.94 acres of medium high density residential, and 6.23 acres of commercial retail land use designations. Primary access is provided by "D" Street, Avenue 62 and Polk Street. It is anticipated that the planned commercial land uses will provide employment to project area residents and will service an area larger than the site itself. A Major Entry is located at the intersection of Polk Street and Avenue 62.

NEIGHBORHOOD F PLANNING STANDARDS

- (1) Portions of the commercial land uses in Neighborhood F are constrained by the OSZ and ERC airport safety zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within zones B1, C and D of the CLUP as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Major project entry is located at the intersection of Avenue 62 and Polk Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (3) An intersection detail is planned for "D" Street and Avenue 62 to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) "D" Street will provide access to Planning Areas F-4, F-2 and F-3.
- (5) Avenue 62 provides access to Planning Area F-3 and F-4.
- (6) Polk Street provides access to Planning Area F-3.
- (7) Polk Street will be developed in accordance with **Figures 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (8) "D" Street will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (9) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**.



LEGEND

- PROJECT AREA BOUNDARY
- EXPRESSWAY STREETSCAPE, FORMAL CANOPY
- ARTERIAL STREETSCAPE 2, INFORMAL CANOPY
- COLLECTOR STREETSCAPE 2, INFORMAL CANOPY
- WINDROW PLANTING
- CLASS I BIKE PATH
- RIGHT IN/RIGHT OUT ACCESS
- FULL ACCESS
- INTERSECTION DETAIL
- MAJOR PROJECT ENTRY
- MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
F-2	Medium Density Residential (MDR)	143	71.64	2.0–5.0
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0–8.0
F-4	Commercial Retail (CR)		6.23	
TOTAL		778	157.81	

NEIGHBORHOOD F PLANNING AREAS**F-2: MEDIUM DENSITY RESIDENTIAL**

Planning Area F-2 provides for the development of 71.64 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 under cluster development. A target of 143 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area F-2 is located within Zones B1, C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-2 is accessed from “D” Street.
- (3) The edge treatment for the interface between the planning area and adjacent drainage channel is shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

F-3: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area F-3 provides for the development of 79.94 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 635 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area F-3 is located within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-3 is accessed from “D” Street, Avenue 62 and Polk Street.
- (3) A Major Project Entry is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-3, Major Entry**.
- (4) A Minor Entry is located in the southeast corner of the planning area to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (5) An intersection detail is located in the northwest corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards

F-4: COMMERCIAL RETAIL

Planning Area F-4 provides for the development of 6.23 acres of commercial land uses.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

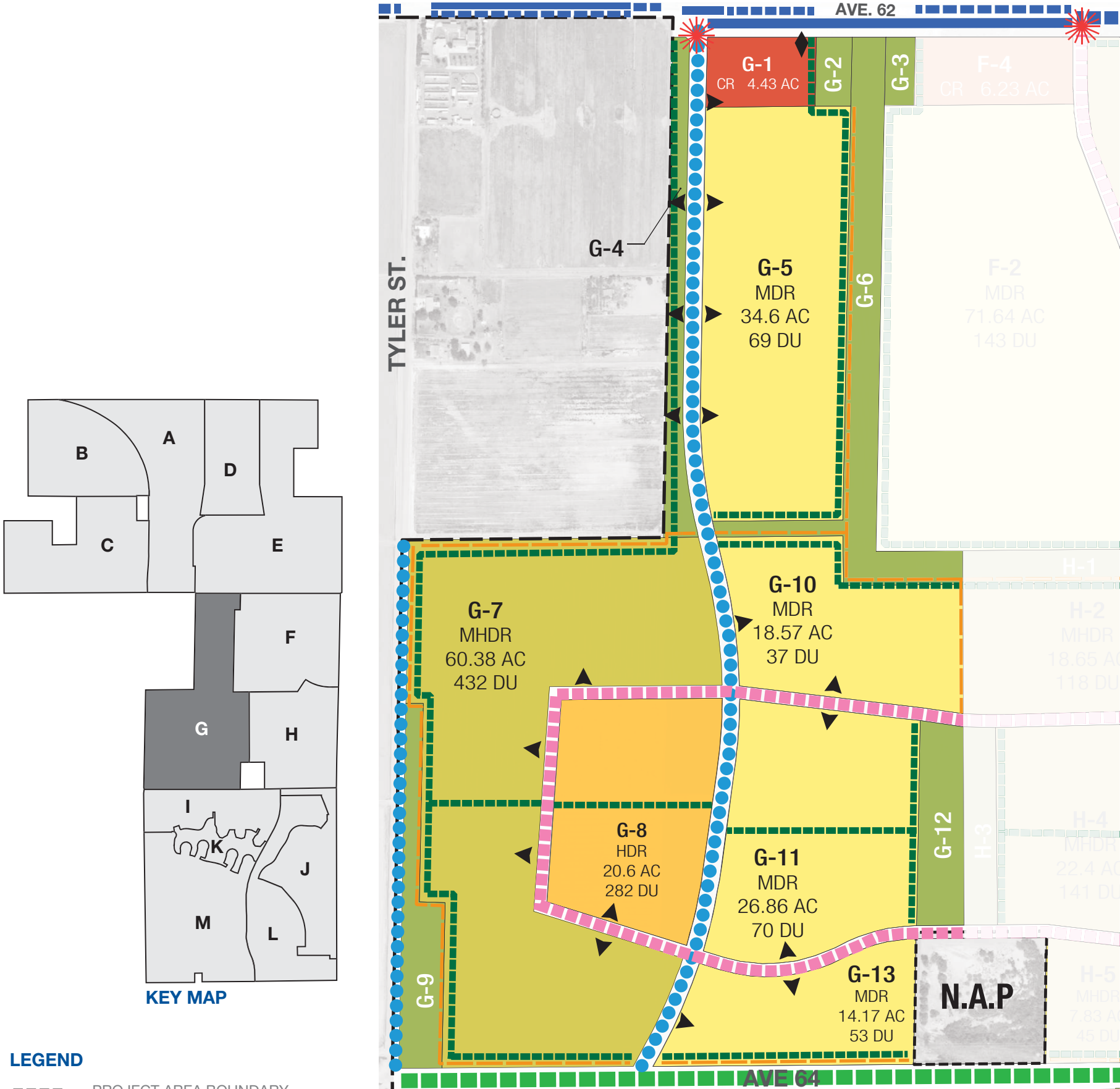
- (1) Planning Area F-4 is located within Zone B1 and C of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-4 is accessed from Avenue 62 and “D” Street.
- (3) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.7 Neighborhood G

Neighborhood G is a predominantly residential neighborhood located roughly in the center of the project site, just east of Tyler Street as depicted in **Figure 3.2-8, Neighborhood F**. Primary access to the neighborhood is provided by "C" Street and Avenue 62. This is a self-contained, inward-focused community that is physically separated from the airport and major circulation routes. A small neighborhood, commercial retail area (4.43 acres) at the northern end of the neighborhood is intended to serve residents of Neighborhood G as well as Neighborhoods C and H. Residential uses include: High Density Residential (20.60 acres), Medium High Density Residential (60.38 acres) and Medium Density Residential (94.20 acres). It is envisioned that this 213.01-acre neighborhood will be a retirement/second home community with a target of 943 dwelling units. Airport safety zones impacting the neighborhood dictate the location of medium density residential uses to the east of "C" Street and discourage the siting of public facilities within the neighborhood. Consequently, the neighborhood is served by public uses to the south in Neighborhood I, located south of Avenue 64. Open space totals 33.40 acres, functioning as part of the storm drainage system and enhanced by windrow plantings, provides a buffer from adjacent neighborhoods and off-site uses. A neighborhood park is located along the eastern edge of the neighborhood, serving Neighborhood H as well. A component of the project-wide trail system is integrated with a power line easement along the neighborhood's southern edge. This trail connects local parks to the north with public facilities to the south.

NEIGHBORHOOD G PLANNING STANDARDS

- (1) Portions of the residential and open space land uses in Neighborhood G are constrained by the OSZ, ERC and ETZ airport zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within Zones B1, C and D of the CLUP for the airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) "C" Street provides access to Planning Areas G-1, G-4, G-5, G-6, G-8, G-10, G-11 and G-13.
- (3) Avenue 62 provides access to Planning Area G-1, G-2, G-3 and G-6.
- (4) The interior east-west loop road provides access to Planning Areas G-7, G-8, G-9, G-10, G-11, G-12 and G-13.
- (5) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**, with the exception of the ETZ in which no trees shall be planted.
- (6) "C" Street will be developed in accordance with **Figure 3.4-13, Secondary Streetscape 1**.
- (7) The interior loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (8) Windrow plantings will be provided as a buffer between land uses and along drainage channels, and will be integrated into the trail system.
- (9) An intersection detail is planned for "C" Street and Avenue 62 to be developed in accordance with **Figure 3.4-5, Intersection Detail**.



NEIGHBORHOOD G PLANNING AREAS**G-1: COMMERCIAL RETAIL**

Planning Area G-1 provides for the development of 4.43 acres of neighborhood commercial land uses.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Areas G-1 is located within Zone B1 of the CLUP for the Jacqueline Cochran Regional Airport and the OSZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-1 is accessed from Avenue 62 and “C” Street.
- (3) The edge treatment between the commercial and adjacent land uses is illustrated in **Figure 3.4-31, Commercial Buffer Streetscape**.
- (4) An intersection detail is located in the northwest corner of this planning area to be developed in accordance with **Figures 3.4-5, Intersection Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-2: OPEN SPACE

Planning Area G-2 provides for 1.44 acres of open space that will be developed as a local park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-2 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and ETZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-2 will be developed as a local park.
- (3) Planning Area G-2 is accessed from Avenue 62.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

G-3: OPEN SPACE

Planning Area G-3 provides of 1.19 acres of open space that will be developed as a local park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-3 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and ETZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-3 will be developed as a local park.
- (3) Planning Area G-3 is accessed from Avenue 62.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

G-4: OPEN SPACE

Planning Area G-4 provides for the development of 5.60 acres of open space that will be part of the drainage system for the site.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-4 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area G-4 is accessed from "C" Street.
- (3) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) This planning area will be developed with the cross-section shown in **Figure 3.4-22, Project Edge**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-5: MEDIUM DENSITY RESIDENTIAL

Planning Area G-5 provides for the development of 34.60 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 for cluster development as defined in Section 2 – Specific Plan Zoning Ordinance. A target of 69 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) The northern half of Planning Area G-5 is within Zone B1 of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. The southern half is located with Zone C of the CLUP. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-5 is accessed from "C" Street.
- (3) The edge treatment between the residential land use and the adjacent commercial uses to the north is illustrated in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) The edge treatment between the residential land use and the adjacent drainage channel is shown in **Figure 3.4-27, Channel Buffer Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-6: OPEN SPACE

Planning Area G-6 provides for the development of 13.65 acres of open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-6 is within the Jacqueline Cochran Regional Airport ETZ and ERC airport safety zones and Zone C of the CLUP. Development within these zones will comply with all applicable restrictions and requirements.
- (2) The northern most portion of the planning area (1.37 acres) will be developed as a local linear park.
- (3) The remainder of this planning area (12.28 acres) will be developed as a graded drainage channel, with a trail provided to connect with local parks to the north and to the south.
- (4) Planning Area G-6 is accessed from Avenue 62 and "C" Street.
- (5) This Planning Area will be developed with the channel cross-section described in **Figure 3.4-27, Channel Buffer Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

G-7: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area G-7 provides for the development of 60.38 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 432 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-7 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-7 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface between this planning area and the adjacent Evacuation Channel is shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (4) The Project Edge Condition for this planning area adjacent to Tyler Street is shown in **Figure 3.4-22, Project Edge**.
- (5) The Project Edge Condition for this planning area and adjacent properties is shown in **Figure 3.4-29, out Parcel/Adjacent Property Buffer**.
- (6) Planning Area G-7 will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (7) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

G-8: HIGH DENSITY RESIDENTIAL

Planning Area G-8 provides for the development of 20.60 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional lots and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 282 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-8 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-8 is accessed from the interior east-west loop road.
- (3) Planning Area G-8 will be developed with a windrow/trail detail, as shown in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

G-9: OPEN SPACE

Planning Area G-9 provides of 6.17 acres of open space to be developed as part of the project drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area G-9 is accessed from the interior east-west loop road.
- (3) The project edge condition for this planning area is indicated in **Figure 3.4-22, Project Edge**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The edge treatment for the interface between this planning area and the adjacent Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

G-10: MEDIUM DENSITY RESIDENTIAL

Planning Area G-10 provides for the development of 18.57 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 under cluster development. A target of 37 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-10 is within the Jacqueline Cochran Regional Airport ERC and Zone C of the CLUP for the airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-10 is accessed from “C” Street and the interior east-west loop road.
- (3) The edge treatment for the interface between the residential uses and adjacent drainage facilities is illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (4) The trail along the eastern edge of the planning area will be developed as described in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-11: MEDIUM DENSITY RESIDENTIAL

Planning Area G-11 provides for the development of 26.86 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 70 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-11 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-11 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface with the adjacent park will be as described in **Figure 3.4-25, Park Buffers**.
- (4) Planning Area G-11 will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-12: OPEN SPACE

Planning Area G-12 provides for 5.35 acres of open space to be developed as a local park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-12 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport ERC and. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-12 will be developed as a local park.
- (3) Planning Area G-12 is accessed from the interior east-west loop road.
- (4) The park buffer between adjacent residential uses is illustrated in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

G-13: MEDIUM DENSITY RESIDENTIAL

Planning Area G-13 provides for the development of 14.17 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 53 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area G-13 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-13 is accessed from "C" Street and the interior east-west loop road.
- (3) The edge treatment for the interface with the adjacent Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

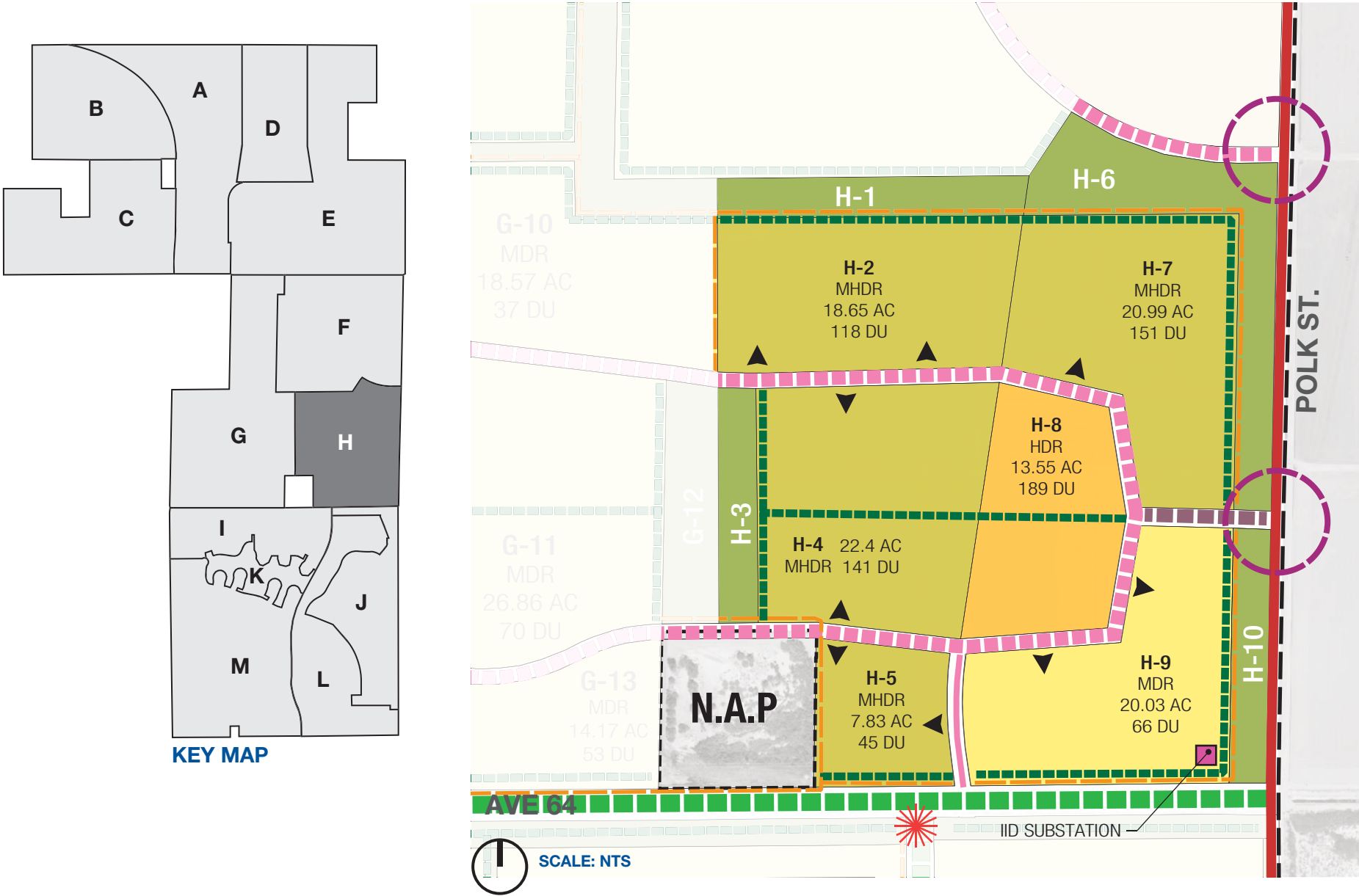
3.2.8 Neighborhood H

Neighborhood H, like Neighborhood G, is a residential neighborhood located in the central portion of the site as depicted in **Figure 3.2-9, Neighborhood H**. It is bounded by Polk Street on the east and the Avenue 64 Evacuation Channel to the south. The neighborhood is accessed from Polk Street and "C" Street. It is separated from the residential uses to the north and off-site uses to the east by a landscaped buffer. Residential uses include: High Density Residential (13.55 acres), Medium High Density Residential (69.87 acres) and Medium Density Residential (20.03 acres). A target of 710 dwelling units is planned in this 127.75-acre retirement/second home community. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Open space totals 24.3 acres, functioning as part of the storm drainage system, and enhanced by windrow plantings, and provides a buffer from adjacent neighborhoods and off-site uses. A neighborhood park is located along the western edge of Neighborhood H, serving Neighborhood G as well. A component of the project-wide trail system is integrated with a power line easement along the neighborhood's southern edge. This trail connects local parks to the north with public facilities to the south.

NEIGHBORHOOD H PLANNING STANDARDS

- (1) The neighborhood is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) The interior loop road provides access to Planning Areas H-1, H-2, H-3, H-4, H-5, H-6, H-7, H-8, H-9 and H-10.
- (3) Polk Street provides access to Planning Areas H-6 and H-10.
- (4) The interior east-west loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (5) The interior north-south loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (6) The project entry from Polk Street will be developed in accordance with **Figure 3.4-16, Industrial Collector**.
- (7) A minor project entry is located at the intersection of Polk Street and the entry to the north-south loop road to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (8) Polk Street will be developed in accordance with **Figure 3.4-10, Arterial Streetscape 2 – Polk Street Streetscape 2B**.
- (9) Windrow plantings will be provided as a buffer between land uses and along drainage channels, and will be integrated into the trail system.



LEGEND

- PROJECT AREA BOUNDARY
- ARTERIAL STREETSCAPE 2, INFORMAL CANOPY
- MAJOR STREETSCAPE 1, PALM TREE
- COLLECTOR STREETSCAPE 2, INFORMAL CANOPY
- COLLECTOR STREETSCAPE 1, FORMAL CANOPY
- INDUSTRIAL COLLECTOR STREETSCAPE, INFORMAL CANOPY
- WINDROW PLANTING
- PROJECT TRAIL
- FULL ACCESS
- INTERSECTION DETAIL
- SUBSTATION
- MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
H-1	Open Space (OS)		5.12	
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0–8.0
H-3	Open Space (OS)		3.79	
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0–8.0
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0–8.0
H-6	Open Space (OS)		11.43	
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0–8.0
H-8	High Density Residential (HDR)	189	13.55	8.0–14.0
H-9	Medium Density Residential (MDR)	66	20.03	2.0–5.0
H-10	Open Space (OS)		3.96	
TOTAL		710	127.75	

NEIGHBORHOOD H PLANNING AREAS**H-1: OPEN SPACE**

Planning Area H-1 provides for the development of 5.12 acres of open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-1 is within Zone C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-1 is accessed from the interior east-west loop road, through Planning Areas H-2 and H-6.
- (3) This planning area will be developed with the channel cross-section illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

H-2: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area H-2 provides for the development of 18.65 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 118 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-2 is within Zones C and of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-2 is accessed from the interior east-west loop road.
- (3) The trail along the western edge of the planning area will be developed as described in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) The edge treatment for the interface between the planning area and adjacent drainage channel is illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

H-3: OPEN SPACE

Planning Area H-3 provides for 3.79 acres of open space to be developed as a local park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-3 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-3 will be developed as a local park.
- (3) Planning Area H-3 is accessed from the interior east-west loop road.
- (4) The edge treatment for this planning area will be as described in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

H-4: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area H-4 provides for the development of 22.40 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 141 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-4 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-4 is accessed from the interior east-west loop road.
- (3) The edge treatment for this interface between the planning area and the adjacent local park will be as illustrated in **Figure 3.4-25, Park Buffers**.
- (4) Planning Area H-4 will be developed with a buffer between the medium density residential uses in the planning area and the high density residential uses in the adjacent planning area as depicted in **Figure 3.4-32, Product Type Buffers**.
- (5) This planning area will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

H-5: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area H-5 provides for the development of 7.83 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 45 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-5 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-5 is accessed from the interior east-west and north-south loop roads.
- (3) The trail along the western edge of the planning area will be as shown in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) The edge treatment for the interface between the planning area residential use and the Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

H-6: OPEN SPACE

Planning Area H-6 provides for the development of 11.43 acres of open space which will be part of the project drainage network and trail system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-6 will be developed to incorporate an element of the project-wide trail system.
- (3) Planning Area H-6 is accessed from Polk Street and the interior east-west loop road.
- (4) This planning area will be developed with the cross-section shown in **Figure 3.4-10, Arterial Streetscape 2/Polk Street Streetscape 2B**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

H-7: MEDIUM HIGH DENSITY RESIDENTIAL

Planning Area H-7 provides for the development of 20.99 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 151 dwelling units is planned within a density range of 5.0–8.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-7 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface between the planning area and adjacent drainage channel is illustrated in **Figure 3.4-10, Arterial Streetscape 2/Polk Street Streetscape 2B**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

H-8: HIGH DENSITY RESIDENTIAL

Planning Area H-8 provides for the development of 13.55 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional lots and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 189 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-8 is accessed from the interior east-west loop road.
- (3) Planning Area H-8 will be developed with a buffer between the high density residential uses in the planning area, and the medium density residential uses in the adjacent planning area as depicted in **Figure 3.4-32, Product Type Buffer**.
- (4) This planning area will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

H-9: MEDIUM DENSITY RESIDENTIAL

Planning Area H-9 provides for the development of 20.03 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 66 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area H-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-9 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface with the planning area and the adjacent drainage channel is illustrated in **Figure 3.4-10, Arterial Streetscape 2/Polk Streetscape 2B**.
- (4) The edge treatment for the interface with the Evacuation Channel is shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

H-10: OPEN SPACE

Planning Area H-10 provides for the development of 3.96 acres of open space as part of the project drainage network and trail system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

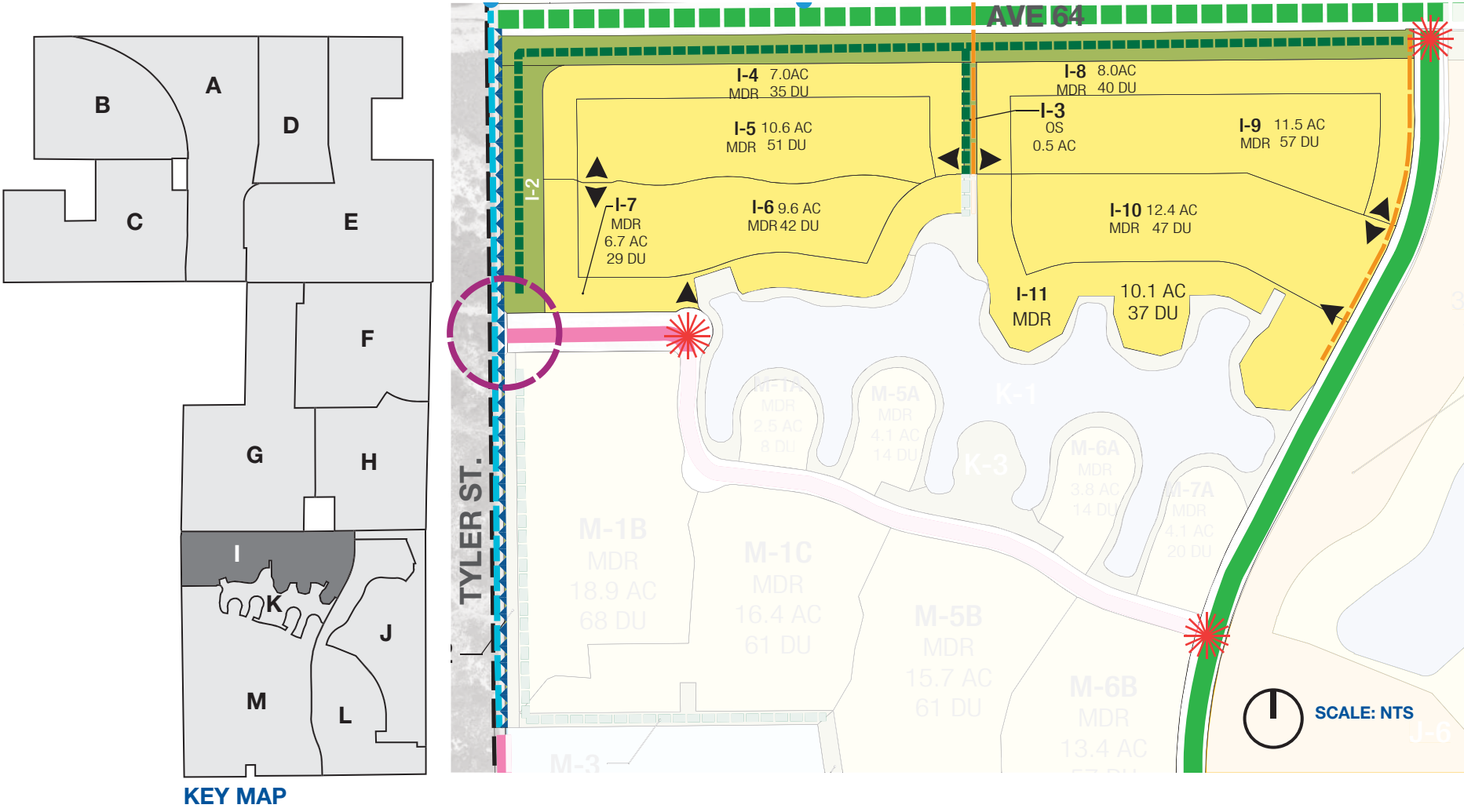
- (1) Planning Area H-10 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-10 is accessed from Polk Street and the interior east-west loop road.
- (3) The edge treatment between the planning area and adjacent residential uses is depicted in **Figure 3.4-10, Arterial Streetscape 2/Polk Streetscape 2B**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.10 Neighborhood I

Neighborhood I is a predominantly residential neighborhood located just south of the Avenue 64 Evacuation Channel, between Tyler and Polk Streets as depicted in **Figure 3.2-10, Neighborhood I**. This neighborhood contains eleven planning areas comprised of 89.9 acres and is the northernmost neighborhood outside of the airport Zone C. Proposed land uses include Medium Density Residential (75.9 acres) and Open Space to be developed as part of the project storm drainage system (14.0 acres). A target of 338 dwelling units is planned for Neighborhood I. The open space area serves as a land use buffer and part of the drainage network. The neighborhood is accessed from Tyler Street, "E" Street, "F" Street and Avenue 64. Within the Kohl Ranch Circulation Plan, "E" Street has been realigned to circulate north and south between Avenue 64 and Avenue 66, replacing its previously proposed curvilinear location south of the evacuation channel.

NEIGHBORHOOD I PLANNING STANDARDS

- (1) The neighborhood is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A Minor entry is located at the intersection of Tyler Street and "F" Street to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (3) Primary access to the neighborhood is provided from Tyler Street, "E" Street, "F" Street, Avenue 64 and the interior loop roads.
- (4) "E" Street provides access to Planning Area I-8, I-9, I-10 and I-11 through interior loop roads.
- (5) 5) Avenue 64 provides access to Planning Areas I-1.
- (6) "F" Street provides access to Planning Areas I-2, I-3, I-4, I-5, I-6, I-7 and I-8 through interior loop roads.
- (7) Tyler Street provides access to Planning Area I-1 and I-2.
- (8) "E" Street and Avenue 64 will be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (9) Tyler Street will be developed in accordance with **Figure 3.4-14, Secondary Streetscape 2**.
- (10) The Evacuation Channel in Planning Area I-1 will be developed with the Canal Buffer depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (11) The intersection of Avenue 64 and "E" Street will be developed with the Intersection Detail as shown in **Figures 3.4-5, Intersection Detail**.
- (12) The east side of Tyler Street will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



NEIGHBORHOOD I PLANNING AREAS**I-1: OPEN SPACE**

Planning Area I-1 provides for the development of 9.6 acres of open space that includes the Avenue 64 Evacuation Channel and land adjacent to the channel.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-1 is accessed from Avenue 64.
- (3) An Intersection Detail will be located at the northeast corner of the planning area as illustrated in **Figure 3.4-5, Intersection Detail**.
- (4) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Planning Area I-1 will be developed with a windrow/trail detail as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) A Class I Bike Path is located in this planning area on the east side of Tyler Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

I-2: OPEN SPACE

Planning Area I-2 provides for the development of 3.9 acres of open space to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from Tyler Street and “F” Street.
- (3) This planning area will be developed with the project edge treatment for the interface between the drainage facilities and project boundary, as shown in **Figure 3.4-22, Project Edge**.
- (4) A Minor Project Entry is located at the southwest corner of this planning area to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (5) A Class I Bike Path is located in this planning area on the east side of Tyler Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

I-3: OPEN SPACE

Planning Area I-3 provides for the development of 0.5 acres of open space that abuts the Avenue 64 Evacuation Channel to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-3 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-3 is accessed from “F” Street through interior loop roads.
- (3) This planning area will be developed with the drainage channel buffer for the interface with adjacent residential uses, as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) The drainage way in Planning Area I-3 will be designed to provide a pedestrian trail that connects with the project-wide trail system as depicted in **Figure 3.4-21 Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

I-4: MEDIUM DENSITY RESIDENTIAL

Planning Area I-4 provides for the development of 7.0 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 35 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-4 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from "F" Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

I-5: MEDIUM DENSITY RESIDENTIAL

Planning Area I-5 provides for 10.6 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development. A target of 51 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-5 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-5 is accessed from "F" Street through interior loop roads.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

I-6: MEDIUM DENSITY RESIDENTIAL

Planning Area I-6 provides for the development of 9.6 acres of medium density residential uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 42 dwelling units is planned within a density range 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-6 is accessed from “F” Street through the interior loop road.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

I-7: MEDIUM DENSITY RESIDENTIAL

Planning Area I-7 provides for the development of 6.7 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 29 dwelling units is planned within a density range 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-7 is accessed from “F” Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

I-8: MEDIUM DENSITY RESIDENTIAL

Planning Area I-8 provides for the development of 8.0 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 40 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-8 is accessed from “E” Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

I-9: MEDIUM DENSITY RESIDENTIAL

Planning Area I-9 provides for the development of 11.5 acres of medium density residential uses with minimum lot sizes of 5,000 square feet for conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 57 dwelling units is planned within a density range 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from “F” Street through interior loop roads.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

I-10: MEDIUM DENSITY RESIDENTIAL

Planning Area I-10 provides for the development of 12.4 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development. A target of 47 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-10 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-10 is accessed from “E” Street.
- (3) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

I-11: MEDIUM DENSITY RESIDENTIAL

Planning Area I-11 provides for the development of 10.1 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 37 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area I-11 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-11 is accessed from “E” Street.
- (3) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.11 Neighborhood J

Neighborhood J is located in the southern section of the project site as depicted in **Figure 3.2-11, Neighborhood J**. The 110.66-acre neighborhood is located between "E" Street and Polk Street, just south of Avenue 64. It contains eight planning areas consisting of the following land uses: Medium Density Residential (70.06 acres) and Open Space (40.6 acres). A target of 332 units is planned for Neighborhood J. Access to the neighborhood is provided from "E" Street and Polk Street. The significant amount of open space within and adjacent to the neighborhood functions as part of the drainage system and provides a scenic amenity and buffer along Polk Street, as well as a local park. If developed with a golf course, a permitted use under provisions of Section 2 – Specific Plan Zoning, Neighborhood J would be equipped with a clubhouse which would provide meeting, dining and recreational opportunities to neighborhood residents.

NEIGHBORHOOD J PLANNING STANDARDS

- (1) A golf course use is permitted in all of the planning areas. A clubhouse is permitted in one of the planning areas. Additionally, recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted in all of the planning areas.
- (2) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (3) Primary access to Neighborhood J is from "E" Street, Avenue 64, Polk Street, and the interior loop roads.
- (4) "E" Street provides access to Planning Area J-7.
- (5) Polk Street provides access to Planning Areas J-1 through J-8.
- (6) An Intersection Detail is located at the intersection of Avenue 64 and "E" Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (7) A Minor Entry is located at the intersection of Polk Street and the interior east-west loop road to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (8) "E" Street will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (9) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (10) Avenue 64 will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (11) The west side of Polk Street will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



NEIGHBORHOOD J PLANNING AREAS**J-1: MEDIUM DENSITY RESIDENTIAL**

Planning Area J-1 provides for 30.64 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 143 dwelling units is planned for this planning area within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, demonstrations, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-1 is accessed from “E” Street, Polk Street, and interior loop roads.
- (4) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Planning Area J-1 will be developed in accordance with **Figure 3.4-32, Product Type Buffer** between the medium density residential uses within the planning area and the high density residential uses in the adjacent planning area.
- (6) The edge treatment for the interface between the residential uses and adjacent drainage facilities is shown in **Figure 3.4-28, Airpark Drainage Buffer**.
- (7) A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

J-2: OPEN SPACE

Planning Area J-2 provides for the development of 22.56 acres of open space to be developed as a lake and as part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-2 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, demonstrations, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) If the golf course or recreational lakes are not developed, this planning area will be developed with the edge treatment shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (4) Planning area J-2 is accessed from Polk Street, “E” Street, and interior loop roads.
- (5) The edge treatment for the interface between the drainage facilities and adjacent residential uses is shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

J-3: OPEN SPACE

Planning Area J-3 provides for the development of 11.24 acres of open space to be a landscaped perimeter to the lake, containing a walking trail for the use and enjoyment of project residents.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-3 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, demonstrations, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-3 is accessed from Polk Street, “E” Street, and the interior loop roads. All planning areas abutting the lake will have access to the lake perimeter trail.
- (4) The edge treatment for the interface with the adjacent drainage facilities is shown in **Figure 3.4-23, Drainage Canal Edge Condition.**
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

J-4: MEDIUM DENSITY RESIDENTIAL

Planning Area J-4 provides for the development of 39.42 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 189 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-4 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course, including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, demonstrations, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-4 is accessed from Polk Street.
- (4) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-24, Golf Course (Racetrack) Edge Condition.**
- (5) A pedestrian trail is located along the eastern boundary of this planning area to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail.**
- (6) A minor project entry is located in the southeast corner of this planning area to be developed in accordance with **Figure 3.4-4, Minor Entry.**
- (7) If the golf course or recreational lakes are not developed, this planning area will be developed with the edge treatment shown in **Figure 3.4-23, Drainage Canal Edge Condition.**
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

J-5: OPEN SPACE

Planning Area J-5 provides for the development of 1.2 acres of open space to be developed as a local park or open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-5 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, demonstrations, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-5 is accessed from Polk Street and through interior loop roads.
- (4) The edge treatment for the interface between the park and adjacent residential uses is shown in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

J-6: OPEN SPACE

Planning Area J-6 provides for the development of 4.1 acres of open space to be developed as a park or open space.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-6 is accessed from “E” Street, Polk Street., and through interior loop roads.
- (4) The edge treatment for the interface between the park and adjacent residential land uses is illustrated in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

J-7: OPEN SPACE

Planning Area J-7 provides for the development of 4.24 acres of open space that includes the Avenue 64 Evacuation Channel and land adjacent to the channel.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning area J-7 is accessed from Avenue 64.
- (3) An Intersection Detail is located in the northwest corner of the planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) A pedestrian trail will be developed along the eastern boundary of the planning area to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (5) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

J-8: OPEN SPACE

Planning Area J-8 provides for the development of 1.16 acres of open space to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area J-8 is within Zones D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area J-8 is accessed from Polk Street.
- (3) The drainage way in Planning Area J-8 will be designed to provide a pedestrian trail that connects with the project-wide trail system as depicted in **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figures 3.1-10, Public Facilities**.
- (4) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land uses is depicted in **Figure, 3.4-23, Drainage Canal Edge Condition**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

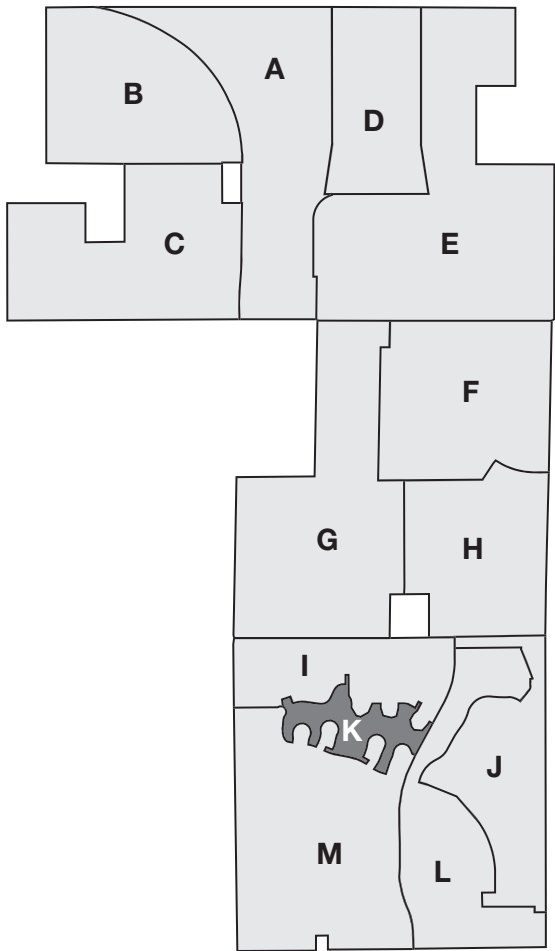
3.2.12 Neighborhood K

Neighborhood K is located in the southern portion of the project site as depicted in **Figure 3.2-12, Neighborhood K**. The neighborhood is 34.4 acres of open space with no dwelling units planned. The lake provides a scenic amenity, includes support of open space features such as a local park and recreation center, and functions as the dual water system (irrigation storage) and as part of the project drainage network. Primary access to the neighborhood is provided from "E" Street and "F" Street.

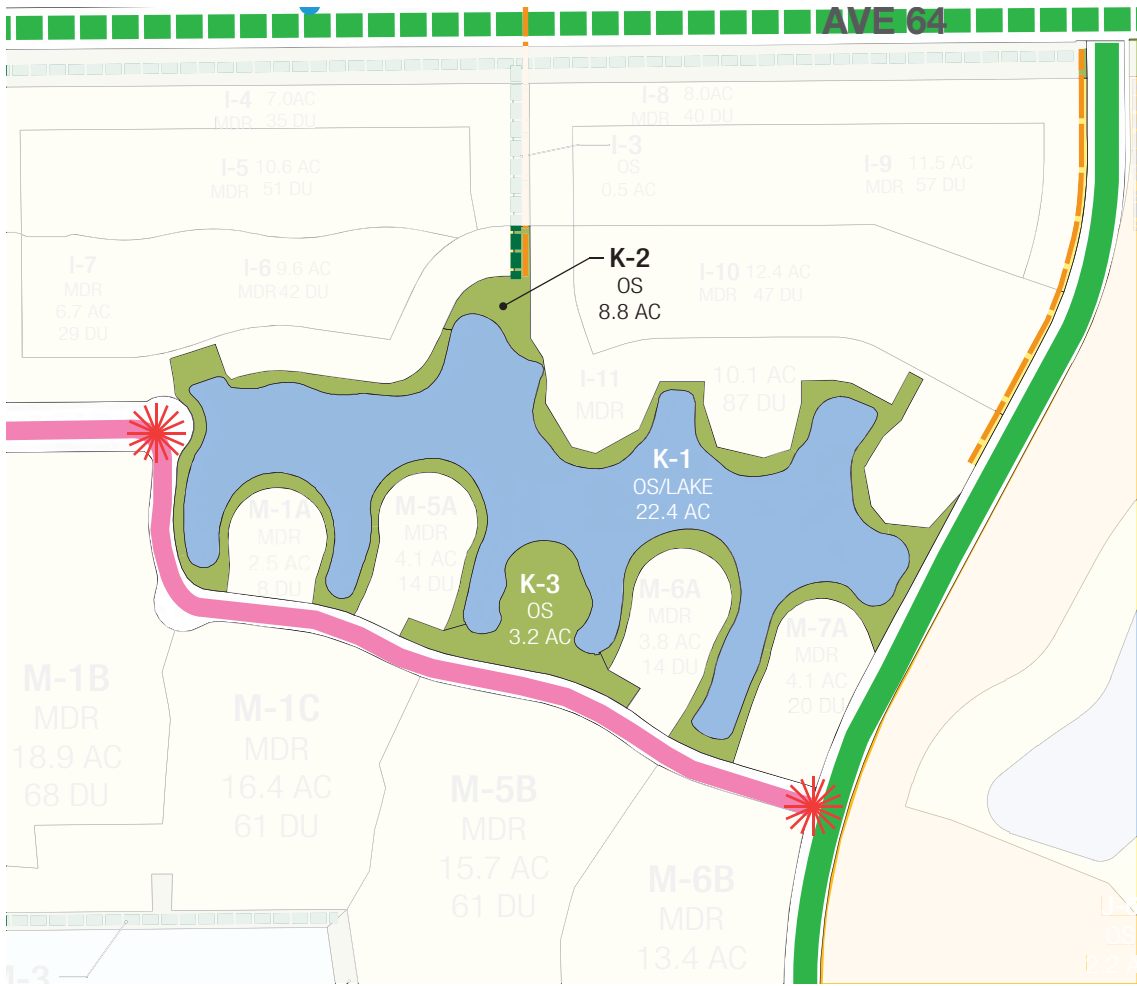
NEIGHBORHOOD K PLANNING STANDARDS

- (1) The neighborhood is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) is a permitted use in all of the planning areas.
- (3) Primary access to the neighborhood is provided by "E" Street and "F" Street as depicted in **Figure 3.3-1, Community Character**.
- (4) "E" Street provides access to Planning Area K-1 to K-3.
- (5) "F" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (6) "E" Street will be developed in accordance with **Figure 3.4-12, Major Streetscape 2**.

FIGURE 3.2-12 NEIGHBORHOOD K



KEY MAP



SCALE: NTS

LEGEND

- MAJOR STREETScape 1, PALM TREE
- MAJOR STREETScape 2, FORMAL CANOPY
- COLLECTOR STREETScape 1, FORMAL CANOPY
- WINDROW PLANTING
- PROJECT TRAIL
- INTERSECTION DETAIL

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
K-1	Open Space/Lake (OS-LAKE)		22.4	
K-2	Open Space (OS)		8.8	
K-3	Open Space (OS)		3.2	
TOTAL		0	34.40	

NEIGHBORHOOD K PLANNING AREAS**K-1: Open Space**

Planning Area K-1 provides for the development of 22.4 acres of open space to be developed as a lake for the for the dual water system (irrigation storage), aesthetics and detention as part of the project drainage network.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area K-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-1 is accessed from “E” Street and “F” Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

K-2: Open Space

Planning Area K-2 provides for 8.8 acres of open space to be a landscaped perimeter to the lake, containing a walking trail for the use and enjoyment of project residents.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area K-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-2 is accessed from “E” Street and “F” Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

K-3: Open Space

Planning Area K-3 provides for the development of 3.2 acres of open space to be developed as a park.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

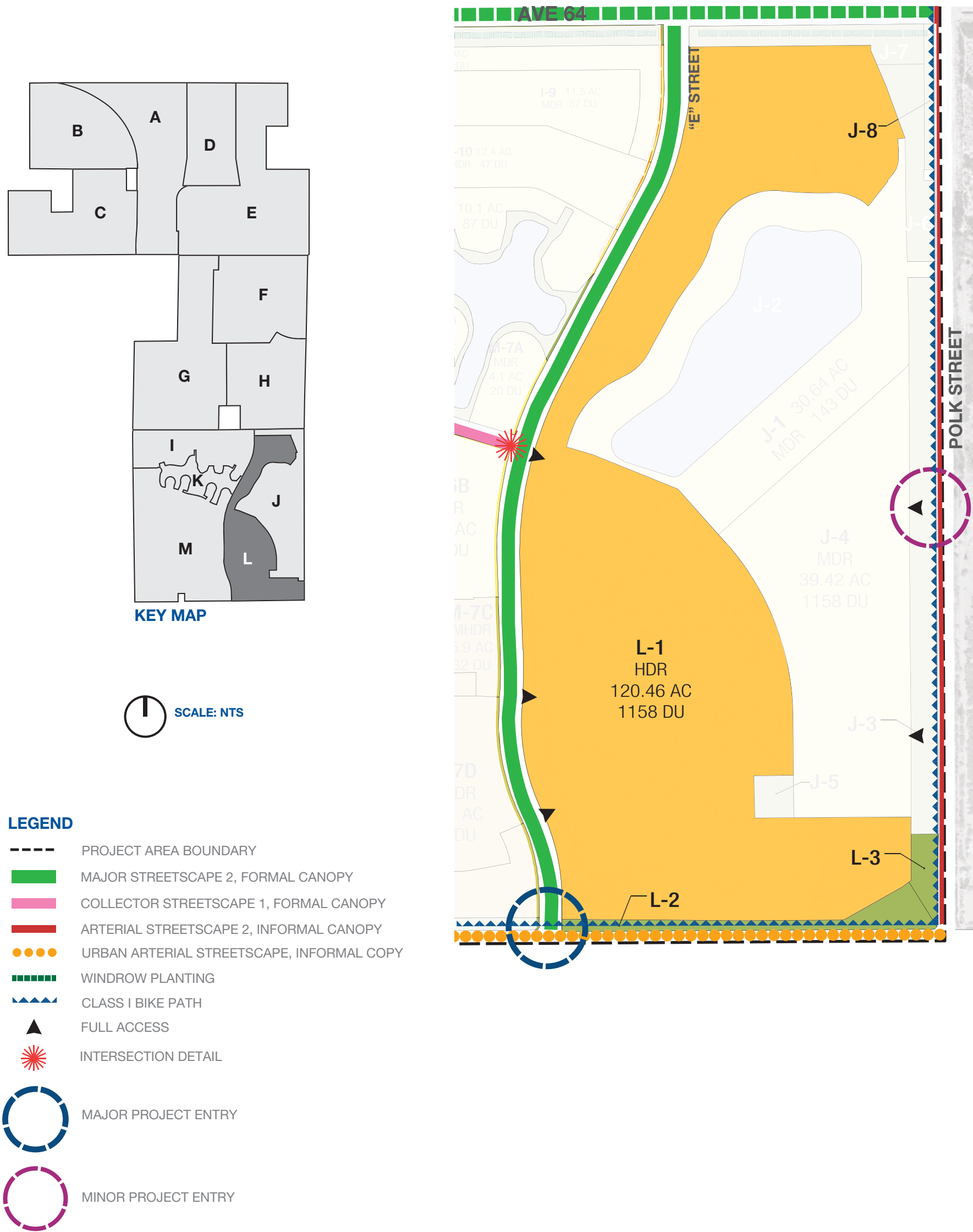
- (1) Planning Area K-3 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-3 is accessed from “F” Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.13 Neighborhood L

Neighborhood L is located in the southern portion of the project site as depicted in **Figure 3.2-13, Neighborhood L**. The 125.96-acre neighborhood is located in the southeast corner of the project site and is bounded by and accessed from Polk Street on the east, Avenue 66 on the south and "E" Street on the west. Land uses consist of High Density Residential (120.46 acres) and open space (5.5 acres). A target of 1,158 dwelling units is planned for Neighborhood L. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Higher density residential uses are located at the project periphery, with lower density uses more interior to the site. If developed with a golf course, a permitted use under the provisions of Section 2 – Specific Plan Zoning, Neighborhood L would be equipped with a significant recreational and visual amenity.

NEIGHBORHOOD L PLANNING STANDARDS

- (1) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use and/or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) is a permitted use in all of the planning areas.
- (3) Primary access is taken from "E" Street, Avenue 66 and Polk Street.
- (4) "E" Street provides access to Planning Area L-1
- (5) Avenue 66 provides access to Planning Areas L-2.
- (6) Polk Street provides access to Planning Area L-3.
- (7) A Major Project Entry is located at the intersection of "E" Street and Avenue 66 to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (8) A Minor Entry is located at the intersection of Polk Street with the interior loop road to be developed in accordance with **Figure 3.4-4, Minor Project Entry**.
- (9) Avenue 66 will be developed in accordance with **Figure 3.4-7, Urban Arterial Streetscape**.
- (10) "E" Street will be developed in accordance with **Figure 3.4-12, Major Streetscape 2**.
- (11) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (12) The west side of Polk Street and the north side of Avenue 62 will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
L-1	High Density Residential (HDR)	1,158	120.46	8.0–14.0
L-2	Open Space (OS)		4.1	
L-3	Open Space (OS)		1.40	
TOTAL		1158	125.96	

NEIGHBORHOOD L PLANNING AREAS**L-1: HIGH DENSITY RESIDENTIAL**

Planning Area L-1 provides for the development of 120.46 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional development and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 1,158 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area L-1 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course, including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, demonstrations, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-1 is accessed from Avenue 66 and “E” Street.
- (4) The edge treatment for the interface between the residential land use and adjacent park use is shown in **Figure 3.4-25, Park Buffers**.
- (5) The edge treatment for the interface between the residential land uses and adjacent drainage facilities is shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

L-2: OPEN SPACE

Planning Area L-2 provides for the development of 4.1 acres of open space to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area L-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, demonstrations, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-2 is accessed from Avenue 66.
- (4) A major project entry is located in the southwest corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land use is depicted in **Figure 3.4-28, Airpark Drainage Buffer**.
- (6) The drainage way in Planning Area L-2 will be designed to provide a pedestrian trail that connects with the project-wide trail system in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the north side of Avenue 66 as illustrated in **Figure 3.1-10, Public Facilities**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

L-3: OPEN SPACE

Planning Area L-3 provides for the development of 1.4 acres of open space to be developed as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

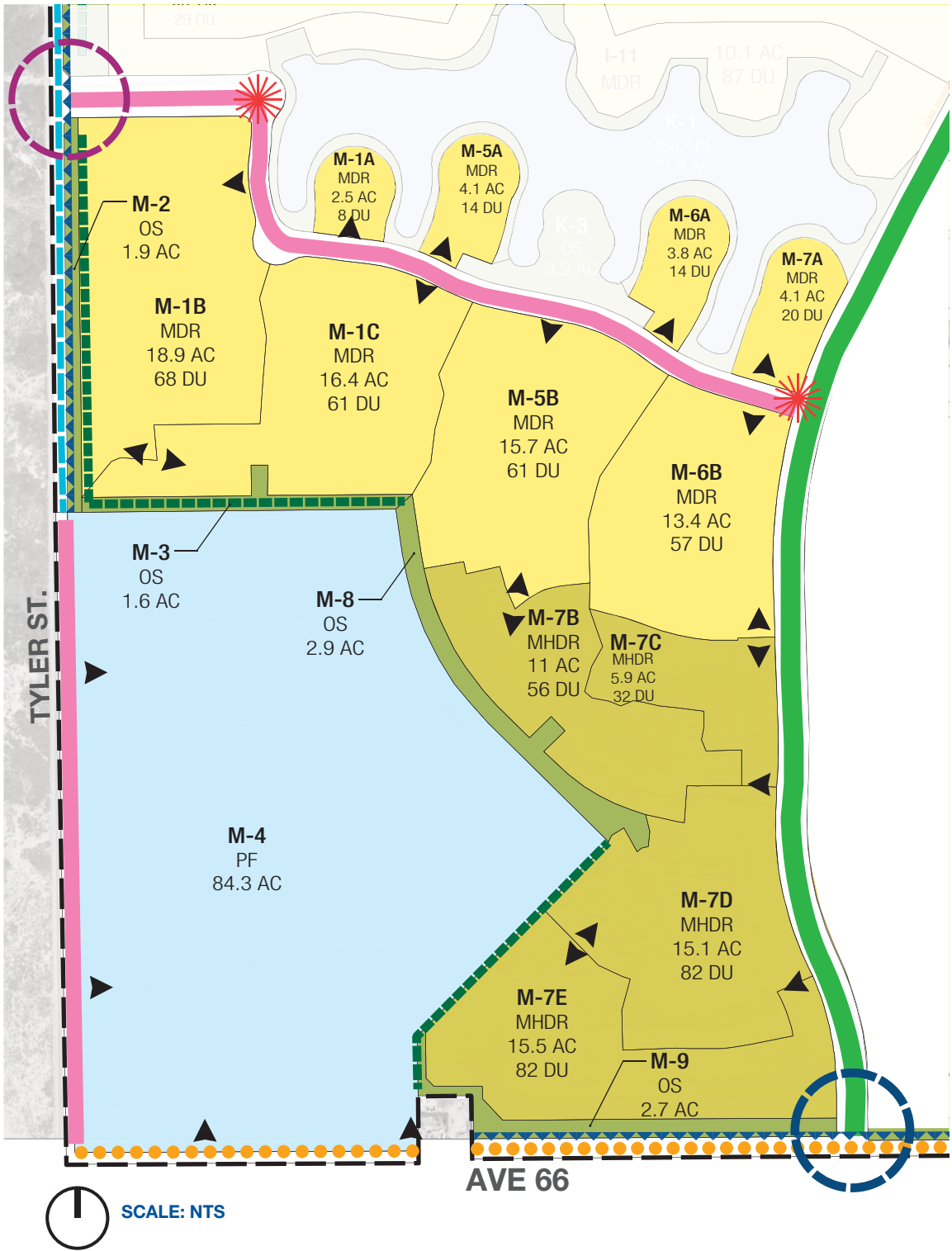
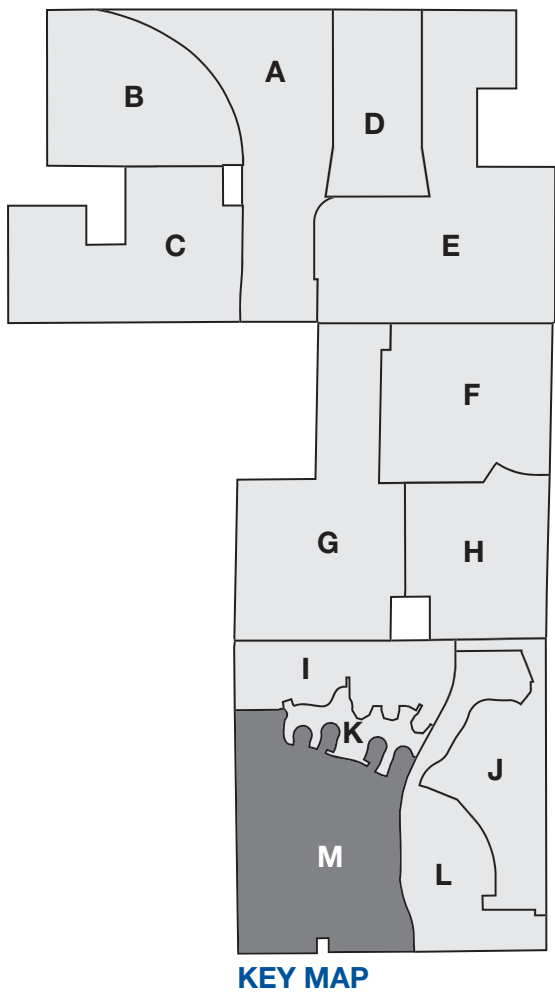
- (1) Planning Area L-3 is within Zone E of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, surfing, swimming, demonstrations, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-3 is accessed from Polk Street.
- (4) A major project entry is located in the southwest corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The drainage way in Planning Area L-3 will be designed to provide a pedestrian trail that connects with the project-wide trail system to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (6) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land uses is depicted in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

3.2.14 Neighborhood M

Neighborhood M is located in the southwest corner of the project site, north of Avenue 66 to "F" Street between Tyler Street and "E" Street as depicted in **Figure 3.2-14, Neighborhood M**. This 219.8-acre neighborhood contains Medium Density Residential (78.90 acres) and Medium High Density Residential (47.50 acres). A target of 555 dwelling units is planned for Neighborhood M. This neighborhood also includes 84.3 acres of Public Facilities/Schools. Open Space totals 9.1 acres and functions as part of the drainage network, provides a buffer along Tyler Street, and provides a buffer along the residential interface with the school site. Under a golf course scenario and recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) scenario, open space would provide active recreational benefits as well. Primary access to Neighborhood M is from Tyler Street, "E" Street, Avenue 66 and "F" Street.

NEIGHBORHOOD M PLANNING STANDARDS

- (1) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course and/or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in all of the planning areas.
- (3) Primary access is provided by Tyler Street, Avenue 66, "E" Street and "F" Street.
- (4) A Major Project Entry is located at the intersection of "E" Street and Avenue 66 to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) A Minor Project Entry is located at "F" Street and Tyler Street to be developed in accordance with **Figure 3.4-4, Minor Project Entry**.
- (6) "F" Street provides access to Planning Areas M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-6B and M-7A.
- (7) The intersection of "F" Street and "E" Street will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (8) "F" Street, just east of the Minor Project Entry on Tyler Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (9) "E" Street provides access to Planning Areas M-6B, M-7C, M-7D, M-7E and M-9.
- (10) "E" Street will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (11) "F" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (12) Avenue 66 will be developed in accordance with **Figure 3.4-7, Urban Arterial Streetscape**.
- (13) Tyler Street, south of "F" Street to the boundary of Planning Area M-2 will be developed in accordance with **Figure 3.4-15, Modified Secondary Streetscape 2**. Tyler Street, south of Planning Area M-2 to Avenue 66 will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (14) The north side of Avenue 62 will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



LEGEND

- MAJOR STREETSCAPE 2, FORMAL CANOPY

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COLLECTOR STREETSCAPE 1, FORMAL CANOPY

●●●●

URBAN ARTERIAL STREETSCAPE, INFORMAL COPY

MODIFIED SECONDARY STREETSCAPE, FORMAL CANOPY

■

WINDROW PLANTING

▲▲▲▲

CLASS I BIKE PATH

▲

FULL ACCESS

✱

INTERSECTION DETAIL
- MAJOR PROJECT ENTRY

MINOR PROJECT ENTRY

Planning Area	Land Use Code	Target Dwelling Units	Acres (Label)	Density Range
M-1A	Medium Density Residential (MDR)	8	2.5	2.0–5.0
M-1B	Medium Density Residential (MDR)	68	18.9	2.0–5.0
M-1C	Medium Density Residential (MDR)	61	16.4	2.0–5.0
M-2	Open Space (OS)		1.9	
M-3	Open Space (OS)		1.6	
M-4	Public Facility (PF)		84.3	
M-5A	Medium Density Residential (MDR)	14	4.1	2.0–5.0
M-5B	Medium Density Residential (MDR)	61	15.7	2.0–5.0
M-6A	Medium Density Residential (MDR)	14	3.8	2.0–5.0
M-6B	Medium Density Residential (MDR)	57	13.4	2.0–5.0
M-7A	Medium Density Residential (MDR)	20	4.1	2.0–5.0
M-7B	Medium High Density Residential (MHDR)	56	11	5.0–8.0
M-7C	Medium High Density Residential (MDR)	32	5.9	5.0–8.0
M-7D	Medium High Density Residential (MDR)	82	15.1	5.0–8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0–8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
TOTAL		555	219.80	

NEIGHBORHOOD M PLANNING AREAS**M-1A: Medium Density Residential**

Planning Area M-1A provides for the development of 2.5 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 8 dwelling units is planned within a density range of 2.0–5.0 du/acre.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-1A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-1A is accessed from “F” Street.
- (4) The edge treatment for the interface between the residential uses and the lake uses in the adjacent planning area is depicted in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

M-1B: Medium Density Residential

Planning Area M-1B provides for the development of 18.9 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development. A target of 68 dwelling units is planned within a density range 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-1B is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area M-1B is accessed from “F” Street.
- (3) The buffer between the Planning Area M-1B’s residential uses and the adjacent lake facilities and will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

M-1C: Medium Density Residential

Planning Area M-1C provides for the development of 16.4 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 61 dwelling units is planned within a density range 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-1C is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area M-1C is accessed from “F” Street.
- (3) The buffer between the Planning Area M-1C’s residential uses and the adjacent lake facilities and will be developed in accordance with **Figure 3.4-23, Drainage Canal Edge Condition**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-2: Open Space

Planning Area M-2 provides for the development of 1.9 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-2 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-2 is accessed from Tyler Street.
- (4) The northwest corner of this planning area will be developed in accordance with a **Figure 3.4-4, Minor Project Entry**.
- (5) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) The buffer between the Planning Area M-2's drainage facilities and the adjacent residential uses will be developed as shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

M-3: Open Space

Planning Area M-3 provides for the development of 1.60 acres of open space to contain part of the southern drainage channel.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-3 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-3 is accessed through the loop circulation system within Planning Area M-1B, from “F” Street.
- (4) This planning area will be developed with the edge conditions for the interface between drainage facilities and adjacent residential land uses, as shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (5) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

M-4: Public Facilities/Schools

Planning Area M-4 is an existing 84.30-acre public facilities/schools site owned by the Coachella Valley Unified School District housing the Las Palmitas Elementary School, Toro Canyon Middle School, and Desert Mirage High School.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-4 is accessed from Tyler Street and Avenue 66.
- (2) Planning Area M-4 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

M-5A: Medium Density Residential

Planning Area M-5A provides for the development of 4.1 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 14 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-5A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-5A is accessed from “F” Street.
- (3) This planning area will be developed in accordance with **Figure 3.4-25, Park Buffers** between planning areas residential and adjacent park uses.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-5B: Medium Density Residential

Planning Area M-5B provides for the development of 15.7 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 61 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-5B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-5B is accessed from “F” Street.
- (3) This planning area will be developed in accordance with **Figure 3.4-23, Drainage Canal Edge Condition** for the interface with adjacent drainage facilities.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-6A: Medium Density Residential

Planning Area M-6A provides for the development of 3.8 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 14 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-6A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-6A is accessed from “F” Street.
- (3) This planning area will be developed with in accordance with **Figure 3.4-25, Park Buffers** between residential land and adjacent park uses.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-6B: Medium Density Residential

Planning Area M-6B provides for the development of 13.4 acres of medium density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 57 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-6B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-6B is accessed from “E” Street and “F” Street.
- (3) The northeast corner of this planning area will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-7A: Medium Density Residential

Planning Area M-7A provides for the development of 4.1 acres of medium density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 20 dwelling units is planned within a density range of 2.0–5.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7A is accessed from “E” Street and “F” Street.
- (3) The southeast corner of this planning area will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-7B: Medium High Density Residential

Planning Area M-7B provides for the development of 11.0 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 56 dwelling units is planned within a density range of 5.0–8.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7B is accessed from “E” Street through interior loop roads.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-7C: Medium High Density Residential

Planning Area M-7C provides for the development of 5.9 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 32 dwelling units is planned within a density range of 5.0–8.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7C is within Zone E of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7C is accessed from "E" Street.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

M-7D: Medium High Density Residential

Planning Area M-7D provides for the development of 15.1 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 82 dwelling units is planned within a density range of 5.0–8.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7D is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7D is accessed from "E" Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-7E: Medium High Density Residential

Planning Area M-7E provides for the development of 15.5 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 82 dwelling units is planned within a density range of 5.0–8.0 du/ac.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-7E is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7E is accessed from “E” Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-8: Open Space

Planning Area M-8 provides for the development of 2.9 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-8 is accessed from the loop circulation road within Planning Area M-5B and from the loop circulation road within Planning Area M-7B via “E” Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

M-9: Open Space

Planning Area M-9 provides for the development of 2.7 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

Land Use and Development Standards

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

Planning Standards

- (1) Planning Area M-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-9 is accessed from the intersection of Avenue 66 and "E" Street.
- (3) This planning area will be developed with the edge conditions for the interface with the drainage facilities and adjacent residential land use, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) A Major Project Entry is located in the southeast corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The drainage way in Planning Area M-9 will be designed to provide a pedestrian trail that connects with the project-wide trail system to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the north side of Avenue 66 as illustrated in **Figures 3.1-10, Public Facilities**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

3.3 DESIGN GUIDELINES

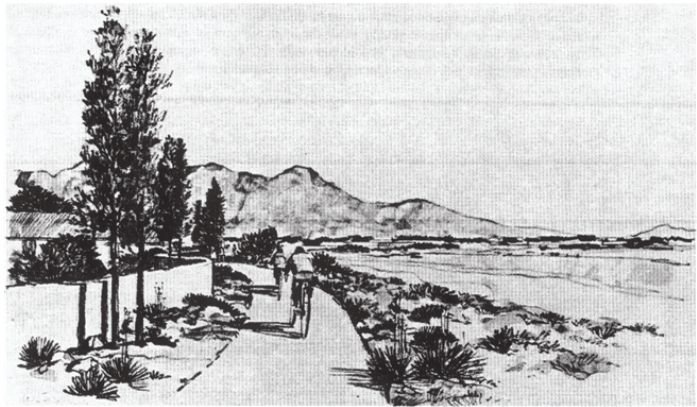
3.3.1 Purpose and Intent

The following Design Guidelines have been developed as a method of achieving a high-quality, cohesive design fabric for the Kohl Ranch Specific Plan area. The objectives of these guidelines are:

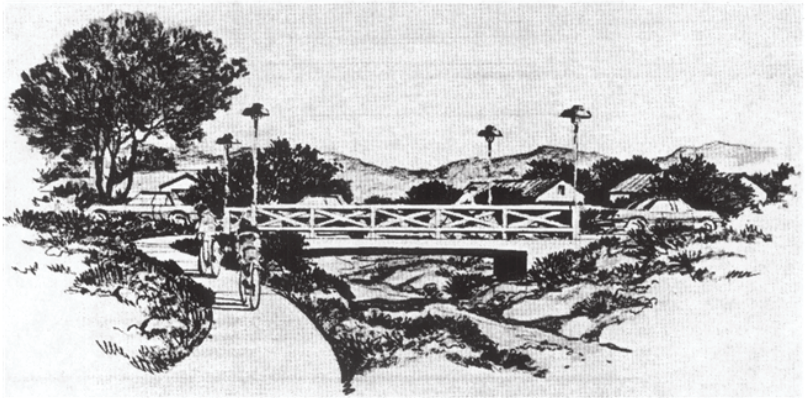
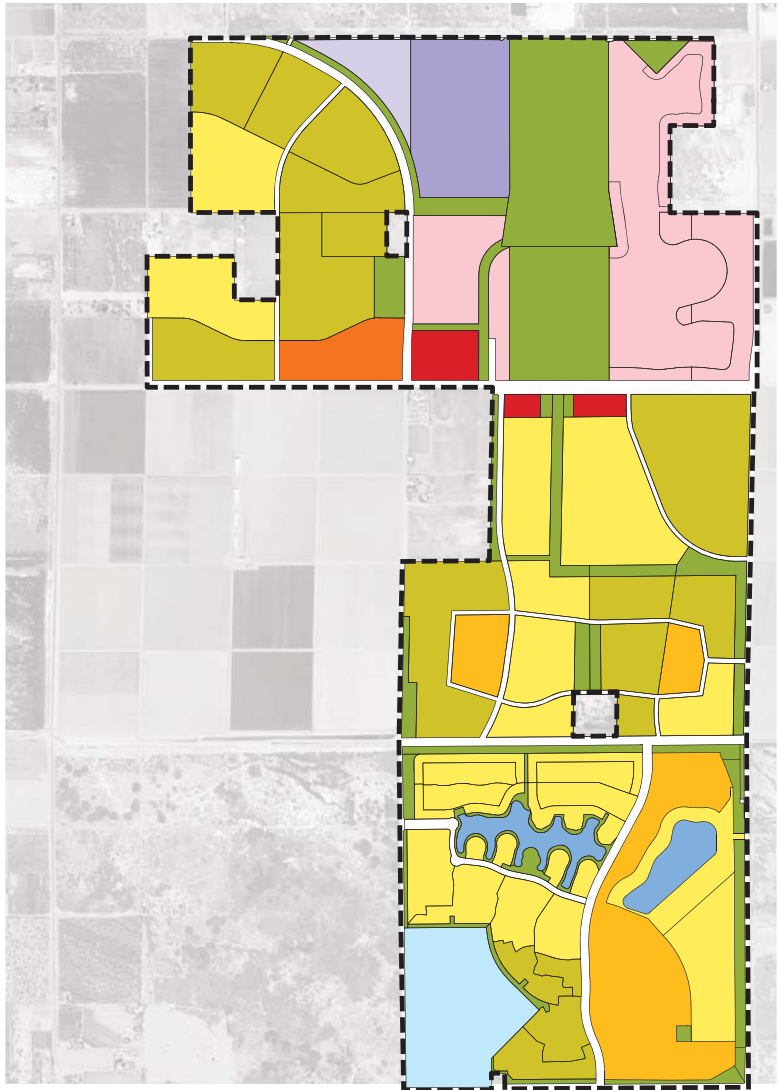
- to provide the County of Riverside with the necessary assurance that the Specific Plan area will develop in accordance with the quality and character proposed herein;
- to serve as design criteria for developers, builders, engineers, architects and landscape architects, and other professionals in preparing plans for construction; and
- to lend guidance to staff, the Joint Powers Authority and the Board of Supervisors in the review and evaluation of future development projects in the Specific Plan area.

Certain key design elements will contribute significantly to the visual order and consistency of the entire Specific Plan area. These common features—site planning, architecture, landscape architecture, signage, lighting and other details—are the subject of the Design Guidelines. The guidelines express the desired character of future development, which is represented conceptually in **Figure 3.3-1, Community Character**. Each guideline will be considered in terms of how it relates to a given development proposal, during the review process, and will be applied to the selected land use concept, as appropriate.

The Design Guidelines are intended to be flexible and are illustrative in nature. Therefore, over time they can respond to unanticipated conditions, such as changes in taste, community desires and the marketplace. Fundamental principles that are the foundation of the Design Guidelines include the coordinated use of land for passive and active recreational open space, to handle storm drainage and to comply with restrictions of the Jacqueline Cochran Regional Airport Safety Zones. In addition, use of landscaping materials and architectural treatments should be compatible with the desert landscape as well as the rich agricultural tradition of the Coachella Valley.



TRAIL ALONG EDGE



TRAIL



PARK



GOLF COURSE EDGE



COMMERCIAL



STREETSCAPE

Compliance with County Design Guidelines

The Design Guidelines and Development Standards for the Kohl Ranch Specific Plan are intended to establish community guidelines and standards for the project to ensure the creation of a quality and aesthetically pleasing environment. Section 3.4, Landscape Guidelines, contains the following subsections: Landscape Concept, Entries, Intersections, Streetscapes, Edges, Buffers, Landscape Palette, Paths and Trails, Walls and Fences, Lighting and Signage. Section C.4., Site Development Guidelines, contains the following subsections: Site Planning and Architectural Guidelines.

The Kohl Ranch Design Guidelines and Development Standards were developed prior to adoption of the Countywide Design Standards and Guidelines criteria (adopted January 13, 2004). Nonetheless, the adopted standards support the development of neighborhoods within the Specific Plan which will meet or exceed the intent and standards contained within the County-wide document. The Kohl Ranch Design Guidelines and Development Standards are consistent with the intent of the Design Strategies and Planning Objectives contained in the Countywide Design Standards and Guidelines (Sections I, Introduction, II, Residential, III, Residential Street Design, and IV, Landscape Design Standards). The Kohl Ranch Specific Plan complies with the standards, goals and objectives by, *"creating quality and visually distinctive development responsive to the natural and built environment of Riverside County"* (Countywide document, page 6).

Prior to approval of tentative tract maps, the Planning Commission and the Board of Supervisors may seek more site-specific information about whether or not the Countywide Design Standards and Guidelines will be met. Standards and features of design implementation illustrating specifically how these general guidelines will be implemented may help guide decision makers during public hearings. Therefore, a project proponent shall submit a design manual to the Planning Department for review and approval prior to public hearings on individual tentative tract maps. Each design manual shall be keyed to the Specific Plan planning area within the project site, and each shall identify how and where particular design features of note will be realized within the development.

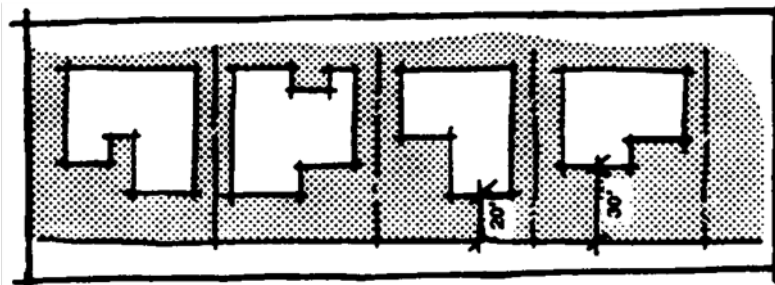
3.3.2 Site Development Guidelines

Site Planning

The intent of site planning is to integrate buildings and site improvements into a unified setting with minimal impact of the development on adjacent land uses. Site planning concentrates on the proper placement of buildings, roads and services.

The goal of residential site planning is to create a neighborhood fabric that offers a unique, safe and visually appealing environment to those in and around the development. The site plan should:

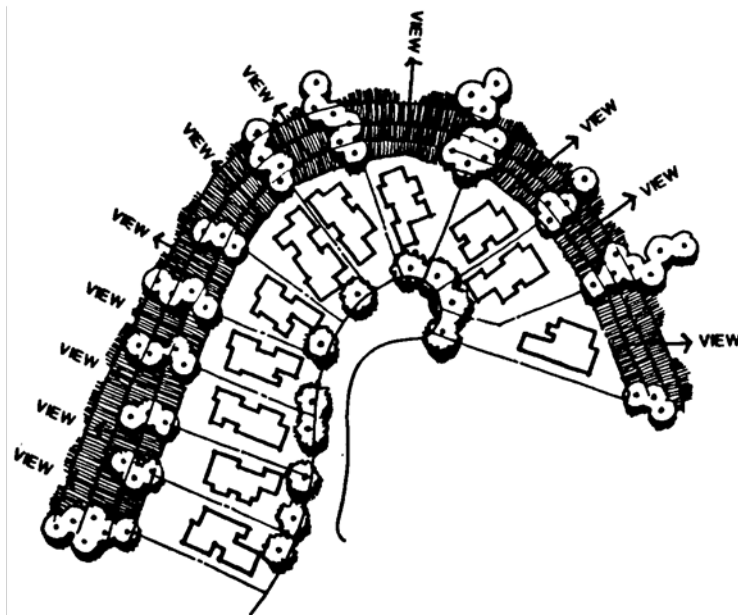
- Effectively accommodate the residential unit types proposed.
- Maximize unit exposure to parks, greenbelts and other amenities.
- Use of geometric "grid" layouts should be allowed.
- Cul-de-sac and curvilinear street layouts should be encouraged.
- Employ the latest techniques of energy-efficient/cost-effective subdivision design.
- Accommodate street drainage or underground drainage in accordance with generally accepted principals.
- Subdivision layouts should discourage through traffic while still permitting adequate emergency vehicle access.
- Variations of the building footprint with cut-outs and pop-outs can assure variety in a plan. Further variety and interest can be developed with plans by varying setback dimensions and positions of buildings.



Varying Setback Dimensions

Single-Family Detached Residential Site Planning

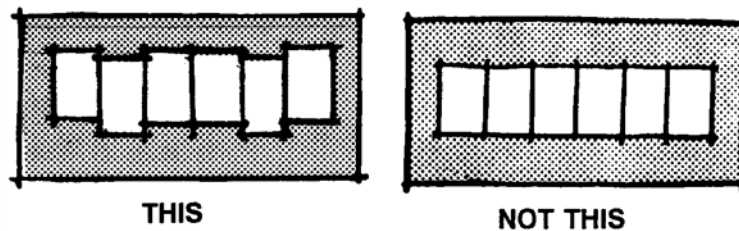
- To the extent possible, units should have views from the rear of the units toward special features, including golf course and water features. Views should be enhanced with landscaping which does not block the view of adjacent lots.
- Single-family detached residential uses shall be buffered from existing adjacent land uses, and high traffic arterials. Landscape design should be used to provide privacy between units.
- A variety of building pad configurations can create further interest.
- Provide for variety along the streetscape.
- Orient views of the units towards special features, distant mountains, or common area open space. Views should be enhanced with tree plantings framing the desired view, but shall not block the view of adjacent lots.



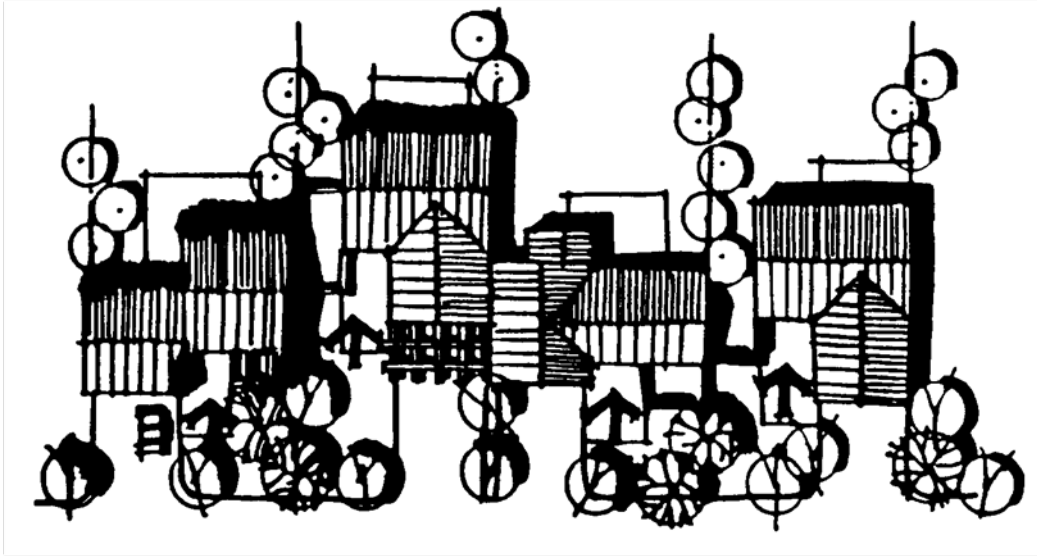
Views from Residential Units

Multi-Family Residential Site Planning**Multi-Family Housing Facades**

- Multi-family housing facades shall be detailed to give a varied quality to the building exterior rather than one long, continuous building. Attached buildings shall not be more than six dwelling units per floor.



- In higher density areas, where lot sizes permit, individual units shall be oriented in a variety of ways to avoid the monotony of garage door corridors.
- Maximization of open space is an important element within a higher density project. Curving the streets and orienting road axis to open areas and views can attain the feeling of open space. Open parking areas can be treated as landscaped plazas and courts.
- In multi-family and attached housing, it is important to provide each unit with its own entry and identity. This can be accomplished by offsetting and staggering each separate unit and by combining one and two-story building forms to separate massing. This also will provide variety to the streetscape.



Streetscape Variety

Non-Residential Site Planning

Non-residential areas will serve the project site as well as the regional community. Businesses which locate here will rely on a high degree of visibility.



Mixed Use/Air Park

Strong Linkages

- Links within commercial developments shall be reinforced by building and landscape elements. In the case of landscape elements, these links may be made with both hardscape (such as paving), or softscape.
- Although the area is to be auto-oriented with regard to attracting those shoppers from the local community, the site planning shall emphasize pedestrian, and bicycle links to minimize auto trips from within the Kohl Ranch. Interaction of residential, park and commercial areas is encouraged.
- In some instances objectionable elements may require visual screens and sound barriers. Otherwise, these treatments should be minimized and used only when necessary.



Avenue 66 Commercial

Orientation of Structures

- Buildings shall be arranged so that most store fronts are oriented toward and visible from the street and parking areas. The shape and configuration of the buildings will be determined by the site constraints. On corners or at major developments, satellite buildings shall be located closer to the street or at the corner, while the bulk of the building is placed far enough back from the street to allow circulation across the front of the parcels.
- Developments that are inward facing and that preclude through access are not encouraged, except when major attractions are destinations.
- Courtyards and outdoor seating areas are encouraged, and other ancillary structures, service areas, and trash enclosures shall be planned as part of the project, and placed or screened in a position of low prominence.
-

**Commercial**

- Landscaping shall be used to break up otherwise uninterrupted building mass, frame views, and connect with development on adjacent pads.
- Building placement along a street should be varied to avoid creating a harsh, monotonous blockface or streetscape. Buildings should be sited so their entrances are generally oriented toward the street and parking areas.

**Office**

Mixed Use Site Planning

Mixed Use planning areas are established to encourage an innovative array of uses to include a combination of commercial, business, industrial, recreational, and residential uses.

- Mixed-use developments contain different building types. A variety of building types are permitted; commercial, business, industrial, race related facilities, residential, and duplex units.
- Where residential uses are mixed with nonresidential uses, the residential use should be located either above the nonresidential component or adjacent to the nonresidential component, as allowed by Section 2 – Zoning Ordinance.
- Proper building massing may be achieved through the use of sufficient vertical, horizontal and roof articulation of a building. Combinations of one and two story elements on the same building are encouraged to facilitate articulation.
- Consistent with the architectural style of the development, street facing facades should incorporate articulation and mix of color and materials to create diversity in the streetscape.
- Buildings within a development shall use colors, materials and architectural details that are compatible among buildings within the same development.

Climatic Conditions

The effects of seasonal wind on development at the Kohl Ranch will, in some instances, be extensive. Wind may be intercepted, diverted or lessened. The following are suggested techniques of wind management:

Shelter Belts on a Community Scale

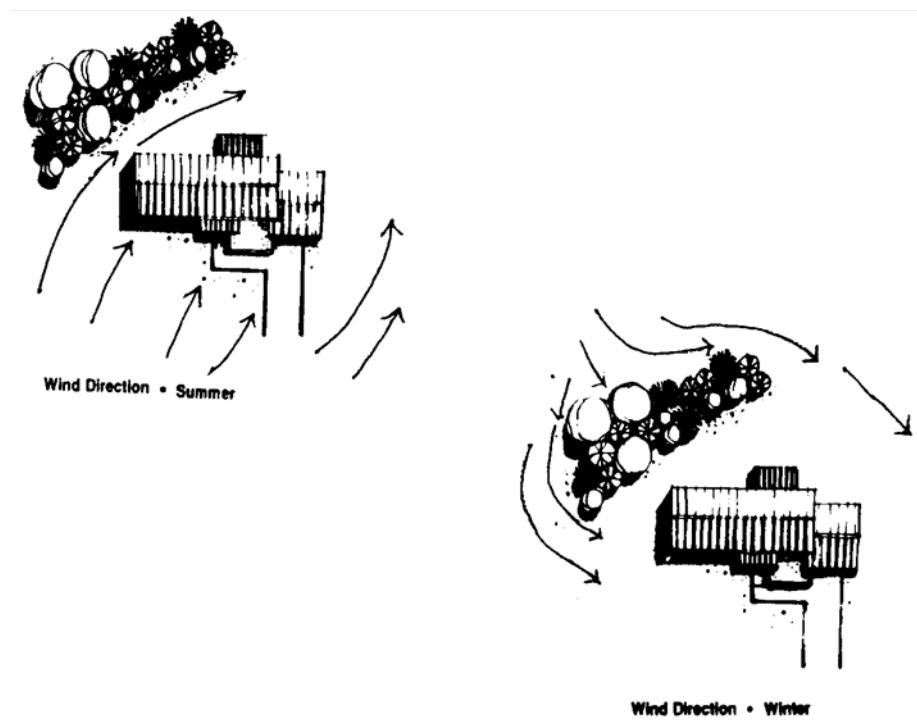
Shelter belts may be used in parallel rows to produce shelter on the leeward side. Belts with pinched profiles are less efficient in halting winds than belts with vertical edges.

Solid Barriers

Fences and walls are effective for protection but are very hard edges and should be limited in use. Barriers penetrated by the wind create more effective and wider wind shadows.

Wind Breaks for Residential Use

Wind breaks placed on residential property can route winter wind around the home and allow cooling breezes during the summer to penetrate the rear yard.

***Wind Breaks***

3.3.3 Architectural Guidelines

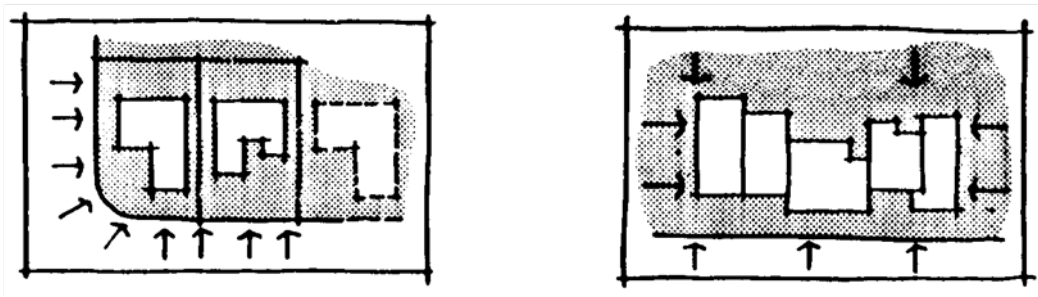
The architectural guidelines for the Kohl Ranch are intended to produce a feeling of authenticity without creating rigid, over-specific product/style requirements which often lead to architectural blandness and homogeneity. Rather, the Kohl Ranch should encourage a program of innovative design that reflects the historical desert landscape. The development programs should apply the guidelines in either literal or abstract forms.

These architectural guidelines are intended to establish design standards and form an encyclopedia of alternative design concepts.

Elevation Concepts

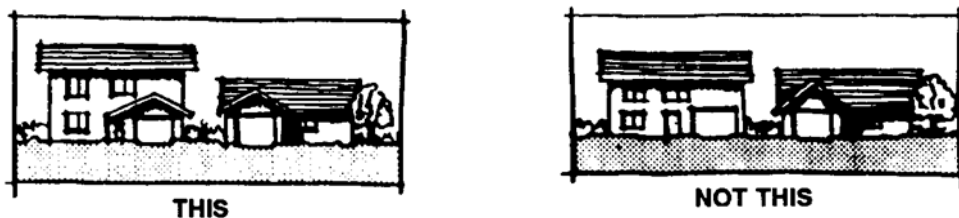
- These guidelines are primarily concerned with the physical appearance of the community environment as perceived by residents and visitors. Therefore, it is the intent of these guidelines to apply to the portion of a building that is visible to the public.
- Primary elevations are the surfaces of a building structure or yard that can be seen from public view (either vehicular or pedestrian). In a single-family project it usually means the front and/or exterior side yard views. In multi-family projects it may be all four elevations of a structure.

Primary Elevations



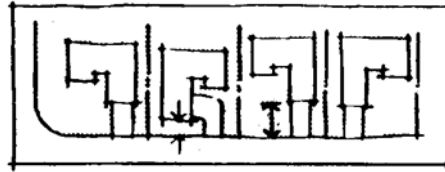
- Each building shall relate in terms of bulk and mass but should not be identical. A two-story building is more harmonious to a neighboring one-story building if it contains a one-story element.

Harmony of Elevations



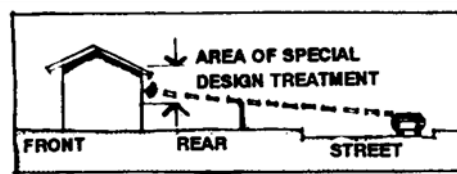
- Avoid stereotyped units which will produce monotonous elevations and street scene.
- A variety of elevations utilizing differing setbacks such as swing-in garages and reverse plans will ensure a varied street scene.

Variety of Elevations



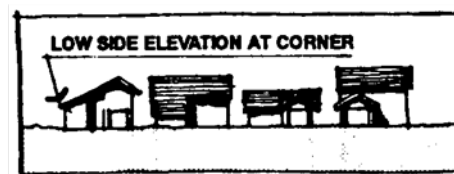
- Where applicable, the rear side of the building located adjacent to streets and other areas of high visibility should have similar treatment as with the primary elevation criteria. If a fence is used as a screening device, then it too should be designed to be part of the architecture.

Special Treatment Area



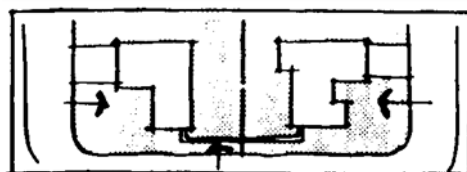
- Corner Elevations - Place the short side elevation on the corner to reduce the feeling of height and mass at the corner. Auto entry on a corner lot is also an important consideration. The high side can be placed on the corner as long as the side elevation is treated as a primary elevation.

Corner Elevations

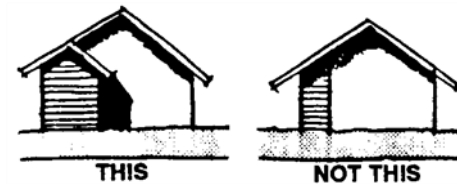


- Elevations and Side Streets - Fences or walls that connect two separate units should be of the same material and color and be compatible with the architecture of the buildings.

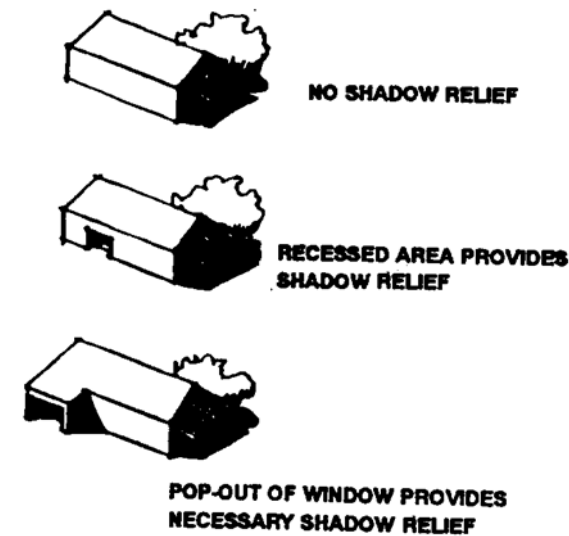
Elevations and Side Streets



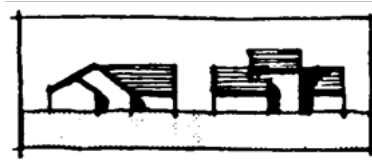
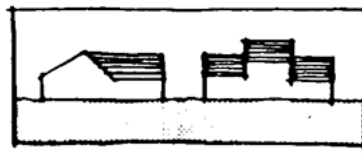
- Facades - If material changes do not occur in the same plane and if they intercept an architectural element, they are more effective.

Facades

- Allow for visual relief for interest and to break up long planar surfaces. Offsets, pop-outs, overhangs, and recesses all may be used to produce effective shadow interest areas. Larger buildings require more relief than do smaller buildings.

Shadow Interest Areas

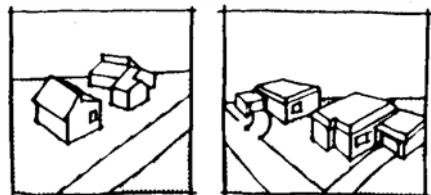
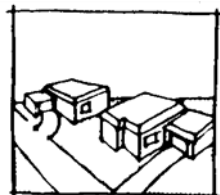
- It is desirable to accompany a plan offset with a change in the roof pitch orientation. This is important on the primary elevations.

Roof Pitch**THIS****NOT THIS**

- The design theme should be continued throughout the project, including accessory items such as lighting fixtures and address plaques.

Design Theme

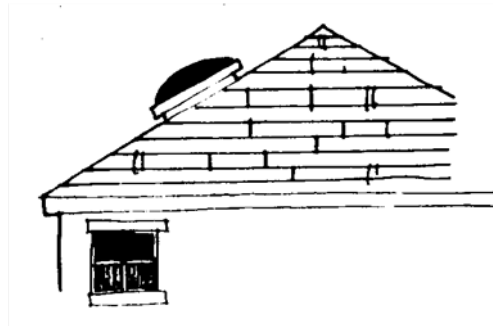
- Roof Design - Flat roof design for residential structures should be discouraged. If used, flat roofs should be a secondary design element only and should be contained within the scheme of an individual building design. Particular consideration as to color and material should be given to the design and treatment of roofs because of their visual impact.

Roof Design**THIS****NOT THIS**

Details

There are many building elements that come into view. Usually they are secondary adjuncts to major element, such as a skylight on a roof. In some cases they become no more than a texture, such as a trellis. It is important in all cases, however, that these elements integrate with the whole. What may start out as a minor feature can become a distracting eyesore, if not thought through.

- Skylights should be integrated into the roof form.

Skylights

- Trellis/Pergolas can be very pleasing and functional structures, (i.e., carport, entry canopy). They provide partial shade, screens for privacy or an arbor for climbing plants. They offer the warmth and texture of wood at a relatively low cost. It is important to consider a trellis as a permanent structure and design it accordingly. Avoid corrugated fiberglass metal, or other temporary type material. Use adequately heavy members for the support structure working down to no less than 2" nominal for the smallest dimension of the lighter members. The trellis will look more substantial and will not bow from old age prematurely.

Trellis/Pergola

- Chimneys are usually very strong roof elements. Codes require that they extend higher than adjacent roof lines. Careful choice of proportion and material should give them a substantial and stable appearance.

- The use of prefab fireplaces and flues is economically sound. However, they need not have the stove-pipe appearance, depending of which spark arrester is used or required by fire code. Enclosing the flue in a masonry or wood chimney with substantial proportions will solve the problem.

Materials and Finish

In keeping with the goals of the Kohl Ranch to maintain and preserve the natural surroundings, designs in harmony with nature should be extended to material, finish and color selection.

- Natural materials by their nature inherently work well with the surroundings. Natural colors or transparent finishes on these materials not only enhance them, but actually improve with age and are low maintenance. Man-made materials of natural colors and textures are also suitable.
- Brick, natural tile, concrete, steel and glass, if used honestly and in a straight-forward manner, can be pleasant. Large expanses of a single material, especially if unbroken by detail or depth, can become overpowering to the rest of the building form and the surroundings. Conversely, over-detailing with use of too many different materials or textures can create confusion and distract from an otherwise good design.

3.3.4 Lighting

Lighting should be designed to ensure compatibility with the community architectural and street furniture themes, and the safety of the site users. An effort should be made to emphasize "human scale" in public areas adjacent to buildings and along walks. Streetlight standards, traffic signal poles, and pedestrian and building lighting play a vital role in defining a safe and secure appearance. This lighting concept has been devised to provide a hierarchy of lighting effects which will contribute to the overall cohesiveness of the community image.

General Guidelines

- Warm white lighting is encouraged. Bright colored or blinking lights are not encouraged except in theme restaurants and shops of commercial development areas.
- Building or roof outline tube lighting shall be subject to County of Riverside approval.
- Design and placement of site lighting should minimize glare affecting adjacent properties, buildings, and roadways.
- Careful consideration and coordination shall be given to avoid any potential conflicts with Jacqueline Cochran Regional Airport operations.
- Lighting shall be designed to minimize sky glow and effects on the Mt. Palomar Observatory and the nighttime desert sky.
- Fixtures and standards shall conform to state and local safety and illumination requirements. In particular, lighting shall conform to Riverside County Ordinance No. 655, which includes requirements related to the Mt. Palomar Observatory.
- Automatic timers on lighting should be designed to maximize personal safety during nighttime use while saving energy.

Roadway Lighting

Lighting fixtures and standards within the public right-of-way play a crucial role in displaying a level of quality for the development.

- Lighting shall be positioned to enhance the safety of vehicular and pedestrian flows at key points along the roadway. Light shall be concentrated at intersections and pedestrian crosswalks. Intersections may have increased wattage for definition and to alleviate automobile/pedestrian conflicts.
- The maximum height of roadway lighting should be twenty-five (25) feet with a minimum clearance of sixteen (16) feet above roadways.

Parking Area Lighting

Since landscaped islands within parking areas are adjacent to high use commercial, office, and recreation buildings, they typically may be hidden from view by landscaping and trees, therefore requiring a less overt lighting standard. Conversely, they may be highly refined and emphasized as a design element within commercial areas where visibility to buildings is crucial.

- Stone or concrete, limited to twenty-four (24) inches in height, may be used for light standard bases in parking area islands to protect lighting from automobile damage.

Pedestrian and Entry Area Lighting

Pedestrian and entry area lighting should be provided at plaza areas, improved trails, entryways, courtyards, parking lots and other public spaces requiring night illumination.

- For all uses permitted within the Kohl Ranch, where above ground illumination is desired, a lighting fixture and standard compatible with the community theme is recommended.
- In pedestrian areas the use of low level walkway lighting is preferred over lighting from above. Lights shall be positioned no higher than four feet above grade.
- Concrete or metal light bollards not exceeding four feet in height may be used to accentuate building entrance areas or to provide separation for pedestrians from vehicular traffic lanes.
- For pedestrian walkways, point-to-point lighting is acceptable with no specific illumination levels required. The main emphasis shall be to clearly identify the pedestrian walkway and direction of travel. These lamp elements may become an important design element on the landscape, or they may be sublime and unobtrusive.
- Fixtures which shield and limit spillover light into the night sky are recommended.

Architectural Lighting

- To highlight monument signs or architectural features such as walls, entry ways or lobbies, dramatic lighting should be used. Spillover lighting from lobby areas is acceptable although glare onto adjacent properties should not occur. Service area lighting shall be contained within service yard boundaries, with light sources concealed.
- Building illumination and architectural lighting shall be creative and must reinforce the design theme. Indirect wall lighting or "wall washing", overhead down lighting, or interior illumination which spills outside is encouraged. Wall "washing" effects shall be sparingly used to accentuate architecture at key visual areas and not an attempt to advertise buildings to roadways.
- Lighting shall not cast any glare onto adjacent lots and streets in such a manner as to decrease the safety of pedestrian and vehicular movement.
- Architectural lighting shall be used to articulate the particular building design. Rim lighting of eaves, bending, uplighting, wall washing, and other effects shall be used in compliance with the design theme for the planning area. Architectural lighting shall be integrated with building elements and concealed flush with grade wherever possible.
- Building or wall lighting shall be indirect. A limited number of lights may be used to create shadow, relief and outline effects.
- Lighting shall be used to enhance landscaping and reinforce architecture, with dramatic uplighting or wall shadow effects with plant materials encouraged.
- Parking lot light fixtures and bollards shall be consistent in styling with the design theme proposed for that subarea.
- Light standards shall not exceed 20 feet in height or the height of the building, whichever is less.
- All architectural lighting should be carefully integrated into design themes and should not be visually obtrusive and incongruent during the daytime.

Landscape Lighting

- Landscape lighting can be used to highlight landscape features such as specimen trees and pedestrian areas. As with architectural lighting, light sources should be designed as an integral design element, concealed flush with grade, or hidden by vegetation during the daytime.
- String lights, with small nonflashing bulbs, may be used to highlight trees and similar features within the golf course clubhouse, commercial areas in interior courts only, and other similar outdoor areas at night to create excitement and a festive ambiance.

Athletic Field, Court and Driving Range Lighting

For proper lighting of athletic fields, driving ranges and courts in recreation facilities of parks and schools, spillover lighting may occur. Lighting shall be designed to minimize the spillover effect. These areas shall be carefully site planned to impact as few neighbors as possible. Hours of operation shall be established which restrict intrusion of nighttime lighting, and posted at visible locations at these recreations areas.

3.3.5 Signage

General Guidelines

- All signs within the project should be designed to provide a consistent reinforcement of the Kohl Ranch character. Signs must communicate not only specific information but should, in themselves, add to the attractiveness of the area.
- Signs in the project area should advertise a place of business, a residential area or provide directions and information.
- Signs should not compete with each other or dominate the setting via inconsistent height, size, shape, number, color, lighting or movement.
- Signs shall conform to the guidelines set forth herein and with Riverside County Ordinances.
- No signs shall be placed upon or project into or above public property or the public right-of-way, except as noted herein.
- Signage typeface should be consistent with building architectural style.
- Public signs should be limited to those absolutely necessary for smooth traffic flow, safety, and public information.
- Clear sight triangles should be observed and enforced at all intersections, corners, parking areas, and drives at all times.
- All signage should be maintained in good repair, including the display surface, which shall be kept neatly painted or finished.
- The exposed backs of all signs visible to the public should be suitably finished and maintained.
- All signage should be designed free of bracing, angle-iron, guy wires, cables, or similar devices, except for commercial advertising signs as hereafter noted.
- Painted signs should use fade-resistant, durable, exterior finish paint. No luminous, fluorescent or iridescent paints or plastics are permitted. Permanent wooden signs or materials that may be degraded by the climate should be avoided unless adequately sheltered.
- An effort shall be made to achieve consistency between building style and sign design. In all cases, signage should be complimentary to the exterior treatment of the building or location involved.
- Color schemes for signage shall relate to other signs, graphics and color schemes in the vicinity in order to achieve an overall sense of identity.
- Signs may be lighted; however, no light that flashes or blinks or affects changes in hue or intensity of illumination is permitted. Illumination sources for any sign shall be hidden from view.

Community Monuments

The following sign standards will effectively regulate the placement, erection and maintenance of permanent signage within the Kohl Ranch. These standards are intended to provide equitable standards for the protection of property values, visual quality and public health, safety and general welfare. The hierarchy of signs for the Kohl Ranch is as follows:

Project Entries

- Monument signs shall identify the Kohl Ranch community at key project entry points. These monuments should be the most prominent in scale and should set the overall theme for the entire community.
- The wall or monument materials should establish the project theme which other developments should use as a general guide.

- Project entry signs should be designed with the angle, distance and length of view in mind. Signage viewed from vehicles should be large and/or conspicuous enough to be read quickly. Signs designed to be viewed from slower speeds or at shorter distance may have more attention given to details such as texture, and may have a reduced text size.
- Building materials should consist of native stone, river rock cobblestone, concrete, brick, stuccoed concrete block, wood rails and boards, metal lettering systems and logos, sheet metals and wires, and special architectural elements such as weather vanes or windmills.
- Maximum dimensions for message areas of signs should be limited to those established in the County of Riverside Ordinance No. 348.
- Lighting should be limited to those fixtures which are at grade or are concealed by landscape planting.
- Signage typography should be limited to bronze, copper, or any quality steel logos or channel letters.

Residential Neighborhood Entries

- Residential neighborhood entries should establish the theme of the individual development while continuing the community theme.
- Recommended building materials may consist of native stone, river rock cobblestone, concrete, brick, stuccoed concrete block, wood rails and boards, and special architectural theme elements.
- Residential entry monuments should be less grand in scale than the major intersection treatments. The monuments should be in proportion with the surrounding streetscape and should be highlighted by landscape plant materials and site furnishings. The monument may be part of a wall, or stand alone.
- Maximum dimensions for message sign areas shall be limited to dimensions established by Riverside County Ordinance No. 348.
- Graphic signage materials should be limited to bronze, copper or any other high quality steel logos or channel letters.
- Back-lit letters or a concealed lighting source, located flush with grade or within a covered entry structure may be used.

Commercial Entries

- Commercial entries should incorporate the commercial center building materials, while continuing the community theme. Entry monuments should be subdued and not over-powering. They should simply and discreetly identify the development rather than create a grand entry statement.
- The maximum dimensions of the sign base and message area shall adhere to Riverside County Ordinance No. 348.
- Signage typography should be limited to bronze, copper or quality finished steel logos or channel letters. Sign text should be limited to identification of the street address, project name, and major retailers.
- Back-lit letters or a concealed lighting source, located flush with grade may be used.
- Signs may be constructed on brick, native stone, wood or metal. Letters may be mounted upon the signage base.
- Monuments shall be integrated with berming and landscape materials with established streetscape design themes.

Retail Signs

- Only one monument sign shall be allowed per building. These signs shall be located at entry points from parking areas, adjacent to walkways, in planting beds or within building setback landscape areas adjacent to the street right-of-way.
- Signs constructed of brick and/or native stone base, a smooth finished concrete, carved wood or metal panel systems are recommended.
- Sign dimensions shall adhere to those established by County Ordinance No. 348. Letters should be consistent with the architectural material of the establishment.
- A street address is recommended to be incorporated into the sign banner. Lighting should be concealed by plants or flush with grade.

Directional Signs

Directional signage, used to identify and direct vehicular and pedestrian traffic to on-site destinations shall be provided along roadways and within all multi-parcel and multi-tenant developments. Directional signage should be of consistent design throughout each project. All such signage should be fabricated from the same materials, with a consistent color palette and common graphic theme. The use of materials compatible with the architectural design of each project and its corresponding site furnishings is encouraged. Directional signs should be of consistent dimensions, and located in a visually logical order.

These signs should provide on-site directional information and should not be used for advertising. They should include the following:

- Directory monuments
- Public service signs
- Traffic safety signs
- Residential street signs

Directory Monuments

- Directory monuments should be permitted as ground signs or on the faces of buildings or structures to illustrate the project layout and locate the establishments or residential buildings of the development.
- The sign area of such location markers should be designed for either pedestrian or automobile users.
- The location and number of these markers should be determined at the individual project design level.
- All dimensions, material types and signage area requirements shall conform to County Ordinance No. 348.

Public Service Signs

Service signs should only provide general public information to direct the way to public facilities such as information centers, rest rooms, telephones, emergency stations, etc. Service signs should be permitted as wall or ground signs and should be limited in number and location as outlined below.

- Signs are permitted on the faces of buildings or structures provided that such signs should be placed at eye level above the immediately adjacent ground.
- Signs setting forth the location of, or directions to, parking or buildings located on the premises, or regulating the flow of on-premise traffic, should be permitted as part of the separate free-standing ground structure or kiosk. Such signs may include pictorial and decorative designs.

- The signs should include a minimum of words and numbers necessary to accurately communicate the required information.
- The signs shall be compatible with the adjacent architectural theme.

Traffic Safety

Traffic safety signs shall be subject to the standards of the County and State agencies.

Residential Street Signs

All street signs shall be consistent with the overall Kohl Ranch theme.

Temporary Signs

Signage that identifies uses or activities temporary in nature, such as that associated with real estate sales and leasing or the development and construction of buildings, shall be allowed on a temporary basis if consistent with the design character of the surrounding area. No illumination of temporary signs should be permitted. Temporary signage should be located on or in close proximity to the uses identified. The multitude of signs associated with development, design, construction and leasing should be combined into one sign for each project and the sign should be located on site.

Project Marketing

- These signs should provide community recognition for the Kohl Ranch during the construction and sales period.
- Wood with a painted sign surface is the recommended construction material. Signs may be double-sided. Signs may be painted onto the temporary construction safety walls erected to hide construction from view.

Non-Residential Construction and Leasing

- These signs are intended to inform the viewer of new buildings and leasing opportunities, opening dates, and builder names and telephone numbers of individual parcels within the Kohl Ranch project.
- Wood construction with a painted sign surface is recommended for this application.
- One sign per individual parcel or project should be allowed and should be located on-site.
- Sign face may include any sketch or architectural rendering of the proposed use.
- Sign removal should occur after lease out.

Residential Sales and Leasing

- Temporary ground signs for real estate sales, leasing, construction or model homes are permitted. One project identification monument or ground sign should be permitted for each major entrance to a development. Sign copy is limited to project name and address in addition to the word "sales" or "leasing."

Prohibited Signs

- Obsolete or Abandoned Signs. Any sign located on vacant or unoccupied property that was erected for a business which no longer exists, or any sign which pertains to a time, event or purpose which no longer exists, shall be removed within 30 days after the use has been abandoned.
- Signs constituting a potential traffic hazard or which simulates or imitates in size, color, lettering or design any traffic sign or signal.

- Temporary signs on public property (street, median island, parkway, sidewalks, traffic control sign posts, utility poles, park land, trees, etc.)
- Animated or moving signs: signs consisting of any moving, swinging, rotating, flashing, blinking or otherwise animated components.
- Off-premise signs: any sign, other than a simple directional sign, installed for the purpose of advertising a project, development, event, person or subject not related to the premises upon which sign is located.
- Vehicle signs: signs on or affixed to trucks, vans, automobiles, trailers or other vehicles which identify, or provide direction to a use or activity not related to its lawful making of deliveries or sales of merchandise or rendering of service from such vehicles.
- Portable signs: a freestanding sign not permanently affixed, anchored, or secured to the ground or structure it occupies, including tailored signs, except as approved by the County of Riverside.
- Roof signs: any sign erected, constructed and maintained upon or over the roof of any building, unless it is a projecting canopy sign or is an explicit part of the building architecture.
- Other prohibited signs: advertising signs and billboards, inflatable signs or balloons, inflatable animals, magnetic signs, plastic flags unless otherwise specified in these guidelines or approved by the County of Riverside.

3.3.6 Walls and Fences

Walls and fences are of particular functional importance for the Kohl Ranch as depicted in **Figure 3.3-2, Wall and Fence Plan**. Other allowed uses in open space that are in the vicinity of the airport safety zones will also be appropriately fenced or walled. If the golf course or motor sports race track are implemented, an individual fencing plan for those uses will be prepared.

Walls and fences establish enclosure, confer physical and visual privacy and also provide vertical texture. Walls and fences are especially important in creating a theme for the residential portion of the community. They should, however, be used sparingly and with great consideration so as not to detract from the open space or to simply demark property lines. The material, style and height of walls and fences shall provide an element of continuity throughout the project to insure visual consistency. The walls and fences should be designed with the intent of furthering the architectural character of the site.

Walls may be solid, perforated, or hidden from view when security is required, but view retention is desired. As fences are subject to severe exposure they must be well-made. All community and perimeter project walls and fencing are to be provided by the project builder at the time of development. Wherever practical, plant material should be used as a barrier rather than walls and fences.

General Guidelines

The horizontal mass of continuous walls should be softened by landscaping as specified in the landscape guidelines. No wall or fence shall exceed 6 feet in height unless it is used for noise attenuation where a combination of wall and berming is not sufficient. If retaining walls are necessary they should be terraced and should not exceed this 6-foot height limit.

Long stretches of unrelieved walls and fences should be broken up with varied setbacks or recesses for plantings. When a change in pad elevation occurs, the wall or fence should be stepped in equal vertical intervals. No step should exceed 12 inches in height.

Community Walls and Fences

Community walls and fences will provide community identity, security, privacy and sound buffering for residential units adjacent to project streets or incompatible land uses as depicted in **Figure 3.3-3, Wall and Fence Types**.

- Solid theme walls may be required along major thoroughfares where sound attenuation and privacy are required and when residential areas are adjacent to schools, commercial areas, or parks.
- Solid walls along residential development edges visible from surrounding arterial roadways shall be composed of solid masonry covered with smooth stucco. The wall will stand six (6) feet in height. Walls should be planted with vines or screened with other plant materials to reduce their visual impact. This will contribute to the shade and residential environment of the development the wall is intended to screen.
- Wall materials shall consist of simple masonry construction finished with colored smooth stucco, consistent with the desert color palette.

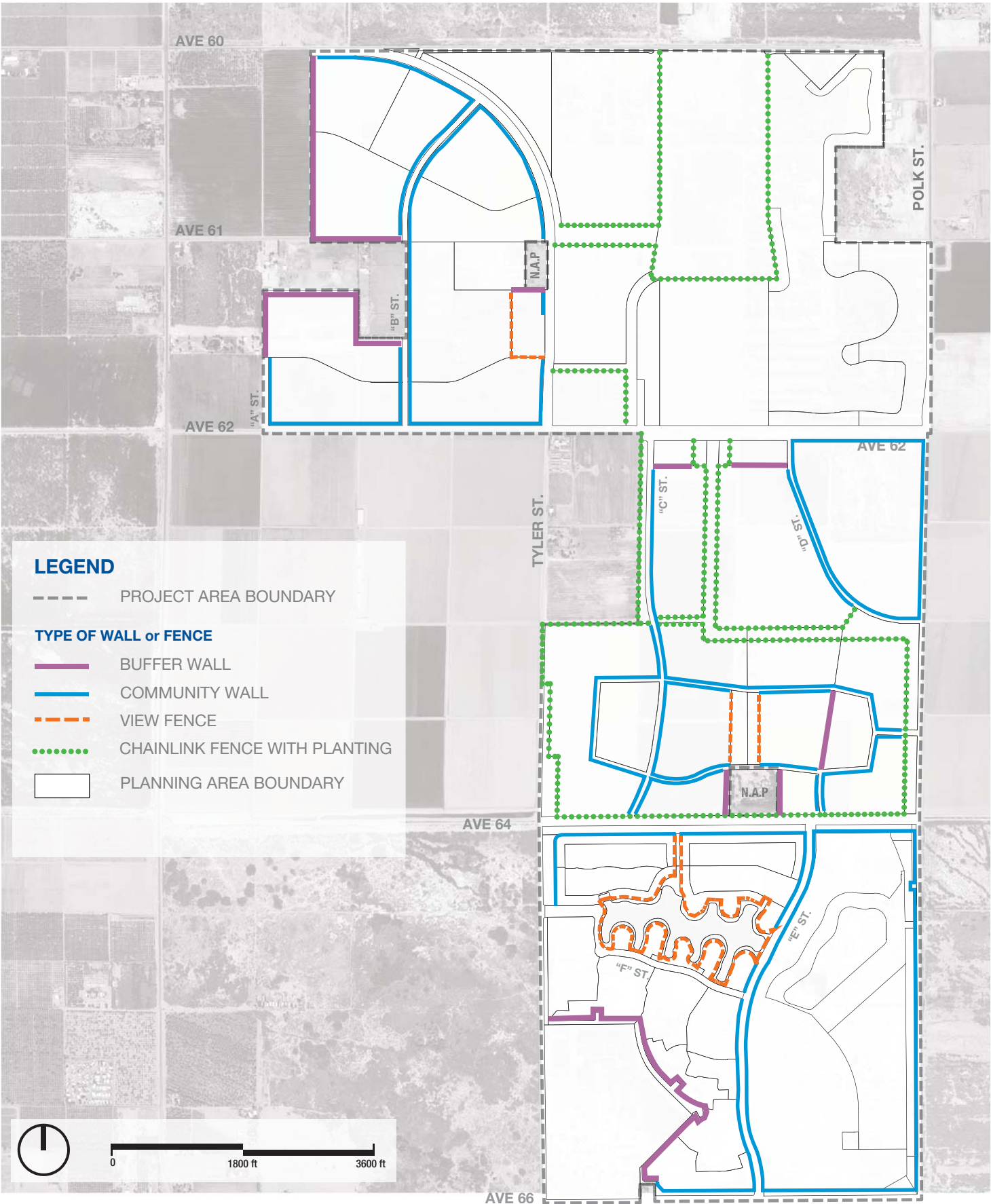
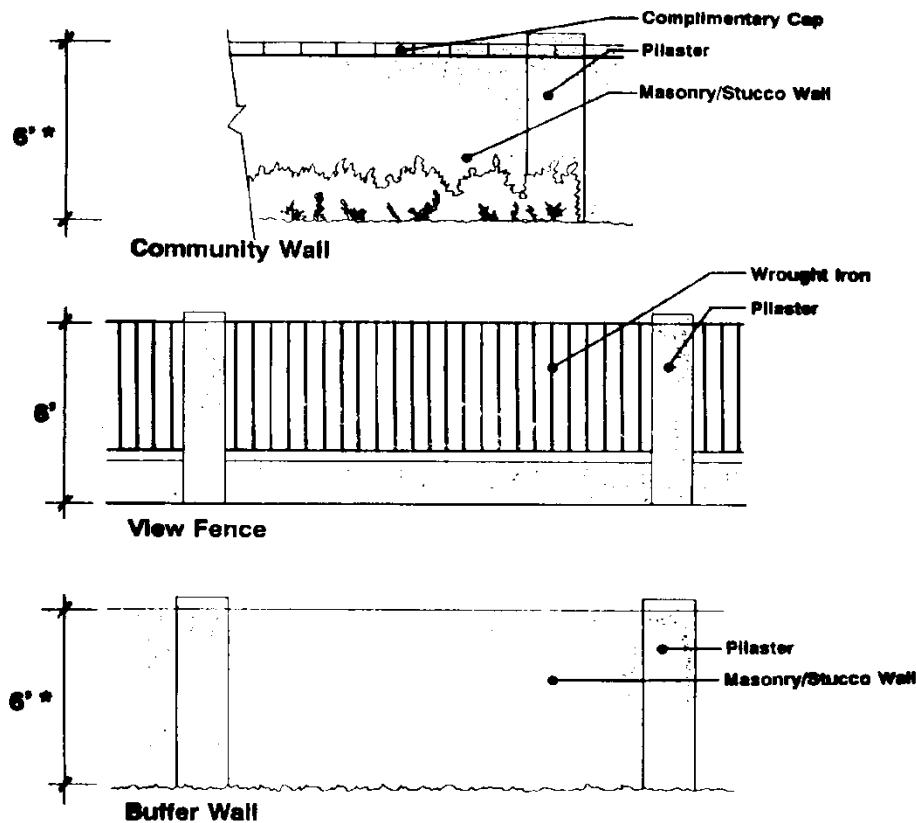


Figure 3.3-3, Wall and Fence Type



*Unless otherwise determined by a noise study for an adjacent land use.

Perimeter Walls and Fences

Fencing should be allowed within the project area and along the project boundaries. Landscape hedges and other screening measures should be used to reduce the visual incongruity of such fences.

View Fences

These fences should be used to increase the sense of openness in rear yards, along open space, golf courses, parks and schools.

- If golf courses are built, fences adjacent to golf courses visible from surrounding arterials should be view fences. Recommended material includes wrought iron, wood pickets, or welded wire on wood posts. Spires, spikes or other detailing consistent with the community theme may be used to impede trespassing over metal rails.

Privacy Walls and Fences

The privacy walls and fences are utilized alongside and rear lot lines where residential units need more privacy. Recommended material includes wood panels, masonry, stucco and stone.

Theme Walls and Fences

In certain instances such as project entries and at restaurants, thematic fences or walls may be appropriate. An example would be a western steak house with a low profile split rail fence at the entry. Special conditions that warrant thematic uses shall be approved by the County of Riverside.

3.4 LANDSCAPE DESIGN GUIDELINES

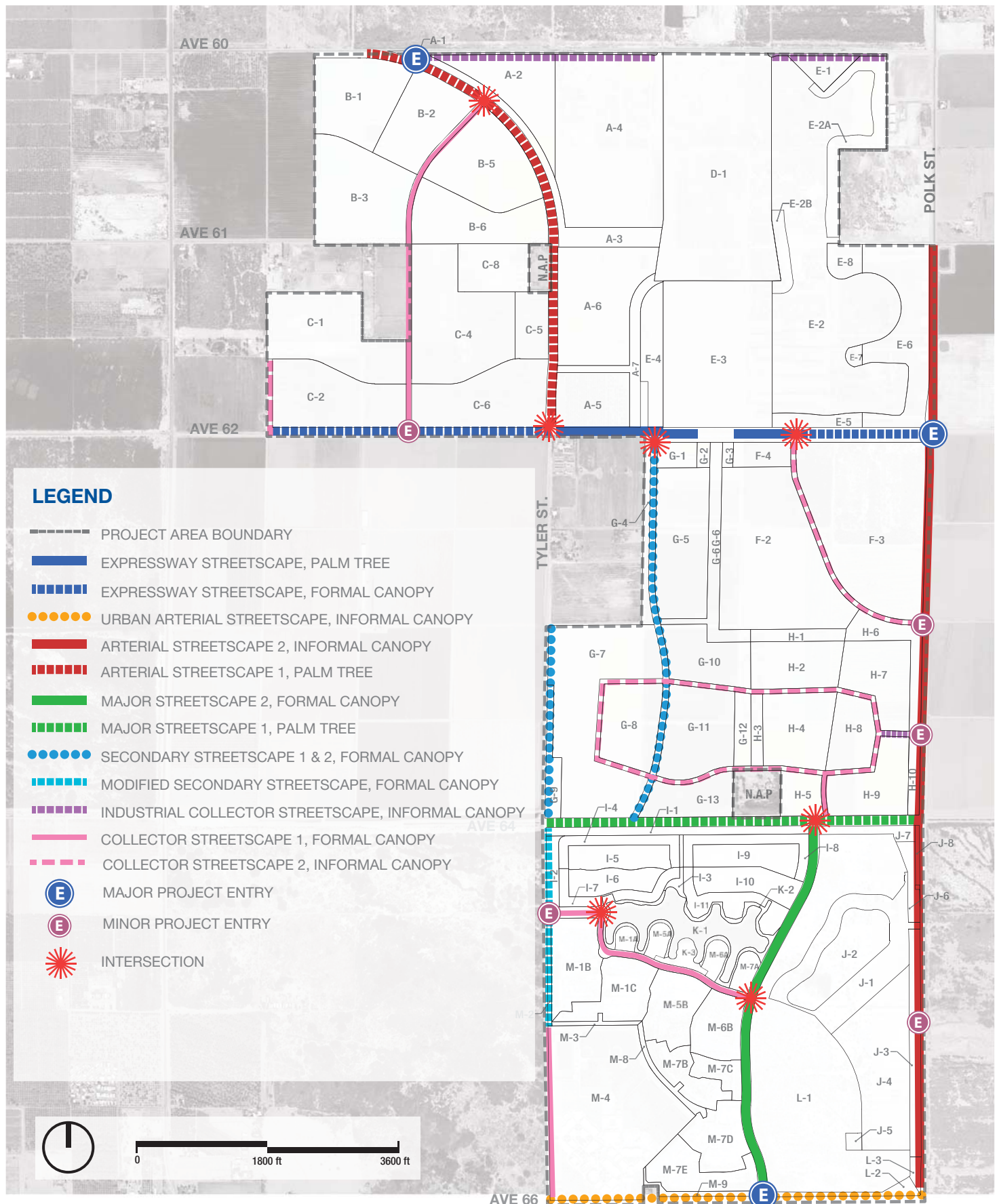
3.4.1 Landscape Concept

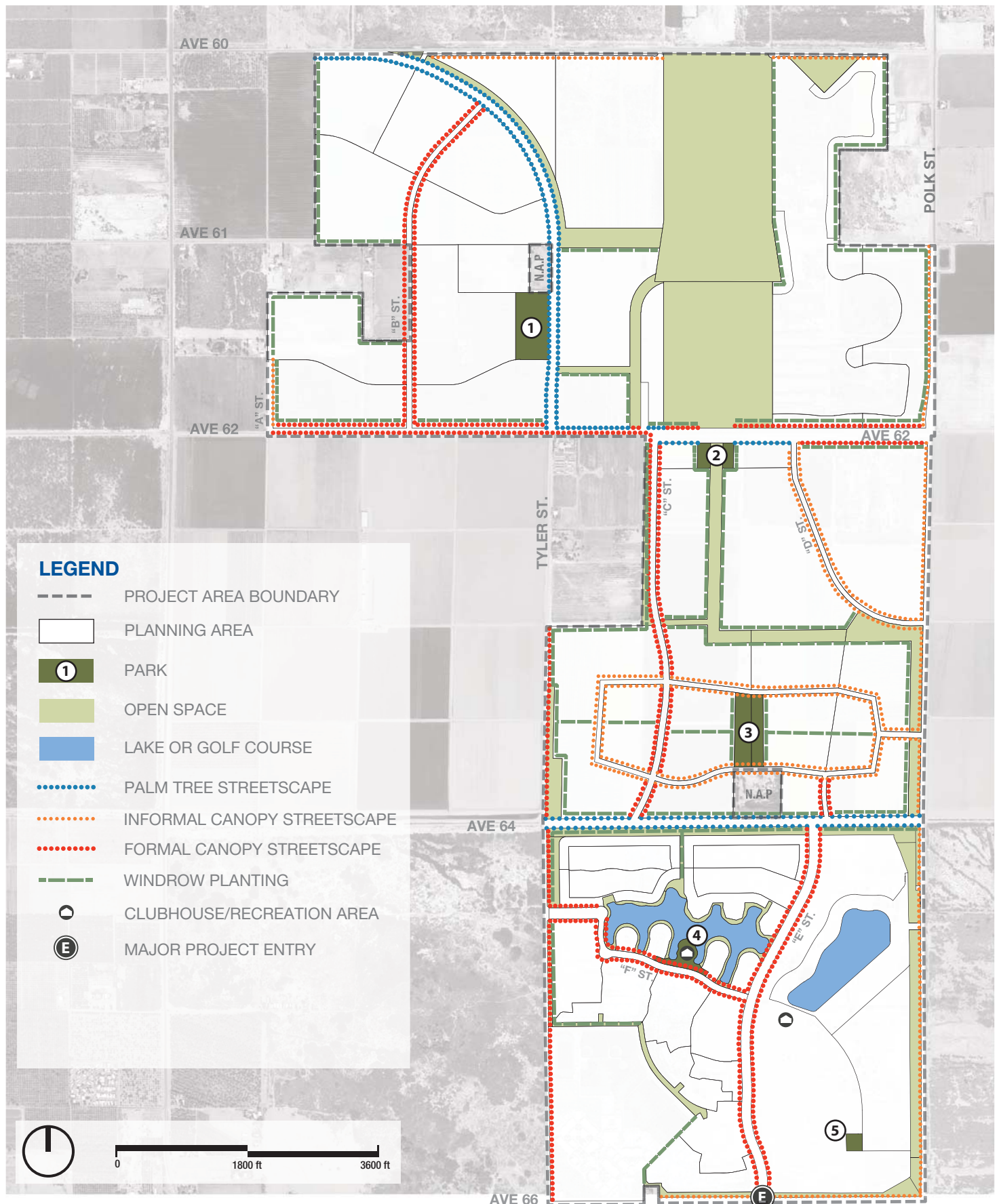
The landscape guidelines for the Kohl Ranch shall contribute greatly to the establishment of a community theme for the project. The landscape concept shall transcend all individual parcel boundaries and unify the community "edges," those areas along roadways, parks, commercial shopping areas, undisturbed areas and drainage ways and the adjacent residential development areas.

The landscape concept provides criteria for the treatment of all areas within roads and easements, medians, development edges, and project entries. The concept has been conceived to organize and present a memorable image for the Kohl Ranch project. Its purpose is to establish a unified landscape framework that provides continuity throughout the project area and encourages the desert community theme. To accomplish this purpose, these guidelines should be consistently applied to define major project roadways (expressways, arterials, secondary and major highways, collectors, industrial collectors, and loop roads) roadway medians, internal circulation routes, major entries, and intersections as reflected in **Figure 3.4-1, Landscape Key Map**. Landscape materials within these areas should direct and guide the resident or visitor to the development, screen sensitive views, and frame or create focal points and views as the motorist, pedestrian, or bicycle moves through and around the project. A high degree of landscape quality is to be maintained throughout the Kohl Ranch Specific Plan area. Key elements of the concept are presented in **Figure 3.4-2, Community Structure Plan**.

The following are key objectives that the landscape concept should foster:

- The Kohl Ranch project should create a landscape theme that is compatible with the native desert community and architectural and site planning themes.
- The landscape is intended to work in association with the architectural guidelines and the grading plan to create screens and buffers where needed and views where the opportunity exists.
- The landscape should serve to augment sound attenuation efforts made with the landform or hardscape elements.
- Plant material selection for streetscapes, entry areas and development areas should be limited to those plants listed in the suggested plant palette.
- Landscape plant materials should be "long lived" varieties. "Short lived" materials such as flowering annuals and perennials may be utilized to accent or augment the "longer lived" base elements which form the landscape framework.
- Landscape elements within the front building setback visible from the public rights-of-way should blend with street edge landscaping.





- All common landscaped areas shall be designed with the objective of reducing long-term water use to a minimum in accordance with the Coachella Valley Water District Ordinance 1302.1 Landscape and Irrigation System Design Criteria and the Kohl Ranch Plant Palette. Native and drought tolerant plant materials and water-efficient irrigation practices shall be used. Within the golf course(s), if developed, water-efficient landscaping shall be encouraged in the "rough" and peripheral areas.
- Recycling of irrigation water is encouraged.

It is the intent of the landscape guidelines to foster innovative design and discourage the bland and mediocre. The design of the Kohl Ranch draws upon the character of the natural setting and applies this theme to concepts which are both cost effective and maintainable. Furthermore, the landscape guidelines foster a spirit of stewardship for the project open spaces and create a symbiotic existence with the developed areas, ultimately creating a unique recreational amenity for the community.

The essence of the landscape guidelines consists of the elements described below.

Community Identity

These guidelines establish a benchmark for high quality community landscape architecture by establishing guidelines for essential landscape elements that carry forth the landscape concept rather than relying upon a static and all inclusive plant palette. This flexible concept will allow for personal expression for individual builders and their designers within the community while maintaining a community structure and theme.

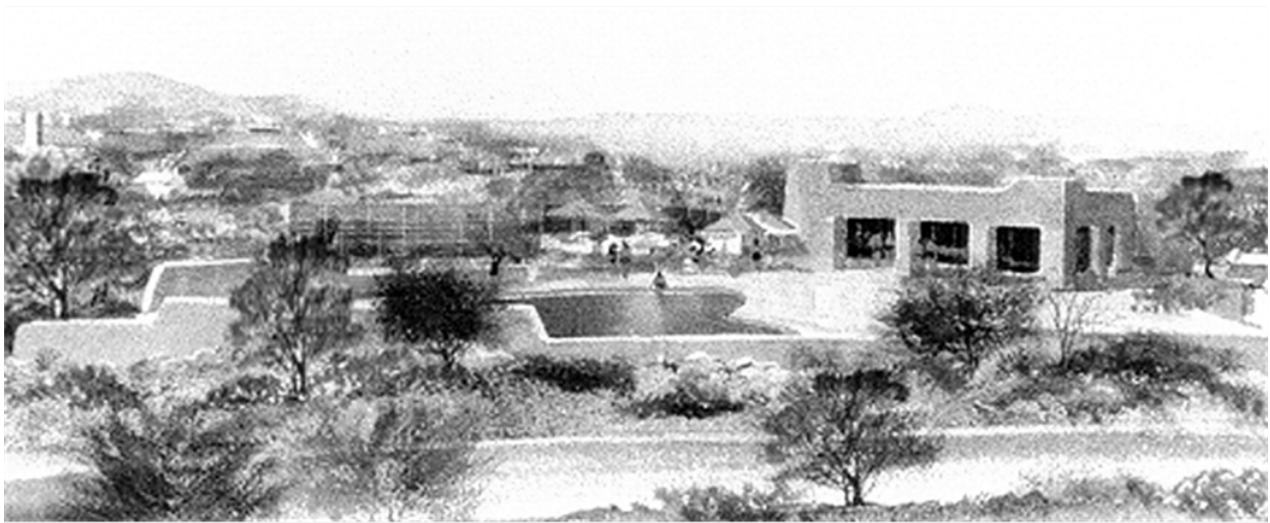
Visual Screens

The guidelines form a program which mitigates the effects of the built environment upon the undisturbed open space. The landscape guidelines also provide concepts for buffering buildings from adjacent land uses, both on and off site.

Conservation of Resources

The landscape plan and suggested plant materials have been selected with careful regard for the Coachella Valley's water supply and CVWD requirements and therefore, the resulting long-term viability of the landscape. The landscape will be efficient in terms of water consumption as well as maintenance requirements. Resources shall be concentrated in those areas of most intense human use, such as parks and other high-use areas. All common landscaped areas shall be designed with the objective of reducing long-term water use to a minimum. The use of native and drought tolerant plant materials and the use of water-efficient irrigation practices and the development of programs which "wean" plant materials from irrigation after their establishment are encouraged, while the use of high water-consuming plant materials shall be limited to selected areas. The landscape concept should encourage minimizing long-term maintenance for the majority of landscaped areas. This will be achieved by limiting areas of turf, clipped hedges, and exotic ornamentals to special places where they can be emphasized. Drainage channels through the site will be developed with native desert landscaping.

Alternative sources of irrigation water shall be considered, including reclaimed water, Colorado River water and surface runoff. The proximity of the site to Water Reclamation Plant No. 4 would allow for the use of reclaimed water for irrigation once tertiary water becomes available.

**Neighborhood Park**

The landscape guidelines encourage the use of the landscape as more than a visual aesthetic by creating environments that use plant materials as architectural elements which define space, establish proportion, and influence climate. The guidelines will encourage spaces which will, through the thoughtful arrangement of plant materials, encourage a range of emotions from intimate enclosure to vast openness. The landscape should affect the tactile senses by creating microclimates where people can escape the heat of summer or be warmed by the sun in the winter. The landscape should provide pleasing odors of flowers, rustling sounds of leaves in the wind, in addition to visual screens and focal points.

Application of Concepts

The guidelines provide a detailed, user-friendly landscape plan and plant matrix which outlines specific treatments and suggested plant materials to carry out the community theme. This plan and matrix should be easily understood by individual developers, builders, and designers.



The Community Structure Plan (Figure 3.4-2) shows how the various landscape conditions interact with one another to create the overall community landscape theme. In general, the plan is concerned with those areas within road rights-of-way, drainage areas, parks, and visually sensitive development areas such as entries and focal points.



3.4.2 Landscape Plant Palette





The following plant palette has been derived to enhance the Kohl Ranch desert landscape theme. The plant material has been selected to accent the desert environment while providing important functional characteristics. This list is intended to be a menu from which landscape architects can mix and match plant materials for various situations. The following general criteria have been established to determine plant selections:


- Desert Compatible Plants – Plants shall be tolerant of the harsh desert environment.
- Low/Moderate Water Consumption – Plants should not be high water consumers. Vast expanses of lawn areas are discouraged. Golf courses shall be primary water consumers and shall be irrigated with canal water to the extent possible.
- Broadleaf Deciduous Trees – These plants should be used where the need for summer shade and winter sunshine is important.
- Groundcover – Use of drought tolerant groundcovers and decomposed granite should be encouraged to reduce blowing dust.
- Maintenance of Established Themes – The Kohl Ranch streetscape and entry themes should be maintained (i.e., Palm trees evenly spaced with citrus understory and desert groundcover throughout).




Table 3.4-A, Kohl Ranch Plant Palette

Plant Form	Botanical Name	Common Name	Location
PALMS			
	<ul style="list-style-type: none"> • <i>Arecastrum romanzoffianum</i> • <i>Brahea armata</i> • <i>Chamaerops humilis</i> • <i>Phoenix canariensis</i> • <i>Phoenix dactylifera</i> • <i>Washingtonia filifera</i> • <i>Washingtonia robusta</i> 	Queen palm Mexican blue palm Mediterranean fan palm Canary Island date palm Date palm California fan palm Mexican fan palm	Palm Tree Streetscape for Major and Minor entries, Intersection Details, Expressway Streetscape, Urban Arterial Streetscape, Arterial Streetscape 1 and Major Streetscape 1.
LARGE SHADE TREES			
	<ul style="list-style-type: none"> • <i>Brachychiton populneus</i> • <i>Cassia leptophylla</i> • <i>Dalbeggia sissoo</i> • <i>Fraxinus velutina</i> • <i>Koelreuteria bipinata</i> 	Bottle tree Golden Medallion Rosewood Arizona ash Chinese Flame Tree	Formal Canopy Streetscape for Expressway Streetscape, Major Streetscape 2, Secondary Streetscape 1, Secondary Streetscape 2, and Collector Streetscape 1.

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> • <i>Olea europaea</i> • <i>Pinus Eldarica</i> • <i>Platanus wrightii</i> • <i>Quercus virginiana</i> • <i>Tipuana tipu</i> • <i>Ulmus parvifolia</i> 	'Swan Hill' olive Afgan Pine Arizona sycamore Southern live oak Tipu Tree Evergreen Elm	
	<ul style="list-style-type: none"> • <i>Acacia salicina</i> • <i>Acacia farnesiana</i> • <i>Acacia smallii</i> • <i>Acacia stenophylla</i> • <i>Cercidium floridum</i> • <i>Cercidium h. 'Desert Museum'</i> • <i>Chitalpa Tashkentensis</i> • <i>Geijera parvifolia</i> • <i>Parkinsonia aculeata</i> • <i>Prosopis alba</i> • <i>Prosopis chilensis</i> • <i>Prosopis hybrid 'Phoenix'</i> • <i>Rhus lancea</i> 	Willow acacia Sweet acacia Sweet acacia Shoestring acacia Blue palo verde Hybrid palo verde Pink Dawn Australian willow Mexican palo verde Argentine mesquite Chilean mesquite Hybrid mesquite African sumac	Formal Canopy Streetscape for Expressway Streetscape, Major Streetscape 2, Secondary Streetscape 1, Secondary Streetscape 2, and Collector Streetscape 1.
STREET TREES (NON-DESERT)			
	<ul style="list-style-type: none"> • <i>Brachychiton populneus</i> • <i>Eucalyptus species</i> • <i>Fraxinus velutina</i> • <i>Platanus wrightii</i> 	Bottle tree Various Arizona ash Arizona sycamore	Informal Canopy Streetscape for Urban Arterial Streetscape, Arterial Streetscape 2, Polk Street Streetscape 2a and 2b, Industrial Collector Streetscape, Collector Streetscape 2 and Tyler Street Project Edge.
STREET TREES (DESERT THEME)			
	<ul style="list-style-type: none"> • <i>Cercidium floridum</i> • <i>Cercidium h. 'Desert Museum'</i> 	Blue palo verde Hybrid palo verde	Informal Canopy Streetscape for Urban Arterial Streetscape, Arterial Streetscape 2, Polk Street Streetscape 2a and 2b, Industrial

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> • <i>Cercidium praecox</i> • <i>Chilopsis linearis</i> • <i>Prosopis alba</i> • <i>Prosopis chilensis</i> 	Sonoran palo verde Desert willow Argentine mesquite Chilean mesquite	Collector Streetscape, Collector Streetscape 2 and Tyler Street Project Edge.
ACCENT TREES			
	<ul style="list-style-type: none"> • <i>Albizia julibrissin</i> • <i>Bauhinia variegata</i> • <i>Jacaranda mimosifolia</i> • <i>Lagerstroemia indica</i> • <i>Pistacia 'Red push'</i> • <i>Thevetia peruviana</i> • <i>Citrus sp</i> 	Silk tree Purple orchid tree Jacaranda Crape myrtle Red push pistache Yellow oleander Orange	Palm Tree Streetscape for Major and Minor Entries, Intersection Details, Expressway Streetscape, Urban Arterial Streetscape, Arterial Streetscape 1, and Major Streetscape.
WINDROW			
	<ul style="list-style-type: none"> • <i>Eucalyptus species</i> • <i>Oleander</i> 	Various Various	Edge and buffer conditions.
LARGE SHRUBS			
	<ul style="list-style-type: none"> • <i>Baccharis sarothoides</i> • <i>Caesalpinea pulcherrima</i> • <i>Cassia nemophylla</i> • <i>Cistus purpureus</i> • <i>Cistus ladaniferus</i> 	Desert broom Red bird of paradise Senna Orchid rock rose Crimson spot rock rose Hopseed bush	Throughout Streetscapes and Edge and Buffer conditions.

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> • <i>Dodonaea viscosa</i> • <i>Fouquieria splendens</i> • <i>Leucophyllum frutescens</i> • <i>Liqustrum japonicum</i> • <i>Nerium oleander</i> • <i>Photinia fraseri</i> • <i>Pittosporum tobira</i> • <i>Pittosporum tobira</i> • <i>'variegata'</i> • <i>Xylosma congestum</i> 	<p>Ocotillo</p> <p>Texas Ranger</p> <p>Japanese privet</p> <p>Petite Pink & 'Petite Salmon'</p> <p>Fraser photinia</p> <p>Mock orange</p> <p>Yellow oleander</p> <p>Shiny xylosma</p>	
MEDIUM/SMALL SHRUBS			
	<ul style="list-style-type: none"> • <i>Agave shawii</i> • <i>Aloe species</i> • <i>Bougainvillea 'Oh La La'</i> • <i>Carissa g.</i> • <i>Encelia farinosa</i> • <i>Euonymus japonica</i> • <i>Euryops virides</i> • <i>Ferocactus species</i> • <i>Hemerocallis</i> • <i>Hesperaloe parviflora</i> • <i>Lantana camara</i> • <i>Larrea tridentate</i> • <i>Leucophyllum f.</i> • <i>'Green Cloud'</i> • <i>Muhlenbergia capillaries</i> • <i>Muhlenbergia rigens</i> • <i>Opuntia violacea</i> • <i>Santolina virens</i> • <i>Simmondsia chenensis</i> 	<p>Shaw's century plant</p> <p>Aloe</p> <p>Bougainvillea</p> <p>Green Carpet</p> <p>Encelia</p> <p>Euonymus</p> <p>Green euryops</p> <p>Barrel cactus</p> <p>Daylily</p> <p>Red yucca</p> <p>Radiation lantana</p> <p>Creosote bush</p> <p>Texas Sage</p> <p>Regal Mist</p> <p>Deer Grass</p> <p>'San Rita' prickly pear</p> <p>Santolina</p> <p>Jojoba</p>	Throughout Streetscapes and Edge and Buffer conditions.
GROUNDCOVER			

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> • <i>Acacia redonlens</i> 'prostrata' • <i>Baccharis pilularis</i> • <i>Dalea capitata</i> • <i>Lantana montevidensis</i> • <i>Lantana</i> 'New Gold' • <i>Lantana sellowiana</i> • <i>Lonicera japonica</i> • <i>Santolina chamaecyparissus</i> • <i>Trachelospermum jasminoides</i> • <i>Verbena gooddingii</i> 	Trailing acacia 'Centennial' Sierra Gold Trailing lantana New Gold lantana Trailing lantana Honeysuckle Lavender cotton Star jasmine Sandpaper Verdena	Throughout Streetscapes and Edge and Buffer conditions.
VINES			
	<ul style="list-style-type: none"> • <i>Bougainvillea spectabilis</i> • <i>Ficus pumila</i> • <i>Lonicera japonica</i> • <i>Rose banksiae</i> 	Various Creeping fig Hall's honeysuckle Lady Bank's rose	Throughout Streetscapes and Edge and Buffer conditions.
TURF			
	<ul style="list-style-type: none"> • <i>Hybrid bermuda</i> 	Tifgreen or 'Santa Ana'	Throughout Streetscapes and Edge and Buffer conditions.

3.4.3 Entries

Entries or focal points are those key areas along the roadway where change in travel direction may occur. Often at these points a different land use may occur and the landscape should announce this change. Working in concert with signage, lighting and hardscape elements, the landscape plant materials will create "outdoor rooms" with an individual identity and level of importance that creates a unique mood as the motorist, pedestrian or cyclist passes through each space. Often a stoppage in movement occurs at these points; therefore, design elements that create a more lush effect of color, texture, and form should be concentrated at these locations.

Major Project Entries

The landscapes at the project entries along with the signage, lighting and hardscape elements will form a gateway into the Kohl Ranch and set the stage for the community identity. The landscape should draw views into the site and toward important development features. Tasteful plantings should indicate a transition from the surrounding land uses to the more formal roadways. Water resources should be concentrated toward irrigation of trees and shrubs. Ground covers may be of a more xeric nature.



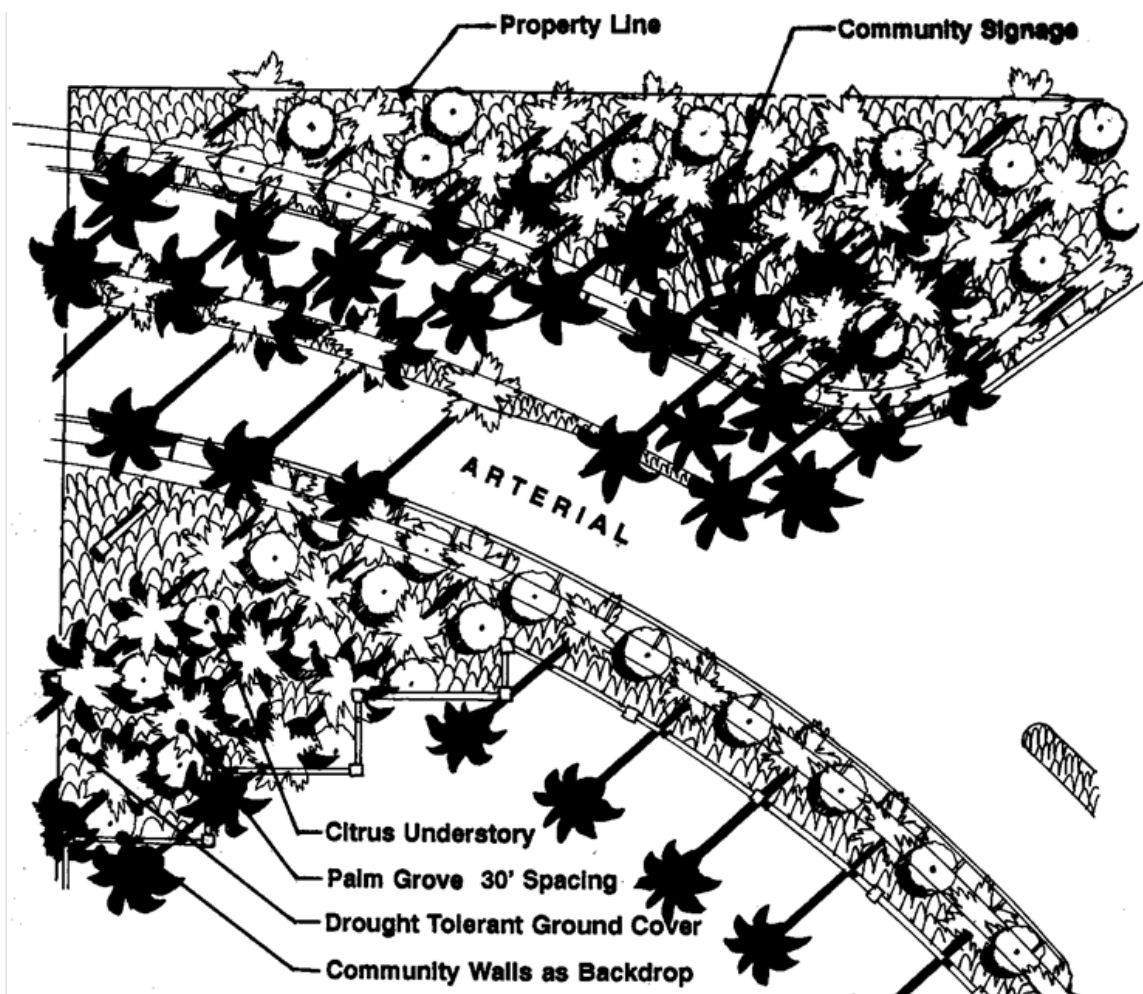
Major Entry

Major project entries are located at the following intersections:

- Avenue 60, north of "B" Street;
- Avenue 62 and Polk Street; and
- "E" Street and Avenue 66.

Major project entries will be landscaped with palm trees, with a maximum spacing of 30 feet on-center, with a citrus understory as designated in **Figure 3.4-3, Major Project Entry**. These plantings will be underlain by a drought tolerant groundcover. Community walls will create a backdrop for the entry landscape features.

Figure 3.4-3, Major Project Entry



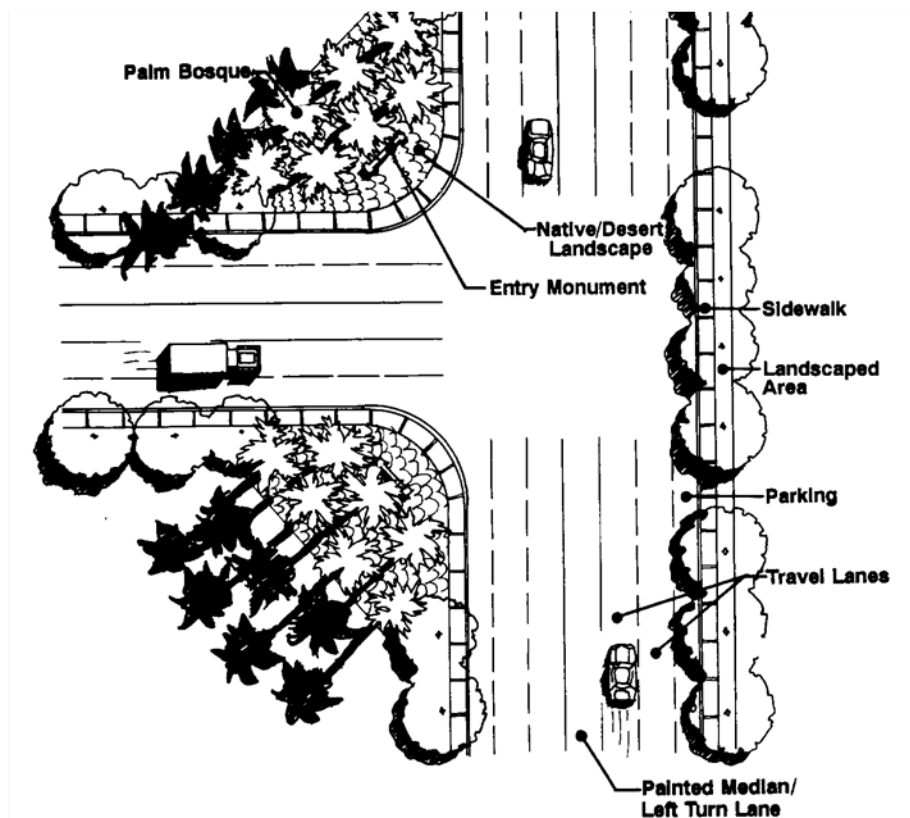
Minor Entries

The minor entries should convey the same objectives as the major entries but on a less grand and more intimate scale. **Figure 3.4-4, Minor Entry**, depicts a typical minor entry at the following locations:

- "B" Street and Avenue 62;
- "F" Street and Tyler Street;
- "D" Street and Polk Street;
- the interior loop road and Polk Street; and
- Polk Street and boundary of Planning Areas J-4 and L-3.

The plantings which create the walls of the outdoor "room" should be pulled in and a greater sense of enclosure should be created. Elements of color, form, and texture should be highly developed for these intensely visible areas. Palm tree groves should be supplemented with native/desert landscape elements.

Figure 3.4-4, Minor Entry

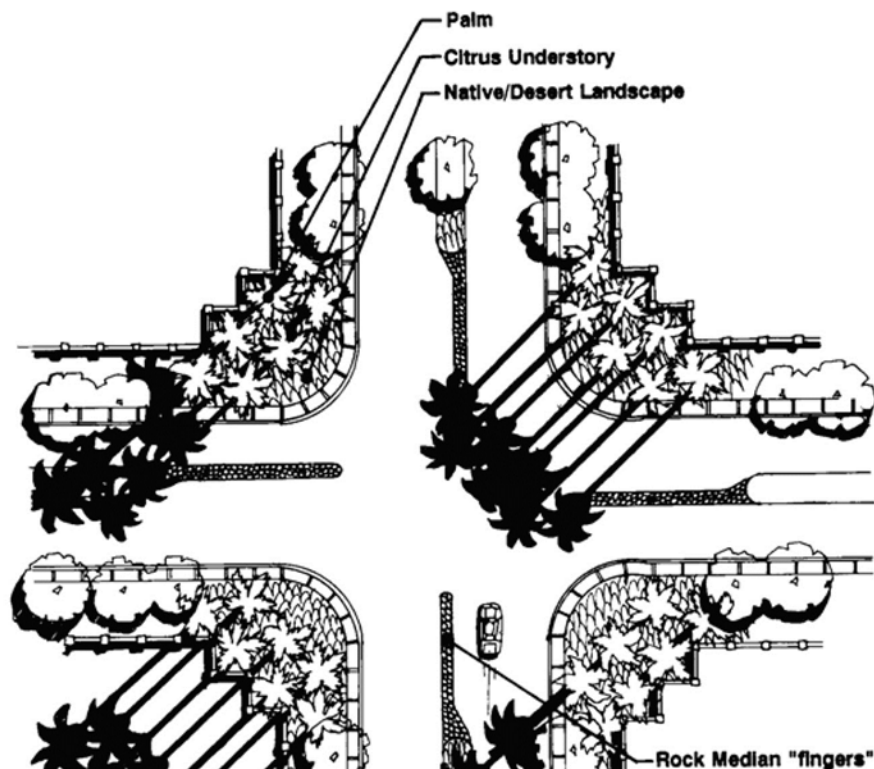


3.4.4 Intersections

Within the project site, certain key intersections will have special landscape treatments to denote the entry into a project neighborhood. This intersection treatment is illustrated in **Figure 3.4-5, Intersection Detail**. The landscape concept will be adapted to three-way intersections. Three-way intersections include the intersection of Avenue 62 and "C" Street, the intersection of "E" Street and Avenue 64, the intersection of "B" Street and Tyler Street, the intersection of "B" Street and Avenue 62, the intersection of "D" Street and Avenue 62, the intersection of "C" Street and Avenue 62, and the intersection of "E" Street and "F" Street. Intersections with special intersection treatments also include "F" Street and it turns south adjacent to Planning Area M-1B.

Major intersection landscapes should act as terminus points along the project roadways. They should appear as large enclosed outdoor rooms where the traveler can easily decide their direction of travel, react, then continue moving through the site. The edges of the room should be vertical to provide enclosure, with plant material located so that signage is emphasized. Color, form and texture should further define the importance of these decision points through an increased emphasis on water resources and plant materials.

Figure 3.4-5, Intersection Detail



Note: Landscape concept will be adapted to 3-way intersections.

3.4.5 Streetscapes

In order to achieve a cohesive overall circulation system for the Kohl Ranch and provide a strong community structure and identity, a consistent level of landscape design quality for public and private rights-of-way will be maintained. A consistent streetscape treatment will be implemented throughout the project as well as for the roadways surrounding the site. The general streetscape concept combines the use of palm trees with citrus understory, massed in selected locations, supported by turf and plantings of lush desert plants and annual color. The streetscapes are broken down into five major categories with special features and plant forms as indicated, and are applied to the hierarchy of the project roadways:

- Palm Tree Streetscape
- Formal Canopy Streetscape
- Informal Canopy Streetscape
- Windrow Planting
- Expressway Planting

Plant materials within these areas are intended to create a mood of movement and procession. Plant materials should direct and guide the traveler, screening sensitive views, or framing and creating focal points or "rooms" as the motorist, cyclist or pedestrian moves through the streetscape. Water resources should be minimized through the use of drip irrigation systems for trees and shrubs, and groundcovers which are attractive and thrive with limited irrigation or seasonal rains.

The five types of streetscape planting schemes listed above are applied to the hierarchy of project roadways which are broken down into seven roadway categories with special features and suggested plant forms as follows.

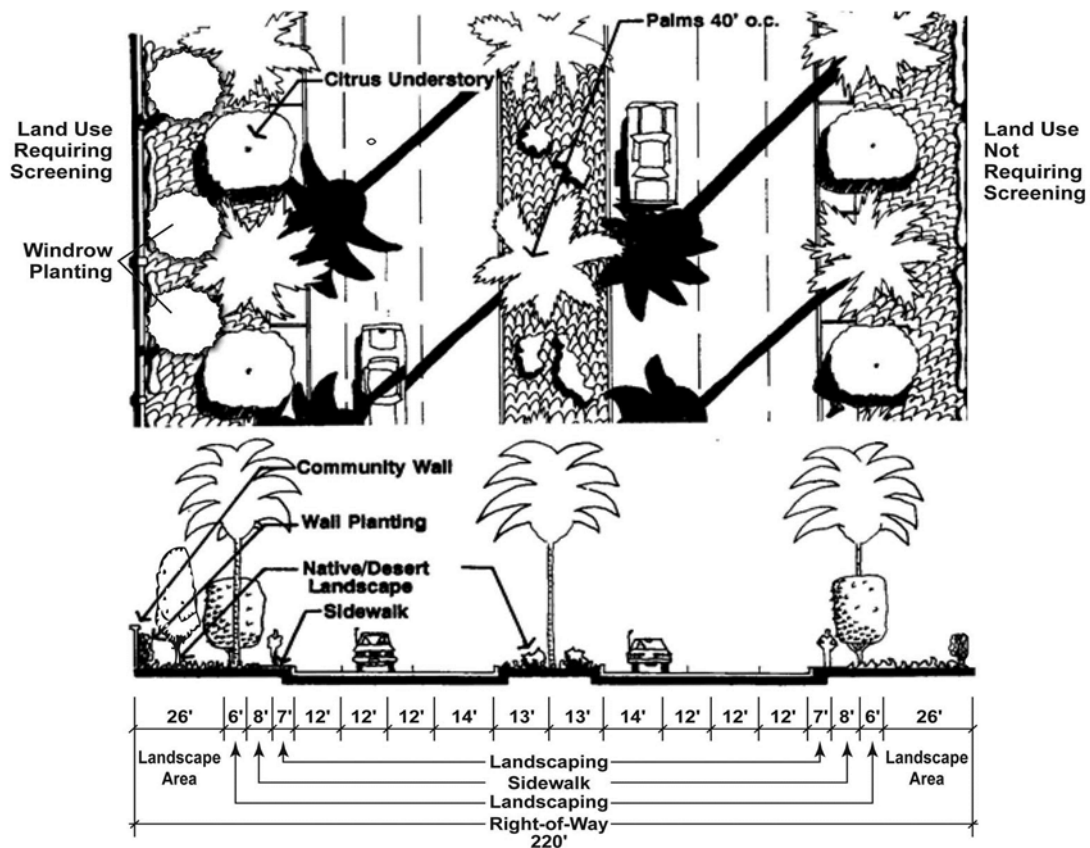


Expressway Streetscapes

Expressway Streetscape (220' ROW)

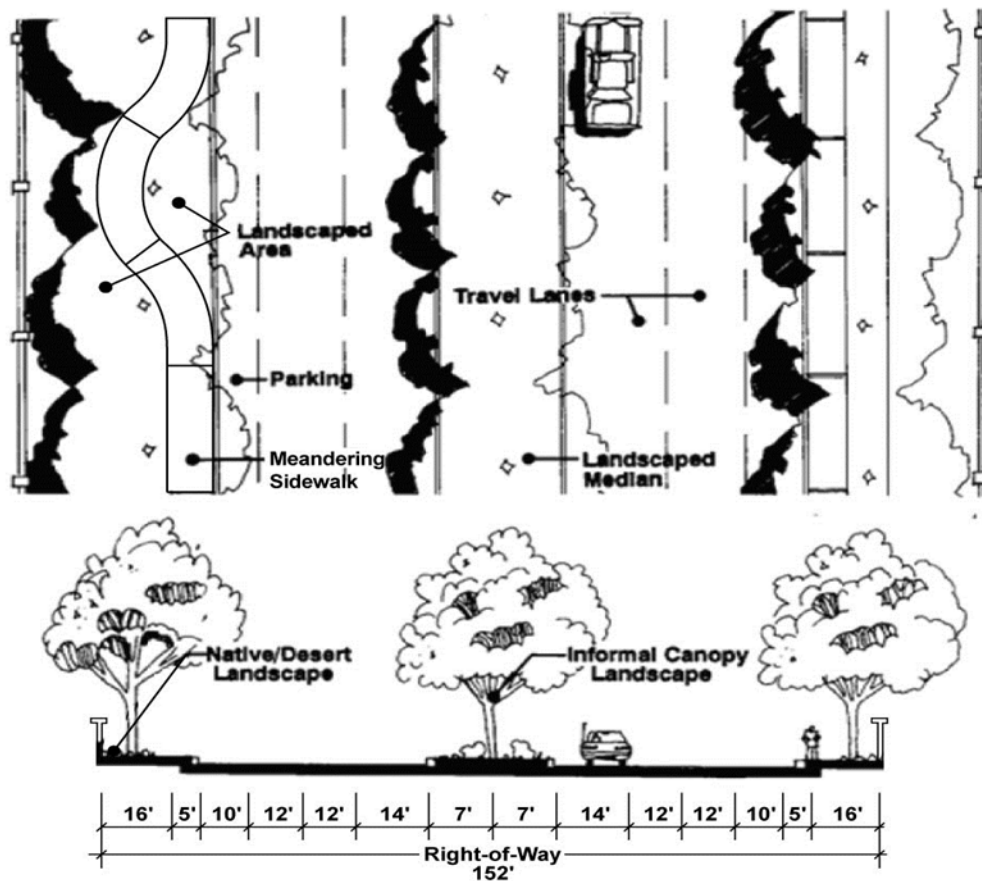
This streetscape applies to Avenue 62, the single project expressway, to serve as a formal progression through the site in the "grand allée" tradition of an estate roadway. The Palm Tree Streetscape is used as the basis for this streetscape, providing continuity across the project site. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial or residential, while streetscapes in front of commercial uses and/or parks would not require screening. **Figure 3.4-6, Expressway Streetscape**, shows the concept for either situation.

- Where the expressway passes through the airport Emergency Touchdown Zone (ETZ), no trees should be planted.

Figure 3.4-6, Expressway Streetscape

Urban Arterial (152' ROW)

This streetscape is intended for Avenue 66 only, and is interspersed with the Informal Canopy Streetscape, which should include trees that create a canopy by touching at their edges providing an overhead plane to impart a feeling of enclosure and to provide dappled shade as reflected in **Figure 3.4-7, Urban Arterial Streetscape**.

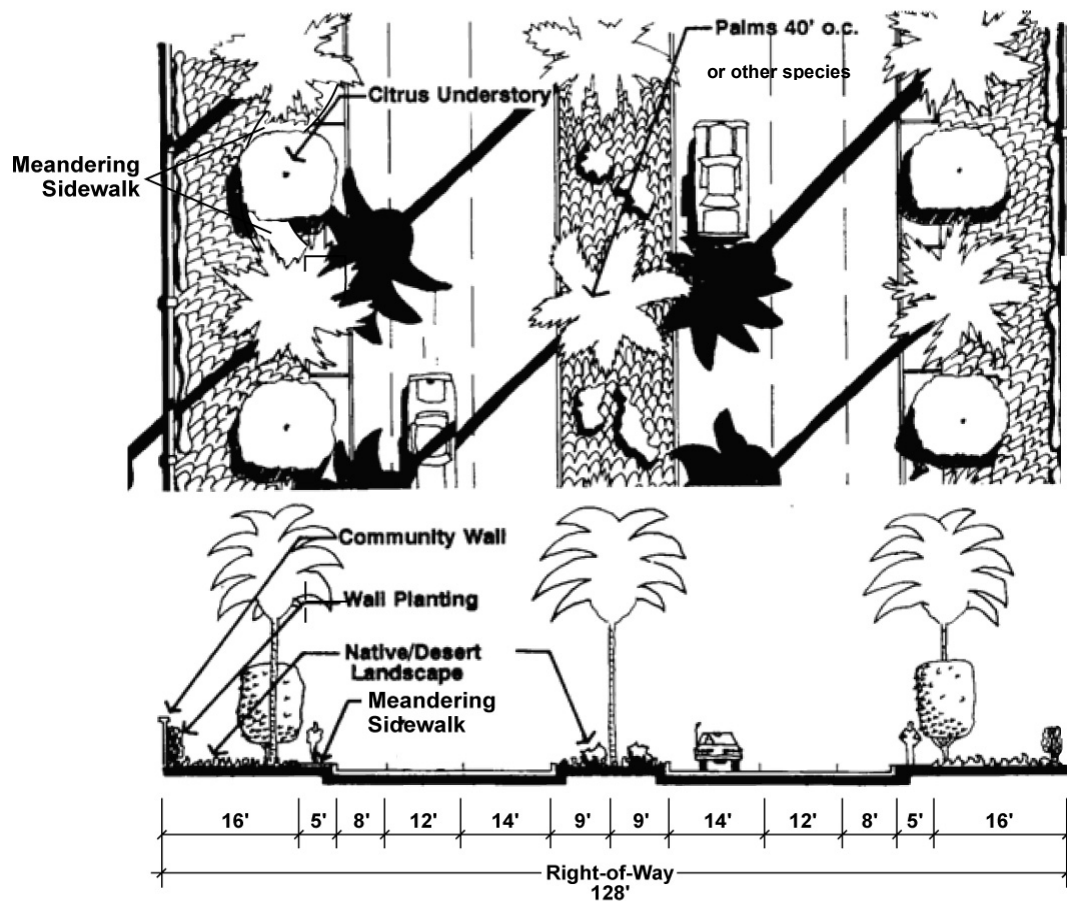
Figure 3.4-7, Urban Arterial Streetscape

Arterial Highway Streetscape (128' ROW)

This streetscape is intended for Tyler Street north of Avenue 62, and Polk Street. Two different streetscapes are intended for these two arterials since Tyler Street is a major roadway within the project and Polk Street is along the edge of the project.

Arterial Streetscape 1 –Tyler Street, north of Avenue 62

- This roadway will include automobiles, bicycle and pedestrian traffic; therefore, foreground treatment and trees should be designed with regard to automobile speeds and/or the pedestrian experience depending on whether off-pavement trails are provided. The Palm Tree Streetscape is used as the basis for Arterial Streetscape 1. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial or residential, while streetscapes in front of commercial or airport mixed uses would not require screening. **Figure 3.4-8, Arterial Streetscape 1**, shows the concept for either situation. Although palms may be used, other columnar tree types may also be used to better address pedestrian needs
- Arterial Streetscape 1 should include more formal elements near the entry focal points including a citrus understory to further emphasize the project entry.

Figure 3.4-8, Arterial Streetscape 1

Arterial Streetscape 2 – Polk Street

The Informal Canopy Streetscape is used as the basis for Arterial Streetscape 2. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial, while streetscapes adjacent to the drainage canal also varies from a typical condition. **Figure 3.4-9, Arterial Streetscape 2 - Polk Street Streetscape 2A**, shows the typical situation. Polk Street, north of Avenue 62, would include Windrow Plantings, if screening is needed.

Figure 3.4-9, Arterial Streetscape 2, Polk Street Streetscape 2A

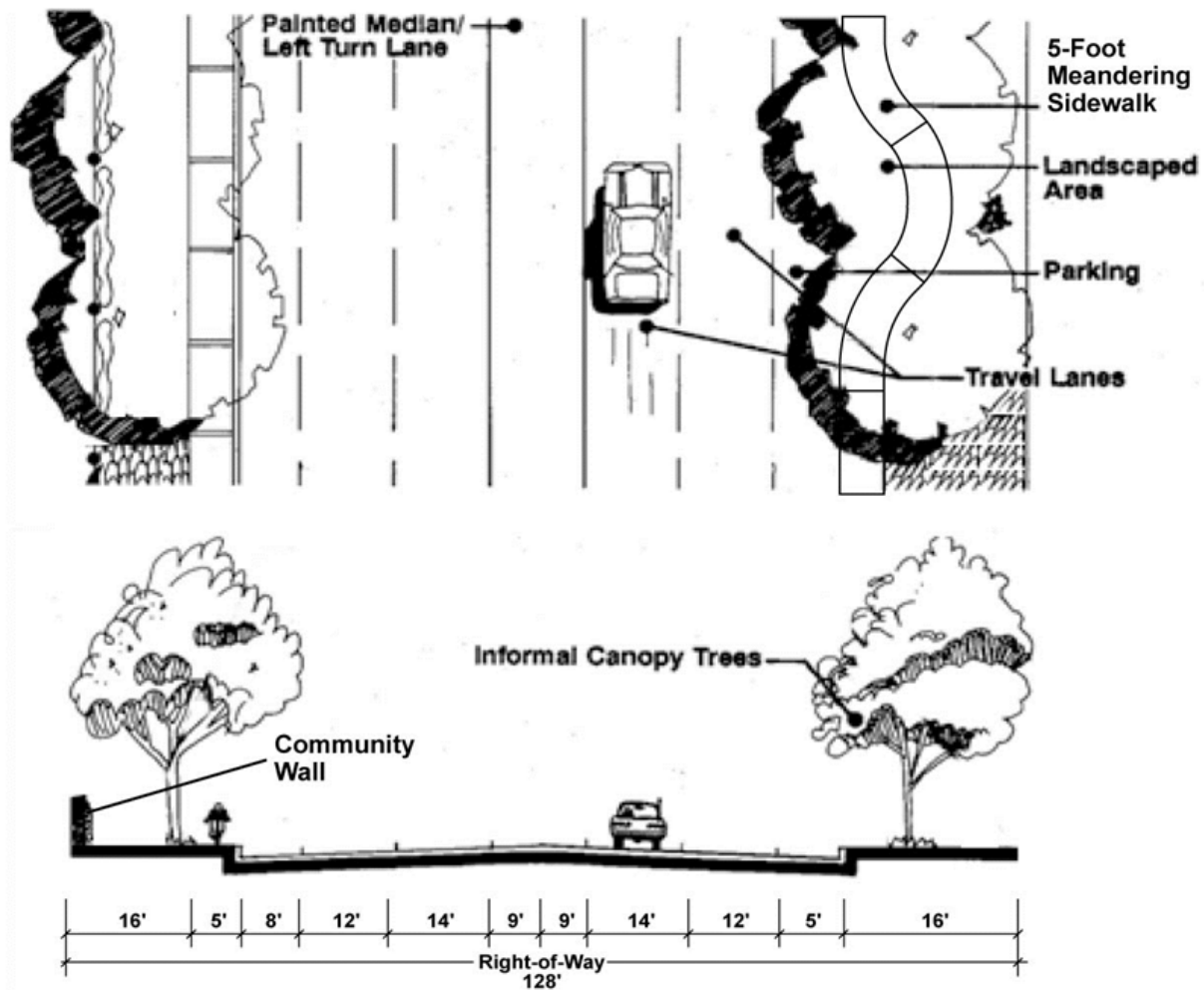
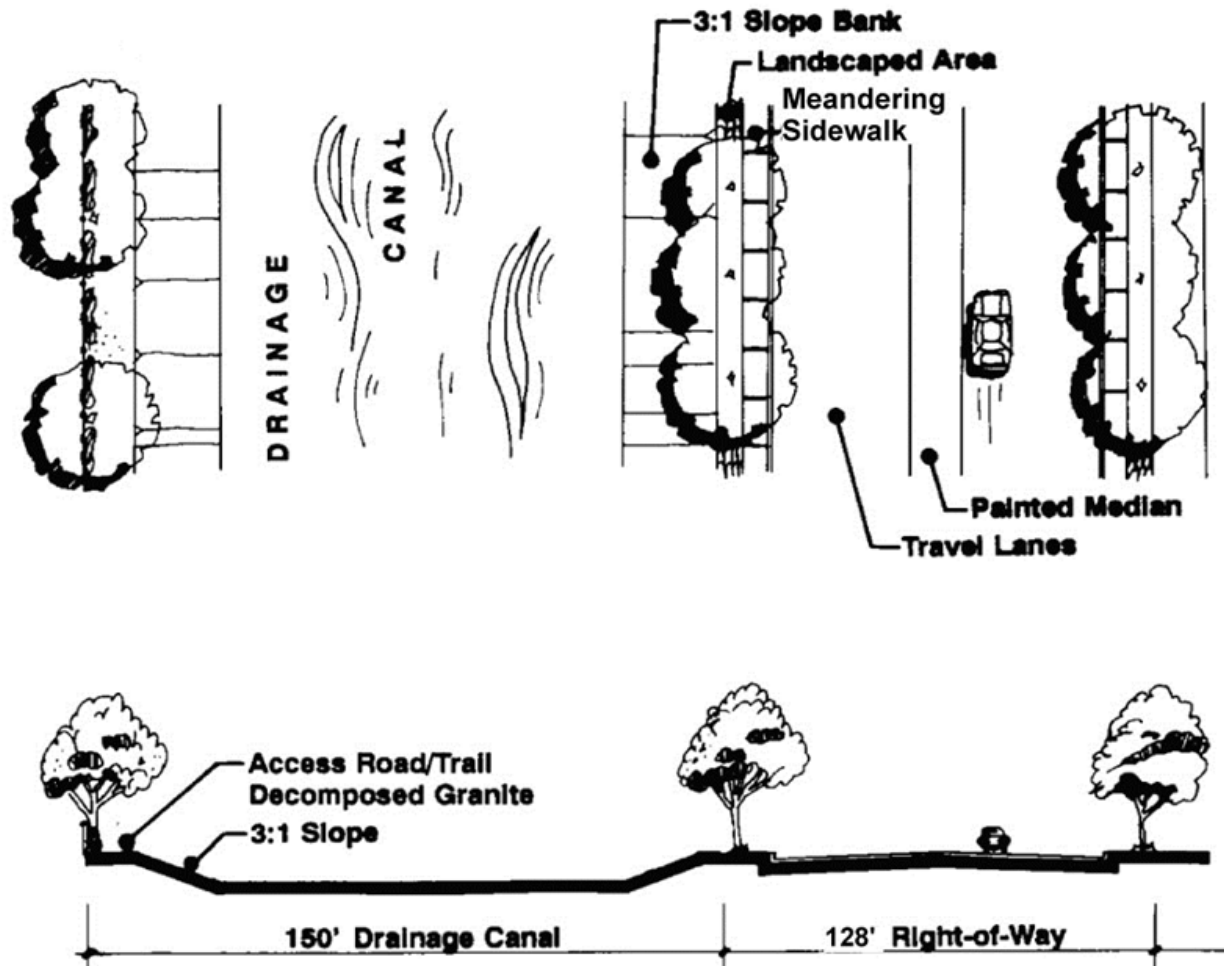


Figure 3.4-10, Arterial Streetscape 2, Polk Streetscape 2B depicts the project edge where the drainage facilities abut Polk Street.

Figure 3.4-10, Arterial Streetscape 2, Polk Street Streetscape 2B

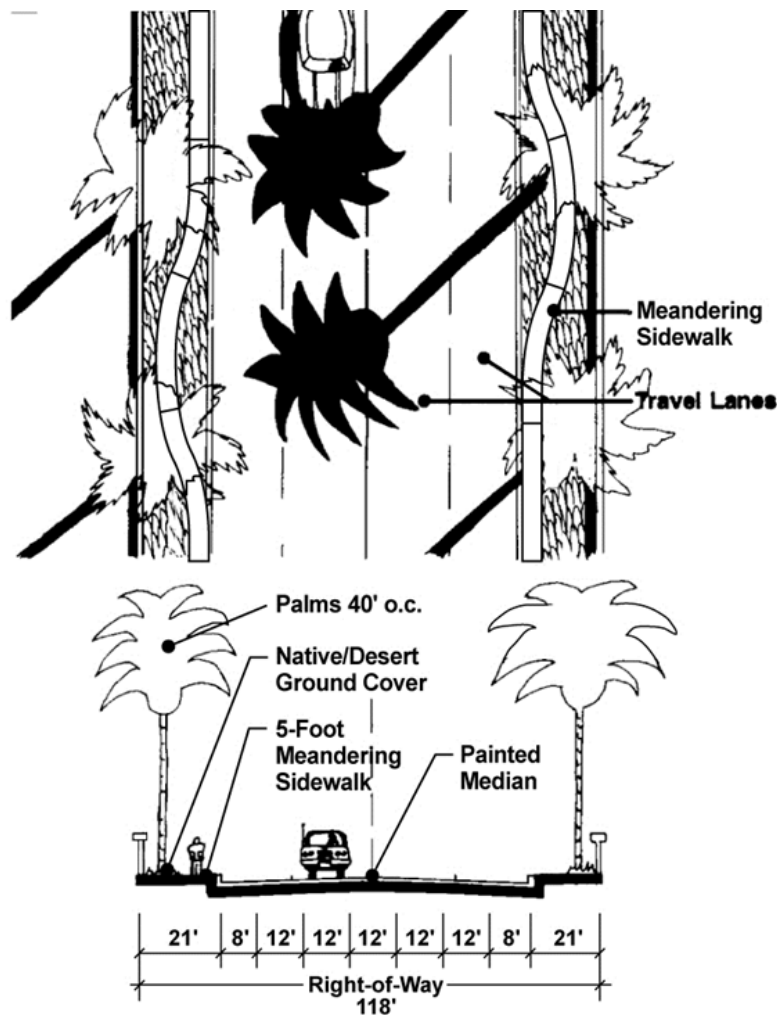


Major Highway Streetscape (118' ROW)

This streetscape is intended for Avenue 64 and "E" Street. Two different streetscapes are intended for these two majors since Avenue 64 is bordered by the utility corridor and evacuation channel while "E" Street meanders through residential areas.

Major Streetscape 1 – Avenue 64

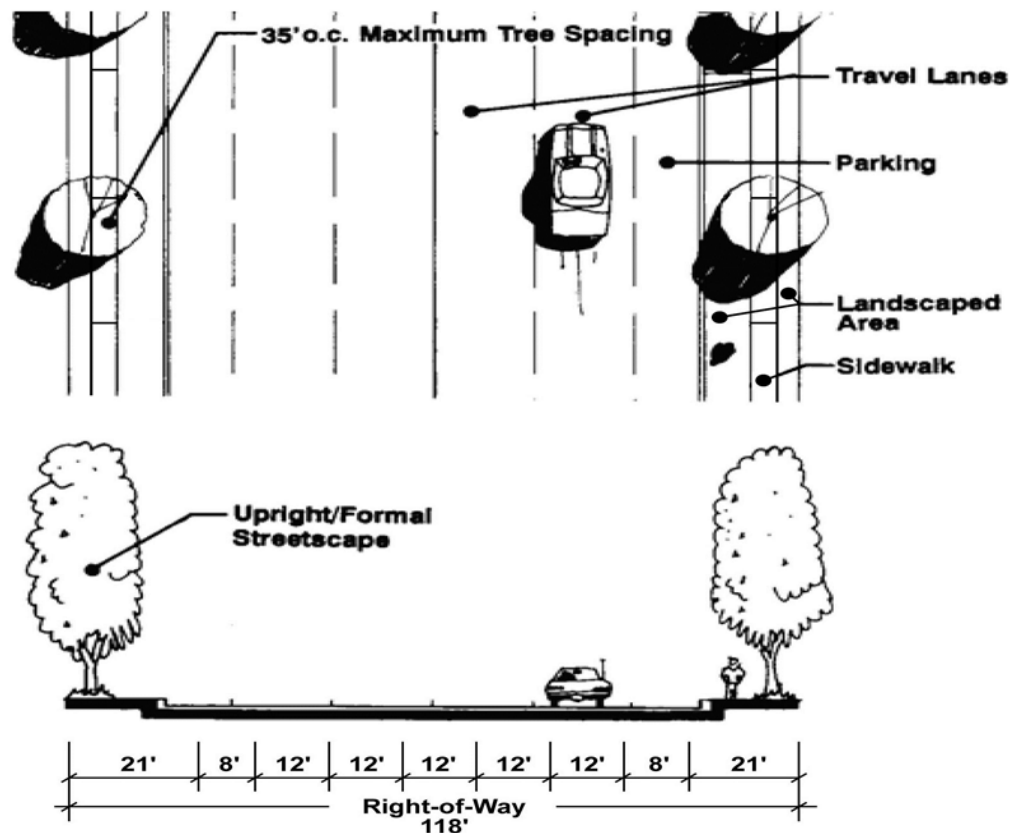
The Palm Tree Streetscape is used as the basis for **Figure 3.4-11, Major Streetscape 1**. This streetscape is intended for Avenue 64 as it traverses the project site. Palm trees provide a vertical element that denote the important roads through the site, and should be spaced at 40 feet on center with a native/desert ground cover. Avenue 64 is flanked by the utility corridor and the evacuation canal so screening of these areas is appropriate by adding Windrow Planting at the back of the ROW to provide a pleasant driving experience through the project area.

Figure 3.4-11, Major Streetscape 1

Major Streetscape 2 – “E” Street

Figure 3.4-12, Major Streetscape 2, illustrates the Formal Canopy Streetscape, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area. This streetscape should be used for “E” Street.

Figure 3.4-12, Major Streetscape 2



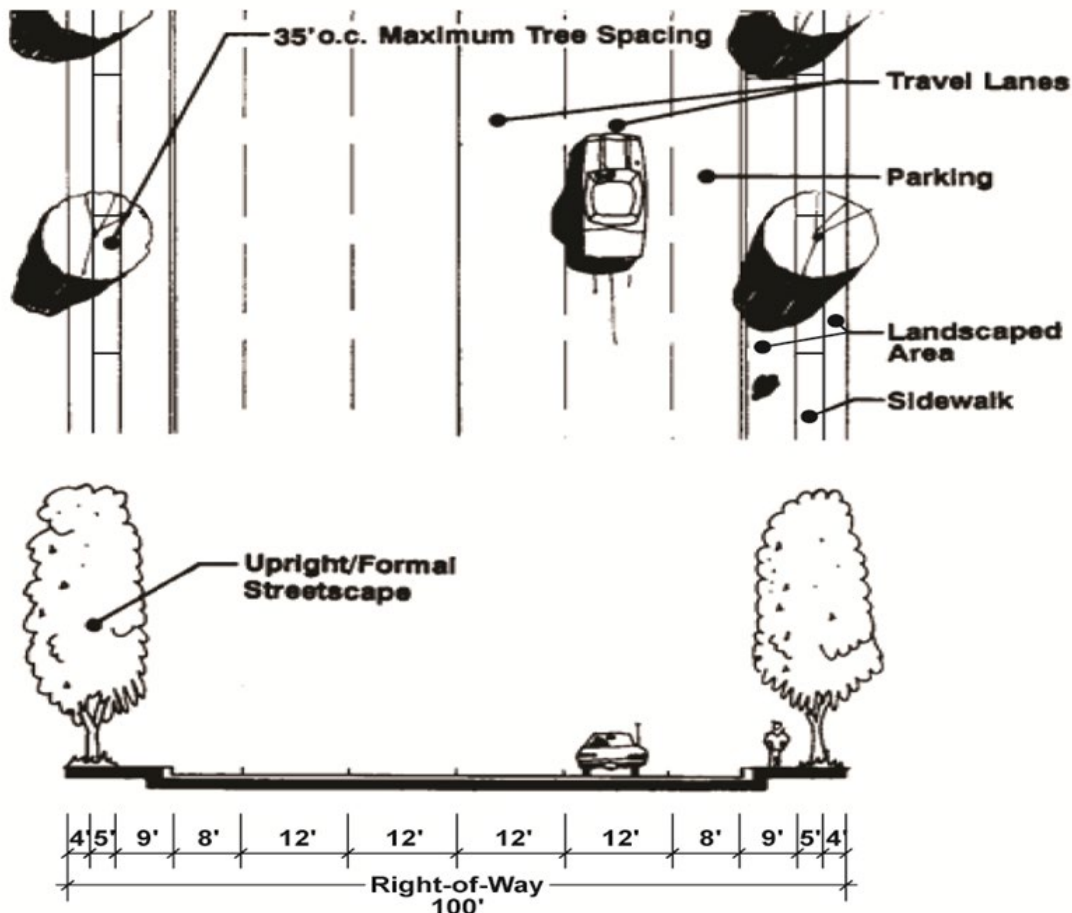
Secondary Streetscape (100' ROW)

This roadway treatment is intended be the next progression from an automobile dominated streetscape to a more bicycle and pedestrian-oriented streetscape. Design speeds along this roadway will be slower, views from passing motorists will be longer, and occasional bicyclists and pedestrians will be present, therefore plant material color and texture will be of importance along this roadway. Just as along the expressway and arterial streetscape, the plant material will open and close to provide or control the view along the roadway. Three streetscapes are proposed for the Secondary Highways in the Kohl Ranch Specific Plan area.

Secondary Streetscape 1

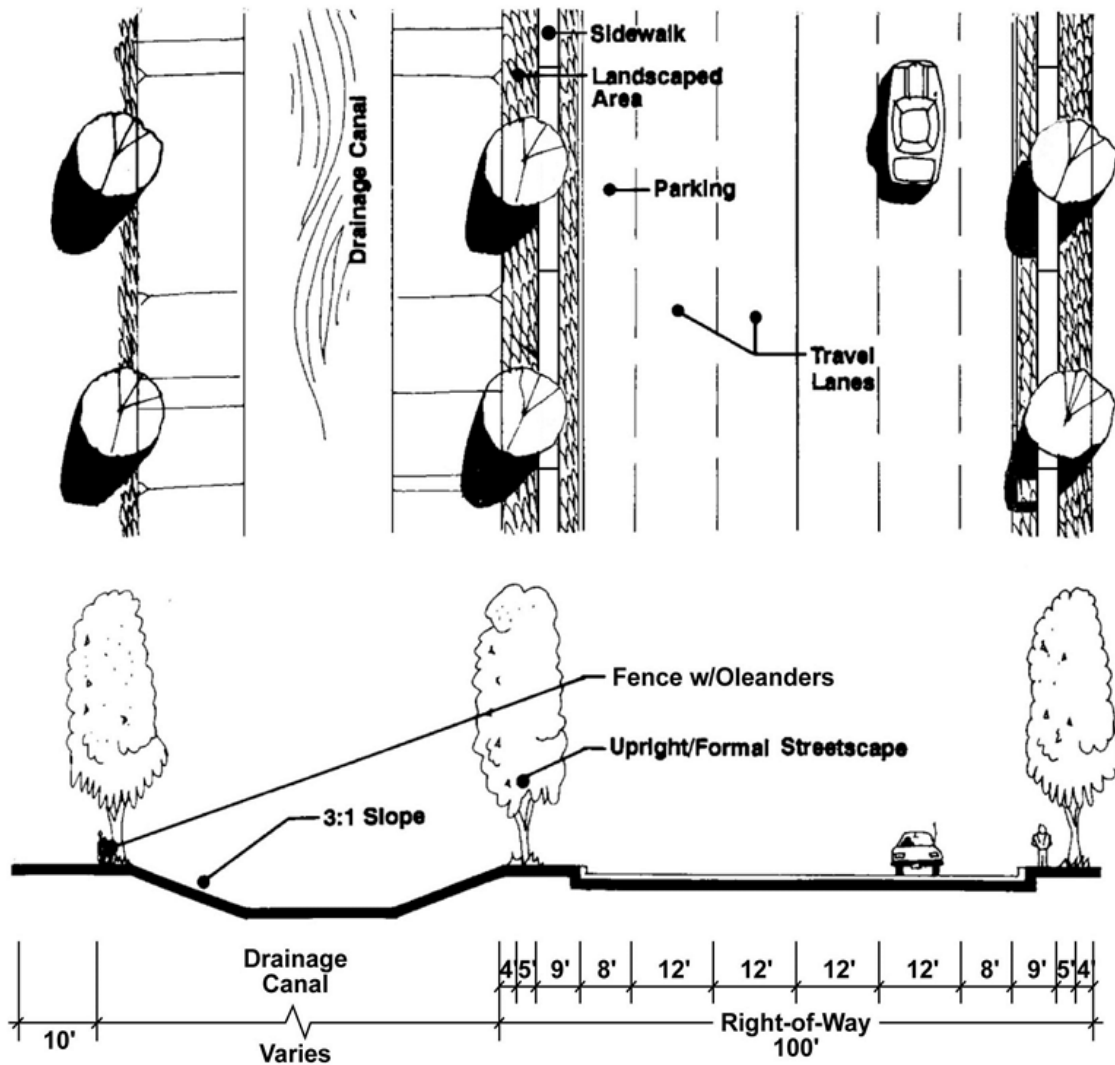
Figure 3.4-13, Secondary Streetscape 1, illustrates the Formal Canopy Streetscape, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area. This streetscape should be used for "C" Street and Tyler Street, north of Avenue 64.

Figure 3.4-13, Typical Secondary Streetscape 1



Secondary Streetscape 2

Figure 3.4-14, Secondary Streetscape 2 illustrates the Formal Canopy Streetscape alongside the drainage channel, which could occur along Tyler Street, north of Avenue 64. Upright trees are used at the edge between the roadway and the 3:1 side slope of the drainage facilities.

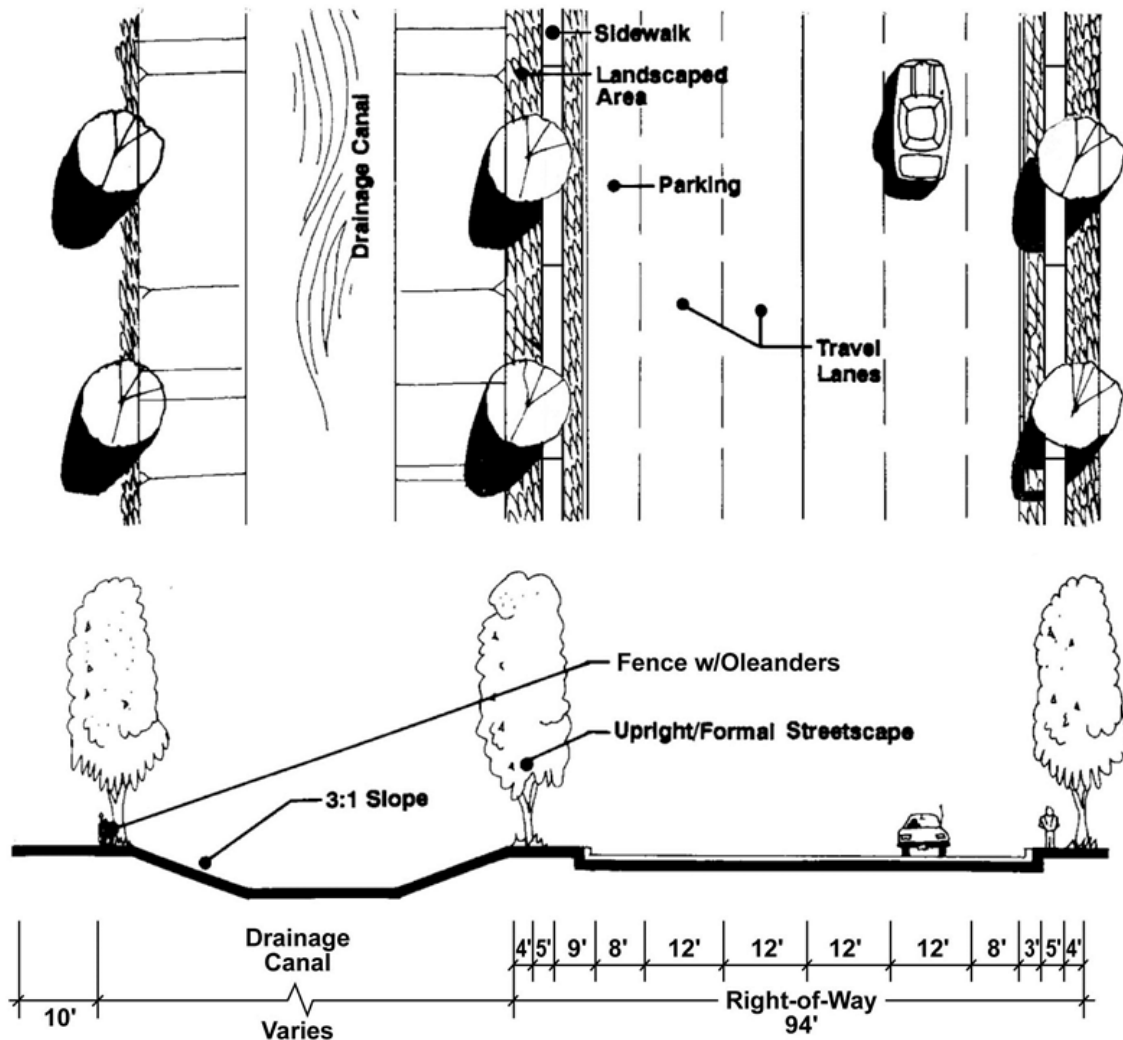
Figure 3.4-14, Secondary Streetscape 2

Note: Typical channel section - widths may vary.

Modified Secondary Streetscape

Tyler Street is also a secondary highway with a modified cross section south of Avenue 64. **Figure 3.4-15, Modified Secondary Streetscape 2** illustrates the Formal Canopy Streetscape alongside the drainage channel, which could occur along Tyler Street. Upright trees are used at the edge between the roadway and the 3:1 side slope of the drainage facilities.

Figure 3.4-15, Modified Secondary Streetscape 2

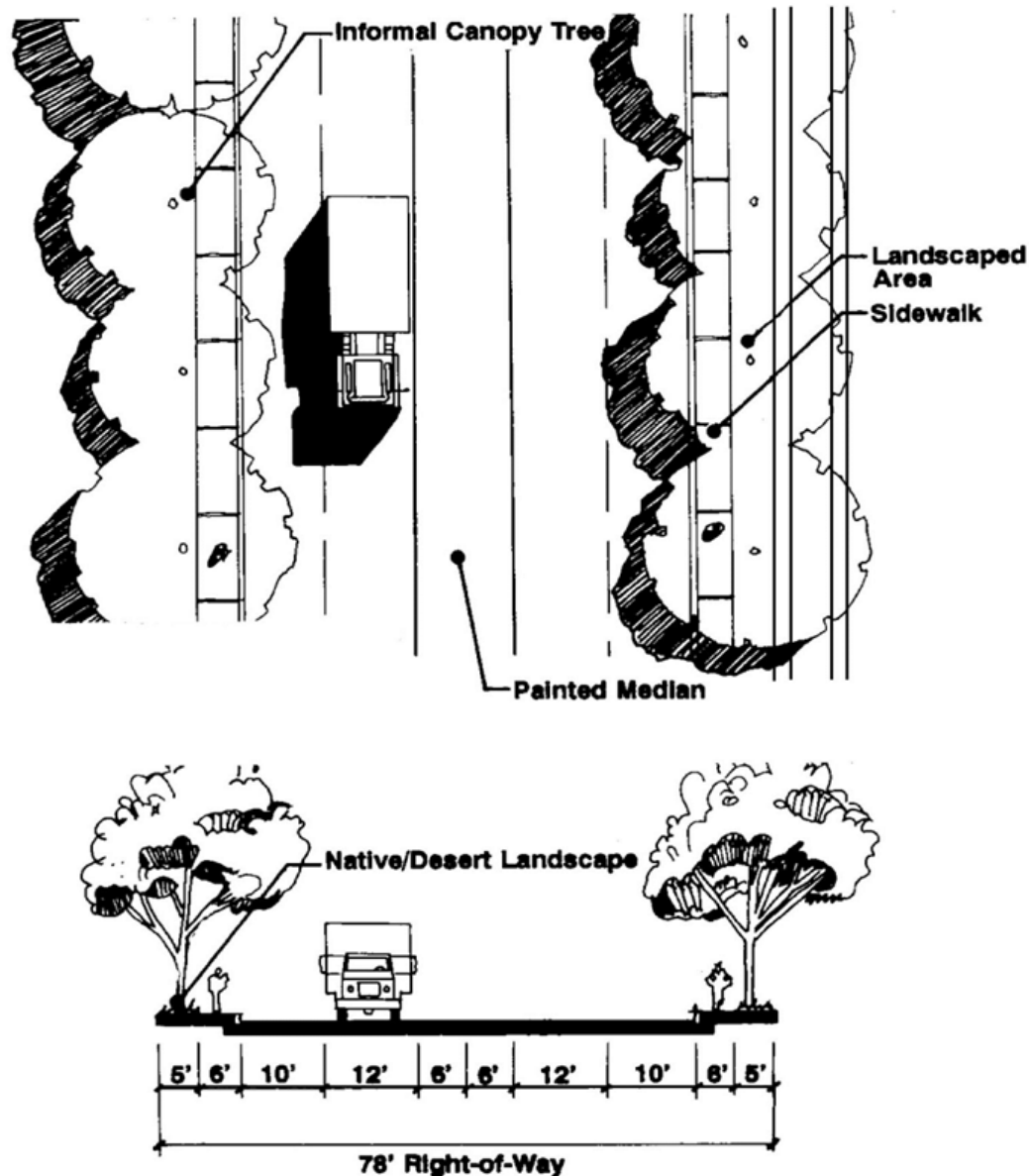


Note: Typical channel section - widths may vary.

Industrial Collector Streetscape (78' ROW)

The streetscape proposed for the Industrial Collector roadway classification is shown on **Figure 3.4-16, Industrial Collector Streetscape**, which illustrates Avenue 60 adjacent to heavy industrial and airport mixed use areas. Informal canopy trees should be used to frame the roadway, augmented by native/desert ground cover. A painted median is proposed.

Figure 3.4-16, Industrial Collector Streetscape



Collector Streetscape (74' ROW)

Collector roadways provide access to residential areas beyond the arterial roadways. Pedestrian and bicycle uses will be of a major concern in the development of the landscape plan for these intimate streetscapes. Front yard landscapes may be increased along this low speed roadway to create a more detailed streetscape of interesting forms, colors, textures, odors and sounds. Residential units front this roadway and access to driveways and compatibility of right-of-way landscapes to residential front yard landscapes must be carefully integrated. Two streetscapes are proposed for the Collector Streets within the Kohl Ranch Specific Plan area.

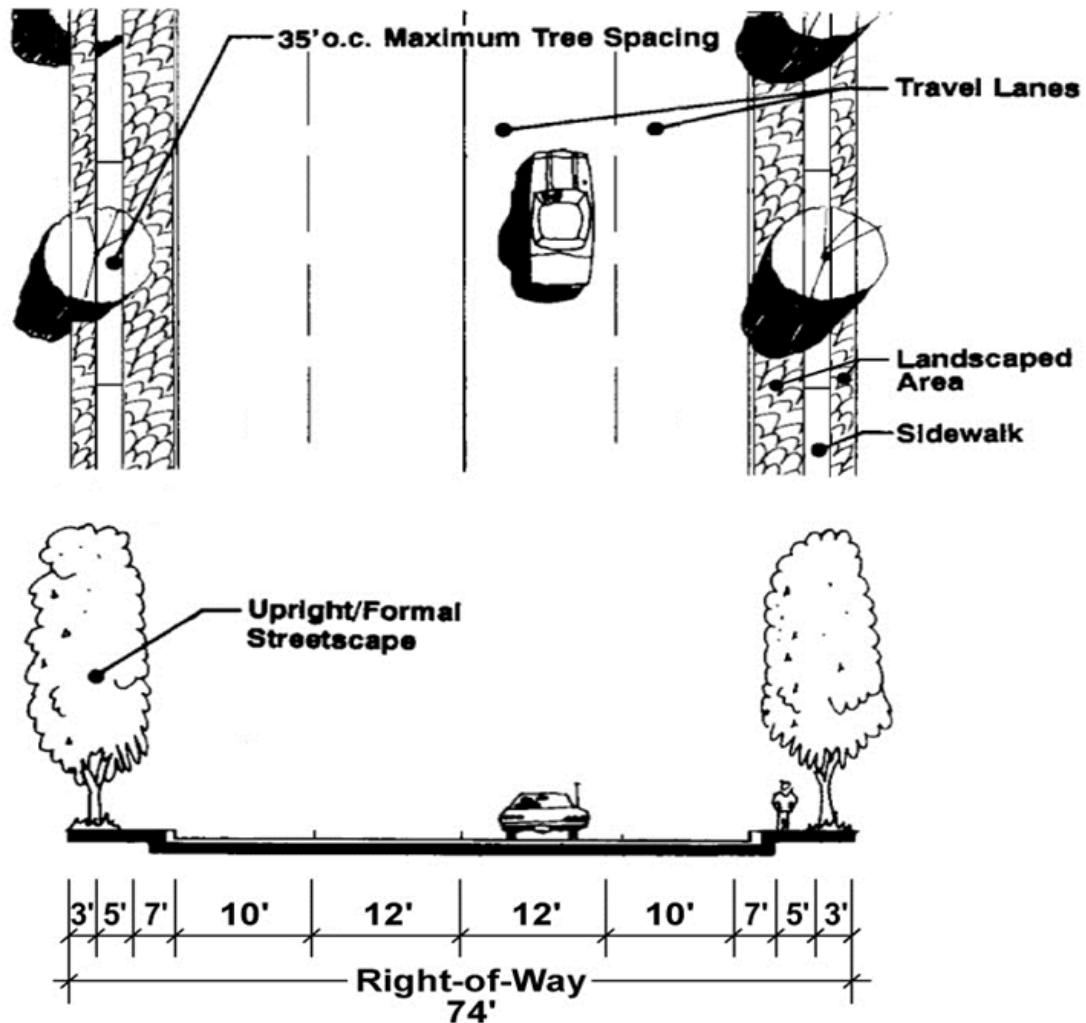


Residential Collector

Collector Streetscape 1

Figure 3.4-17, Collector Streetscape 1 illustrates the landscaping for "B" Street and Tyler Street north of Avenue 66, adjacent to Planning Area M-4. The Formal Canopy Streetscape is incorporated in Collector Streetscape 1, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area.

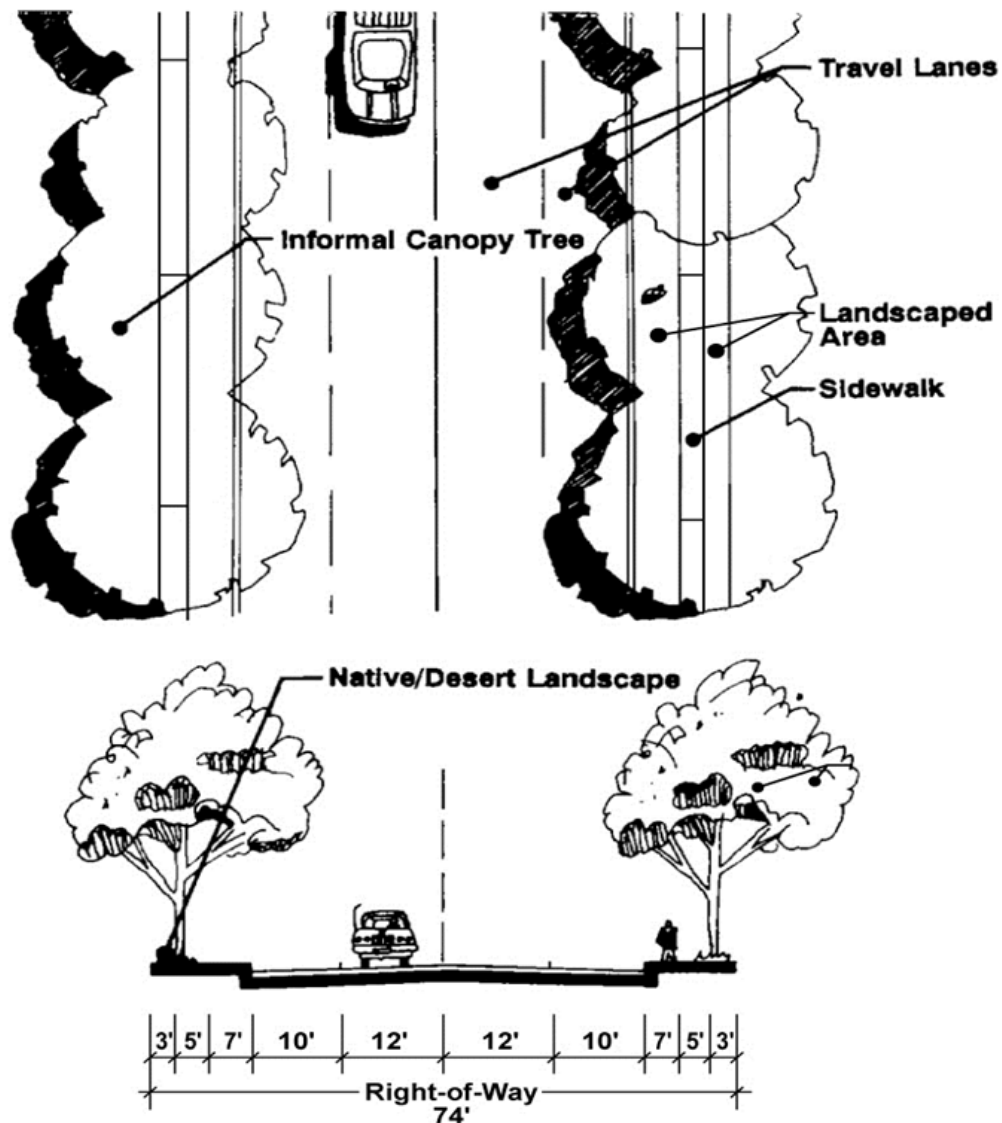
Figure 3.4-17, Collector Streetscape 1



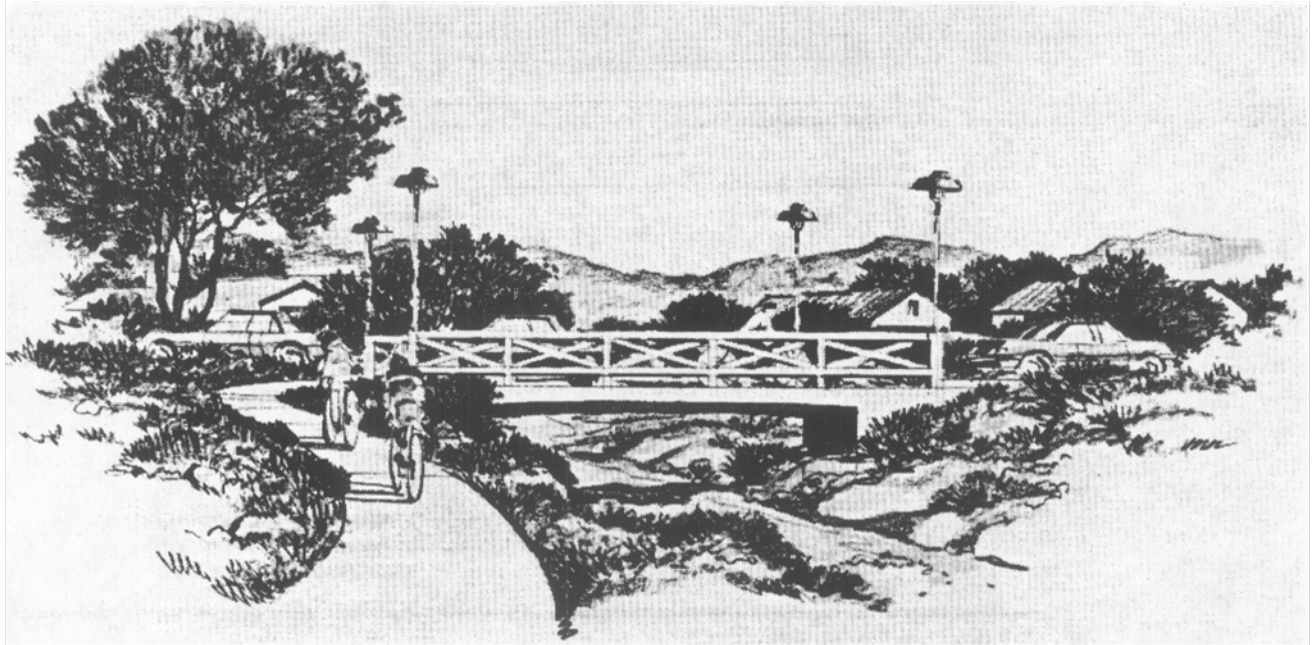
Collector Streetscape 2

The second collector streetscape is utilized on the more interior loop roads within the residential neighborhoods. It should be developed with the Informal Canopy Streetscape shown in **Figure 3.14-18, Collector Streetscape 2**.

Figure 3.14-18, Collector Streetscape 2



3.4.6 Paths and Trails



Paths and Trails

Project Trails

The Kohl Ranch shall have improved trails which will access the extensive open space and recreation areas of the site, and connect with the county trail system. The trails should be incorporated into the project drainage network, and should connect residential areas with schools, commercial areas, public facilities and parks. Improved trails dimensions and surface material should be governed by the County of Riverside standards for construction. Special landscape statements should be created for locations where trail segments intersect, and especially where they cross roadways. Boulders can be used to call attention to trail crossings and intersections, and can function as seats. A large shade tree also should be provided.

Bicycle Paths

Bicycle trails may be incorporated as a portion of the project roadways. Mountain bicycles should not be allowed off-road except on improved trails.

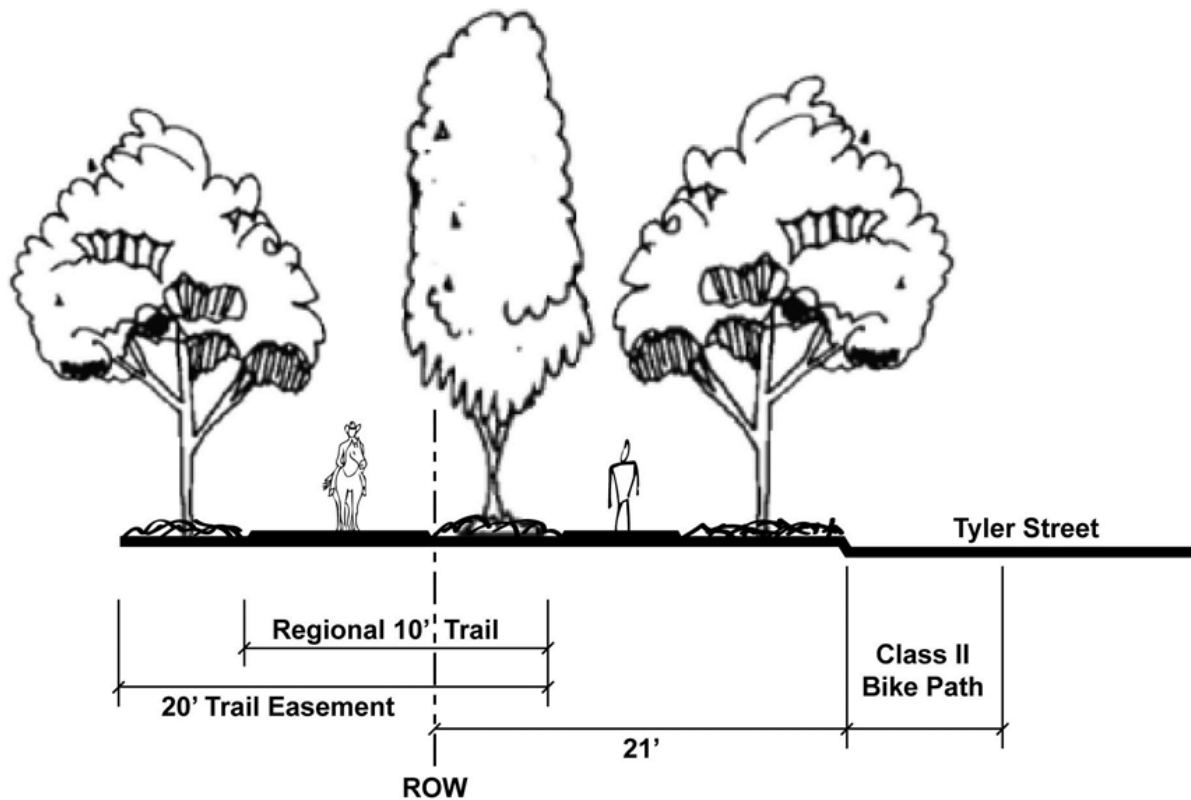
Tyler Street Trail

Tyler Street will contain both a Class II Bike Path and Regional Trail connection between Avenue 60 and Avenue 62 on the east side of Tyler Street. Regional trails are the main primary long distance trails within the County and are typically designed to provide linkages between communities, regional parks, and open space areas. Regional trails are designed to serve users needing soft trail surfaces, including equestrians, pedestrians, joggers, and mountain bikers and should consist of decomposed granite.

Class II Bike Paths provide a striped lane for one-way bike travel on a street or highway.

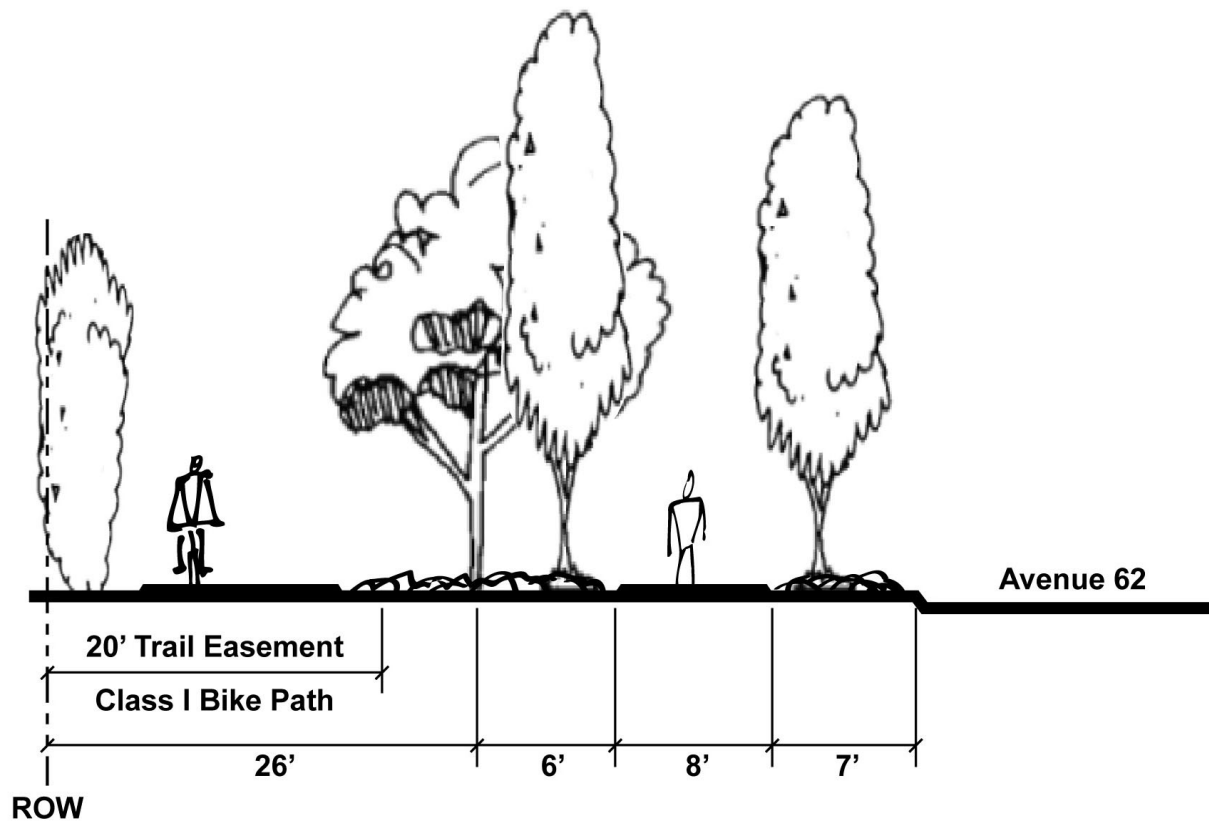
The Class II Bike Path along Tyler Street shall be a striped lane located within the right-of-way on pavement as illustrated in **Figure 3.4-19, Tyler Street Trail** or as agreed to by the Riverside County Regional Parks and Recreation District.

Figure 3.4-19, Tyler Street Trail



Avenue 62 Trail

Avenue 62 will provide for a Class I Bike Path on the north side of Avenue 62. Generally speaking, Class I Bike Paths are located in an easement outside the right-of-way. In the case of Avenue 62, however, the Class I Bike Path shall be located within the right-of-way as illustrated in **Figure 3.4-20, Avenue 62 Trail**.

Figure 3.4-20, Avenue 62 Trail

3.4.7 Edges

Landscape edges are the landscape treatment areas that create an envelope around roadways and development parcels, softening and buffering their harsh effects upon the landscape. These landscapes are very important in developing the community theme spilling over into residential and roadway landscape treatments. Resources of water and plant materials should be minimized in these areas with the intent of forming landscapes which require minimal maintenance. These areas additionally may be used as a continuation of adjacent passive recreation or open space areas.

Formal landscape treatment areas are intended to establish an organized, structured and even at times homogeneous landscape that is characteristic of more ordered forms. These treatment areas should appear lush and should contrast rather than blend with the natural desert landscape. The plant material should be used as a tool for space articulation, emphasizing topography and creating overt spatial relation statements. A grove should be visualized as the ultimate intent of treatment when designing these rural themed landscapes. Further, areas that are in direct contact with development areas may include plant materials which will foster a sense of security and privacy while maintaining views. Plant materials along these contact points may be slightly taller to screen and buffer the effects of rooflines and building facades.

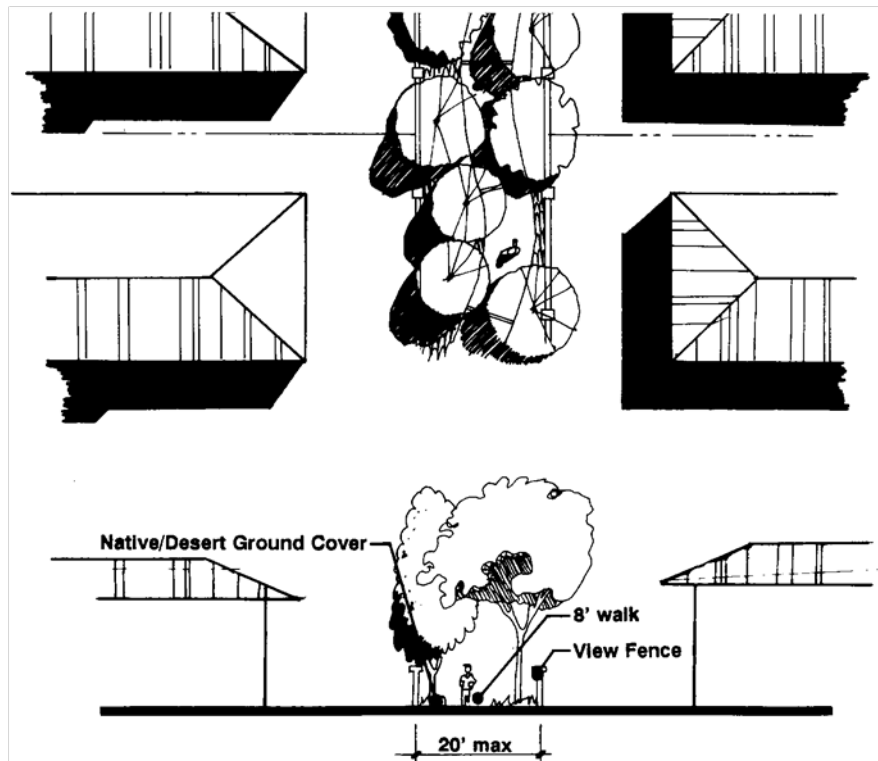
Natural landscapes composed of xeric (drought resistant) plant species are intended to blend with the existing landscape in color, form and texture. These landscapes occur along the project edges and drainage corridors, and will require a minimum of water resources. Only sporadic irrigation should be used to establish growth and little or no irrigation should be required after maturity. They should be organically arranged, reflecting the growth patterns of existing vegetation. Their primary intent is to screen and buffer residential developments.

- Landscape plantings are used as screens to block views, create microclimates, or as architectural space articulation elements. Plant materials should be thick and full, defining an edge of space or thick elongated masses. These screens may occur along the edges of development or within development parcels.
- Small structures can be easily screened with shrubbery, larger two-story structures can be buffered through the use of vertical trees. All plants should be chosen from the plant matrix. Care should be taken when selecting plants to consider their size, so that desirable views are not blocked.
- In certain cases, proper screening may be more easily achieved by building a wall or fence. Material selection must be compatible with the adjacent architecture. It is encouraged to soften walls and fences with vines and shrubs. Combinations of tree masses will be effective in screening non-desirable views from one area to another. Shrub masses can also help when placed correctly.

Windrow/Trail

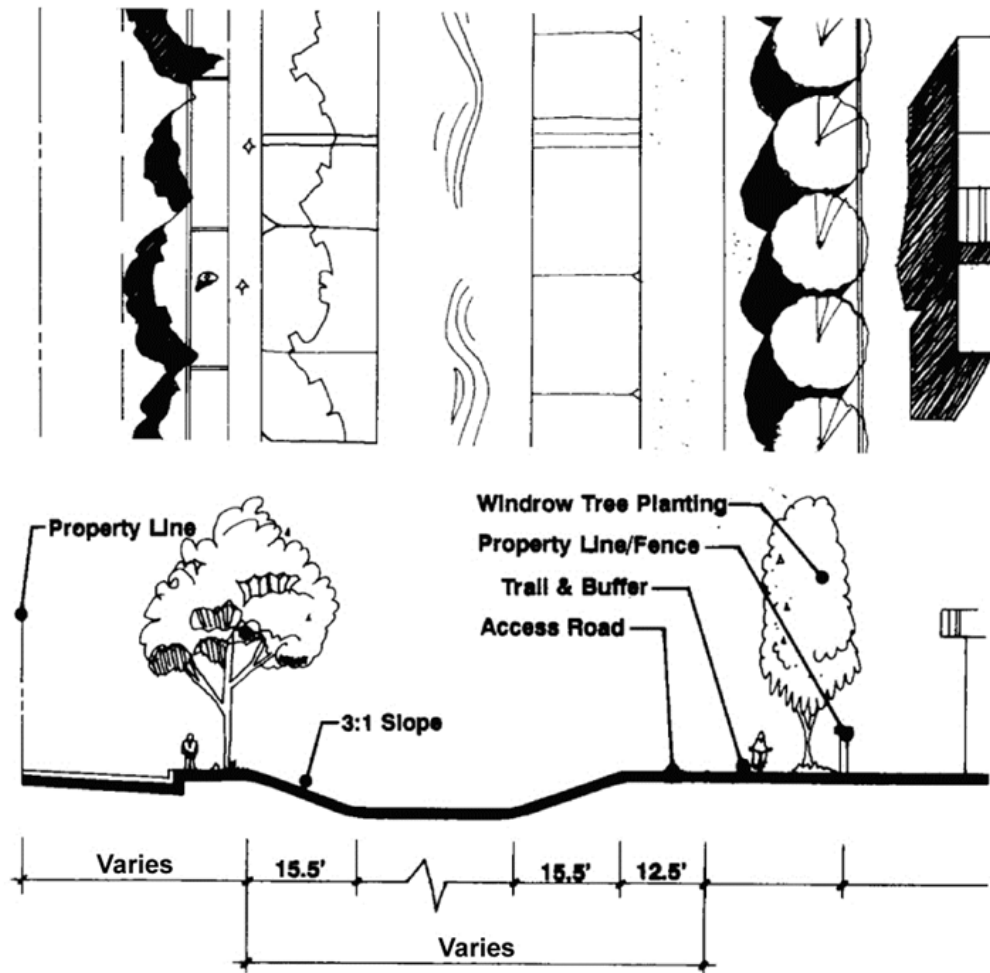
Windrows are used to define land use areas, provide a wind break, visually screen road and/or uses from one another, and to structure and enclose pedestrian trails throughout the residential neighborhoods. The Windrow/Trail Detail in **Figure 3.4-21, Windrow/Trail Detail** illustrates this landscape edge.

Figure 3.4-21, Windrow/Trail Detail



Drainage/Project Edge

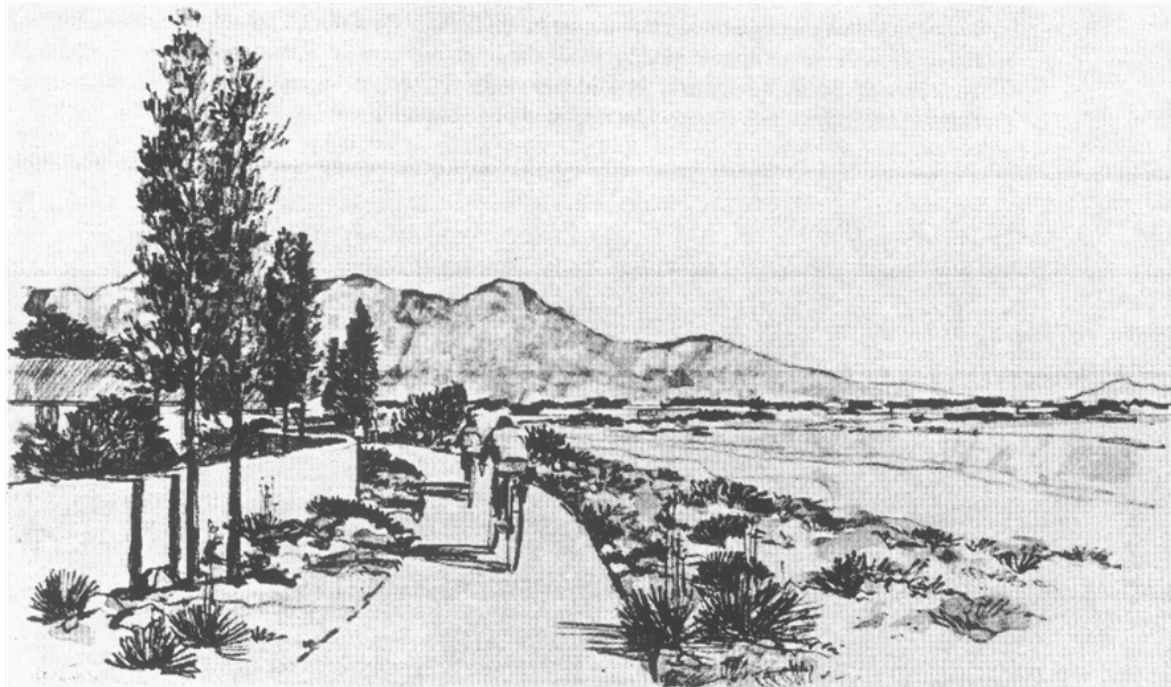
Where drainage facilities are proposed along the Kohl Ranch property boundary, adjacent to residential uses, a special edge treatment is needed. **Figure 3.4-22, Project Edge** illustrates this condition, and the use of windrow plantings to buffer the residences.

Figure 3.4-22, Project Edge

Note: Typical channel section - widths may vary.

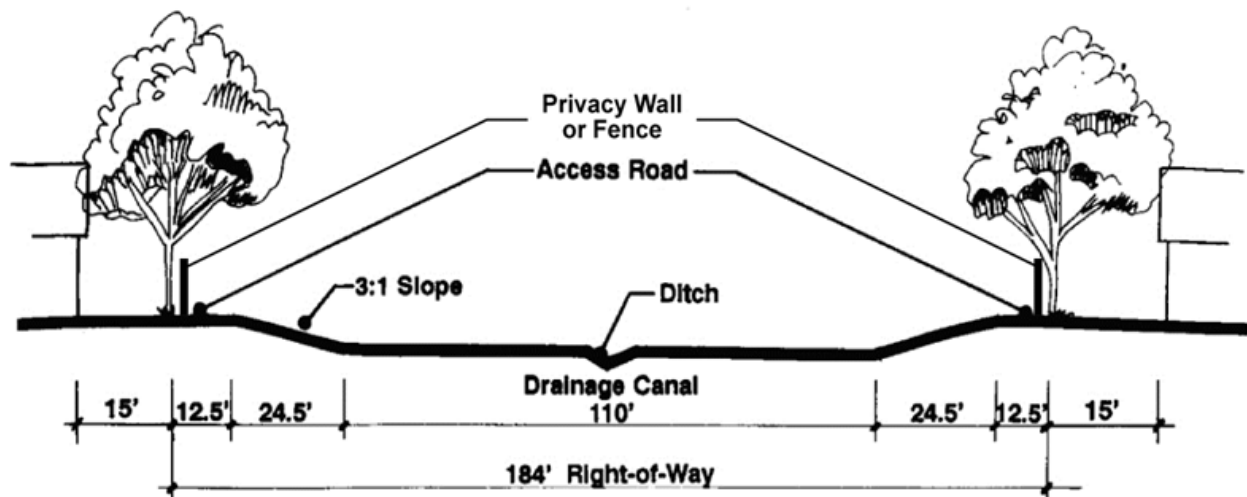
Drainage Canal Edge Conditions

In a number of instances, through the Kohl Ranch site, drainage facilities are integrated into residential neighborhoods, commercial areas, mixed use and the Mixed Use/Air Park center.

**Drainage Channel and Trail**

The edge condition created by these relationships is shown in **Figure 3.4-23, Drainage Canal Edge Condition**.

Figure 3.4-23, Drainage Canal Edge Condition



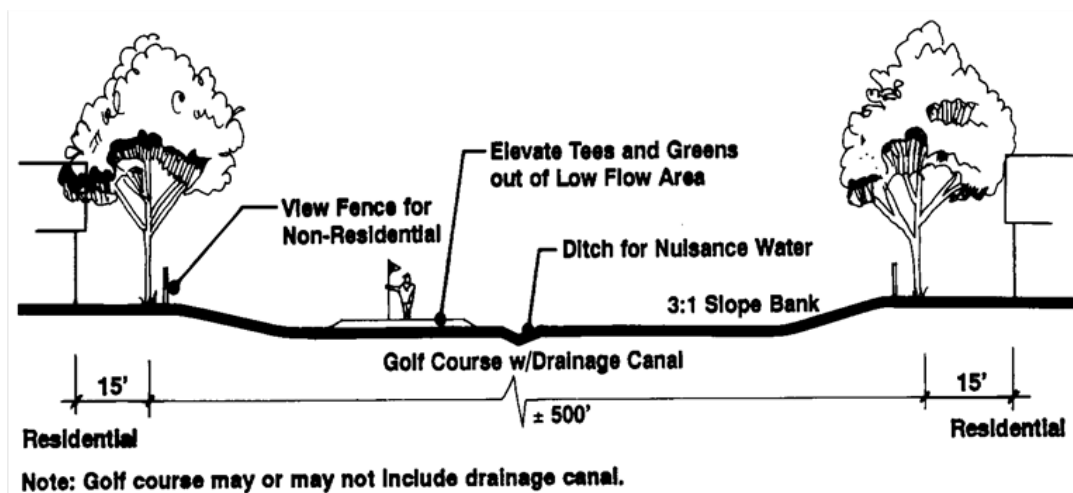
Major Recreation Use – Golf Course or Race Track

If a golf course alternative is developed, the approximately 500-foot section (widths may vary) should include an area of elevated trees and greens out of the low flow area, and a ditch for nuisance water. A view fence should be placed along the interface with non-residential uses.

**Golf Course Edge**

If a race track is developed, edges adjacent to roads and non-race track uses need to be buffered for both aesthetic purposes and to reduce noise from the race cars as depicted in Figure 3.4-24, Golf Course/Racetrack Edge Condition.

Figure 3.4-24, Golf Course/Racetrack Edge Condition



3.4.8 Buffers

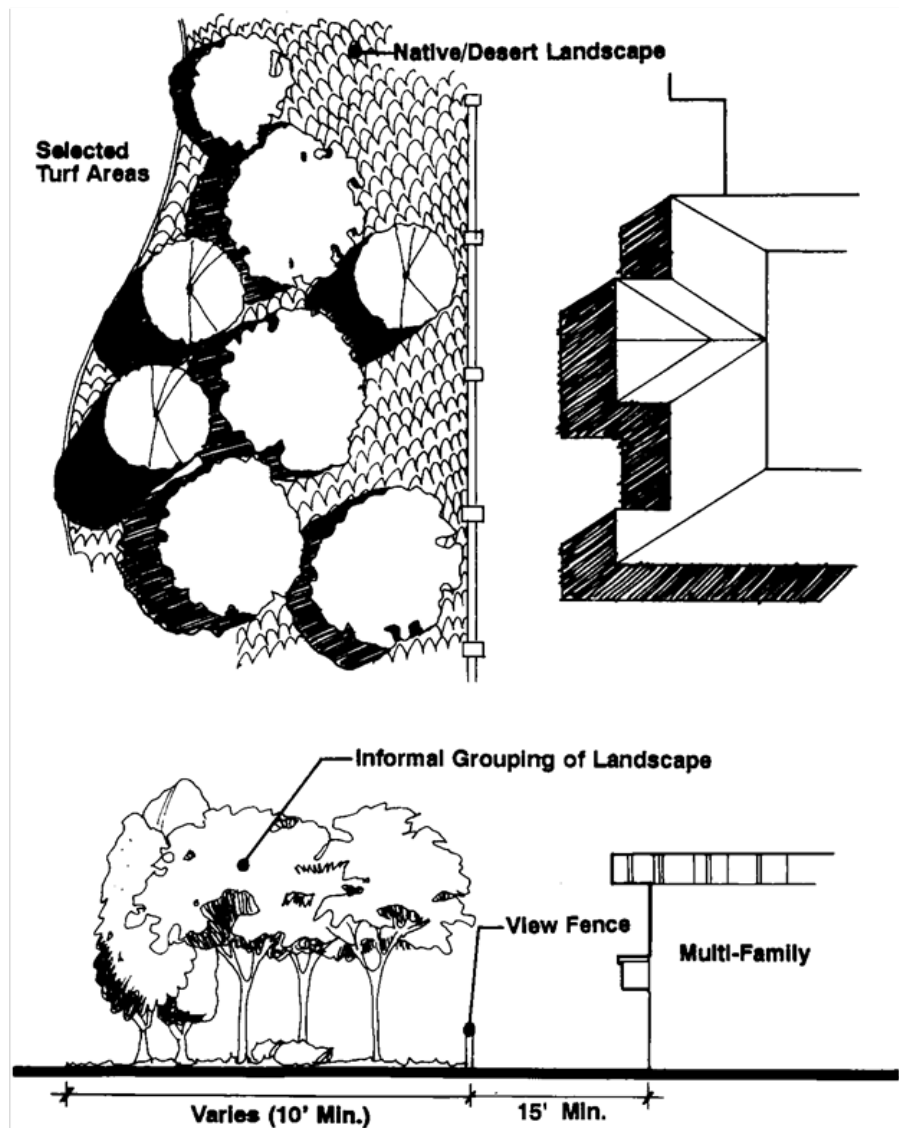
Landscape treatments are used to buffer adjacent land uses within the Kohl Ranch Specific Plan area. These landscape treatments should be a thoughtful blending of edges of plant materials from one treatment type to another. The transitions may be abrupt or a slow fading effect, depending on the desired effect.

- In order to provide privacy from one unit to another, it is encouraged that vertical tree masses be planted along side yards slopes where practical.
- Human use of natural open space areas will be accommodated by a limited number of trails and parks. These areas shall consist of native and xeric plant materials which shall be retained in their natural condition with no water, other than natural rainfall.
- Maintenance will consist of only occasional trail maintenance. This landscape will serve as a visual amenity for the community, and will also buffer adjacent uses outside the Specific Plan area.

If the race track is developed, buffers may include fences or walls, landscaping used to screen views, berms, buildings, a combination of the preceding, or other means as may be acceptable to the County and/or needed to reduce noise. Concepts for buffer edge conditions shall be submitted with plot plan/site plan for the race track use.

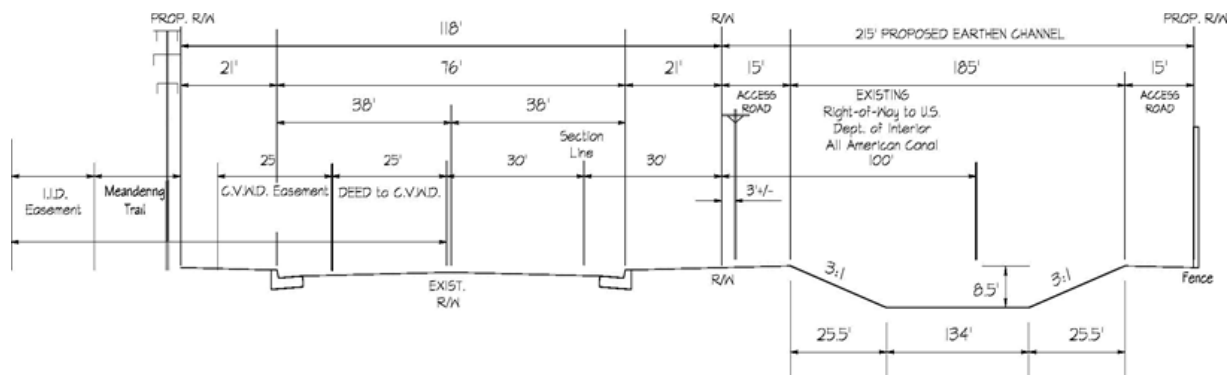
Park

Special landscape treatments are recommended where residential dwellings abut the high-use local parks. An informal landscape grouping at the park edge, with native/desert groundcover should be planted along the view fence, to ensure privacy and to aid in noise attenuation. A 15-foot minimum rear yard setback is recommended between the residential building and the view fence as depicted in **Figure 3.4-25, Park Buffers**.

Figure 3.4-25, Park Buffers

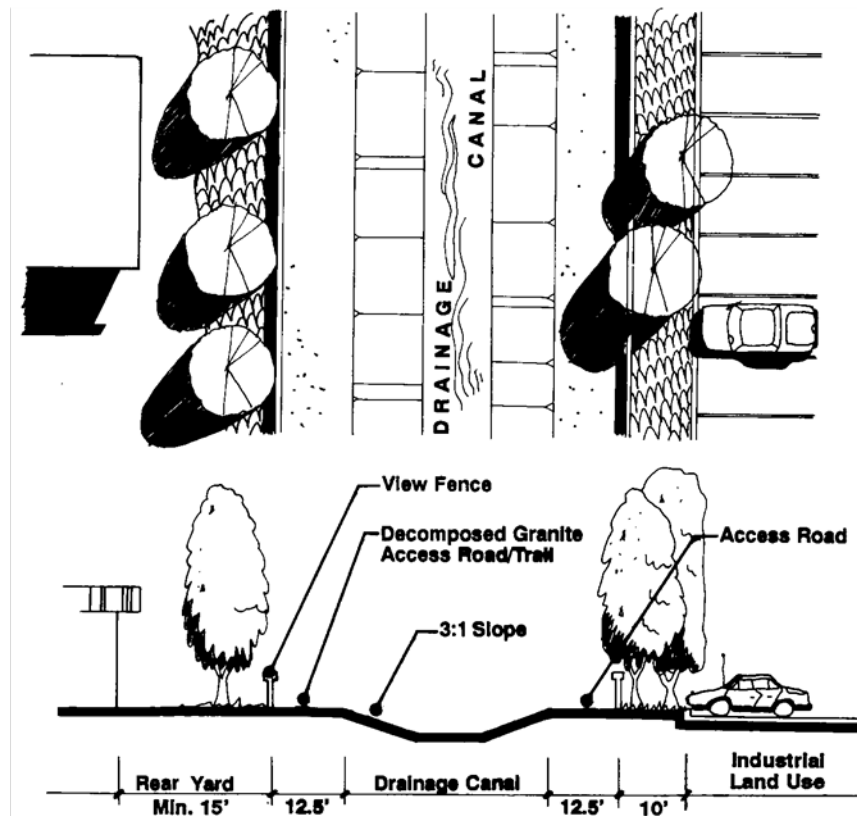
Evacuation Channel and Utility Easement Beltway

The Avenue 64 Evacuation Channel runs across the Kohl Ranch site in an east-west direction, between Tyler and Polk Streets. At its proposed final elevation, the channel should be lower than the adjacent land on either side of the channel, screening the concrete bottom from view as reflected in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**. The Evacuation Channel is within the utility easement beltway which also runs east-west through the site. This beltway contains easements for CVWD irrigation pipelines, the channel, and a power line easement for an existing 161 kV power line. This area will remain in open space and will incorporate a segment of the project-wide trail system. Residential land uses should be clustered to the north of the beltway, to preserve this area as a common open space amenity.

Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway

Drainage Channel

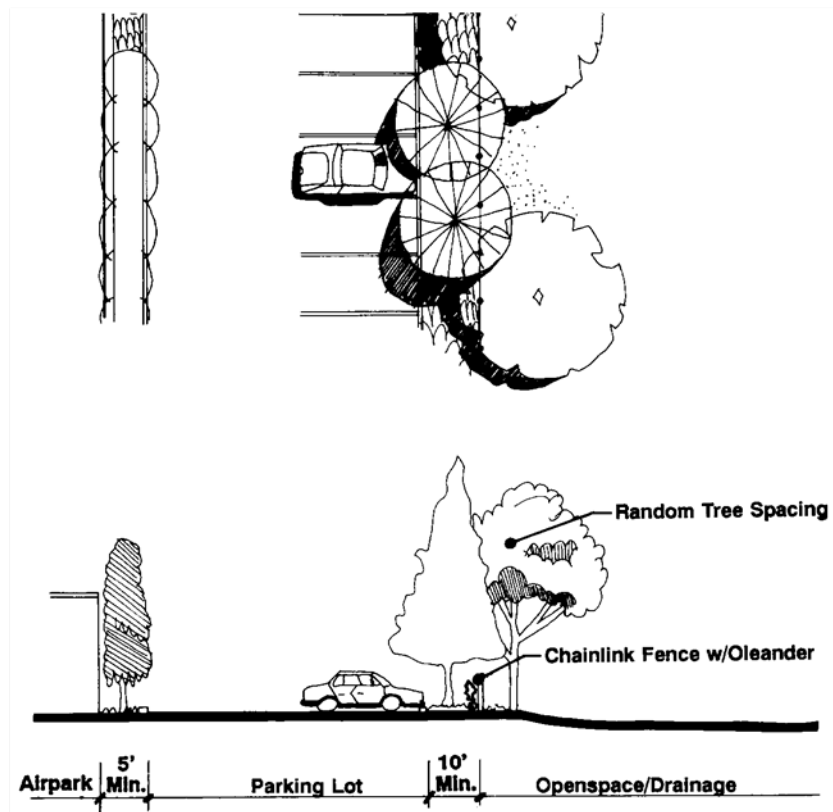
Drainage channels are located throughout the Kohl Ranch, to handle storm flows and to provide a linkage between park and open space amenities. The proposed buffers for these drainage facilities are indicated in **Figure 3.4-27 Channel Buffer Detail**. As indicated, informal plantings are recommended adjacent to the view fence to screen adjacent residential and non-residential land uses. Trails should be sited along the top of the 3:1 side slope and should consist of decomposed granite.

Figure 3.4-27, Channel Buffer Detail

Airpark/Drainage

The drainage channel(s) which pass through or adjacent to the airpark/mixed use development should receive special buffer treatment. A fence with oleander plantings should be provided along the rear parking lot of the airpark uses. Tree plantings with random spacing should be used to provide additional screening and to discourage unwanted use of intrusion into the drainage areas as reflected in **Figure 3.4-28, Airpark Drainage Buffer**.

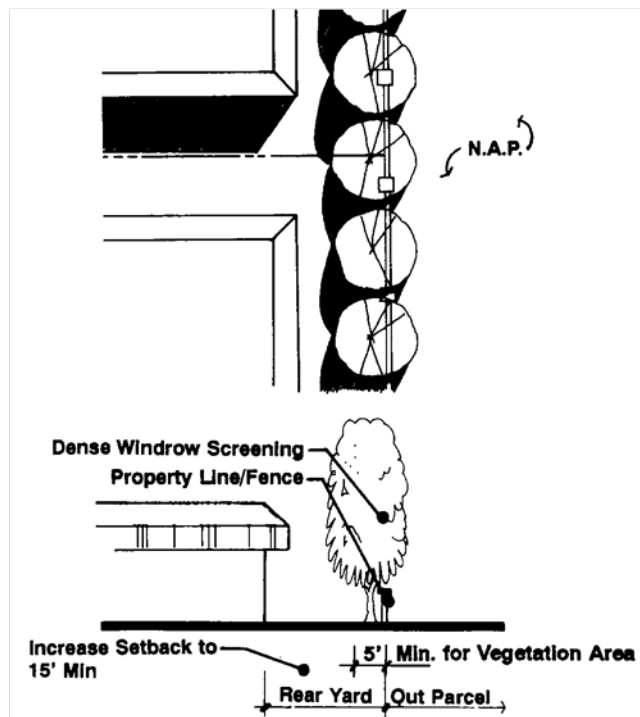
Figure 3.4-28, Airpark Drainage Buffer



Out Parcel/Adjacent Properties

Where proposed land uses are located adjacent to properties that are "Not a Part" of the Kohl Ranch Specific Plan area, buffers are recommended. Dense windrow plantings along the rear yard property line, along with fencing and an increased rear yard setback of 15 feet, should serve to minimize potential conflicts as reflected in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.

Figure 3.4-29, Out Parcel/Adjacent Property Buffer



Residential/Non-Residential

As part of the town center concept, residential uses are located near commercial areas to minimize vehicle trips and instill a sense of community. At the interface between residential and commercial land uses, buffers are needed to minimize potential adverse effects. As indicated in **Figure 3.4-30, Land Use Buffer**, a 15-foot backyard setback for the residential uses is recommended. In addition, a windrow screening tree mass should be planted along the rear property line, along an 8-foot solid wall, creating a 10-foot wide buffer area. A minimum 50-foot setback for the commercial property, inclusive of this buffer, should be maintained with parking; without parking, the buffer should be 25 feet.

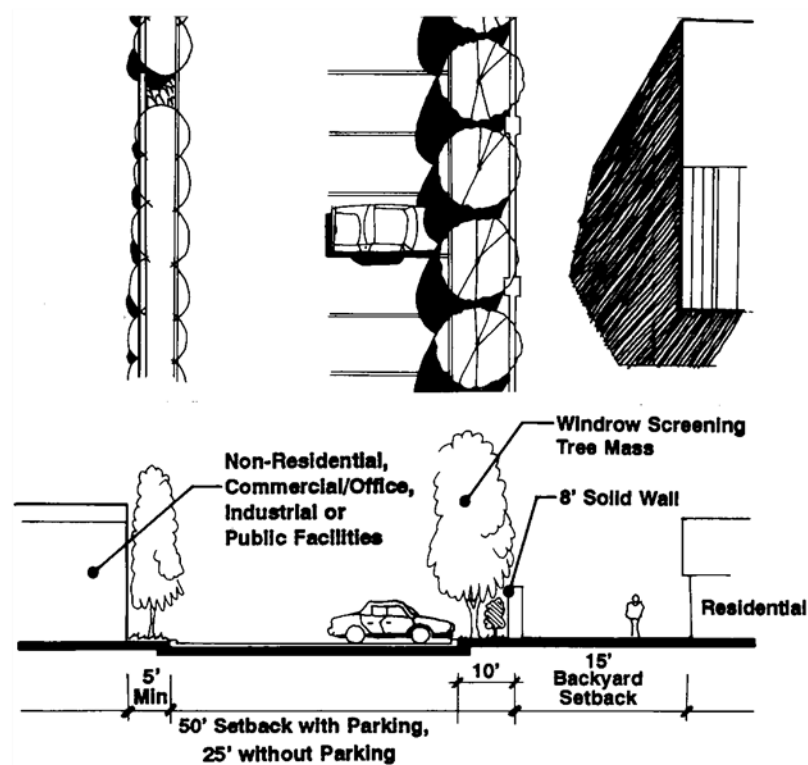
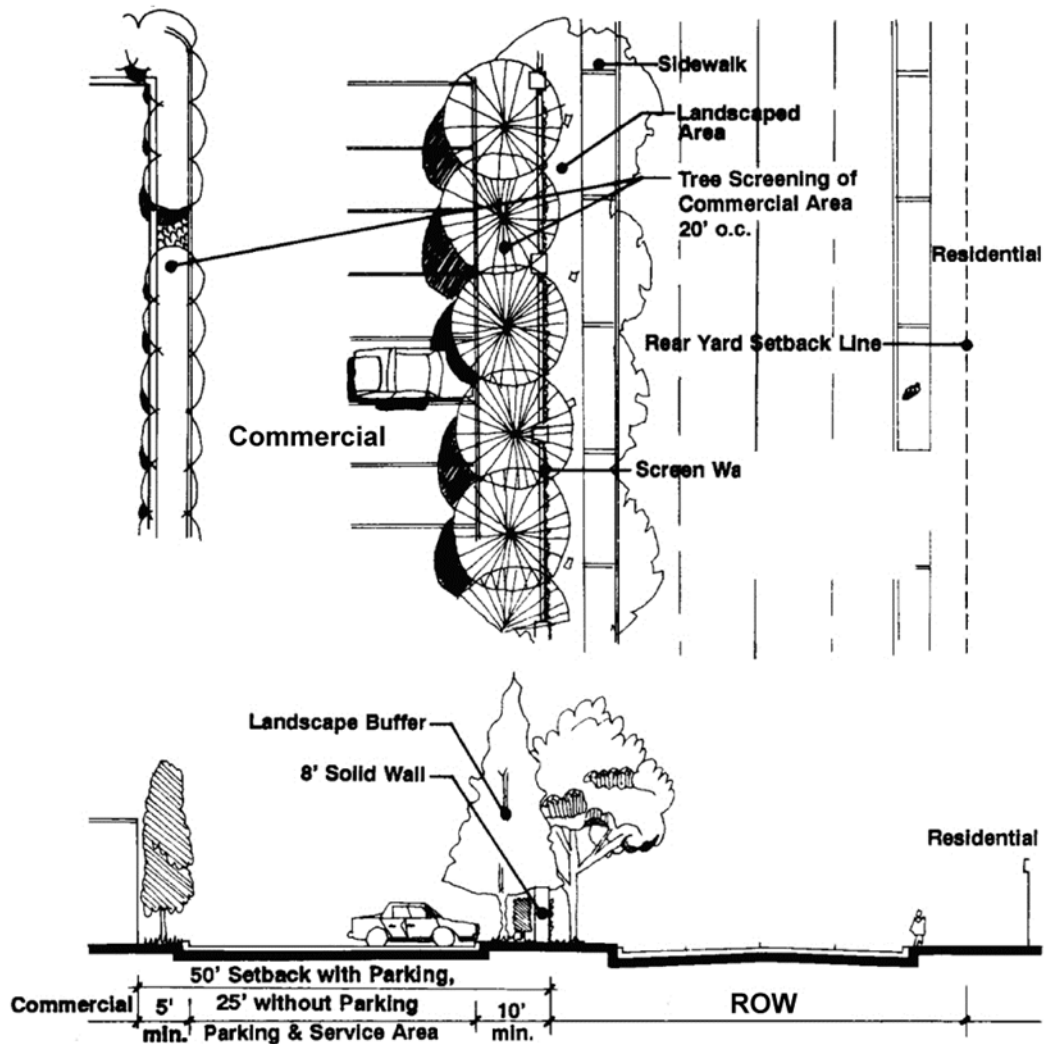
Figure 3.4-30, Land Use Buffer

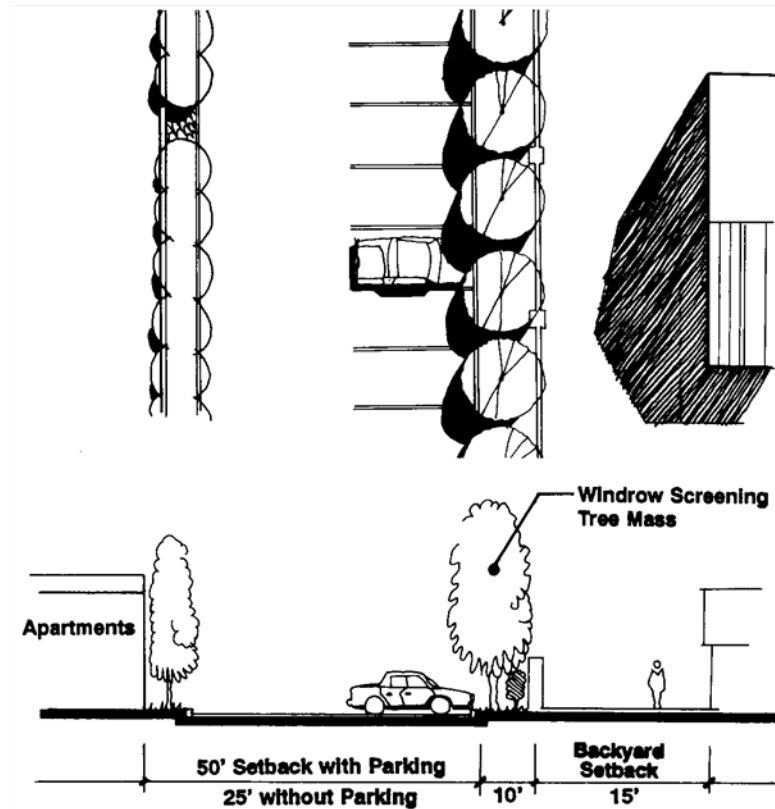
Figure 3.4-31, Commercial Buffer Streetscape depicts the scenario where commercial uses are located across the street from residential uses, as is the case along Tyler Street just north of Avenue 62 or on "D" Street. A landscape buffer should be planted adjacent to an 8-foot solid wall and the parking and service area for the commercial use, within the 50-foot setback. This will provide both a physical and visual separation between the commercial and residential uses.

Figure 3.4-31, Commercial Buffer Streetscape



Single-Family/Multi-Family Residential

Where single-family residential dwellings are located adjacent to multi-family residential units, a product type buffer should be implemented, consisting of windrow screening along the back side of the multi-family parking area, a community wall, and an increased backyard setback for the single-family uses as reflected in **Figure 3.4-32, Product Type Buffer**.

Figure 3.4-32, Product Type Buffer

3.5 RCIP GENERAL PLAN POLICY CONSISTENCY ANALYSIS

3.5.1 RCIP General Plan and Eastern Coachella Valley Area Plan Policy Consistency Analysis

Table 3.5-A, RCIP General Plan and ECVAP Policies Consistency Analysis, below, relates the proposed The Kohl Ranch Specific Plan No 303, Amendment 4 to relevant policies in the General Plan ("General Plan" or "RCIP"), as well as any relevant policies from the Eastern Coachella Valley Area Plan. Policies deemed not relevant to the project, based on proposed land uses, are not included in this table. Based on the brief analysis here and the overall information provided in The Kohl Ranch Specific Plan No. 303 Amendment 4, EIR 396 and Addendum (EA43068), a notation of **"C"** meaning generally consistent, or **"I"** meaning generally inconsistent, or **"N/A"** meaning not applicable, is given each policy to reflect the project's response to the intent of the General Plan or Area Plan policy.

Table 3.5-A, RCIP General Plan and ECVAP Policy Consistency Analysis

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Aesthetics	LU 14.1	Preserve and protect outstanding scenic vistas and visual features for the enjoyment of the traveling public.	The project area is relatively flat and because of the limited development on the site, views of the nearby mountains framing the valley floor are generally unobstructed. The Santa Rosa and San Jacinto Mountains are important visual resources in the Coachella Valley. Views of these resources have been preserved in the Kohl Ranch Specific Plan Design Guidelines. C
Aesthetics	LU 14.2	Incorporate riding, hiking, and bicycle trails and other compatible public recreational facilities within scenic corridors.	Approximately 379.28 acres of open space is included in The Kohl Ranch Specific Plan. Additionally, the Project includes a trail system consisting of local and regional connections to link parks, open space and community facilities. C
Aesthetics	LU 14.3	Ensure that the design and appearance of new landscaping, structures, equipment, signs, or grading within Designated and Eligible State and County scenic highway corridors are compatible with the surrounding scenic setting or environment.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	LU 14.4	Maintain at least a 50-foot setback from the edge of the right-of-way for new development adjacent to Designated and Eligible State and County Scenic Highways.	The proposed Project is not located within a scenic highway or corridor. The closest State-eligible scenic highway is State Route 111, from Bombay Beach on the Salton Sea to State Route 195 near Mecca located approximately 4.4 miles to the southeast. N/A
Aesthetics	LU 14.5	Require new or relocated electric or communication distribution lines, which would be visible from Designated and Eligible State and County Scenic Highways, to be placed underground.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	LU 14.6	Prohibit offsite outdoor advertising displays that are visible from Designated and Eligible State and County Scenic Highways.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	LU 14.7	Require that the size, height, and type of on-premise signs visible from Designated and Eligible State and County Scenic Highways be the minimum necessary for identification. The design, materials, color, and location of the signs shall blend with the environment, utilizing natural materials where possible.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	LU 14.8	Avoid the blocking of public views by solid walls.	The Design Guidelines of the proposed project requires the use of view fencing along open space, golf courses, parks and schools to avoid blocking of public views. C
Aesthetics	ECVAP 4.1	Require the inclusion of outdoor lighting features that would minimize the effects on the nighttime sky and wildlife habitat areas.	The Design Guidelines of the proposed project require lighting to be designed to minimize sky glow and the nighttime desert sky. The aesthetics section of this EIR and the Aesthetics section of the Addendum evaluated potential impacts and determined that all potential significant adverse impacts

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
			associated with the proposed project were found to be less than significant without mitigation. In addition, conditions recommended by the Airport Land use Commission also limit lighting within the area due to its proximity to the airport so the effects of nighttime lighting will be minimized. C
Aesthetics	ECVAP 4.2	Adhere to the County's lighting requirements for standards that are intended to limit light leakage and spillage that may interfere with the operations of the Palomar Observatory.	The Kohl Ranch Specific Plan is located within Zone B of the Mt. Palomar Nighttime Lighting Policy Area. Mitigation measures herein reduce potential significant impacts to less than significant. Future development proposals will be required to comply with County Ordinance No. 655 and will be reviewed at such time as development application is files with the Planning Department. C
Aesthetics	ECVAP 10.1	Adhere to the Advertising Regulations of the County Land Use Ordinance, Section 19, regarding outdoor advertising for all development within the Eastern Coachella Valley.	Section 19 does not allow outdoor advertising within land zoned Specific Plan (SP). C
Aesthetics	ECVAP 10.2	Prohibit the placement of billboards within the Eastern Coachella Valley.	The zoning ordinance prohibits outdoor advertising displays. C
Aesthetics	ECVAP 10.3a	For premises adjacent to the right-of-way of scenic corridors, single support free-standing signs for on-site advertising shall be prohibited. A sign affixed to buildings, a free-standing monument sign, or a free-standing sheathed-support sign which has minimal impact on the scenic setting shall be utilized for on-site advertising purposes along the below-referenced scenic corridors. a. For purposes of this policy, scenic corridors include: <ul style="list-style-type: none">• State Route 86; and• State Route 111.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	ECVAP 10.3c	On-site advertising signs for businesses located along freeway scenic corridors shall comply with the following:(1) Businesses located within 660 feet of the terminus of a freeway exit ramp or the origination of a freeway entrance ramp may utilize either monument or sheathed-support signs in addition to signs affixed to buildings.i. A free-standing monument sign for a single business or tenant may be approved with a maximum height of 10 feet and a maximum surface area of 150 square feet. A free-standing monument sign for multiple businesses or tenants may be approved with an overall height of 12 feet or less and a maximum surface area of 200 square feet.ii. A free-standing sheathed-support sign for a single business or tenant may be approved with a maximum height of 35 feet and a maximum surface area of 150 square feet. A free-standing sheathed-support sign for multiple businesses or tenants may be approved with a maximum height of 35 feet. The maximum surface area shall be the greater of either 150 square feet, or .25 percent (1/4 of 1 percent) of the total existing building floor area, except that in any event, no sign shall exceed 200 square feet in surface area.iii. A sign affixed to a building, advertising the business contained therein, shall not exceed 10 percent of the surface area of the building wall facing the freeway. A single sign, or a total of all signs, affixed to a building and advertising multiple businesses contained therein shall not exceed 10 percent of the surface area of the building wall facing the freeway.(2) Businesses located within 330 feet of the nearest edge of a freeway right-of-way line, but farther than 660 feet from the terminus of a freeway exit ramp or the origination of a freeway entrance ramp, may utilize either monument or sheathed-support signs in addition to signs affixed to buildings.i. A free-standing monument sign for a single business or tenant may be approved with a maximum height of 10 feet and a maximum surface area of 150 square feet. A free-standing monument sign for multiple businesses or tenants may be approved with an overall height of 12 feet or less and a maximum surface area of 200 square feet.ii. A free-standing sheathed-support sign for a single business or tenant may be approved with a maximum height of 25 feet, or the actual height of the primary building advertised, whichever is less, and a maximum surface area of 150 square feet. A free-standing sheathed-support sign for multiple businesses or tenants may be approved with an overall height of 25 feet, or the actual height of the primary building	The Project is not located within a scenic highway or corridor. N/A

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
		advertised, whichever is less. The maximum surface area shall be the greater of either 150 square feet, or .25 percent (1/4 of 1 percent) of the total existing building floor area, except that in any event, no sign shall exceed 200 square feet in surface area.iii. A sign affixed to a building, advertising a single business contained therein, shall not exceed 10 percent of the surface area of the building wall facing the freeway. A single sign, or a total of all signs, affixed to a building and advertising multiple businesses contained therein shall not exceed 10 percent of the surface area of the building wall facing the freeway.	
Aesthetics	ECVAP 10.3d	On-site advertising signs for businesses located along highway scenic corridors shall comply with the following:(1) Notwithstanding the other provisions of this policy, a single-business monument sign may be approved with a maximum height of 10 feet, and a maximum 150 square feet of sign surface area. A multiple-business monument sign may be approved with a maximum height of 12 feet or less, and a maximum 200 square feet of sign surface area.(2) Notwithstanding the other provisions of this policy, a single-business sheathed-support sign or a multiple-business sheathed-support sign shall not be erected along a highway scenic corridor.(3) A sign affixed to a building, advertising the business contained therein, shall not exceed 10 percent of the surface area of the building wall facing the highway. A single sign, or a total of all signs, affixed to a building and advertising multiple businesses contained therein shall not exceed 10 percent of the surface area of the building wall facing the highway.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	ECVAP 15.1	Protect the scenic highways in the Eastern Coachella Valley from change that would diminish the aesthetic value of adjacent properties in accordance with the Scenic Corridors section of the General Plan Land Use, Multipurpose Open Space, and Circulation Elements.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	ECVAP 17.1	Refer to the Ridgeline policies in the Hillside Development and Slope section of the General Plan Land Use Element and the Scenic Resources policies in the General Plan Multipurpose Open Space Element.	The ridgeline of the Santa Rosa mountains along the western edge of the Eastern Coachella Valley is an important natural resource. The project area is relatively flat and because of the limited development on the site and requirements in the Kohl Ranch Specific Plan Design Guidelines, views of the nearby mountains framing the valley floor are generally unobstructed. C
Aesthetics	OS 21.1	Identify and conserve the skylines, view corridors, and outstanding scenic vistas within Riverside County.	The project area is relatively flat and because of the limited development on the site, views of the nearby mountains framing the valley floor are generally unobstructed. The Santa Rosa and San Jacinto Mountains are important visual resources in the Coachella Valley. Views of these resources have been preserved in the Kohl Ranch Specific Plan Design Guidelines. C
Aesthetics	OS 22.1	Design developments within designated scenic highway corridors to balance objectives of maintaining scenic resources with accommodating compatible land uses.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	OS 22.3	Encourage joint efforts among federal, state, and County agencies, and citizen groups to ensure compatible development with scenic corridors.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	OS 22.4	Impose conditions on development within scenic highway corridors requiring dedication of scenic easements consistent with the Scenic Highways Plan, when it is necessary to preserve unique or special visual features.	The Project is not located within a scenic highway or corridor. N/A
Aesthetics	OS 22.5	Utilize contour grading and slope rounding to gradually transition graded road slopes into a natural configuration consistent with the topography of the areas within scenic highway corridors.	The Project is not located within a scenic highway or corridor. N/A
Agriculture	ECVAP 5.1	Retain and protect agricultural lands through adherence to the policies contained in the Agriculture section of the General Plan Land Use Element.	Section V.C.2 of the EIR and the Agricultural Resources section of the Addendum incorporates mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
			impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. The Kohl Ranch Specific Plan does not include any agriculturally designated land and no changes are proposed that would affect other areas of agricultural use. N/A
Agriculture	ECVAP 5.2	Refer to the General Plan Certainty System in the General Plan Administrative Element. An exception is provided allowing limited changes from the Agriculture designation to be processed and approved.	There are no agriculturally designated lands in the Kohl Ranch Specific Plan Amendment No. 4. N/A
Agriculture	ECVAP 6.1	Allow farmworker housing that meets basic safety standards in agriculturally designated areas per the land use designations section of the General Plan Land Use Element, and the Five-Year Action Plan and Special Housing Need sections of the Housing Element. Provided that adequate provisions for public services and compatibility with adjacent uses is achieved, farm worker housing projects of both 1-12 dwelling units and greater than 12 units are permitted in the Agriculture designation in the Eastern Coachella Valley Area Plan.	There are no agriculturally designated lands in the Kohl Ranch Specific Plan Amendment No. 4. N/A
Agriculture	LU 20.1	Encourage retaining agriculturally designated lands where agricultural activity can be sustained at an operational scale, where it accommodates lifestyle choice, and in locations where impacts to and from potentially incompatible uses, such as residential uses, are minimized, through incentives such as tax credits.	The Eastern Coachella Valley Area Plan contains 41,403 acres of AG designated land which is 9.1 percent of all the land comprising this Area Plan. Within Riverside County, only the San Jacinto Valley Area Plan designates a greater percentage of land to AG with 9.4 percent. The Kohl Ranch Specific Plan would not retain the Agriculture (AG) designated lands within its project area. Section V.C.2 of the EIR and the Agricultural Resource section of the Addendum analyzed impacts and determined these to be significant and unavoidable. A statement of overriding considerations was adopted with certification of the EIR November 16, 1999. A General Plan Amendment was approved to change land use to that of an adopted specific plan and a zone change changed the zoning to Specific Plan (SP). This policy is therefore, not applicable to the Kohl Ranch Specific Plan Amendment No. 4 with the exception of minimizing impacts to agriculturally designated lands from incompatible uses. EIR section V.C.2 incorporates mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. N/A, C
Agriculture	LU 20.2	Protect agricultural uses, including those with industrial characteristics (dairies, poultry, hog farms, etc) by discouraging inappropriate land division in the immediate proximity and allowing only land uses and intensities that are compatible with agricultural uses.	Section V.C.2 of the EIR and the Agricultural Resources section of the Addendum incorporate mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. No such offensive agricultural uses exist in proximity to the Kohl Ranch. C
Air Quality	LU 11.1	Provide sufficient commercial and industrial development opportunities in order to increase local employment levels and thereby minimize long-distance commuting (AI 18)	The Land Use Designations in the area north of Avenue 62 and east of Tyler Street is intended for industrial and airport-related business park development. Additional land use designations along both sides of Avenue 62 are intended for commercial development. These uses will increase the available employment opportunities and help minimize long-distance commuting. Bus stops and coordination with the transit agency for increased routes will also provide alternatives to long-distance commutes. C
Air Quality	LU 11.2	Ensure adequate separation between pollution producing activities and sensitive emission receptors, such as hospitals, residences, and schools.	EIR section V.C.6 and the Air Quality section of the Addendum incorporate mitigation measures to reduce impacts to sensitive receptors. C
Air Quality	LU 11.3	Accommodate the development of community centers and concentrations of development to reduce reliance on the automobile and help improve air quality.	The Kohl Ranch Specific Plan creates its own master-planned community consisting of medium density, medium high density, high density, and very high density residential types, three existing schools, commercial, industrial, parks and open space that collectively may contain the attributes that contribute to a community center. EIR section V.D.1 and the Air Quality section of the Addendum incorporates mitigation to provide for potential bus transit as well as the opportunity for Park and Ride facilities. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will

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			promote non-vehicular access to on-site recreational areas as well as to the schools thus reducing the reliance on automobiles. C
Air Quality	LU 11.4	Provide options to the automobile in communities, such as transit, bicycle and pedestrian trails, to help improve air quality.	EIR section V.D.1 and the Air Quality section of the Addendum incorporates mitigation to provide for potential bus transit as well as the opportunity for Park and Ride facilities. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. C
Air Quality	AQ 4.7	To the greatest extent possible, require every project to mitigate any of its anticipated emissions that exceed allowable emissions as established by the SCAQMD, MDAQMD, SCAB, the Environmental Protection Agency, and the California Air Resources Board.	The project is located within the jurisdiction of the South Coast Air Quality Management District (SCAQMD). The air quality section of the EIR and the Air Quality section of the Addendum requires that the project comply with the requirements set forth by the SCAQMD, the Environmental Protection Agency, and the California Air Resources Board and propose to mitigate, to the greatest extent possible, project-related emissions. C
Air Quality	AQ 8.2	Emphasize job creation and reductions in vehicle miles traveled in job-poor areas to improve air quality over other less efficient methods.	The proposed project includes a commercial and industrial uses only along Avenue 62 and east of Tyler Street which will create jobs and concentrate growth around the center which will help reduce vehicle miles traveled. C
Air Quality	AQ 8.4	Support new mixed-use land use patterns and community centers which encourage community self-sufficiency and containment, and discourage automobile dependency.	EIR section V.D.1 and the Air Quality section of the Addendum incorporates mitigation to provide for potential bus transit as well as the opportunity for Park and Ride facilities. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. C
Biology	ECVAP 16.1	Protect visual and biological resources in the Eastern Coachella Valley Area Plan through adherence to General Plan policies found in the Preservation section of the Multipurpose Open Space Element, as well as policies contained in the Coachella Valley Multiple Species Habitat Conservation Plan..	The proposed Project site has been extensively disturbed from agricultural activities. Section V.C.3 of this EIR and the Biological Resource section of the Addendum analyzed potential impacts to biological resources. The EIR incorporates a mitigation measure within the Biology section, which requires a Pre-Construction Survey for burrowing owls. No other impacts were found to be significant. Additionally, the Addendum incorporates a mitigation measure to account for the Coachella Valley Multiple Species Habitat Conservation Plan requiring the payment of fees prior to grading. C
Cultural	OS 19.1	Cultural resources (both prehistoric and historic) are a valued part of the history of the County of Riverside..	Section V.C.12 of this EIR and the Cultural Resource section of the Addendum analyzed potential impacts to archeological resources. The EIR incorporates mitigation measures within the cultural resources section that reduce impacts to archeological sites and resources. C
Cultural	OS 19.2	The County of Riverside shall establish a Cultural Resources Program in consultation with Tribes and the professional cultural resources consulting community that , at a minimum would address each of the following: application of the Cultural Resources Program to projects subject to environmental review; government-to-government consultation; application processing requirements; information database(s); confidentiality of site locations; content and review of technical studies; professional consultant qualifications and requirements; site monitoring; examples of preservation and mitigation techniques and methods; curation and the descendant community consultation requirements of local, state and federal law. (AI 144).	Section V.C.12 of this EIR and the Cultural Resource section of the Addendum analyzed potential impacts to archeological resources but the Project is not responsible for establishing a Cultural Resource program with the Tribes. N/A
Cultural	OS 19.3	Review proposed development for the possibility of cultural resources and for compliance with the cultural resources program.	The Kohl Ranch Specific Plan Amendment No. 4 is required to comply with Senate Bill 18. C

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Cultural	OS 19.4	To the extent feasible, designate as open space and allocate resources and/or tax credits to prioritize the protection of cultural resources preserved in place or left in an undisturbed state. (AI 145).	EIR section V.C.12 and the Cultural Resources section of the Addendum incorporate mitigation measures to reduce impacts to historical sites and resources including discovered resources. The proposed project has been reviewed by the Regional Park and Open Space District. C
Cultural	OS 19.5	Exercise sensitivity and respect for human remains from both prehistoric and historic time periods and comply with all applicable laws concerning such remains.	Section V.C.12 of this EIR and the Cultural Resource section of the Addendum analyzed potential impacts to human remains. In the event that unknown human remains are uncovered during construction activities, the Project will be in compliance with Sections 7052 and 7050.5 of the California Health and Safety Code (HSC) requiring that the Riverside County Coroner's Office must be contacted within 24 hours and all work shall be halted until a clearance is given by that office and any other involved agencies and with the requirements of Public Resources Code Section 5097.98, as amended. C
Cultural	OS 19.6	Whenever existing information indicates that a site proposed for development has high paleontological sensitivity as shown on Figure OS-8, a paleontological resource impact mitigation program (PRIMP) shall be filed with the County Geologist prior to site grading. The PRIMP shall specify the steps to be taken to mitigate impacts to paleontological resources.	Section V.C.12 of the EIR and the Cultural Resource section of the Addendum includes mitigation measures which call for paleontological monitoring within areas identified as potentially significant and includes measures should inadvertent discoveries be made. C
Cultural	OS 19.7	Whenever existing information indicates that a site proposed for development has low paleontological sensitivity as shown on Figure OS-8, no direct mitigation is required unless a fossil is encountered during site development. Should a fossil be encountered, the County Geologist shall be notified and a paleontologist shall be retained by the project proponent. The paleontologist shall document the extent and potential significance of the paleontological resources on the site and establish appropriate mitigation measures for further site development.	Cultural and paleontological assessments have been performed for this project and are discussed in Section V.C.12 of this EIR and the Cultural Resource section of the Addendum which includes mitigation measures to lessen potential impacts. C
Cultural	OS 19.8	Whenever existing information indicates that a site proposed for development has undetermined paleontological sensitivity as shown on Figure OS-8, a report shall be filed with the County Geologist documenting the extent and potential significance of the paleontological resources on site and identifying mitigation measures for the fossil and for impacts to significant paleontological resources prior to approval of that department.	Section V.C.12 of the EIR and the Cultural Resource section of the Addendum includes mitigation measures which call for paleontological monitoring within areas identified as potentially significant and includes measures should inadvertent discoveries be made. C
Cultural	OS 19.9	Whenever paleontological resources are found, the County Geologist shall direct them to a facility within Riverside County for their curation, including the Western Science Center in the City of Hemet.	Section V.C.12 of the EIR and the Cultural Resource section of the Addendum includes mitigation measures which call for paleontological monitoring within areas identified as potentially significant and includes measures should inadvertent discoveries be made. C
Geology & Soils	ECVAP 18.1	Protect life and property from wildfire hazards through adherence to the Fire Hazards section of the General Plan Safety Element	The proposed Project is located in an area considered to be at very low susceptibility for wildfire. N/A
Geology & Soils	ECVAP 20.1	Protect life and property from seismic-related incidents through adherence to the Seismic Hazards section of the General Plan Safety Elements.	Section V.C.4 of the EIR and the Geology and Soils section of the Addendum analyzed potential impacts from seismic-related incidents and the proposed Project site has the potential to be subject to strong seismic ground shaking events. The EIR incorporates mitigation measures within the geology and seismicity section to reduce impacts by requiring that structures be constructed per the California Building Code. C
Geology & Soils	ECVAP 21.1	Protect life and property through adherence to the Hillside Development and Slope section of the General Plan Land Use Element and the Slope and Soil Instability Hazards section of the General Plan Safety Element.	The project is not in an area of steep slope. Section V.C.1 of the EIR and the Geology and Soils section of the Addendum analyzed potential impacts from slope and instability hazards. The EIR incorporates mitigation measures within the land form and topography section to reduce impacts by requiring that grading activities be in conformance with the California Building Code and Riverside County Ordinance 457. C

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Geology & Soils	ECVAP 21.2	Refer to the Rural Mountainous and Open Space-Rural land use designations in the General Plan Land Use Element.	The project site does not contain these land use designations. N/A
Geology & Soils	ECVAP 22.1	Minimize damage from and exposure to wind erosion and blowsand through adherence to the Slope and Soil Instability Hazards section of the General Plan Safety Element.	Implementing projects within the proposed Specific Plan will be required to comply with Riverside County Ordinance 742. C
Geology & Soils	ECVAP 22.2	Require protection of soil in areas subject to wind erosion or blowsand. Mitigation measures that may be required include, but are not limited to, windbreaks, walls, fences, vegetative groundcover, rock, other stabilizing materials, and installation of an irrigation system or provision of other means of irrigation.	Implementing projects within the proposed Specific Plan will be required to comply with Riverside County Ordinance 742. Additionally, the Specific Plan landscape guidelines incorporate windrow plantings to provide wind breaks. C
Geology & Soils	ECVAP 22.3	Control dust through the policies of the Particulate Matter section of the General Plan Air Quality Element.	EIR section V.C.6 and the Geology and Soils section of the Addendum incorporates mitigation to control fugitive dust and particulate matter. C
Geology & Soils	ECVAP 22.4	Preserve the environmentally sensitive alluvial fan areas flowing out of the canyons of the Santa Rosa Mountains.	EIR sections V.C.1 and V.C.12 and the Geology and Soils section of the Addendum incorporate mitigation measures to ensure protection of alluvial fan areas. C
Geology & Soils	S 2.5	Require that engineered slopes be designed to resist seismically induced failure. For lower-risk projects, slope design could be based on pseudo-static stability analyses using soil engineering parameters that are established on a site-specific basis. For higher-risk projects, the stability analyses should factor in the intensity of expected ground shaking, using a Newmark-type deformation analysis.	This project will comply with Ordinance 457, which requires that manufactured slopes are designed to be seismically safe. Additionally, Section V.C.1 of the EIR and the Geology and Soils section of the Addendum includes mitigation measures which call for grading activities to conform with the California Building Code and Ordinance 457. C
Geology & Soils	S 3.5	During permit review, identify and encourage mitigation of on-site and off-site slope instability, debris flow, and erosion hazards on lots undergoing substantial improvements.	During the review of implementing applications (residential subdivisions, use permits, etc.) of The Kohl Ranch Specific Plan, these geological issues will be analyzed and addressed on an as-needed basis. C
Geology & Soils	LU 12.1c	Require that areas with slope be developed in a manner to minimize the hazards from erosion and slope failures.	The project area is relatively flat and is not in an area of steep slope. Sections V.C.1 and V.C.5 of the EIR and the Geology and Soils section of the Addendum include mitigation measures for erosion control. C
Geology & Soils	S 7.7b	Require mitigation measures to reduce potential damage caused by ground failure for sites determined to have potential for liquefaction. Such measures shall apply to critical facilities, utilities, and large commercial and industrial projects as a condition of project approval.	The proposed project is designated as having a high potential for liquefaction. A Geotechnical Report prepared for the project site found the potential to be low, however, and sections V.C.4 of the EIR and the Geology and Soils section of the Addendum includes mitigations measures for further site specific investigations once location and nature of structures are known. Additionally, conformance with California Building Code standards, enforcement of the maintenance of the tile drain system, seismic safety standards and sound grading practices will help to mitigate any potentially concerning circumstances. C
Geology & Soils	S 4.1	For new construction and proposals for substantial improvements to residential and nonresidential development with 100-year floodplains as mapped by FEMA or as determined by site specific hydrologic studies for areas not mapped by FEMA, the County shall apply a minimum level of acceptable risk; and disapprove projects that cannot mitigate the hazard to the satisfaction of the Building Official or other responsible agency.	Implementing projects within the proposed Specific Plan will comply with the Flood and Inundation section of the General Plan Safety Element. C
Hazards	ECVAP 19.1	Protect life and property from wildfire hazards through adherence to the Fire Hazards section of the General Plan Safety Element	The Kohl Ranch is located in an area considered to be at a very low susceptibility for wildfire. C
Hazards	S 6.1	Enforce the land use policies and siting criteria related to hazardous materials and wastes through continued implementation of the programs identified in the County of Riverside Hazardous Waste Management Plan including the following: (AI 98) a. Ensure county	The project will be in compliance with existing regulations such as South Coast Air Quality Management District Rules and Regulations pertaining to asbestos, Department of Transportation (DOT) office of Hazardous Materials Safety regulations, and Titles 8, 22, and 26 of the California Code of

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		businesses comply with federal, state and local laws pertaining to the management of hazardous wastes and materials including all Certified Unified Program Agency (CUPA) programs; b. Ensure active public participation in hazardous waste and hazardous materials management decisions in Riverside County through the County's land use and planning processes; and c. Encourage and promote the programs, practices, and recommendations contained in the Riverside County Hazardous Waste Management Plan, giving the highest waste management priority to the reduction of hazardous waste at its source.	Regulations, would ensure that the public would not be exposed to any unusual or excessive risks related to hazardous materials. C
Hazards	S 7.1	Continually strengthen the Multi-Hazard Functional Plan and maintain mutual aid agreements with federal, state, local agencies and the private sector to assist in: a. clearance of debris in the event of widespread slope failures, collapsed buildings or structures, or other circumstances that could result in blocking emergency access or regress; b. heavy search and rescue; c. fire suppression; d. hazardous materials response; e. temporary shelter; f. geologic and engineering needs; g. traffic and crowd control; and h. building inspection.	The project site along with the unincorporated Riverside County areas are contracted with the Riverside County Fire Department for emergency response. Emergency response and emergency evacuation are regulated under one agency in the project area and surrounding areas. Therefore project development will not interfere with existing emergency response and evacuation, but will be consistent with the existing system because the same agency regulates all of the surrounding areas. C
Hydrology	ECVAP 18.1	Protect life and property from the hazards of flood events through adherence to the Flood and Inundation Hazards section of the General Plan Safety Element.	The proposed The Kohl Ranch Specific Plan is not within the 100-year flood plain but the area is subject to ponding and flash flooding. EIR sections V.C.5 and V.D.2 incorporate mitigation measures to mitigate against impacts from flood damage, surface ponding and erosion. C
Hydrology	ECVAP 18.2	Adhere to the flood proofing, flood protection requirements, and Flood Management Review requirements of the Riverside County Ordinance No. 458 Regulating Flood Hazard Areas.	The proposed The Kohl Ranch Specific Plan is not within the 100-year flood plain but the area is subject to ponding and flash flooding. EIR sections V.C.5 and V.D.2 and the Hydrology section of the Addendum incorporate mitigation measures to mitigate against impacts from flood damage, surface ponding and erosion. Additionally, the proposed project will comply with Ordinance 458. C
Hydrology	ECVAP 18.3	Require that proposed development projects that are subject to flood hazards, surface ponding, high erosion potential or sheet flow be submitted to the Coachella Valley Water District for review.	The proposed The Kohl Ranch Specific Plan is not within the 100-year flood plain but the area is subject to ponding and flash flooding. EIR sections V.C.5 and V.D.2 and the Hydrology section of the Addendum incorporate mitigation measures to mitigate against impacts from flood damage, surface ponding and erosion. C
Hydrology	OS 3.3	Minimize pollutant discharge into storm drainage systems and natural drainage and aquifers.	The Hydrology section of this EIR and the Hydrology section of the Addendum evaluated potential impacts to water quality and determined that all potential significant adverse impacts associated with the proposed project were found to be less than significant. With the implementation of the Regional WQMP, as well as subsequent tract specific WQMPs that will be prepared at the time of tract map submittal, impacts to water quality are anticipated to be less than significant. C
Hydrology	OS 5.3	Based on site specific study, all development shall be set back from the floodway boundary a distance adequate to address the following issues: a. public safety; b. erosion; c. riparian or wetland buffer; d. wildlife movement corridor or linkage; e. slopes; f. type of watercourse; and g. cultural resources.	Implementing projects within the proposed Specific Plan will comply with the Flood and Inundation section of the General Plan Safety Element. Upon completion, the entirety of development of the Kohl Ranch is outside of any Flood Hazard areas. C
Hydrology	OS 5.4	Consider designating floodway setbacks for greenways, trails, and recreation opportunities on a case-by-case basis.	The project is not located within a floodway but is subject to ponding and flash flooding. EIR section V.C.5 and V.D.2 and the Hydrology section of the Addendum incorporate mitigation measures to reduce impacts. C
Hydrology	S 4.8	Allow development within the floodway fringe, if the proposed structures can be adequately flood-proofed and will not contribute to property damage or risks to public safety.	The project is not located within a floodway but is subject to ponding and flash flooding. EIR section V.C.5 and V.D.2 and the Hydrology section of the Addendum incorporate mitigation measures to reduce impacts. C
Hydrology	S 4.9	Within the floodway fringe of a floodplain as mapped by FEMA or as determined by site specific hydrologic studies for areas not mapped by FEMA, require development to be	The project is not located within a floodway but is subject to ponding and flash flooding. EIR section V.C.5 and V.D.2 and the Hydrology section of the Addendum incorporate mitigation measures to

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		capable of withstanding flooding and to minimize use of fill. However, some development may be compatible within flood plains and floodways, as may some other land uses. In such cases, flood proofing would not be required. Compatible uses shall not, however, obstruct flows or adversely affect upstream or downstream properties with increased velocities, erosion backwater effects, or concentrations of flows.	reduce impacts. C
Hydrology	S 4.18	Require that the design and upgrade of street storm drains be based on the depth of inundation, relative risk to public health and safety, the potential for hindrance of emergency access and regress from excessive flood depth, and the threat of contamination within the top of curbs and the 100-year flood flows within the street right of way.	As shown in the Drainage Plan portion of the Specific Plan, the Kohl Ranch will utilize streets, underground storm drains, swales, drainage pipes, collection basins, and catch basins to collect the on-site and off-site storm water, and convey it through the project and discharge over wiers on the east side of the project site. Facilities will be required to accommodate developed 100-year storm runoff through the project. The backbone drainage plan facilities are designed to protect habitable dwelling units from flooding. C
Land Use & Planning	ECVAP 3.1	To provide for the orderly development of Desert Resorts Regional Airport and Chiriaco Summit Airport and the surrounding area, comply with the Airport Land Use Compatibility Plan for Desert Resorts Regional Airport and Chiriaco Summit Airport as fully set forth in Appendix L and as summarized in Table 4, as well as any applicable policies related to airports in the Land Use, Circulation, Safety and Noise Elements of the Riverside County General Plan.	The Airport Land Use Commission has found the Kohl Ranch Specific Plan to be conditionally consistent with the 2005 Jacqueline Cochran Regional Airport (formerly Desert Resorts Regional Airport) Land Use Compatibility Plan (JCRAUCP). EIR Section V.D.12 analyzed and incorporates mitigation measures to ensure compliance with the JCRAUCP. The Kohl Ranch is not located within the policy area of the Chiriaco Summit Airport. C
Land Use & Planning	ECVAP 7.1	Ensure proper service provision, land use compatibility, design standards, safety, and accessibility for recreational vehicle development in the Eastern Coachella Valley area through adherence to General Plan policies found in the Land Use Element.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan area, therefore such policies do not apply to the project. N/A
Land Use & Planning	ECVAP 7.2	Allow Resort Recreational Vehicle developments within the following land use designations: Low Density Residential, Medium Density Residential, Medium High Density Residential, High Density Residential, Very High Density Residential, Commercial Tourist, and Open Space-Recreation.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan area, therefore such policies do not apply to the project. N/A
Land Use & Planning	ECVAP 7.3	Limit Resort Recreational Vehicle developments to a density of sixteen (16) spaces per acre.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan area, therefore such policies do not apply to the project. N/A
Land Use & Planning	ECVAP 7.4	Allow Remote recreational vehicle developments within the following land use designations: Very Low Density Residential, Estate Density Residential, Rural Residential, Rural Mountainous, Rural Desert, Open Space-Recreation, and Open Space-Rural.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan area, therefore such policies do not apply to the project. N/A
Land Use & Planning	ECVAP 7.5	Limit Remote recreational vehicle developments to a density of seven (7) spaces per acre.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan area, therefore such policies do not apply to the project. N/A
Land Use & Planning	ECVAP 8.1	Encourage industrial uses related to agriculture to continue and expand within this area plan.	The zoning ordinance allows for light agriculture on industrial land use designations where lots are 20 acres or greater. C
Land Use & Planning	ECVAP 8.2	Discourage industrial uses that may conflict with agricultural or residential land uses either directly or indirectly within the Eastern Coachella Valley Area Plan.	Section V.C.2 of the EIR and the Land Use & Planning section of the Addendum incorporates mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. As a planned community, potential industrial uses are planned nearest the airport and appropriate design features, such as roads and landscaping, are proposed to buffer between proposed residential land uses and industrial uses. C
Land Use & Planning	ECVAP 8.3	Discourage industrial uses which use large quantities of water in manufacturing or cooling processes and result in subsequent effluent discharges.	EIR sections V.D.2 and the Land Use & Planning section of the Addendum incorporate mitigation measures to mitigate against impacts from usage of large quantities or subsequent effluent

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			discharges. C
Land Use & Planning	ECVAP 8.4	Discourage industrial uses which produce significant quantities of toxic emissions into the air.	The project is located within the jurisdiction of the South Coast Air Quality Management District (SCAQMD). The air quality section of the EIR requires that the project comply with the requirements set forth by the SCAQMD, the Environmental Protection Agency, and the California Air Resources Board and propose to mitigate, to the greatest extent possible, project-related emissions. C
Land Use & Planning	ECVAP 8.5	Encourage industrial uses that can best utilize the transportation facilities of the Jacqueline Cochran Regional Airport.	All industrial uses are located within the vicinity of the Jacqueline Cochran Regional Airport (formerly Desert Resorts Regional Airport) within the Kohl Ranch Specific Plan Land Use Plan. C
Land Use & Planning	ECVAP 8.6	Encourage industrial uses related to aviation to locate in the vicinity of the Jacqueline Cochran Regional Airport.	All industrial uses are located within the vicinity of the Jacqueline Cochran Regional Airport (formerly Desert Resorts Regional Airport) within the Kohl Ranch Specific Plan Land Use Plan. C
Land Use & Planning	LU 2.1	Accommodate land use development in accordance with the patterns and distribution of use and density depicted on the General Plan Land Use Map (RCIP Figure VI-1) and the Area Plan Land Use Maps, in accordance with the following:	Kohl Ranch Specific Plan proposed a reallocation of land uses previously approved in the Kohl Ranch Specific Plan. The proposed uses will be the same intensity as the uses permitted under the current Kohl Ranch Specific Plan Amendment No. 1. C
Land Use & Planning	LU 2.1a	Provide a land use mix at the countywide and area plan levels based on projected need and supported by evaluation of impacts to the environment, economy, infrastructure, and services.	The population of Riverside County is expected to be approximately 3.4 million by 2030 creating an increasing need for a broader variety of job and services available to the residents of the area. More housing units will be needed to accommodate new residents. At the countywide level, the RCIP Vision Statement sets forth various topical visions aimed at proactively dealing with the aforementioned need for growth with its social and development issues. By offering a broader mix of land use as proposed in the Kohl Ranch Specific Plan, the community can be better served. The proposed project provides a mix of land uses within a master planned community setting including a range of residential types, open space, public facilities, commercial and industrial use. This comprehensive approach will assist the County in meeting its vision and accommodating project growth. The EIR evaluated in detail impacts to the environment. Infrastructure necessary to serve the project area is being constructed and is evaluated in the specific plan; and public services will be provided as identified in the specific plan and addressed the EIR. C
Land Use & Planning	LU 2.1b	Accommodate a range of community types and character, from agricultural and rural enclaves to urban and suburban communities.	The Kohl Ranch Specific Plan with its variety of land uses will accommodate a range of community types and character that will be compatible with both the County General Plan and Area Plans. C
Land Use & Planning	LU 2.1c	Provide for a broad range of land uses, intensities, and densities, including a range of residential, commercial, business, industry, open space, recreation, and public facilities uses.	The Kohl Ranch Specific Plan will create a master-planned community consisting of medium high density, high density, and very high density residential types, four potential K-8 schools, mixed-use commercial/residential, open space, and public and civic facilities. C
Land Use & Planning	LU 2.1d	Concentrate growth near community centers that provide a mixture of commercial, employment, entertainment, recreation, civic, and cultural uses to the greatest extent possible.	The Eastern Coachella Valley area is a rural area of Riverside County which has limited employment, commercial and entertainment opportunities currently available to residence in the vicinity of the Project site. The Kohl Ranch Specific Plan concentrates the commercial and industrial uses along Avenue 62 to allow for the growth of a community center. C
Land Use & Planning	LU 2.1e	Concentrate growth near or within existing urban and suburban areas to maintain the rural and open space character of Riverside County to the greatest extent possible.	The Eastern Coachella Valley area is a rural area of Riverside County. The area around Jacqueline Cochran Airport is envisioned by the County as an area where growth should be focused. The Kohl Ranch Specific Plan is consistent with this goal for the area. C
Land Use & Planning	LU 2.1f	Site development to capitalize upon multi-modal transportation opportunities and promote compatible land use arrangements that reduce reliance on the automobile.	The Eastern Coachella Valley area is a rural area of Riverside County which has limited multi-modal transportation opportunities currently available to residence in the vicinity of the Project site. The Kohl Ranch development is expected to encourage the development of future bus routes to the area. The proposed Kohl Ranch Specific Plan offers a mix of residential/commercial services, job producing areas, schools and parks all of which are linked with a trail system so residents will be able to access

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			either by bike or foot, reducing their reliance on automobiles. C
Land Use & Planning	LU 2.1g	Prevent inappropriate development in areas that are environmentally sensitive or subject to severe natural hazards.	The Kohl Ranch Specific Plan, being part of the Eastern Coachella Valley area, is designated as a high liquefaction potential, however a Geotechnical Report prepared for the project site as part of the analysis for the EIR found the potential to be low and section V.C.4 of the EIR and the Land Use & Planning section of the Addendum includes mitigations measures for further site specific investigations once location and nature of structures are known. Additionally, conformance with California Building Code standards, enforcement of seismic safety standards and sound grading practices will help to mitigate any potentially concerning circumstances. While the Project site is not within the 100-year flood plain, the area is subject to ponding and flash flooding. EIR sections V.C.5 and V.D.2 incorporate mitigation measures to mitigate against impacts from flood damage. C
Land Use & Planning	LU 3.1	Accommodate land use development in accordance with the patterns and distribution of use and density depicted on the General Plan Land Use Maps (RCIP Figure LU-1) and the Area Plan Land Use Maps in accordance with the following concepts:	See LU2.1 for setting details. C
Land Use & Planning	LU 3.1a	Accommodate communities that provide a balanced mix of land uses, including employment, recreation, shopping, public facilities, and housing.	See response to Policy LU2.1, above. C
Land Use & Planning	LU 3.1b	Assist in and promote the development of infill and underutilized parcels which are located in Community Development areas, as identified on the General Plan Land Use Map.	The Kohl Ranch is not located in a Community Development area. N/A
Land Use & Planning	LU 3.1d	Create street and trail networks that directly connect local destinations, and that are friendly to pedestrians, equestrians, bicyclists, and others using non-motorized forms of transportation.	Regional, county and project trails are incorporated throughout the Kohl Ranch project site. All levels of street hierarchy include a sidewalk and/or trail to facilitate pedestrian travel and to permit pedestrian access to areas and trails within the community. C
Land Use & Planning	LU 3.1e	Re-plan existing urban cores and specific plans for higher density, compact development as appropriate to achieve the RCIP vision.	It is the intent of this policy to achieve the RCIP vision for higher density and compact development by re-planning existing urban core and specific plan areas. Although the proposed project promotes higher density, compact development as appropriate to achieve the RCIP vision, this project area for The Kohl Ranch is not part of an existing urban core. The Kohl Ranch Specific Plan Amendment No. 2 is proposing to reallocate land uses within an existing specific plan but not plan for a higher density. Accordingly, this policy is does not apply to The Kohl Ranch. N/A
Land Use & Planning	LU 3.1g	Provide the opportunity to link communities through access to multi-modal transportation systems.	See response to Policy LU 2.1.f, above. C
Land Use & Planning	LU 3.2	Use open space, greenways, recreational lands, and watercourses as community separators.	The Kohl Ranch will provide buffers and use of open space areas to create separation from land uses. C
Land Use & Planning	LU 4.1	Require that new developments be located and designed to visually enhance, not degrade the character of the surrounding area through consideration of the following concepts:	The Kohl Ranch Specific Plan has developed its own Design Guidelines. New developments will be required to comply with these guidelines to contribute to the visual order and consistency of the entire project site. C
Land Use & Planning	LU 4.1a	Compliance with the design standards of the appropriate area plan land use category.	The Kohl Ranch Specific Plan has developed its own Design Guidelines. The Thermal Design Guidelines were created and adopted a number of years after approval of the Kohl Ranch Specific Plan. The Thermal Design Guidelines would apply to the Kohl Ranch if the SP guidelines are silent. Thus, the project will adhere to the design standards approved for the area. C
Land Use & Planning	LU 4.1b	Require that structures be constructed in accordance with the requirements of the County's zoning, building, and other pertinent codes and regulations.	The Kohl Ranch Specific Plan has established its own set of zoning standards, and development will occur in accordance with those requirements and with all other applicable zoning regulations not in the specific plan. Building codes and other pertinent regulations will be applied to the project as it

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			develops. C
Land Use & Planning	LU 4.1c	Require that an appropriate landscape plan be submitted and implemented for development projects subject to discretionary review.	Prior to the issuance of building permits, the Planning Dept. must approve plans for developed common open space areas, including landscaping and irrigation plans. Plans include final grading plans, irrigation plans certified by a landscape architect, certified landscape plans, fence treatment, and special treatment/buffer area treatment plans. The Kohl Ranch Specific Plan Development Standards support this. C
Land Use & Planning	LU 4.1d	Require that new development utilize drought tolerant landscaping and incorporate adequate drought-conscious irrigation systems.	The Kohl Ranch Specific Plan's Landscape Guidelines require the implementation of water use efficiency measures such as use of drought tolerant plants, grouping plants with similar irrigation requirements, use of mulch, and water efficient irrigation systems, consistent with County Ordinance 859, Coachella Valley Water District Landscape Ordinance 1302.1 and the Thermal Design Guidelines plant palette. C
Land Use & Planning	LU 4.1e	Pursue energy efficiency through street configuration, building orientation, and landscaping to capitalize on shading and facilitate solar energy, as provided for in Title 24 of the California Administrative Code.	Passive energy efficiency techniques will occur within the proposed The Kohl Ranch Specific Plan, including orientation of buildings, planting trees to take advantage of sun and adequate roof overhangs. Active energy efficiency measures will be addressed and required in Title 24 for such methods as proper wall and ceiling insulation. C
Land Use & Planning	LU 4.1f	Incorporate water conservation techniques, such as groundwater recharge basins, use of porous pavement, drought tolerant landscaping, and water recycling, as appropriate.	The Kohl Ranch Specific Plan contains water conservation measures that incorporate a dual water system consisting of installation of potable water lines for domestic purposes and to provide water for fire protection. Non-potable water lines will be installed for irrigation purposes and drought tolerant landscaping will be implemented throughout the Specific Plan. C
Land Use & Planning	LU 4.1g	Encourage innovative and creative design concepts.	The Kohl Ranch Specific Plan has developed its own Design Guidelines. New developments will be required to comply with these guidelines to contribute to the visual order and consistency of the entire project site. C
Land Use & Planning	LU 4.1h	Encourage the provision of public art that enhances the community's identity, which may include elements of historical significance and creative use of children's art.	Although not specifically discussed in the Specific Plan, public art is not discouraged. As individual development proposals are reviewed by the County, public art could be included. C
Land Use & Planning	LU 4.1i	Include consistent and well-designed signage that is integrated with the building's architectural character.	At the specific plan level, no particular signs on buildings are proposed. General signage guidelines are included within the Specific Plan and are limited in scope but will conform to the project's design guidelines and Riverside County Ordinance 348. C
Land Use & Planning	LU 4.1j	Provide safe and convenient vehicular access and reciprocal access between adjacent commercial uses.	The project's circulation system will be designed to provide safe and convenient vehicular access to all commercial uses that complies with Riverside County Transportation Department and Fire Department requirements. During future site plan reviews assessments can be made for the possibility of reciprocal access between adjacent uses. C
Land Use & Planning	LU 4.1k	Locate site entries and storage bays to minimize conflicts with adjacent residential neighborhoods.	At the time of site plan review the Riverside County Planning Department will apply all applicable design standards to minimize conflicts with adjacent residential neighborhoods. C
Land Use & Planning	LU 4.1l	Mitigate noise, odor, lighting, and other impacts on surrounding properties.	To the extent feasible at the specific plan level, the EIR and the Land Use & Planning section of the Addendum has found these types of impacts to be less than significant or reduced to less than significant through mitigation included in the EIR. C
Land Use & Planning	LU 4.1m	Provide and maintain landscaping in open spaces and parking lots.	The Design Guidelines of The Kohl Ranch Specific Plan provide guidance for designing open space landscaping in common areas. The Specific Plan requires that a special district or Home Owners Association be established to maintain all common landscape areas. Parking lots are required to be

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			landscaped in accordance with Riverside County Ordinance No. 348 requirements. C
Land Use & Planning	LU 4.1n	Include extensive landscaping.	The Kohl Ranch Specific Plan has developed unique Design Guidelines which will encourage appropriate landscaping. The tree planting and landscaping program in The Kohl Ranch Specific Plan is proposed to be extensive with the use of drought tolerant and non-invasive plant species. The proposed The Kohl Ranch Specific Plan will adhere to standards detailed in the Specific Plan's landscaping standards and guidelines, and in Riverside County Ordinance Nos. 348 and 859 as well as the Thermal Design Guidelines and Coachella Valley Water District Landscape Ordinance 1302.1. C
Land Use & Planning	LU 4.1o	Preserve natural features, such as unique terrain, drainage ways, arroyos, canyons and native vegetation, wherever possible, particularly where they provide continuity with more extensive regional systems.	The Kohl Ranch Specific plan lies on relatively flat terrain with high disturbed soils due to past and present agriculture. N/A
Land Use & Planning	LU 4.1p	Require that new development be designed to provide adequate space for pedestrian connectivity and access, recreational trails, vehicular access and parking, supporting functions, open space and other pertinent elements.	Approximately 379.28 acres of open space will be provided within the Project for use as trails and parks. An extensive trail system connects parks, open space, and community facilities. Landscaped streets include sidewalks for pedestrian access. The residential community will have access to sidewalks and on-street bike lanes and off-street bike paths within the Project site. C
Land Use & Planning	LU 4.1q	Design parking lots and structures to be functionally and visually integrated and connected.	Development within the Kohl Ranch will comply with the specific plan's design guidelines and applicable Riverside County zoning ordinance requirements regarding the design and screening of parking areas. C
Land Use & Planning	LU 4.1r	Site building access points along sidewalks, pedestrian areas, and bicycle routes, and include amenities that encourage pedestrian activity.	The Design Guidelines encourage buildings to be oriented toward and visible from the street and parking areas to provide for easy access to bicycle routes and sidewalks. Courtyards and outdoor seating areas are encouraged as well as other ancillary structures and service areas. C
Land Use & Planning	LU 4.1s	Establish safe and frequent pedestrian crossings.	Internal sidewalks will connect The Kohl Ranch Specific Plan development to internal parks and to other project areas. Pedestrian connectivity and movement is emphasized in street and parking areas designs with numerous crossing points at parking and street intersections. C
Land Use & Planning	LU 4.1t	Create a human-scale ground floor environment that includes public open space areas that separate pedestrian space from auto traffic and where mixed, it does so with special regard to pedestrian safety.	Courtyards and outdoor seating areas are encouraged as well as other ancillary structures and service areas. C
Land Use & Planning	LU 4.2	Require property owners to maintain structures and landscaping to a high standard of design, health, and safety through the following:	The Kohl Ranch Specific Plan Design Guidelines will complement the County-wide Design Guidelines applicable to this area. C
Land Use & Planning	LU 4.2a	Provide proactive code enforcement activities.	The Kohl Ranch Specific Plan requires that a Homeowners' Association shall be established for the specific plan area, to assume ownership and maintenance responsibility for all common recreation, open space, private circulation systems within gated communities and within multi-family complexes, and landscaped areas. The Homeowners' Association will enforce any property and landscaping maintenance requirements established by the association and as appropriate will provide homeowners with information regarding maintenance methods. C
Land Use & Planning	LU 4.2b	Promote programs and work with local service organizations and educational institutions to inform residential, commercial, and industrial property owners and tenants about property maintenance methods.	The Kohl Ranch Specific Plan requires that a Homeowners' Association shall be established for the specific plan area, to assume ownership and maintenance responsibility for all common recreation, open space, private circulation systems within gated communities and within multi-family complexes, and landscaped areas. The Homeowners' Association will enforce any property and landscaping maintenance requirements established by the association and as appropriate will provide homeowners with information regarding maintenance methods. C

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Land Use & Planning	LU 4.2c	Promote and support community and neighborhood based efforts for the maintenance, upkeep, and renovation of structures and sites.	Please see discussion of Policy LU 4.2.b, above. C
Land Use & Planning	LU 7.1	Require land uses to develop in accordance with the General Plan and area plans to ensure compatibility and minimize impacts.	See discussions related to General Plan consistency under Policy LU 2.1, above. Setbacks and buffers incorporated in the specific plan and mitigation measures included in the EIR reduce all potential significant impacts to less than significant levels which ensure compatibility issues are minimized. C
Land Use & Planning	LU 7.3	Consider the positive characteristics and unique features of the project site and surrounding community during the design and development process.	The Kohl Ranch Specific Plan has developed its own Design Guidelines. New developments will be required to comply with these guidelines to contribute to the visual order and consistency of the entire project site. C
Land Use & Planning	LU 7.4	Retain and enhance the integrity of existing residential, employment, agricultural, and open space areas by protecting them from encroachment of land uses that would result in impacts from noise, noxious fumes, glare, shadowing, and traffic.	Currently, the Kohl Ranch Specific Plan project site and surrounding lands are currently used for open farmland, agricultural activities and rural residential living. However, the Eastern Coachella Valley area is rapidly developing with residential land uses in the cities of Coachella, La Quinta, Indio, Palm Desert. Design Guidelines of the Specific Plan will provide for the buffering of the surrounding community from this development. C
Land Use & Planning	LU 7.5	Require buffering to the extent possible between urban uses and adjacent rural/equestrian oriented land uses.	The Kohl Ranch Specific Plan contains a regional trail which may be used for equestrian purposes that will continue to provide linkages to some of the rural/equestrian oriented land uses; and the Specific Plan proposes various trails along some of the perimeters of the project site adjacent to existing rural uses to preserve access to open space areas and offer a buffer between the project's urban development and those uses. In addition, The Kohl Ranch will be buffered from the existing rural and equestrian properties through the use of equestrian trails, open space areas, and site-specific designs. A 300-foot buffer between project development and active agricultural uses is required by mitigation within this DEIR. C
Land Use & Planning	LU 8.1	Accommodate the development of a balance of land uses that maintain and enhance the County's fiscal viability, economic diversity, and environmental integrity.	The Kohl Ranch is a primarily residential specific plan, with some commercial, industrial and public facilities uses also allowed. The Fiscal Impact Report for this specific plan shows that the project will have a positive impact the County's General Fund, and other public facilities funds, thus enhancing the County's fiscal viability. The project allows for the development of approximately 379,000 square feet of retail/commercial uses and 6,486,000 square feet of industrial uses. The environmental integrity of the project area and the County is being preserved through the conservation of 376 acres of open space. C
Land Use & Planning	LU 8.2	Promote and market the development of a variety of stable employment and business uses that provide a diversity of employment opportunities.	The Kohl Ranch Specific Plan will include approximately 379,000 square feet of commercial/retail development and 6,486,000 square feet of industrial development, which would provide the area with new businesses and increase employment opportunities. C
Land Use & Planning	LU 8.3	Promote the development of focused employment centers rather than inefficient strip commercial development.	The Kohl Ranch Specific Plan proposal includes approximately 379,000 square feet of commercial development within an area comprised of 31.06 acres along Avenue 62. The Specific Plan designates four locations within the 31.06 acres for the commercial uses. The placement of the commercial development will be concentrated at the four proposed locations and the surrounding area will be developed with medium density, medium high density and very high density residential. The focused commercial development avoids the strip commercial concept and meets the intent of Policy LU 8.3. C
Land Use & Planning	LU 8.12	Improve the relationship and ratio between jobs and housing so that residents have an opportunity to live and work within the County.	The Kohl Ranch Specific Plan will include approximately 379,000 square feet of commercial development, which would provide the area with new businesses and increase employment opportunities for residents within the project area. C

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Land Use & Planning	LU 9.1	Provide for permanent preservation of open space lands that contain important natural resources cultural resources, hazards, water features, watercourses including arroyos and canyons, and scenic and recreational values. (AI 10)	The Kohl Ranch Specific Plan includes no important natural resources. N/A
Land Use & Planning	LU 9.2	Require that development protect environmental resources by compliance with the Multipurpose Open Space Element of the General Plan and Federal and State regulations such as CEQA, NEPA, the Clean Air Act, and the Clean Water Act.	The project is in compliance with all applicable federal, state and local regulations that protect environmental resources, including the Western Riverside MSHCP, the General Plan's Open Space Element, Clean Air Act and the Clean Water Act. The proposed Specific Plan and all future implementing development proposals will comply with applicable provisions of the California Environmental Quality Act (CEQA). C
Land Use & Planning	LU 9.3	Incorporate open space, community greenbelt separators, and recreational amenities into Community Development areas in order to enhance recreational opportunities and community aesthetics, and improve the quality of life.	The Kohl Ranch Specific Plan proposes to establish 379.28 acres of Open Space with trails. Additionally, the Specific Plan includes parks which could include play equipment, pool, picnic tables, and passive play areas. These amenities will enhance recreational opportunities and community aesthetics and improve the quality of life compliant with Policy LU 9.3. A potential lake is proposed in Neighborhoods K and J which will be used as part of the dual water system and part of the projects drainage network but will provide a scenic amenity and enhance recreational opportunities by providing the use of a project trails surrounding the lakes. C
Land Use & Planning	LU 9.4	Allow development clustering and/or density transfers in order to preserve open space, natural resources, cultural resources, and biologically-sensitive resources. Wherever possible, development on parcels containing 100-year floodplains, blueline streams and other higher-order watercourses, and areas of steep slopes adjacent to them shall be clustered to keep development out of watercourse and adjacent steep slope areas, and to be compatible with other nearby land uses. (AI 1, 9)	The project proposes to cluster development into medium-high, high density, and very-high residential housing in part to address airport master plan requirements. The increase in residential density and clustering will allow for preservation of 379.28 acres of open space, however no natural resources needing preservation exist on-site. C
Land Use & Planning	LU 10.1	Require that new development contribute their fair share to fund infrastructure and public facilities such as police and fire facilities.	The Kohl Ranch will be required to pay its fair share of public safety and infrastructure-related mitigation fees as per Riverside County Ordinance No. 659.6. Additionally, the proposed project will allow for the development of public facilities within the project area. C
Land Use & Planning	LU 10.2	Require a fiscal impact analysis for specific plans and major development proposals so as not to have a negative fiscal impact on the County.	The Fiscal Impact Report shows a net fiscal surplus for the County as a result of implementation of the proposed project. C
Land Use & Planning	LU 11.4	Provide options to the automobile in communities, such as transit, bicycle and pedestrian trails, to help improve air quality.	The Kohl Ranch Specific Plan proposes sidewalks along all streets and an extensive network of trails along most major roadways as a way to offer pedestrian and bicycle "friendly" means of travel with the intent of providing options to the use of the automobile for internal project area travel as a way to help improve air quality. C
Land Use & Planning	LU 20.4	Encourage conservation of productive agricultural lands. Preserve prime agricultural lands for high-value crop production.	The Eastern Coachella Valley Area Plan contains 41,403 acres of AG designated land which is 9.1 percent of all the land comprising this Area Plan. Within Riverside County, only the San Jacinto Valley Area Plan designates a greater percentage of land to AG with 9.4 percent. The majority of the Eastern Coachella Valley AG designated land is concentrated along the northern boundary of Lakeview area and separated from other communities by dedicated conservation lands or park lands. The Kohl Ranch Specific Plan would not retain the Agriculture (AG) designated lands within its project area. Section V.C.2 of the EIR and the Land Use & Planning section of the Addendum analyzed impacts and determined these to be significant and unavoidable as it eliminated 1,468 acres of Prime Farmland, 171 acres of Farmland of Statewide Importance and 480 acres of Farmland of Local Importance. A statement of overriding considerations was adopted with certification of the EIR November 16, 1999. A General Plan Amendment was approved to change land use to that of an adopted specific plan and a zone change changed the zoning to Specific Plan (SP). This policy is therefore, not applicable to the Kohl Ranch Specific Plan Amendment No. 2 with the exception of minimizing impacts to agriculturally designated lands from incompatible uses. EIR section V.C.2 incorporates mitigation requiring

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			adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. N/A, C
Land Use & Planning	LU 20.5	Continue to participate in the California Land Conservation Act (the Williamson Act) of 1965.	There are currently no Williamson Act contracts on any properties within The Kohl Ranch Specific Plan. N/A
Land Use & Planning	LU 20.6	Require consideration of State agricultural land classification specifications when a 2½-year Agriculture Foundation amendment to the General Plan is reviewed that would result in a shift from an agricultural to a non-agricultural use.	The Kohl Ranch Specific Plan would not retain the Agriculture (AG) designated lands within its project area. Section V.C.2 of the EIR and the Agricultural Resources section of the Addendum analyzed impacts and determined these to be significant and unavoidable. A statement of overriding considerations was adopted with certification of the EIR November 16, 1999. Subsequently, the zoning was changed to Specific Plan (SP). This policy is therefore, not applicable to the Kohl Ranch Specific Plan Amendment No. 4. N/A
Land Use & Planning	LU 20.7	Adhere to Riverside County's Right-to-Farm Ordinance.	Riverside County Ordinance No. 625 (Right-to-Farm Ordinance) pertains to residential subdivisions that are to be located within 300 feet of properties zoned for agricultural uses (A-1 (Light Agriculture), A-2 (Heavy Agriculture), A-P (Agriculture-Poultry), A-D (Agriculture-Dairy), or C/V (Citrus/Vineyard). The EIR section V.C.2 and the Agricultural Resource section of the Addendum analyzed impacts to such lands and incorporates mitigation measures to ensure adherence to Ordinance 625. C
Land Use & Planning	LU 22.1	Require that grading be designed to blend with undeveloped natural contours of the site and avoid an unvaried, unnatural, or manufactured appearance.	The project area is relatively flat and will not encroach upon hillsides. N/A
Land Use & Planning	LU 22.3	Ensure that development does not adversely impact the open space and rural character of the surrounding area.	The project area is relatively flat and because of the limited development on the site, views of the nearby mountains framing the valley floor are generally unobstructed. Buffers between land uses will allow for appropriate transitions to surrounding uses and compatibility issues can be addressed to retain rural character adjacent to existing rural areas. The intensity of the development will also place a greater number of people within easy reach of open space. Over 379 acres of open space will be preserved as part of this project. As part of the management of the open space, limited recreational uses will be allowed within this area (trails, horseback riding, etc.). C
Land Use & Planning	LU 22.4	Encourage clustered development where appropriate on lots smaller than the underlying land use designation would allow. The density yield of the underlying land use designation may be clustered on 0.5-acre lots; however, for sites located adjacent to the Community Development Foundation Component, 10,000 square foot minimum lots may be considered.	The Kohl Ranch Specific Plan Amendment No. 2 was a reallocation of land uses allowing for higher density residential designations however, the overall density will not exceed the previously approved maximum allotted dwelling units approved under the original Specific Plan November 16, 1999. C
Land Use & Planning	LU 22.5	Encourage parcel consolidation.	The master developer for The Kohl Ranch has assembled approximately 2,163 acres to form the project site. This consolidation of parcels under one control offers the ability to preserve 379.28 acres of open space as envisioned by the CVMSHCP. C
Land Use & Planning	LU 23.2	Require that structures be designed to maintain the environmental character in which they are located.	Developments within the Project site will adhere to the Kohl Ranch Specific Plan Design Guidelines which address and respect the desert environment. C
Land Use & Planning	LU 28.1	Accommodate the development of single- and multi-family residential units in areas appropriately designated by the General Plan and area plan land use maps.	The Kohl Ranch provides a range of densities that will allow for both single- and multi-family residences to be built within the specific plan area which is consistent with the General Plan vision and the Community Development Foundation. C
Land Use & Planning	LU 28.2	Accommodate higher density residential development near community centers, transportation centers, employment, and services areas.	The very high density residential designation is located in close proximity to commercial and industrial land uses to locate near employment, service and transit centers. C
Land Use &	LU 28.3	Require that adequate and available circulation facilities, water resources, and sewer lines	At the present time there are not adequate facilities available to meet the needs of The Kohl Ranch. However, through County regulations and mitigation measures, which include the payment of

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Planning		exist to meet the demands of the proposed residential land use. (AI 3)	development impact fees, and the construction of utility facilities, facilities can be constructed to meet the needs of the proposed project. The Water Source Assessment prepared for this project indicates that water resources are available to serve this project. Some water and wastewater facilities have already been constructed and all necessary infrastructure is planned. C
Land Use & Planning	LU 28.4	Accommodate the development of a variety of housing types, styles and densities that are accessible to and meet the needs of a range of lifestyles, physical abilities, and income levels.	The Kohl Ranch Specific Plan consists of medium (2 to 5 du/ac), medium-high (5 to 8 du/acre), high density (8 to 14 du/acre), and very high density (14 to 20 du/ac) residential uses. It is intended that the Specific Plan, through the various residential densities, will cater to a range of lifestyles, physical abilities, and income levels. C
Land Use & Planning	LU 28.5	Integrate a continuous network of parks, plazas, public squares, bicycle trails, transit systems, and pedestrian paths to provide both connections within each community and linkages with surrounding features and communities.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads and within roadways. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods, parks, and trailheads. C
Land Use & Planning	LU 28.6	Require setbacks and other design elements that buffer residential units from the impacts of abutting agricultural, roadway, commercial, and industrial uses. (AI 3)	The project design concepts include landscaping and setbacks to buffer residential uses from roads. Drainage facilities are also used as landscaped paseo areas to separate residential areas from roads and other uses. C
Land Use & Planning	LU 28.7	Allow for reduced street widths to minimize the influence of the automobile and improve the character of a neighborhood, in accordance with the Riverside County Fire Department.	Internal roadways within residential tracts of the specific plan may have narrow private streets, the widths of which will be determined in coordination with County Transportation Department and the Riverside County Fire Department. C
Land Use & Planning	LU 28.8	Establish activity centers within or near residential neighborhoods that contain services such as child or adult-care, recreation, public meeting rooms, convenience commercial uses, or similar facilities.	The Kohl Ranch Specific Plan includes commercial and residential land uses. It is foreseeable that the development of this area could include services as desired in this policy. C
Land Use & Planning	LU 28.9	Require residential projects to be designed to maximize integration with and connectivity to nearby community centers, rural villages, and neighborhood centers.	The project includes commercial uses that would be well-integrated into the residential area. Additionally, there are internal trails and roadway that will connect the project site to the surrounding community. C
Land Use & Planning	LU 28.10	Require that residential units/projects be designed to consider their surroundings and to visually enhance, not degrade, the character of the immediate area.	All buildings in the proposed The Kohl Ranch Specific Plan will utilize elements of architectural styles described in the Specific Plan's Design Guidelines and will enhance the character of the area. With sufficient buffering, landscaping, and separation from the adjacent properties as described under previous policies, the project would not degrade the character of the immediate area. C
Land Use & Planning	LU 31.1	Accommodate the development of public facilities in areas appropriately designated by the General Plan and area plan land use maps.	Planning Area M-4 is designated for Public Facilities as this site houses three schools. C
Land Use & Planning	LU 31.2	Protect major public facilities, such as landfill and solid waste disposal sites and airports, from the encroachment of incompatible uses.	The project is located adjacent to the Jacqueline Cochran Regional Airport. The Airport Land Use Commission has found the Kohl Ranch Specific Plan to be conditionally consistent with the 2005 Jacqueline Cochran Regional Airport (formerly Desert Resorts Regional Airport) Land Use Compatibility Plan (JCRALUCP). EIR Section V.D.12 analyzed and incorporates mitigation measures to ensure compliance with the JCRALUCP. C
Land Use & Planning	LU 32.3	Require that new public facilities protect sensitive uses, such as schools and residences, from the impacts of noise, light, fumes, odors, vehicular traffic, parking, and operational hazards.	Public facilities that would be constructed within The Kohl Ranch Specific Plan are operated by Coachella Valley Water District and Riverside County Flood Control District. Implementing projects may require public facilities in the future, such as drainage facilities. Those facilities will be designed to have minimal impact to the surrounding land uses, and thus will comply with this policy. Construction and

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			operations of currently anticipated major facilities have been evaluated in EIR. C
Land Use & Planning	LU 33.4	Require that adequate and available circulation facilities, water resources, and sewer facilities exist to meet the demands of the proposed land use.	At the present time there are not adequate facilities available to meet the needs of The Kohl Ranch. However, through County regulations and mitigation measures, which include the payment of development impact fees, and the construction of utility facilities, facilities can be constructed to meet the needs of the proposed project. The Water Source Assessment prepared for this project indicates that water resources are available to serve this project. Some water and wastewater facilities have already been constructed and all necessary infrastructure is planned. C
Noise	N 1.1	Protect noise-sensitive land uses from high levels of noise by restricting noise producing land uses from these areas. If the noise producing land use cannot be relocated, then noise buffers such as setbacks, landscaping, or block walls shall be used.	The Eastern Coachella Valley area is a rapidly developing area. The Design Guidelines of the Specific Plan will provide for the buffering of the surrounding community from this development. Industrial and commercial uses are planned nearest the airport and clustering of residential units will allow all airport noise related issues to be addressed/avoided. C
Noise	N 1.2	Guide noise tolerant land uses into areas irrevocably committed to land uses that are noise producing, such as transportation corridors, or within the projected noise contours of any adjacent airports.	Industrial and commercial uses are planned nearest the airport and clustering of residential units will allow all airport noise related issues to be addressed/avoided. Community walls, landscaping, setbacks and drainage facilities are used to buffer residential uses from surrounding roads. The EIR and Addendum incorporate mitigation measures to reduce these impacts to less than significant. C
Noise	N 1.5	Prevent and mitigate the adverse impacts of excessive noise exposure on the residents, employees, visitors, and noise sensitive uses of Riverside County.	The project will mitigate adverse impacts of excessive noise to residents, employees, visitors, and noise sensitive uses of Riverside County. Once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. C
Noise	N 4.2	Develop measures to control non-transportation noise impacts.	The exact type of retail/commercial uses that will be occupying the commercial land is unknown at this time; therefore, the types of noise generated by the on-site businesses cannot be determined. However, once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. (The concurrently proposed Thermal Motorsports Park has provided such documentation and been made consistent with this policy.) C
Noise	N 4.3	Ensure any use determined to be a potential generator of significant stationary noise impacts be properly analyzed, and ensure that the recommended mitigation measures are implemented.	The exact type of retail/commercial uses that will be occupying the commercial land is unknown at this time; therefore, the types of noise generated by the on-site businesses cannot be determined. However, once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. (The concurrently proposed Thermal Motorsports Park has provided such documentation and been made consistent with this policy.) C
Noise	N 12.1	Utilize natural barriers such as hills, berms, boulders, and dense vegetation to assist in noise reduction.	The Design Guidelines of the Specific Plan will provide for the buffering methods which do utilize such "natural barriers." C
Noise	N 13.1	Minimize the impacts of construction noise on adjacent uses within acceptable practices.	The project will be subject to Riverside County Ordinance No. 457, which regulates construction practices; and Riverside County Ordinance No. 847, which regulates noise. C
Noise	N 13.2	Ensure that construction activities are regulated to establish hours of operation in order to prevent and/or mitigate the generation of excessive or adverse noise impacts on surrounding areas.	The project will be subject to Riverside County Ordinance No. 457, which regulates construction practices; and Riverside County Ordinance No. 847, which regulates noise. C

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Noise	N 13.4	Require that all construction equipment utilize noise reduction features (e.g. mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer.	The mitigation measures of the noise section of the EIR and Addendum require that all construction equipment shall be required to minimize noise from construction activities. All construction will be required to comply with local noise control ordinances. C
Noise	N 15.1	Minimize the potential adverse noise impacts associated with the development of mixed-use structures where residential units are located above or adjacent to commercial uses.	Mixed use structures are not proposed in the Kohl Ranch Specific Plan. N/A
Noise	N 15.2	Require that commercial and residential mixed use structures minimize the transfer or transmission of noise and vibration from the commercial land use to the residential land use.	Mixed use structures are not proposed in the Kohl Ranch Specific Plan. N/A
Noise	N 15.3	Minimize the generation of excessive noise level impacts from entertainment and restaurant/bar establishments into adjacent residential or noise sensitive uses.	The exact type of retail/commercial uses that will be occupying the commercial land uses is unknown at this time; therefore, the types of noise generated by the on-site businesses cannot be determined. However, once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. C
Noise	C 3.27	Evaluate proposed highway extensions or widening projects for potential noise impacts on existing and future land uses in the area. Require that the effects of truck mix, speed limits, and ultimate motor vehicle volumes on noise levels are also explored during the environmental process.	The exterior noise impacts from traffic to future land uses have been calculated. The Addendum has incorporated mitigation measures regarding the necessary height of noise barriers to obtain the appropriate noise standard levels for sensitive receptors and commercial land uses. C
Population/Housing	5 Year Action Plan: Policy 1.2	Ensure the availability of Suitable sites for the development of affordable housing to meet the needs of all household income levels, including farm workers and other special needs populations.	The Kohl Ranch Specific Plan consists of medium (2 to 5 du/ac), medium-high (5 to 8 du/acre), high density (8 to 14 du/acre), and very high density (14 to 20 du/ac) residential uses. It is intended that the Specific Plan, through the various residential densities, will cater to a range of lifestyles, physical abilities, and income levels. C
Population/Housing	5 Year Action Plan: Policy 1.7	Encourage innovative housing, site plan design and construction techniques to promote new affordable housing by the private sector.	The Kohl Ranch Specific Plan consists of medium (2 to 5 du/ac), medium-high (5 to 8 du/acre), high density (8 to 14 du/acre), and very high density (14 to 22 du/ac) residential uses. It is intended that the Specific Plan, through the various residential densities, will cater to a range of lifestyles, physical abilities, and income levels. C
Population/Housing	5 Year Action Plan: Policy 1.7	Action 1.7a Continue to provide for greater flexibility in the design of single family development through the processing of PDs, Specific Plans, and Area Plans, and application of density bonus provisions, when requested, to allow for varying lot sizes and development standards than normally required in residential districts.	The Kohl Ranch is a Specific Plan which allows for a greater degree of flexibility in lot sizes, density bonuses, and housing product types than is the historical norm in Riverside County. C
Population/Housing	5 Year Action Plan: Policy 1.7	Action 1.7d Encourage new large scale development proposals to provide a range of housing types and densities for all income levels through the use of creative planning concepts as specific plans and mixed-use development.	The Kohl Ranch is a large-scale specific plan which introduces a wide variety of housing types, lot sizes, and densities. The Administrative Section of the specific plan allows for flexibility in product type, lot sizes, and densities. C
Population/Housing	5 Year Action Plan: Policy 5.1	Encourage the use of energy conservation features in residential construction and remodeling.	Passive energy efficiency techniques will occur within the proposed project, including orientation of buildings, planting trees to take advantage of sun and adequate roof overhangs. Active energy efficiency measures will be addressed and required in Title 24 for such methods as proper wall and ceiling insulation. C
Public Services	LU 5.1	Ensure that development does not exceed the ability to adequately provide supporting infrastructure and services, such as libraries, recreational facilities, educational and day care centers, transportation systems, and fire/police/medical services.	Through the payment of Riverside County's required development impact fees (Ordinance No. 659), the implementation of the Periodic Medical Needs Assessment, which is required by Mitigation Measure 4.15.7A of the County General Plan EIR, and the implementation of mitigation measures listed in the EIR and the Public Services section of the Addendum, the impacts of the Kohl Ranch Specific Plan to public facilities and services will be less than significant. C

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Recreation	OS 20.4	Provide for the needs of all people in the system of County recreation sites and facilities, regardless of their socioeconomic status, ethnicity, physical capabilities or age.	The proposed project includes the construction of many types of trail systems, open space areas with passive and active uses available to the general public. The recreation plan is provided to the community as a whole and does not discriminate by socioeconomic status, ethnicity, physical capabilities or age. C
Recreation	OS 20.5	Require that development of recreation facilities occurs concurrent with other development in the area.	Recreational facilities will be built to satisfy the population demand of the proposed project, as referenced in the Kohl Ranch Specific Plan Development Standards. C
Recreation	OS 20.6	Require new development to provide implementation strategies for the funding of both active and passive parks and recreational sites.	The project will provide active parks and passive parks by design. If it is determined that park acreages are insufficient, fees will be paid in accordance with Riverside County Ordinance 460. C
Recreation	C 16.1	Implement the Riverside County trail system as depicted in the Bikeways and Trails Plan, Figure C-6. (AI 3, 33	The County trail system has been implemented in accordance with the Bikeways and Trails Plan and per the County Transportation Department and County Regional Parks and Recreation District. C
Recreation	C 16.2	Develop a multi-purpose trail network with support facilities which provide a linkage with regional facilities, and require trailheads and staging areas that are equipped with adequate parking, equestrian trailer parking (as appropriate), bicycle parking, restrooms, informative signage, interpretive displays, maps, and rules of appropriate usage and conduct on trails accessed from such facilities. (AI 35).	The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods, parks, and trailheads. C
Recreation	C 16.3	Require that trail alignments either provide access to or link scenic corridors, schools, parks, bus stops, transit terminals, park and ride commuter lots, and other areas of concentrated public activity, where feasible.	The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods, parks, and trailheads. C
Recreation	C 17.3	Ensure that the bikeway system incorporates the following: a. interconnection throughout and between cities and unincorporated communities; b. appropriate lanes to specific destinations such as state or county parks; c. appropriate opportunities for recreational bicycle riding and bicycle touring; d. opportunities for bicycle commuting and golf cart commuting within a community, as appropriate for the terrain, traffic levels and proximity to surrounding destinations; e. bikeways connecting to all urban transit centers and systems (bus stops and Metrolink stations) in the vicinity; and f. bicycle parking at transit stops and park-and-ride lots..	The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods, parks, and trailheads. C
Transportation	C 2.1	<p>The following minimum target levels of service have been designated for the review of development proposals in the unincorporated areas of Riverside County with respect to transportation impacts on roadways designated in the Riverside County Circulation Plan (Figure C-1) which are currently County maintained, or are intended to be accepted into the County maintained roadway system:</p> <p>LOS C shall apply to all development proposals in any area of the Riverside County not located within the boundaries of an Area Plan, as well those areas located within the following Area Plans: REMAP, Eastern Coachella Valley, Desert Center, Palo Verde Valley, and those non- Community Development areas of the Elsinore, Lake Mathews/Woodcrest, Mead Valley and Temescal Canyon Area Plans.</p> <p>LOS D shall apply to all development proposals located within any of the following Area Plans: Eastvale, Jurupa, Highgrove, Reche Canyon/Badlands, Lakeview/Nuevo, Sun City/Menifee Valley, Harvest Valley/Winchester, Southwest Area, The Pass, San Jacinto Valley, Western Coachella Valley and those Community Development Areas of the Elsinore, Lake Mathews/Woodcrest, Mead Valley and Temescal Canyon Area Plans.</p> <p>LOS E may be allowed by the Board of Supervisors within designated areas where transit</p>	The project has been analyzed in the Traffic Study to meet the required LOS standards for the various roadway types located within the project's sphere of influence. Section V.D.1 of the EIR and the Transportation section of the Addendum incorporates mitigation measures to reduce impacts and ensure LOS standards meet required levels. C

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		<p>oriented development and walkable communities are proposed.</p> <p>Notwithstanding the forgoing minimum LOS targets, the Board of Supervisors may, on occasion by virtue of their discretionary powers, approve a project that fails to meet these LOS targets in order to balance congestion management considerations in relation to benefits, environmental impacts and costs, provided an Environmental Impact Report, or equivalent, has been completed to fully evaluate the impacts of such approval. Any such approval must incorporate all feasible mitigation measures, make specific findings to support the decision, and adopt a statement of overriding considerations. (AI 3)</p>	
Transportation	C 1.2	Support development of a variety of transportation options for major employment and activity centers including direct access to transit routes, primary arterial highways, bikeways, park-n-ride facilities, and pedestrian facilities.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads and within roadways. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. C
Transportation	C 1.5	Evaluate the planned circulation system as needed to enhance the arterial highway network to respond to anticipated growth and mobility needs.	The Traffic Study analyzed the planned circulation system with respect to the current County of Riverside General Plan Circulation Element and in coordination with future County efforts. C
Transportation	C 1.7	Encourage and support the development of projects that facilitate and enhance the use of alternative modes of transportation, including pedestrian-oriented retail and activity centers, dedicated bicycle lanes and paths, and mixed-use community centers.	The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods and parks and the various land uses. C
Transportation	C 2.3	Traffic studies prepared for development entitlements (tracts, plot plans, public use permits, conditional use permits, etc.) shall identify project related traffic impacts and determine the "significance" of such impacts in compliance with CEQA and the Riverside County Congestion Management Program Requirements. (AI 3).	Potential impacts related to inadequate parking capacity, altered waterborne, rail or air traffic were found to be less than significant in the Notice of Preparation for the Kohl Ranch Specific Plan. The subsequent Addendum for the Kohl Ranch Specific Plan Amendment found no new or substantially increased significant effects. Some design features present on the current and future project roadways, such as curves, could result in potentially significant impacts however, all design features will be designed to meet County Standards. C
Transportation	C 2.4	The direct project related traffic impacts of new development proposals shall be mitigated via conditions of approval requiring the construction of any improvements identified as necessary to meet level of service standards.	The analysis shows that the project will contribute to the exceedance of acceptable levels of service for both intersections and roadway segments, and contribute to substantial increases in the traffic on roads. Mitigation measures in the form of signals and roadway improvements, and fair share fees, listed above, will be required to reduce these potentially significant impacts to below the level of significance. C
Transportation	C 2.5	The cumulative and indirect traffic impacts of development may be mitigated through the payment of various impact mitigation fees such as County Development Impact Fees, Road and Bridge Benefit District Fees, and Transportation Uniform Mitigation Fees to the extent that these programs provide funding for the improvement of facilities impacted by development.	<p>To ensure that area-wide traffic conditions do not worsen as development occurs, the County of Riverside has established "fair share" mitigation fees, which include, but are not limited to, the Coachella Valley Association of Governments (CVAG) Transportation Uniform Mitigation Fee (TUMF), Development Impact Fees (DIF) and a Road and Bridge Benefit District (RBBD) fee, which is currently being established by the County, all described below. The project will participate in the cost of off-site improvements through payment of the following "fair share" mitigation fees:</p> <ul style="list-style-type: none">• Coachella Valley Association of Governments Transportation Uniform Mitigation Fee (TUMF), current at time of construction• Riverside County Traffic Signal Systems Fee Program• RBBD (when approved and enacted) <p>These fees shall be collected and utilized as needed by Riverside County to construct the improvements necessary to maintain the required level of service. C</p>
Transportation	C 3.1	Design, construct, and maintain Riverside County roadways as specified in the Riverside County Road Improvement Standards and Specifications. The standards shown in Figure C-4	The Kohl Ranch Specific Plan includes Circulation Development Standards which are required of the

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
		may be modified by Specific Plans, Community Guidelines, or as approved by the Director of Transportation if alternative roadway standards are desirable to improve sustainability for the area.	project. C
Transportation	C 3.2	Maintain the existing transportation network, while providing for future expansion and improvement based on travel demand, and the development of alternative travel modes.	<p>To ensure that area-wide traffic conditions do not worsen as development occurs, the County of Riverside has established "fair share" mitigation fees, which include, but are not limited to, the Coachella Valley Association of Governments (CVAG) Transportation Uniform Mitigation Fee (TUMF), Development Impact Fees (DIF) and a Road and Bridge Benefit District (RBBD) fee, which is currently being established by the County, all described below. The project will participate in the cost of off-site improvements through payment of the following "fair share" mitigation fees:</p> <ul style="list-style-type: none">• Coachella Valley Association of Governments Transportation Uniform Mitigation Fee (TUMF), current at time of construction• Riverside County Traffic Signal Systems Fee Program• RBBD (when approved and enacted) <p>These fees shall be collected and utilized as needed by Riverside County to construct the improvements necessary to maintain the required level of service. C</p>
Transportation	C 3.10	Require private and public land developments to provide all on-site auxiliary facility improvements necessary to mitigate any development-generated circulation impacts. A review of each proposed land development project shall be undertaken to identify project impacts to the circulation system and its auxiliary facilities. The Transportation Department may require developers and/or subdividers to provide traffic impact studies prepared by qualified professionals to identify the impacts of a development.	The project has been analyzed in the Traffic Study to meet the required LOS standards for the various roadway types located within the project's sphere of influence. Where the LOS standards are expected to be exceeded, mitigation has been included that would reduce it to below the required levels. C
Transportation	C 3.13	Design street intersections, where appropriate, to assure the safe, efficient passage of through traffic and the negotiation of turning movements.	The Kohl Ranch project includes a General Plan Amendment which will modify the existing Circulation Element. The modified circulation plan provides improvement standards to promote efficient and safe movement of people within the project area. C
Transportation	C 3.14	Design curves and grades to permit safe movement of vehicular traffic at the road's design speed. Design speed should be consistent with and complement the character of the adjacent area.	The curvilinear roads proposed within the project site (Figure IV-2, The Kohl Ranch Specific Plan Circulation Plan) have been designed to meet the County Transportation Department's standards. C
Transportation	C 3.15	Provide adequate sight distances for safe vehicular movement at a road's design speed and at all intersections.	Sight distance at the project entrance roadways shall be reviewed with respect to standard County of Riverside sight distance standards at the time of preparation of final grading, landscape, and street improvement plans. C
Transportation	C 3.20	Determine location of General Plan road rights of way and levels of road improvements needed based primarily upon land uses and travel demand.	General Plan roadways and levels of road improvements are based on the Riverside County General Plan Circulation Element and coordination with the Riverside County Transportation Department and South Valley Implementation Plan. C
Transportation	C 3.24	Provide a street network with quick and efficient routes for emergency vehicles, meeting necessary street widths, turn-around radius, and other factors as determined by the Transportation Department in consultation with the Fire Department and other emergency service providers.	The Kohl Ranch Specific Plan includes Circulation Development Standards which are required of the project and are designed to meet County Transportation and Fire Department requirements. C
Transportation	C 3.26	Plan off-street parking facilities to support and enhance the concept of walkable and transit-oriented communities.	Section V.D.1 of the EIR and the Transportation section of the Addendum incorporates mitigation to encourage ridesharing and transit ridership by requiring a portion of commercial parking areas be designated to Park and Ride. Additional mitigation requires the coordination with areas bus transit service provider to incorporate bus stops and turnouts. C

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Transportation	C 4.1	Provide facilities for the safe movement of pedestrians within developments, as specified in the County Ordinances Regulating the Division of Land of the County of Riverside.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. C
Transportation	C 4.2	Maximize visibility and access for pedestrians and encourage the removal of barriers (walls, easements, and fences) for safe and convenient movement of pedestrians. Special emphasis should be placed on the needs of disabled persons considering Americans with Disabilities Act (ADA) regulations.	Sidewalks and trails will be incorporated in compliance with County standards. Special considerations for disabled persons were not addressed specifically at this level of analysis. C
Transportation	C 4.3	Assure pedestrian access from developments to existing and future transit routes and terminal facilities through project design.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools and future bus stops. C
Transportation	C 4.4	Plan for pedestrian access that is consistent with road design standards while designing street and road projects. Provisions for pedestrian paths or sidewalks and timing of traffic signals to allow safe pedestrian street crossing shall be included.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools and future bus stops. C
Transportation	C 6.3	Limit access points and intersections of streets and highways based upon the road's General Plan classification and function. Require that access points be located so that they comply with Riverside County's minimum intersection spacing standards. Under special circumstances the Transportation Department may consider exceptions to this requirement. (AI 3)	Sight distance at the project entrance roadways shall be reviewed with respect to standard County of Riverside sight distance standards at the time of preparation of final grading, landscape, and street improvement plans. C
Transportation	C 7.2	Work with property owners to reserve right-of-way for potential CETAP corridors through site design, dedication, and land acquisition, as appropriate.	Three corridors are being examined in western Riverside County for the preservation of rights-of-way for future multi-modal transportation facilities. These include the Beaumont/Banning to Temecula transportation corridor, the Moreno Valley to San Bernardino corridor, and the Hemet to Corona/Lake Elsinore corridor. None of these corridors pass through the project area. N/A
Transportation	C 7.9	Review development applications in cooperation with RCTC and as appropriate, to identify the precise location of CETAP corridors and act to preserve such areas from any permanent encroachments, pending dedication or acquisition. Coordinate with RCTC to evaluate and update the CETAP corridors periodically as conditions warrant. (AI 50)	Three corridors are being examined in western Riverside County for the preservation of rights-of-way for future multi-modal transportation facilities. These include the Beaumont/Banning to Temecula transportation corridor, the Moreno Valley to San Bernardino corridor, and the Hemet to Corona/Lake Elsinore corridor. None of these corridors pass through the project area. N/A
Transportation	C 11.4	Offer incentives to new development to encourage it to locate in a transit-oriented area such as a community center or along a designated transit corridor near a station.	EIR section V.C.6 and the Transportation section of the Addendum incorporates mitigation to encourage mixed-use services to reduce off-site vehicle trips. Commercial and Industrial uses are concentrated along Avenue 62 which is a main transit corridor and coordination efforts with SunLine Bus Transit are required to incorporate bus routes along this segment. C
Transportation	C 11.5	Accommodate transit through higher densities, innovative design, and right-of-way dedication.	Higher density residential land uses are proposed by the Kohl Ranch Specific Plan. These higher densities will facilitate the need for transit-oriented development. EIR section V.D.1 incorporates mitigation to coordinate with SunLine Bus Agency to incorporate bus stops and turnouts. C
Transportation	C 11.6	Promote development of transit centers and park-n-rides for use by all transit operators, including development of multi-modal facilities.	Section V.D.1 of the EIR and the Transportation section of the Addendum incorporates mitigation to encourage ridesharing and transit ridership by requiring a portion of commercial parking areas be designated to Park and Ride on weekdays between 6:00am and 6:00pm. Additional mitigation requires the coordination with areas bus transit service provider to incorporate bus stops and turnouts.

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			C
Transportation	ECVAP 12.1	Design and develop the vehicular roadway system per Figure 8, Circulation, and in accordance with the functional classifications and standards in the System Design, Construction and Maintenance section of the General Plan Circulation Element.	Roadways have been designed in accordance with the standards in the System Design, Construction and Maintenance section of the General Plan Circulation Element. C
Transportation	ECVAP 12.2	Maintain the County's roadway Level of Service standards as described in the Level of Service section of the General Plan Circulation Element.	The project has been analyzed in the Traffic Study to meet the required LOS standards for the various roadway types located within the project's sphere of influence. Section V.D.1 of the EIR and the Transportation section of the Addendum incorporates mitigation measures to reduce impacts and ensure LOS standards are below the required levels. C
Transportation	ECVAP 12.3	Separate vehicular traffic from pedestrian and equestrian traffic in order to avoid potential hazards and where traffic volumes justify the costs.	Trails are designed in accordance with Riverside County Transportation Department and County Regional Parks and Recreation District standards. C
Transportation	ECVAP 13.1	Coordinate with railroad companies to maintain and enhance railroad facilities south of the City of Coachella in accordance with the Rail System section of the General Plan Circulation Element.	The project does not propose the construction of new or modified existing rail line. The nearest rail line is over 7,500 feet from the project site. N/A
Transportation	ECVAP 13.2	Coordinate with railroad companies to encourage grade-separated crossings in and near Mecca.	The Kohl Ranch Specific Plan is not located in or near Mecca. N/A
Transportation	ECVAP 14.1	Implement the Trails and Bikeway System, Figure 9, as discussed in the Non-motorized Transportation section of the General Plan Circulation Element.	The County Trail and Bikeway System has been implemented in accordance with the Non-motorized Transportation section of the General Plan Circulation Element and per the County Transportation Department and County Regional Parks and Recreation District. C
Transportation	ECVAP 14.2	At signalized intersections, special equestrian push buttons (located at heights usable by persons riding on horseback) will be considered and installed where appropriate. Priority shall be given to those signalized intersections identified as trail crossings.	The inclusion of specialized push buttons would be determined at the time signalized intersections are proposed/built. There are no requirements or prohibitions in the specific plan for special equestrian push buttons. C
Transportation	ECVAP 14.3	As resources permit, consideration should be given to the placement of signs along those public rights-of-way identified as regional or community trail alignments alerting motorists to the possible presence of equestrian, bicycle and pedestrian (i.e. , non-motorized) traffic.	The Kohl Ranch Specific Plan Design Guidelines incorporate requirements for placement of directional, traffic safety and public service signs. C
Transportation	LU 13.1	Provide land use arrangements that reduce reliance on the automobile and improve opportunities for pedestrian, bicycle, and transit use in order to minimize congestion and air pollution.	The Specific Plan proposes sidewalks along all streets and an extensive network of trails along most major roadways as a way to offer pedestrian and "bicycle-friendly" means of travel with the intent of providing alternatives the use of the automobile for internal project area travel as a way to help improve air quality. Trails will also encourage the use of bus transit. C
Transportation	LU 13.2	Locate employment and service uses in areas that are easily accessible to existing or planned transportation facilities.	Specific Plan is a concentrated residential development that proposes commercial and service uses near Avenue 62, an expressway. The SunLine Transit Agency does not currently provide bus service to this area however, section V.D.1 of the EIR and the Transportation section of the Addendum incorporates mitigation to require coordination with the agency to incorporate bus stops and turnouts. Addendum Figure 15, provides potential stops and turnout locations based upon the most current circulation plan. Additionally, mitigation requires a portion of commercial parking areas be designated for Park and Ride facilities. C
Transportation	LU 13.3	Locate transit stations in community centers and at places of public, employment, entertainment, recreation, and residential concentrations.	Addendum #4 Figure 15 identifies potential bus stops and turnouts in areas designated for commercial and industrials uses where employment, public and entertainment concentrations are expected to occur as well as areas designated for residential use. Additionally, section V.D.1 of the EIR and the Transportation section of the Addendum incorporates mitigation to require coordination with the SunLine Transit Agency to incorporate bus stops and turnouts. Further, mitigation requires a portion of

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			commercial parking areas be designated for Park and Ride facilities. C
Transportation	LU 13.4	Incorporate safe and direct multi-modal linkages in the design and development of projects, as appropriate.	The proposed project has been designed to incorporate sidewalks for pedestrians along all roadways. This system will provide the members of community an alternative method of travel in order to reduce residents' reliance on the automobile to reach these destinations. Vehicular access to all portions of the project site will be designed and developed in compliance with Riverside County standards. An extensive system of trails for bikes and pedestrians connects residential areas to local services and shopping, as well as to regional trails and open space. C
Transportation	LU 13.5	Allow traffic-calming elements, such as narrow streets, curb bulbs, textured paving, and landscaping, where appropriate.	Traffic-calming elements such as curvilinear streets and cul-de-sacs are encouraged and proposed within The Kohl Ranch Specific Plan site development guidelines. Landscaping will also be utilized as an integral part of the internal roadways as a traffic-calming element. C
Transportation	LU 13.6	Require that adequate and accessible circulation facilities exist to meet the demands of a proposed land use.	Implementation of mitigation measures described in the Traffic Section of this EIR section V.D.1. and updated measures provided in the EIR Addendum and conditions of approval for SP303A2 will ensure that demands of the project on circulation will be met. C
Utilities	OS 1.1	Balance consideration of water supply requirements among urban, agricultural, and environmental needs so that sufficient supply is available to meet each of these different needs.	A Water Supply Assessment has been prepared which determined that the Coachella Valley Water District will have adequate water to supply the proposed project and its various uses. C
Utilities	OS 2.2	Encourage the installation and use of water conserving systems such as dry wells and graywater systems, where feasible, in new developments. The installation of cisterns or infiltrators shall be encouraged to capture rainwater from roofs for irrigation in the dry season and flood control during heavy storms.	This project will utilize a "dual water system" consisting of potable and non-potable water systems. The potable system will provide water for domestic purposes and provide water for fire protection. The non-potable system will provide water for irrigation purposes. C
Utilities	OS 2.4	Support and engage in educational outreach programs with other agencies, the public, homebuilders, landscape installers, and nurseries that promote water conservation and widespread use of water-efficient technologies. (AI 58).	The Kohl Ranch Specific Plan requires that a homeowners association shall be established for the specific plan area. The homeowners association will enforce any property and landscaping maintenance requirements established by the association and as appropriate will provide homeowners with information regarding maintenance methods. Ongoing educational programs including property maintenance methods are a part of The Kohl Ranch Community. C
Utilities	OS 4.6	Retain storm water at or near the site of generation for percolation into the groundwater to conserve it for future uses and to mitigate adjacent flooding. Such retention may occur through "Low Impact Development" or other Best Management Practice measures. (AI 57)	The project, as indicated on the Drainage Plan of the Specific Plan, on-site runoff will be intercepted and conveyed through the development by means of a conventional catch basin, swales and storm drain system, in accordance with Coachella Valley Water District standards, so that the increase in on-site runoff resulting from the development will be detained on-site and allowed to percolate into the ground or be captured and reused. C
	ECVAP 9.1	Conserve and protect watersheds and water supply through adherence to policies contained in the Open Space, Habitat and Natural Resource Preservation and Land Use Designation Policies sections found in the General Plan Land Use Element, and the Water Resources section of the General Plan Multipurpose Open Space Element.	This project will utilize a "dual water system" consisting of potable and non-potable water systems. The potable system will provide water for domestic purposes and provide water for fire protection. The non-potable system will provide water for irrigation purposes. Additionally, the Kohl Ranch Specific Plan Landscape Guidelines require the implementation of water use efficiency measures such as use of drought tolerant plants, grouping plants with similar irrigation requirements, use of mulch, and water efficient irrigation systems and are consistent with County Ordinance 859, Coachella Valley Water District Landscape Ordinance 1302.1 and the Thermal Design Guidelines plant palette. C
Utilities	S 4.10	Require all proposed projects anywhere in the County to address and mitigate any adverse impacts that it may have on the carrying capacity of local and regional storm drain systems.	The project, as shown on the Drainage section of the Specific Plan, storm flows will be intercepted and conveyed through the project area through utilization of graded swales and drainage pipe where they are discharged over weirs. Additionally, the use of on-site retention basins will control runoff flows. Section V.C.5, V.C.7 and V.D.2 and the Utilities section of the Addendum analyze and incorporate

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
			mitigation to reduce impacts on carrying capacity of local and regional storm drains to less than significant. C
Utilities	LU 5.3	Review all Projects for consistency with individual urban water management plans.	A Water Supply Assessment has been prepared which considered the Coachella Valley Water District's Urban Water Management Plan and determined that the District has adequate water to supply to serve the proposed The Kohl Ranch Specific Plan. C
Utilities	LU 17.2	Require that adequate and available circulation facilities, water resources, sewer facilities and/or septic capacity exist to meet the demands of the proposed land use.	At the present time there are not adequate facilities available to meet all the needs of The Kohl Ranch. However, through County regulations and mitigation measures, which include the payment of development impact fees, and the construction of utility facilities, facilities can be constructed to meet the needs of the proposed project. A Water Supply Assessment has been prepared for this project and indicates that water resources are available to serve this project. C
Utilities	OS 2.2	Where feasible, decrease stormwater runoff by reducing pavement in development areas, and by design practices such as permeable parking bays and porous parking lots with bermed storage areas for rainwater detention.	The project, as shown on the Drainage section of the Specific Plan, storm flows will be intercepted and conveyed through the project area through utilization of graded swales and drainage pipe where they are discharged over weirs. Additionally, the use of on-site retention basins will control runoff flows. C
Utilities	LU 5.2	Monitor the capacities of infrastructure and services in coordination with service providers, utilities, and outside agencies and jurisdictions to ensure that growth does not exceed acceptable levels of services.	The project proponent and utility providers will monitor growth and utility demand to ensure that the utility agency maintains adequate levels of service to the area. The payment of Riverside County's required development impact fees (Ord. No. 659) and the implementation of mitigation measures listed in the EIR shall ensure that growth is provided with appropriate infrastructures for adequate level of services. C
Utilities	LU 5.4	Ensure that development and conservation land uses do not infringe upon existing public facilities and utility corridors, which include county regional landfills, fee owned rights-of-way and permanent easements, whose true land use is that of "public facilities". This policy will ensure that the public facilities designation governs over what otherwise may be inferred by the large scale general plan maps.	The Kohl Ranch designates a Public Facilities planning area and will ensure that land uses do not infringe on existing public utility corridors, such as the Avenue 64 Evacuation Channel that traverses through the project site. C
Utilities	OS 11.3	Permit and encourage the use of passive solar devices and other state-of- the-art energy resources.	The California Green Building Standards Code (proposed Part 11, Title 24) was adopted as part of the California Building Standards Code in the CCR. Part 11 establishes voluntary standards, that will become mandatory in the 2010 edition of the Code, on planning and design for sustainable site development, energy efficiency (in excess of the California Energy Code requirements), water conservation, material conservation, and internal air contaminants. The Project will be subject to these mandatory standards. C
Utilities	OS 16.1	Continue to implement Title 24 of the California Code of Regulations (the "California Building Standards Code") particularly Part 6 (the California Energy Code) and Part 11 (the California Green Building Standards Code), as amended and adopted pursuant to County ordinance. Establish mechanisms and incentives to encourage architects and builders to exceed the energy efficiency standards of within CCR Title 24. (AI 62)	The Greenhouse Gas section of the Addendum incorporates mitigation to continue implementation of Title 24 but with an exceedance of standards by 15%. C
Utilities	OS 16.2	Specify energy efficient materials and systems, including shade design technologies, for County buildings.	Although it not specifically planned at this point, any potential future County buildings will ensure compliance with this policy. C
Utilities	OS 16.3	Implement public transportation systems that utilize alternative fuels when possible, as well as associated urban design measures that support alternatives to private automobile use.	EIR section V.D.1 and the Utilities section of the Addendum incorporates mitigation to provide for potential bus transit. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the

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			schools. C
Utilities	OS 16.5	Utilize federal, state, and utility company programs that encourage energy conservation.	The Greenhouse Gas section of the Addendum incorporates mitigation to continue implementation of Title 24 with an exceedance of standards by 15%. Additionally, EIR section V.C.6 incorporates mitigation to promote usage of energy saving equipment. C
Utilities	OS 16.7	Promote purchasing of energy-efficient equipment based on a fair return on investment, and use energy-savings estimates as one basis for purchasing decisions for major energy-using devices.	EIR section V.C.6 and the Utilities section of the Addendum incorporates mitigation to promote usage of energy saving equipment. C
Utilities	OS 16.8	Promote coordination of new public facilities with mass transit service and other alternative transportation services, including bicycles, and design structures to enhance mass transit, bicycle, and pedestrian use.	EIR section V.D.1 and the Utilities section of the Addendum incorporates mitigation to provide for potential bus transit. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. C
Utilities	OS 16.9	Encourage increased use of passive, solar design and day-lighting in existing and new structures.	The Addendum incorporates mitigation to improve Title 24 standards by 15%. While not specifically required in the Kohl Ranch Specific Plan, use of passive, solar design and day-lighting in new structures could help reach this goal. C
Utilities	AQ 5.1	Utilize source reduction, recycling and other appropriate measures to reduce the amount of solid waste disposed of in landfills.	The project will participate in the County's recycling program. The Greenhouse Gas section of the Addendum incorporates mitigation that will require separate recycling and waste receptacles at each house and commercial site. C